

**RICHARDSON CITY COUNCIL**  
**JANUARY 28, 2013**  
**7:30 P.M.**  
**CIVIC CENTER/CITY HALL, 411 W. ARAPAHO, RICHARDSON, TX**

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- 1. INVOCATION – MARK SOLOMON**
  - 2. PLEDGE OF ALLEGIANCE: U.S. AND TEXAS FLAGS – MARK SOLOMON**
  - 3. MINUTES OF THE JANUARY 14, 2013 MEETING**
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4. VISITORS. (THE CITY COUNCIL INVITES CITIZENS TO ADDRESS THE COUNCIL ON ANY TOPIC NOT ALREADY SCHEDULED FOR PUBLIC HEARING. PRIOR TO THE MEETING, PLEASE COMPLETE A "CITY COUNCIL APPEARANCE CARD" AND PRESENT IT TO THE CITY SECRETARY. THE TIME LIMIT IS FIVE MINUTES PER SPEAKER.)
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**ACTION ITEMS:**

5. CONTINUED CONSIDERATION OF ZONING FILE 12-11: A REQUEST BY JOHN S. KIRK, REPRESENTING EMBREY PARTNERS, LTD., FOR A CHANGE IN ZONING FROM I-FP(2) INDUSTRIAL WITH SPECIAL CONDITIONS TO PD PLANNED DEVELOPMENT FOR THE DEVELOPMENT OF A MULTI-FAMILY COMMUNITY TO BE LOCATED AT THE SOUTHEAST CORNER OF GREENVILLE AVENUE AND COLLINS BOULEVARD. (CONTINUED FROM NOVEMBER 12, 2012 CITY COUNCIL MEETING).

ACTION TAKEN:

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6. CONSIDER THE REQUEST OF THE WARMING HUT FOR A VARIANCE TO THE CITY OF RICHARDSON CODE OF ORDINANCES, CHAPTER 18, ARTICLE III, SECTION 18-96(23)(D)(II) TO ALLOW FOR A 18 FOOT REDUCTION OF THE 30 FOOT SETBACK REQUIREMENT TO ALLOW FOR A 59.88 SQUARE FOOT POLE SIGN, WITH AN ELECTRONIC MESSAGING CENTER TO BE 12 FEET FROM THE ADJOINING PRIVATE PROPERTY LINE AT THE PROPERTY LOCATED AT 331 N CENTRAL EXPRESSWAY; AND TAKE APPROPRIATE ACTION.

ACTION TAKEN:

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7. CONSIDER THE REQUEST OF GOLF CARS OF DALLAS FOR VARIANCES TO THE CITY OF RICHARDSON CODE OF ORDINANCES, CHAPTER 18, ARTICLE III, SECTION 18-96(23)(B)(III)(1) AND SECTION 18-96(23)(C)(III)(1) FOR A 20 FOOT INCREASE IN HEIGHT AND 52 SQUARE FOOT INCREASE IN SIGN AREA TO ALLOW FOR A 40 FOOT IN HEIGHT, 112 SQUARE FOOT MULTI-TENANT POLE SIGN AT THE PROPERTY LOCATED AT 2100 ALAMO ROAD; AND TAKE APPROPRIATE ACTION.

ACTION TAKEN:

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ALL ITEMS LISTED UNDER ITEM 8 OF THE CONSENT AGENDA ARE CONSIDERED TO BE ROUTINE BY THE CITY COUNCIL AND WILL BE ENACTED BY ONE MOTION IN THE FORM LISTED BELOW. THERE WILL BE NO SEPARATE DISCUSSIONS OF THESE ITEMS. IF DISCUSSION IS DESIRED, THAT ITEM WILL BE REMOVED FROM THE CONSENT AGENDA AND WILL BE CONSIDERED SEPARATELY:

8. CONSENT AGENDA:

- A. ADOPTION OF ORDINANCE NO. 3896, CHANGING THE NAME OF "DATACENTER PARK BOULEVARD", IN ITS ENTIRETY, TO "INTEGRITY DRIVE".
- B. CONSIDER RESOLUTION NO. 13-05, ORDERING A GENERAL ELECTION TO BE HELD ON THE 11TH DAY OF MAY 2013, FOR THE PURPOSE OF ELECTING A MAYOR AND SIX (6) MEMBERS OF THE RICHARDSON CITY COUNCIL; DESIGNATING POLLING PLACES; ORDERING NOTICES OF ELECTION TO BE GIVEN; AND AUTHORIZING EXECUTION OF JOINT ELECTION CONTRACT.
- C. AUTHORIZE THE ADVERTISEMENT BID #33-13 – 2013 ANNUAL REQUIREMENTS CONTRACT FOR STREET REHABILITATION. BIDS TO BE RECEIVED BY THURSDAY, FEBRUARY 14, 2013 AT 3:00 P.M.
- D. CONSIDER AWARD OF THE FOLLOWING BIDS:
  1. BID #15-13 – WE RECOMMEND THE AWARD TO QUALITY EXCAVATION FOR THE PAVEMENT/DRAINAGE REHABILITATION IN THE AMOUNT OF \$787,916.90.
  2. BID #19-13 – WE RECOMMEND THE AWARD TO JESKE CONSTRUCTION CO., FOR THE COTTONWOOD HEIGHTS BRIDGE AND CULVERT RAILING IN THE AMOUNT OF \$505,222.
  3. BID #30-13 – WE RECOMMEND THE AWARD TO INSITUFORM TECHNOLOGIES, INC., FOR THE 2013 TRENCHLESS SEWER REHABILITATION PROJECT IN AN AMOUNT NOT TO EXCEED \$400,000
  4. BID #34-13 – WE REQUEST AUTHORIZATION TO ISSUE A COOPERATIVE ANNUAL CONTRACT TO TRAFFIC HIGHWAY PRODUCTS, LTD. FOR TRAFFIC SIGNAL CONTROLLER HARDWARE THROUGH THE CITY OF FORT WORTH INVITATION TO BID #12-0135 PURSUANT TO UNIT PRICES.

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THE RICHARDSON CITY COUNCIL WILL MEET AT 5:30 P.M. ON MONDAY, JANUARY 28, 2013, IN THE RICHARDSON ROOM OF THE CIVIC CENTER/CITY HALL, 411 W. ARAPAHO, RICHARDSON, TEXAS. AS AUTHORIZED BY SECTION 551.071(2) OF THE TEXAS GOVERNMENT CODE, THIS MEETING MAY BE CONVENED INTO CLOSED EXECUTIVE SESSION FOR THE PURPOSE OF SEEKING CONFIDENTIAL LEGAL ADVICE FROM THE CITY ATTORNEY ON ANY AGENDA ITEM LISTED HEREIN. THIS BUILDING IS WHEELCHAIR ACCESSIBLE. ANY REQUESTS FOR SIGN INTERPRETIVE SERVICES MUST BE MADE 48 HOURS AHEAD OF THE MEETING. TO MAKE ARRANGEMENTS, CALL 972-744-4000 VIA TDD OR CALL 1-800-735-2989 TO REACH 972-744-4000.

WORK SESSION – 6:00 P.M.:

- Call to Order
- A. Review and Discuss Items Listed on the City Council Meeting Agenda
- B. Review and Discuss Part 2 of the Final Report and Recommendation, Main Street/Central Expressway Corridor Study - Implementation
- C. Review and Discuss the 2012 Year End Crime Statistics and Program Updates
- D. Report on Items of Community Interest

## EXECUTIVE SESSION

- In compliance with Section 551.087 of the Texas Government Code, Council will convene into a closed session to discuss the following:
  - Deliberation Regarding Economic Development Negotiations
    - Commercial Development – Glenville Dr./Campbell Rd. Area
    - Retail Development – U.S. 75/Arapaho Rd. Area
- Council will reconvene into open session, and take action, if any, on matters discussed in executive session.

I CERTIFY THE ABOVE AGENDA WAS POSTED ON THE BULLETIN BOARD AT THE CIVIC CENTER/CITY HALL ON FRIDAY, JANUARY 25, 2013, BY 5:00 P.M.

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CITY SECRETARY

**MINUTES**  
**RICHARDSON CITY COUNCIL**  
**WORK SESSION MEETING**  
**JANUARY 14, 2013**

**WORK SESSION – 6:00 P.M.:**

• **Call to Order**

Mayor Townsend called the meeting to order at 6:00 p.m. with the following Council members present:

Bob Townsend	Mayor
Laura Maczka	Mayor Pro Tem
Mark Solomon	Councilmember
Scott Dunn	Councilmember
Kendal Hartley	Councilmember
Steve Mitchell	Councilmember
Amir Omar	Councilmember

The following staff members were also present:

Dan Johnson	City Manager
David Morgan	Deputy City Manager
Cliff Miller	Assistant City Manager Development Services
Don Magner	Assistant City Manager Community Services
Shanna Sims-Bradish	Assistant City Manager Admin/Leisure Services
Samantha Woodmancy	Management Analyst
Aimee Nemer	City Secretary
Steve Spanos	Director of Engineering
Jim Lockart	Assistant Director of Engineering

The following special guests were also in attendance:

Dr. David Daniel, President, UTD  
Calvin Jamison, VP for Administration, UTD  
Amanda Rockow, VP for Public Affairs, UTD

**A. Review and Discuss Items Listed on the City Council Meeting Agenda**

**Staff Comments**

Don Magner, Assistant City Manager reviewed three Sign Control Board cases:

- 13-01 Warming Hut, 331 N. Central Expressway requesting variance for a pole sign to be located twelve feet from the adjoining property
- 13-02 Texas Instruments, 300 W. Renner Road requesting variance to allow an eight foot, six inch tall, fifty-five square foot monument sign at the northwest corner of Alma and W. Renner; and to allow a seven foot tall monument sign at the entrance off of W. Renner
- 13-03 Golf Cars of Dallas, 2100 Alamo Road requesting a variance to allow a multi-tenant pole sign that is forty feet in height and one-hundred twelve square feet in area

## **B. Review and Discuss the State of the University of Texas at Dallas**

Dr. David Daniel, President, UTD, reviewed the State of the University of Texas at Dallas:

- Goal to be a Tier One, nationally competitive, research university
- 43 year old university with 19,750 total students and 3560 living on campus
- 1200 university employees are Richardson residents
- Ranked 3<sup>rd</sup> public university in Texas
- Review of campus enhancements and new construction
- Community outreach programs
- City of Richardson partnerships
- Economic impact
- Request assistance from the City in developing property north of main campus

## **C. Review and Discuss the Sidewalk Continuity Survey**

Jim Lockhart, Assistant Director of Engineering, addressed this item with Council.

- Review of recent City projects that include new sidewalk construction
- Survey of public sidewalks – approximately 60 miles of public street parkway does not have a public sidewalk and are categorized as follows:
  - Undeveloped tracts – approximately 24 miles
  - Developed residential – approximately 15 miles
  - Developed commercial – approximately 10 miles
  - City – approximately 7 miles
  - School – less than 2 miles
  - Rail Road – less than 2 miles
- Review of construction candidate sites estimated to cost \$4,000,000
  - UT Dallas Trail Phase 2
  - SH190 west of Alma
  - North Stat – Otto Middle School Route
  - E. Renner – Schell Elementary School Route
  - Canyon Creek Drive east of Custer
  - Collins Overpass
  - Plano at Greenville
  - Centennial at KCS
- Next steps:
  - Proceed with funded City projects that include sidewalk construction
  - Continue to seek funding sources for construction of candidate sites
  - Construct network improvements in conjunction with other adjacent City projects where possible
  - Update Capital Project database to include the results of the survey and Council direction on priority locations

## **Council Discussion**

Council discussed some possible locations that were not included in the survey. City Manager Johnson requested Council to review the location map and report any missing areas to Staff. Mr. Johnson stated that he would provide Council with a list of locations and a larger map. Council requested that the list include cost estimates and potential funding for each location.

#### **D. Report on Items of Community Interest**

There were no Items of Community Interest Reported.

1. **INVOCATION – AMIR OMAR**
2. **PLEDGE OF ALLEGIANCE: U.S. AND TEXAS FLAGS – AMIR OMAR**
3. **MINUTES OF THE DECEMBER 10, 2012 COUNCIL MEETING, DECEMBER 17, 2012 SPECIAL MEETING, AND JANUARY 7, 2013 WORK SESSION.**

#### **Council Action**

Councilmember Omar moved to approve the Minutes as presented. Councilmember Hartley seconded the motion. A vote was taken and passed, 7-0.

#### **4. VISITORS**

The following visitors submitted comments in opposition of Ordinance No. 3847 regarding rental registrations and inspections and requested a review of the ordinance.

David Farnham  
Joshua Callahan  
Mitzi Armado  
Peter Balbus  
Robert Pittana

Mayor Townsend asked City Manager Dan Johnson to briefly address the comments. Mr. Johnson stated that the ordinance was developed with consultation with the City Attorney and staff is competent in its legal structure. He explained that in regards to the Municipal Court proceedings, procedures were followed in accordance with the law. Mr. Johnson stated that Council and staff will reflect on the points of view from tonight's comments as well as previous points of view raised during the development of the ordinance. Mr. Johnson explained that staff will take direction from Council on further review of the ordinance.

5. **CONSIDER RESOLUTION NO. 13-03, DESIGNATING THE REAL PROPERTY LOCATED AT THE 400 BLOCK OF WEATHERRED DRIVE RICHARDSON, TEXAS AS A NEIGHBORHOOD PARK; NAMING THE HEIGHTS NEIGHBORHOOD SMALL PARK “DURHAM PARK”.**

#### **Council Action**

Councilmember Solomon moved to approve a resolution designating the park on Weatherred Drive as “Durham Park”. Mayor Pro Tem Maczka seconded the motion. A vote was taken and passed, 7-0.

#### **Public Comments**

The Durham family was present and thanked the City Council for recognizing their family.

**6. CONSENT AGENDA**

ALL ITEMS LISTED UNDER ITEM 6 OF THE CONSENT AGENDA ARE CONSIDERED TO BE ROUTINE BY THE CITY COUNCIL AND WILL BE ENACTED BY ONE MOTION IN THE FORM LISTED BELOW. THERE WILL BE NO SEPARATE DISCUSSIONS OF THESE ITEMS. IF DISCUSSION IS DESIRED, THAT ITEM WILL BE REMOVED FROM THE CONSENT AGENDA AND WILL BE CONSIDERED SEPARATELY:

**A. ADOPTION OF ORDINANCE NO. 3895, AMENDING THE CODE OF ORDINANCES BY AMENDING CHAPTER 4 ALCOHOLIC BEVERAGES BY AMENDING SECTION 4-7(A) TO ALLOW THE SALE OR SERVING OF ALCOHOLIC BEVERAGES FOR ON-PREMISES CONSUMPTION UPON APPROVAL OF A SPECIAL PERMIT IN ACCORDANCE WITH ARTICLE XXII-A OF THE COMPREHENSIVE ZONING ORDINANCE GRANTED AFTER OCTOBER 1, 2011, FOR A RESTAURANT WITHOUT DRIVE-THROUGH OR CURB SERVICE IN ANY O-M, TO-M OR INDUSTRIAL DISTRICT OR FOR A RESTAURANT WITHOUT DRIVE-THROUGH OR CURB SERVICE WITH A SPECIAL PERMIT FOR A PRIVATE CLUB IN ANY INDUSTRIAL DISTRICT.**

**B. CONSIDER THE FOLLOWING RESOLUTIONS:**

- 1. RESOLUTION NO. 13-01, PROVIDING FOR THE REDEMPTION OF A PORTION OF THE OUTSTANDING “CITY OF RICHARDSON, TEXAS, GENERAL OBLIGATION REFUNDING BONDS, TAXABLE SERIES 2004”; AND RESOLVING OTHER MATTERS INCIDENT AND RELATED TO THE REDEMPTION OF SUCH OBLIGATIONS.**
- 2. RESOLUTION NO. 13-02, APPROVING AND AUTHORIZING PUBLICATION OF NOTICE OF INTENTION TO ISSUE CERTIFICATES OF OBLIGATION.**
- 3. RESOLUTION NO. 13-04, ESTABLISHING FEES AND CHARGES FOR THE USAGE OF CERTAIN PARK AND RECREATION FACILITIES IN THE CITY OF RICHARDSON BY RESIDENTS AND NON-RESIDENTS.**

**C. AUTHORIZE THE ADVERTISEMENT BID #26-13 – 2010 ALLEY RECONSTRUCTION PHASE II WITH SANITARY SEWER REPLACEMENT (GREENLEAF/RIDGEDALE/ LOCKWOOD). BIDS TO BE RECEIVED BY THURSDAY, JANUARY 31, 2013 AT 2:00 P.M.**

**D. CONSIDER AWARD OF THE FOLLOWING BIDS:**

- 1. BID #16-13 – WE RECOMMEND THE AWARD TO INTEGRITY TEXAS CONSTRUCTION FOR THE COMMUNICATIONS HVAC PHASE 2 IN THE AMOUNT OF \$152,700.00 AND TO FEDERAL MECHANICAL SYSTEMS, INC., FOR THE LIBRARY COOLING TOWER IN THE AMOUNT OF \$180,500 FOR A TOTAL AMOUNT OF \$333,200.00.**
- 2. BID #27-13 – WE REQUEST AUTHORIZATION TO ISSUE A CO-OP PURCHASE ORDER TO HEIL OF TEXAS FOR FIVE (5) REAR**

**LOADER REFUSE BODIES FOR SOLID WASTE SERVICES THROUGH THE TEXAS LOCAL GOVERNMENT STATEWIDE PURCHASING COOPERATIVE BUYBOARD CONTRACT #357-10 IN THE AMOUNT OF \$269,150.**

- 3. BID #28-13 – WE REQUEST AUTHORIZATION TO ISSUE A CO-OP PURCHASE ORDER TO SUNBELT POOLS FOR SWIMMING POOL REPLACEMENT FILTERS FOR COTTONWOOD POOL (\$44,750) AND GLENVILLE POOL (\$31,200) THROUGH THE TEXAS LOCAL GOVERNMENT STATEWIDE PURCHASING COOPERATIVE BUYBOARD CONTRACT #367-10 FOR A TOTAL EXPENDITURE OF \$75,950.**

**E. CONSIDER REJECTION OF BID #43-12 – WE RECOMMEND REJECTING ALL BIDS RECEIVED FOR THE 2012 PUBLIC BUILDINGS ENERGY REDUCTION INITIATIVES PROJECT.**

**F. AUTHORIZE THE CITY MANAGER TO EXECUTE CHANGE ORDER NO. 4 TO PURCHASE ORDER NO. 120797 TO CAMINO CONSTRUCTION FOR THE STREET REHAB PHASE III – MELROSE (COIT TO WEST SHORE) & MEADOWVIEW COURT IN THE AMOUNT OF \$185,000.00.**

**Council Action**

Councilmember Mitchell moved to approve the Consent Agenda as presented. Councilmember Hartley seconded the motion. A vote was taken and passed 7-0.

- 7. RECEIVE THE SIGN CONTROL BOARD MINUTES OF THE JANUARY 9, 2013 MEETING.**

**Council Discussion**

Councilmember Omar requested that Council separately review the sign cases on Case 13-01 and 13-02 which are both located on U.S. 75.

**Council Action**

Mayor Pro Tem Maczka moved to approve Sign Case 13-02 and review Sign Case 13-01 and 13-02 separately. Councilmember Dunn seconded the motion. A vote was taken and passed 7-0.

**ADJOURNMENT**

With no further business, the meeting was adjourned at 7:07 p.m.

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MAYOR

ATTEST:

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CITY SECRETARY



City of Richardson  
City Council Meeting  
Agenda Item Summary



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**Meeting Date:** Monday, January 28, 2013

**Agenda Item:** Visitors *(The City Council invites citizens to address the Council on any topic not already scheduled for public hearing.)*

**Staff Resource:** Aimee Nemer, City Secretary

**Summary:** Members of the public are welcome to address the City Council on any topic not already scheduled for public hearing. Speaker Appearance Cards should be submitted to the City Secretary prior to the meeting. Speakers are limited to 5 minutes and should avoid personal attacks, accusations, and characterizations.

In accordance with the Texas Open Meetings Act, the City Council cannot take action on items not listed on the agenda. However your concerns will be addressed by City staff, may be placed on a future agenda, or by some other course of resolution.

**Board/Commission Action:** N/A

**Action Proposed:** Receive comments by visitors.

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# MEMO

**DATE:** January 24, 2013  
**TO:** Honorable Mayor and City Council  
**FROM:** Michael Spicer, Director of Development Services MS  
**SUBJECT:** Zoning File 12-11 – GreenVUE PD – Multi-family Community

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## REQUEST

John S. Kirk of Embrey Partners, Ltd. is requesting approval of a change in zoning from I-FP (2) Industrial with special conditions to PD Planned Development to provide for the development of a multi-family apartment community on a 12.75-acre tract of land located at the southeast corner of Greenville Avenue and Collins Boulevard.

## BACKGROUND

The subject property is an undeveloped tract bounded by the Collins Boulevard overpass to the north, Greenville Avenue to the west, Alma Road to the east, and the KCS Railroad and a vacant industrial/office building to the south. The site is located within the East Arapaho/Collins Enhancement/Redevelopment district as depicted in the 2009 Comprehensive Plan. A study of this district is now underway and is projected to be completed by the first quarter of 2013.

## SEPTEMBER 4, 2012 CITY PLAN COMMISSION MEETING

The applicant presented a 351-unit development on a 12.75-acre tract comprising eight (8), three-story buildings generally arranged parallel to an east/west drive connecting Alma Road and Greenville Avenue. The buildings included only one-bedroom and two-bedroom units. All buildings were clad with brick, stone, concrete block, 3-stage stucco, metal panels and hardipanel. About 70% of the parking provided was surface parking with the remainder either located in a tuck-under configuration on the first floor of the apartment buildings or in free-standing garages. Multiple open space areas were proposed throughout the development; the largest located near the clubhouse. A hike and bike trail, ten (10) feet wide, was proposed along the perimeter of the property on the three sides having public street frontage.

The concept plan presented to the City Plan Commission included gates located at both the Greenville Avenue and Alma Road entrances. The City Plan Commission expressed concerns that the gates were not in keeping with the intended urban character of the project and impaired connectivity.

## PLAN COMMISSION RECOMMENDATION

On September 4, 2012, the City Plan Commission, by a vote of 4-3 (Commissioners Bouvier, Hand, and Linn opposed), recommended approval of the request, including PD standards and a condition that would allow the option of removing the gates at the Greenville and Alma entrances.

## SEPTEMBER 24, 2012 CITY COUNCIL MEETING

The applicant indicated the gates would be removed to address the Commission's concern and requested that rather than having the option to remove the gates, that the requirement for the gates be eliminated. Primary concerns expressed by the City Council included the lack of retail opportunities on the first floor of the buildings along Alma Road and Greenville Avenue and the lack of structured parking – features typical of more urban style development. The applicant requested additional time to revise the proposal in response to City Council concerns. City Council closed the public hearing but voted to continue consideration of the request to October 22, 2012. At that meeting, the applicant requested further continuance until November 12, 2012 in order to finalize plans that might better respond to Council's concerns.

## **NOVEMBER 12, 2012 CITY COUNCIL MEETING**

At the November 12, 2012 City Council meeting, the applicant presented a modified plan that reduced the size of the proposed development from 12.75 acres to 11.13 acres. The south property line was moved north and a 0.85-acre parcel located at the southwest corner of Collins and Alma was set aside for a future park. The applicant also revised the site plan and elevations in an effort establish a stronger urban character. Updates to the site plan included placing the buildings closer to Greenville Avenue; increasing density from twenty-eight (28) units per acre to thirty-six (36) units per acre; increasing the number of units from 351 units to 401; adding deck parking to accommodate additional units without increasing the surface parking area; providing “retail ready” ground floor space along Greenville Avenue and Alma Road; increasing building heights from three (3) to four (4) stories with heights up sixty (60) feet along Greenville Avenue and Alma Road; and providing elevators in all four-story buildings.

The Council expressed concerns related to the on-going East Arapaho/Collins enhancement/redevelopment study; development density; the viability of the project’s retail component; the location and orientation of the parking deck; and the location and ownership of the park. In light of these concerns, the applicant requested that Council continue its consideration of the case to allow time to address the issues. The Council, by a vote of 6-1 (Councilman Mitchell opposed), continued its consideration of the case to January 28, 2013.

## **REVISED SITE PLAN AND ELEVATIONS**

Key changes to the proposed concept plan include: an increase in the number of proposed units from 401 to 408 (36.7 units per acre); the addition of another four-story building along Collins Boulevard that conceals the northern exposure of the parking deck; essentially “wrapping” the parking deck by physically connecting it to the surrounding three buildings; reconfiguration of the buildings fronting Alma Road; and eliminating the ground floor “retail ready” space along Alma Road.

## **ATTACHMENTS**

Special Conditions	Retail Ready Section (Exhibit “D-2”)
CC Public Hearing Notice	Perspective Rendering (Exhibit “D-3”)
City Plan Commission Minutes 09-04-2012	Entry Drive Detail (Exhibit “D-4”)
City Council Minutes 09-24-2012	Site Photos (Exhibits “E-1” through “E-4”)
City Council Minutes 11-12-2012	Proposed PD Conditions (Exhibit “F”)
Staff Report	Market Study Report (Provided by Gateway Planning)
Zoning Map	Applicant’s Statement
Aerial Map	Update to Applicant’s Statement 11-07-2012
Oblique Aerial Looking East	Notice of Public Hearing
Zoning Exhibit (Exhibit “B”)	Notification List
Building Elevations (Exhibit “C-1” and “C-2”)	Previous Exhibits
Rendered Concept Plan (Exhibit “D-1”)	

### **ZF 12-11 Special Conditions**

1. The subject site shall be zoned PD Planned Development for the A-950-M Apartment District and shall be developed in accordance with the attached “GreenVUE Planned Development District Proposed PD Conditions” (Exhibit “F”).

### **CPC Additional Condition**

2. The gates located at the Greenville Avenue and Alma Road entrances may be removed. *“The applicant’s revised Concept Plan does not include internal gates”.*



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Attn. Lynda Black  
Publication for Dallas Morning News – Legals  
Submitted on: 09-6-2012  
Submitted by: City Secretary, City of Richardson

Please publish as listed below or in attachment and provide a publication affidavit to:

City Secretary's Office  
P.O. Box 830309  
Richardson, TX 75083-0309

FOR PUBLICATION ON: September 7, 2012

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**City of Richardson  
Public Hearing Notice**

The Richardson City Council will conduct a public hearing at 7:30 p.m. on Monday, September 24, 2012, in the Council Chambers, Richardson Civic Center/City Hall, 411 W. Arapaho Road, to consider the following requests.

**Zoning File 12-11**

A request by John S. Kirk, representing Embrey Partners, Ltd., for a change in zoning from I-FP(2) Industrial with special conditions to PD Planned Development for the development of a multi-family community to be located at the southeast corner of Greenville Avenue and Collins Boulevard. The property is currently zoned I-FP(2) Industrial.

If you wish your opinion to be part of the record but are unable to attend, send a written reply prior to the hearing date to City Council, City of Richardson, P.O. Box 830309, Richardson, Texas 75083.

CITY OF RICHARDSON  
Aimee Nemer, City Secretary

**EXCERPT  
CITY OF RICHARDSON  
CITY PLAN COMMISSION MINUTES – September 4, 2012**

**PUBLIC HEARING**

**Zoning File 12-11 (continued from August 21, 2012 meeting):** Consider and take necessary action on a request by John S. Kirk, representing Embry Partners, Ltd., for a change in zoning from I-FP(2) Industrial with special conditions to PD Planned Development for the development of a multi-family community. The 12.75-acre site is located at the southeast corner of Greenville Avenue and Collins Boulevard and is zoned I-FP(2) Industrial.

Mr. Shacklett advised the applicant was requesting to rezone the property at the southeast corner of Greenville Avenue and Collins Boulevard for development of a 351-unit multifamily community. He added that Exhibit “F” in the Commission’s packet would be the proposed PD conditions for the development relating to height, buildings, landscaping and other regulations. Mr. Shacklett provided background information on the property including (1) the 2001 land use study for the area in and around the Arapaho DART Station including the subject property and the property located directly south with designated office, open space, and mixed-use office/urban residential, (2) the 2005 City initiated zoning request for a transit oriented development (TOD) around the Station that did not include the subject property (the City Plan Commission recommended approval and the City Council tabled it, but a decision was never made therefore leaving the existing Industrial zoning on the property), (3) the 2009 Comprehensive Plan update designated six areas throughout the City as enhancement/redevelopment with the subject property listed as part of the third study area that will be taking place in the near future.

Mr. Shacklett stated the applicant was proposing 351 multi-family units in a total of eight (8) buildings with the majority of the buildings being 3-stories in height. The only exception would be Building 1 which will have 3-story units that have a second story (loft) within the unit thereby creating a 4-story building. He added that there will be a number of open spaces provided throughout the community including the largest area behind the leasing office/clubhouse where a pool and other amenities would be located.

Mr. Shacklett pointed out that the site would have access from Greenville Avenue and Alma Road via an east/west drive aisle with parallel parking adjacent to the buildings. He added that the applicant was also proposing landscape buffers and fencing around the property with tree and a shrub row alternating on centers along the fence providing a buffer from the property to the south.

Mr. Shacklett reported the applicant was requesting the following amendments to the development standards of the A-950 District:

- Parking ratio of 1.5 per unit, the same as provided at Eastside, due to an apartment mix of 70/30 (one to two bedrooms). Also providing 158 garages and tuck-under spaces.
- Requesting 100 amenity points as opposed to the typical 70 points for each 250 units in A-950 Regulations.

- Requesting the property be considered one large community and that no physical separation be required.
- Reduction in masonry material from 75% to 50% for any one elevation, and the non-masonry materials allowed would be a three-stage stucco, metal and hardy panels. Some elevations will have upward of 77% of masonry, but the lowest would be 50%.

Mr. Shacklett explained that rather than having dumpsters in the parking lot, the applicant was proposing internal trash rooms within each building where residents would take their trash and then maintenance staff would move the trash to an enclosed compactor area on the north side of the property.

In closing his presentation, Mr. Shacklett stated that if the zoning request was approved, the property will be zoned PD Planned Development, developed in accordance with Exhibit “F” (list of conditions), Exhibit “B” and the three elevation sheets.

Vice Chair Hand said he understood the legality of making the property a PD Planned Development district, but had a concern about exempting the property in question from residential requirements listed in the Comprehensive Zoning Ordinance (CZO).

Chairman Gantt replied that the PD would be creating a new ordinance and would define the use and what could be built.

Mr. Chavez added that Section 8(c) of the Proposed Conditions was added to clarify that the property would be a PD as opposed to a residential district, which is where the CZO would apply additional heights limitations and performance standards on adjacent property.

Vice Chair Hand asked if the item was approved, would the surrounding properties be nonconforming and would the new zoning exempt adjacent properties from what a residential property would do to them. He also felt the property did not meet the definition of a PD.

Mr. Chavez replied that based on the staff analysis, and in accordance with the CZO, none of the residential performance standards would apply to the surrounding properties because the property would be zoned PD, which by definition has to be more than 10 acres and the property met that requirement.

Chairman Gantt asked if Mr. Hand’s concern was the property to the south and what might be built there in the future that would normally not be allowed adjacent to a residential area.

Vice Chair Hand confirmed that was an area of concern for him.

Commissioner Linn asked staff if the property in question would be in the Arapaho Collins redevelopment/enhancement study area. He also wanted to know if the item was approved, would the PD supersede the study area recommendations, or would it be removed from the study area.

Mr. Shacklett replied that the 2009 Future Lane Use plan called for six areas to be studied and the study for the proposed area had not been undertaken as yet. In addition, he said the

development might be considered as a factor in the future study, but it was not known at this time if it would be

Commissioner Linn stated the 2000 ULI study for the area surrounding the Arapaho Station called for residential as well as mixed-use retail, similar to other TOD areas, and wanted to know why mixed-use was not part of the proposal. He also wanted to know if there would be sidewalks around the development.

Chairman Gantt pointed out that the ULI study covered a much broader area than the proposed property and Mr. Shacklett noted that there would be a 10-foot trail along all three frontages.

With no further questions for staff, Chairman Gantt opened the public hearing.

Mr. William Dahlstrom, representing Embry Partners, 901 Main Street, Richardson, Texas, stated Embry Partners was a fully-integrated development and property management company with 40 years of experience and they were proposing a high quality urban residential community. He added that the project could be a catalyst for the area and could trigger development around the Arapaho Station.

Mr. John Kirk, Executive Vice President, Embry Partners, 1020 NE Loop, San Antonio, Texas, stated that Embry specialized in multi-family developments and has developed over 30,000 residential units and over 6 million square feet of office in the past 40 years. He added that their projects cover much of the south and they pride themselves on building quality products with long term value.

Mr. Kirk highlighted some of materials to be used in construction of the project including brick, stone, hardy plank, three-stage stucco, and metal accents. In addition, the development would have amenities such a pool, club house/fitness center, dog park, and a hike and bike trail that would tie into the City's trail system via a trail head that will be paid for and constructed by Embry.

Mr. Kirk concluded his presentation noting the high demand for the type of product they were proposing and cited similar projects in the area that are all above 90% occupied.

Mr. Scott Polikov, President, Gateway Planning Group, 101 Summit Avenue, Fort Worth, Texas, stated that the area needed a jump start with an urban residential, TOD project, and felt the proposed project could be the catalyst that has been discussed in some of the redevelopment and enhancement studies in the City. He added that the 2000 Land Use study called for urban residential for the property as opposed to retail, and suggested the project would serve the type of residents who work for the large employers in the Telecom Corridor.

In closing, Mr. Polikov acknowledged that his company did not usually handle this small a project, but when Embry Partners, who has an outstanding reputation, asked his company to participate they were eager to do so.

Mr. Tod Fobare, Property Owner, 5825 Park Lane, Dallas, Texas, stated his company owned a lot of property in the area and felt the proposed project would act as a catalyst to increase

development from Arapaho Road north to Campbell Road. He thought that more vertical developments would follow as properties redevelop closer to the Arapaho DART Rail station and noted that plans are in the works for an office building on the property to the south.

Mr. Dahlstrom stated he wanted to conclude the group's presentation by answering two of the questions posed by the Commission: 1) item 8(c) in the proposed conditions was put in place to head off any unintended consequences from putting a residential use next to a nonresidential use; and 2) mixed-use can be horizontal as well as vertical and the proposed project would be a component of that mixed-use in and around the Arapaho station.

Commissioner Bouvier asked if the hardy panels referred to in the Commission's packet were one solid piece as opposed to the typical hardy planks.

Mr. Eric Ernschaw, BGO Architects, 4144 N. Central Expressway, Dallas, Texas, replied that hardy panels were fiber cement panels that come in 4' by 8' sheets with the joints concealed so there are no battens or reveals and looks like a smooth stucco or wood textured panel.

With no other comments in favor or opposed, Chairman Gantt closed the public hearing.

Commissioner DePuy asked about vertical access to the apartments.

Mr. Kirk replied the units would be accessed via interior corridors and stairwells.

Vice Chair Hand asked why gates were listed on the concept plan when the project was suppose to be urban.

Mr. Polikov replied that there had been concern on the part of the applicant about cut through traffic, but after speaking with staff, an agreement was reached to return to the original proposal and remove the gates. Also, in areas where gates would be needed for internal parking security, the engineering staff thought something could be worked out to avoid turn around conflicts.

Chairman Gantt stated he understood the possibility of security issues, but did not think there would be a problem with cut through traffic and Mr. Polikov agreed.

Vice Chair Hand asked why the applicant was proposing 3-story buildings throughout most of the project as opposed to 4 stories, especially along the frontage road to Highway 75.

Mr. Polikov replied the proposal was the maximum urban format possible under the rent structure in the market place going forward for the next several years. In addition, the proposed development served the mid-level market and more closely matched the wages in the Telecom Corridor at \$1.30 to \$1.40 per square foot for rent. He did not think the market would support rent on podium or structured parking construction.

Regarding 4 stories along the frontage road to Highway 75, Mr. Ernschaw replied there will be three stories of residential units in the building along the frontage road, but the perception of the height of the building will be greater than 3 stories with a vertical elevation of 50 feet.

Vice Chair Hand stated he could accept 3-story buildings down the center boulevard because of the outdoor space being developed, but wanted to know if the green spaces in front of the buildings were patios or yards, which he felt took away from the urban feel of the design.

Mr. Ernschaw replied there would be a meandering sidewalk with 8-foot tree wells against the parallel parking, and between the sidewalk and the building there will be landscaping.

Mr. Shacklett replied that there would be approximately 12 feet between the balcony/patio and the parallel parking (5-foot walkway, 7-foot landscape).

Commissioner DePuy stated that the proposed concept plan reminded her of a project in Uptown Dallas with buildings close to the sidewalk and some landscaping, which makes it a very comfortable environment for the residents. She added that the concept plan made sense to her and thought it was the right fit for the site.

Commissioner Frederick stated she liked the plan and felt it was distinctive enough to attract attention along Highway 75. She felt the green space in front of the buildings softened the hardscape just a little bit, but left the urban feel.

Chairman Gantt stated at first he was having a hard time seeing how the proposed concept plan was a good idea because of the industrial zoning to the south, but after a more detailed look at the plan and listening to the presentation, he concurred that the project was a good fit for the area.

Vice Chair Hand noted that proposals had come before the Commission in the past with plans for small apartments, and he acknowledged that the applicant was proposing high quality premium units with the smallest at 550 square feet, but wanted to know how the Commission could codify that the units would not be small, cheap efficiencies.

Mr. Kirk replied they had designed one-bedroom apartments that were high in quality and efficient, but were not the typical efficiency apartments.

Mr. Polikov stated that he could understand the concern of a smaller unit if there were no other elements in the PD conditions that required quality construction, and felt the type of resident who would live in the development would be interested in the amenities, location, and the lifestyle. In addition, for the City to remain competitive they would have to look at changes in the market and the current zoning ordinance was not nuanced enough to do that so that was why the PD made sense.

Vice Chair Hand stated he understood financially why the applicant was not building podium with retail/live/work type units, but asked why that type of environment was not being built and filled with residential for now.

Mr. Polikov replied the market was in the City and not in the site, and may very well never be in the site. He added the amenity level was part of the rent structure renters would be willing to pay. If the developer wanted to go to podium construction and have the retail space to eventually fill in, there might be a problem because higher rents would have to be charged, higher than what was called for in the market. Polikov urged the Commission to

view the project as an incremental investment to up-tick the market to allow future developments to build mixed-use, podium style construction.

Commissioner Linn stated he thought the proposed development was a good idea for the area, but would prefer to wait until the Arapaho Collins redevelopment/enhancement study was complete and let the study dictate the land use around the station. In addition, he did not feel there was enough data to back up the claim that the project could be a catalyst for the area.

Mr. Polikov replied that his company had worked on other area studies for Cities in the Metroplex, including Richardson, and, based on his opinion, if his company was working on the contract for the Arapaho Collins study, he felt their recommendations would not be that much different from the concept plan being presented to the Commission. He added that the development would act as a catalyst by making a statement to the market that if Embry was willing to invest in the location, then maybe other investors and developers should too.

Mr. Dahlstrom added they had visited with the staff and were told the Arapaho Collins study would be a different type of study and that the request was a reasonable use of the property.

Mr. Chavez stated that the proposed study for Arapaho Collins would be a market study, which could possibly lead to a land use study of the area, but there were no guarantees that would happen and that the market study would be completed in the first quarter of 2013.

Mr. Kirk addressed the catalyst statement by citing a redevelopment project Embry did in San Antonio on the site of a run-down motel and how the area is now home to a new Target, Wal-Mart Supercenter, and Chick-fil-A. He also mentioned some areas in Dallas where residential made an impact and started the turn around process.

Commissioner DePuy stated the area was definitely a TOD area and felt the apartments would appeal to young professionals. She added that to wait on the project would be detrimental to other developments starting to take place along Highway 75.

**Motion:** Commissioner Frederick made a motion to recommend approval of Zoning File 12-11 as presented, with an additional condition to allow the option to remove the gates at both the east and west drives; second by Commissioner DePuy.

Vice Chair Hand stated he thought the proposal was interesting, but was hoping to have more time to deliberate and negotiate with the applicant. He also thought the motion should state the gates should be removed as opposed to having the option of removing them.

Mr. Hand closed his comments by citing a section of the Gateway Planning document about older apartment complexes and cautioned the Commission to apply the lessons learned when making their decision.

Motion passed 4-3 with Vice Chair Hand and Commissioners Bouvier and Linn opposed.

**EXCERPT  
CITY OF RICHARDSON  
CITY COUNCIL MINUTES – September 24, 2012**

**PUBLIC HEARING, ZONING FILE 12-11:** A request by John S. Kirk, representing Embrey Partners, Ltd., for a change in zoning from I-FP(2) Industrial with special conditions to PD Planned Development for the development of a multi-family community to be located at the southeast corner of Greenville Avenue and Collins Boulevard. The property is currently zoned I-FP(2) Industrial.

**Staff Comments**

Michael Spicer, Development Services Director, reviewed this item.

**Council Comments**

Councilmember Dunn asked how many units are proposed. Mr. Spicer replied 351.

Councilmember Omar inquired about the acreage of the property. Mr. Spicer stated it was just under 13 acres.

Councilmember Solomon asked about the landscaping on the area backing to Collins Street. Mr. Spicer explained that it would be buffered by a six foot wrought iron fence and landscaping.

**Public Hearing**

Mayor Townsend opened the Public Hearing at 7:54 p.m.

The following, all representing the applicant, spoke in favor of the request and addressed questions from Council.

Bill Dahlstrom and John Kirk, Embry Partners Ltd.  
Scott Polikov, Gateway Planning Group  
Tod Fobare, property owner

**Council Comments**

Mayor Pro Tem Maczka and Councilmember's Omar and Mitchell expressed significant concerns with no option for retail development. They also commented on the elevations being too low for the property and in regards to urban development.

Councilmember Mitchell stated the proposed development was not his idea of "urban development".

Councilmember Solomon expressed concerns with the amount of landscaping on Collins Street. Mr. Kirk explained that the full landscape plan has not been submitted yet and stated it would include a 10-12 foot hike and bike trail as well as landscaping. Mr. Solomon also expressed concerns with traffic regarding the main entrance off Greenville. Mr. Kirk stated that they have worked extensively with staff on the traffic plans.

Councilmember Dunn inquired about the dog park, specifically, how the park would be kept exclusive to the development. Mr. Kirk replied that the management company would manage the dog park.

Councilmember Hartley commented on the property being located within the Arapaho/Collins study area and inquired if Council should wait for the results of the study before considering this request. Mr. Johnson reviewed the study area and explained that this parcel was not currently included in the study area. Mr. Polikov explained that the proposed development meets all the elements of the study.

Council expressed appreciation to Mr. Fobare and the applicants.

With no further public comments, Mayor Pro Tem Maczka moved to close the Public Hearing seconded by Councilmember Dunn. A vote was taken and passed unanimously.

### **Council Comments**

Councilmember Omar stated that he highly respected all of the players, but does not see the same vision they have presented. He explained that he is okay with apartments, but would like to see the opportunity for the first floor to be mixed use. He also stated he did not like the surface parking.

Mr. Dahlstrom stated that the applicant is willing to look at options for parking and using building one as mixed used in the future.

Mayor Pro Tem Maczka stated that she preferred all buildings facing streets to have adaptability for mixed uses.

Councilmember Mitchell commented that in the end, if approved, Council would be approving 350 apartment units.

After a lengthy discussion on the issues mentioned, Council and the applicants determined that more time was needed to consider the issues and bring back revised plans.

### **Council Action**

Councilmember Omar moved to table this item to October 22, 2012. Mayor Pro Tem Maczka seconded the motion. A vote was taken and passed, 7-0.

**EXCERPT  
CITY OF RICHARDSON  
CITY PLAN COMMISSION MINUTES – November 12, 2012**

**PUBLIC HEARING ITEMS:**

- 1. CONTINUED CONSIDERATION OF ZONING FILE 12-11: A REQUEST BY JOHN S. KIRK, REPRESENTING EMBREY PARTNERS, LTD., FOR A CHANGE IN ZONING FROM I-FP(2) INDUSTRIAL WITH SPECIAL CONDITIONS TO PD PLANNED DEVELOPMENT FOR THE DEVELOPMENT OF A MULTI-FAMILY COMMUNITY TO BE LOCATED AT THE SOUTHEAST CORNER OF GREENVILLE AVENUE AND COLLINS BOULEVARD (CONTINUED FROM OCTOBER 22, 2012, CITY COUNCIL MEETING).**

**Staff/Applicant Comments**

Mr. Spicer reviewed this item for Council. Bill Dahlstrom, the applicant; Todd Fobare, the property owner; and John Kirk, the developer; also addressed Council and were present for questions. Mr. Kirk provided a Power Point and reviewed the density –increased by 30%; urban design with four stories and elevations up to 60 feet; retail ready on first floor with 18 feet in height and 45 feet in depth; structured parking with garage; and new feature of community park.

**Council Comments**

There was significant Council discussion with the following issues being raised as concerns:

- Parking garage –orientation, location, and cladding materials
- Community park – location, ownership, maintenance
- Current study area for Collins –should this piece be included
- Current zoning of property and uses that are allowed by right
- Density of proposed development –not dense enough to support retail

Mr. Kirk requested that Council defer rather than deny the request in order to review the Council's direction.

**Council Action**

Mayor Pro Tem Maczka moved to table this item to the January 28 Council Meeting. Councilmember Solomon seconded the motion. A vote was taken and passed, 6-1 with Councilmember Mitchell voting in opposition.



## Staff Report

**TO:** City Council

**THROUGH:** Michael Spicer, Director of Development Services **MS**

**FROM:** Sam Chavez, AICP, Asst. Dir. of Development Services (Planning) **SC**

**DATE:** January 24, 2013

**RE:** **Zoning File 12-11:** Planned Development – GreenVUE PD

### REQUEST:

Rezone 11.13 acres from I-FP(2) Industrial with special conditions to PD Planned Development for the development of a multi-family community located at the southeast corner of Greenville Avenue and Collins Boulevard.

### APPLICANT / PROPERTY OWNER:

John S. Kirk – Embrey Partners, Ltd. / Leora Azoulay Lesh – SAF CTP, LLC

### EXISTING DEVELOPMENT:

The property is undeveloped.

### ADJACENT ROADWAYS:

**Alma Road:** Four-lane, divided major collector; 3,800 vehicles per day on all lanes, northbound and southbound, south of Collins Boulevard (May 2011).

**Collins Boulevard:** Six lane, divided arterial; 11,000 vehicles per day on all lanes, eastbound and westbound, on the Collins Boulevard overpass (May 2011).

**Greenville Avenue:** Six-lane, divided arterial; no traffic counts available between Arapaho Road and Collins Boulevard.

### SURROUNDING LAND USE AND ZONING:

**North:** Office; PD Planned Development

**South:** Industrial; I-FP(2) Industrial

**East:** Industrial; I-FP(2) Industrial

**West:** Retail/Commercial (across DART and US-75); C-M Commercial

## **FUTURE LAND USE PLAN:**

### **Enhancement/Redevelopment**

*These are areas where reinvestment and redevelopment is encouraged. Further study may be necessary to understand the full potential for redevelopment. This property is located in the East Arapaho/Collins enhancement/redevelopment area and is part of the City's Tax Increment Finance (TIF) district. This area has been challenged in recent years by evolving markets, technology, and user requirements. Redevelopment, enhancement, and building format changes should be considered. Mid-rise office uses are appropriate throughout the area and mixed-use buildings with ground-floor retail could be appropriate at key locations, including adjacent to the Arapaho Center rail transit stations.*

### **Future Land Uses of Surrounding Area:**

North: Office/Industry

South: Enhancement/Redevelopment

East: Enhancement/Redevelopment

West: Community Commercial

## **EXISTING ZONING:**

I-FP(2) Industrial (Ordinance Number 29-A).

## **TRAFFIC/ INFRASTRUCTURE IMPACTS:**

The applicant provided a traffic impact analysis (TIA) and conceptual drainage study per the requirements for a PD Planned Development District. **Based on the TIA, staff has worked with the applicant to provide adequate driveway locations along Greenville Avenue and Alma Road. The proposed driveway along Greenville Avenue was moved south from Collins to provide adequate transition and storage for a left turn lane to the property from Greenville Avenue.** Staff has also reviewed the conceptual drainage plan and determined the proposed use would not negatively impact the City's infrastructure.

## **APPLICANT'S STATEMENT**

(Please refer to the complete Applicant's Statement.)

## **STAFF COMMENTS:**

### **Background:**

The subject site undeveloped lot. It is bounded on the north by the Collins Boulevard overpass. At the west end of the property, the overpass is approximately thirty-five (35) feet above grade and as the road moves eastward, it comes back to grade as it approaches Alma Road (see attached oblique aerial and photos). The site is also partially bounded along the southwest property line by the KCS Railroad and a vacant office/industrial building to the south. The property is located between one-quarter and one-half mile north from the Arapaho DART station, which is connected via a trail along the east side of Greenville Avenue.

In 2001, a land use study was conducted for the Arapaho DART Station area to determine what types of uses and development patterns would be appropriate surrounding a multi-modal transit station. The study led to the creation of a draft station area plan that was developed in response to recommendations from an Urban Land Institute (ULI) Advisory Service Panel Report published in 2000. The station area plan encouraged the development of commercial development around the Arapaho Center station as the dominant land use, but suggested flexibility to allow a mix of land uses on larger parcels to include retail, residential and office uses while allowing developers to respond to market demands. For the subject property and property located directly south, the station area plan designated office, open space, and mixed-use office/urban residential as appropriate land uses.

In 2005, the City submitted an application to rezone the property around the Arapaho Center Station. The area to be rezoned was bounded by Central Expressway to the west, the KCS Railroad to the north and east, and Arapaho Road to the south and did not include the subject property. The proposed PD contained three (3) separate areas around the station that allowed a mix of uses such as retail, commercial, entertainment, office with multi-family allowed by Special Permit on the property between Central Expressway and the DART right-of-way, north of Arapaho Road. The City Plan Commission recommended approval of the zoning change; however, at the City Council meeting, a major property owner within the district stated they were opposed, and the zoning case was tabled for an indefinite period of time. Ultimately, the zoning change was never approved, and the existing Industrial zoning classification is still in place today.

In 2009, the City updated the Future Land Use Plan as part of the update to the City's Comprehensive Plan. The subject property was placed in a future land use category noted as Enhancement/Redevelopment. There were six (6) distinct Enhancement/Redevelopment districts designated in the Plan, and the subject property is located in the East Arapaho/Collins district. The City is in the preliminary stages of a study of this district, which should be completed in the 1<sup>st</sup> quarter of 2013. The study area boundaries have not been finalized, so it is unclear whether the subject property will be included or not. The subject property had previously been designated as a medium density employment/service land use in the 2000 Future Land Use Plan.

#### **Update:**

The City Council considered the applicant's original request on September 24, 2012. The request was tabled to the October 22<sup>nd</sup> City Council meeting. On October 22, 2012, the applicant requested that the item be continued to the November 12, 2012, City Council meeting to allow them the opportunity to further refine the design and modify the PD Planned Development regulations to address the following concerns expressed by the City Council at their September 24, 2012 meeting. Issues addressed at the November 12, 2012 meeting included the following:

- Lack of density – The number of units was increased from 351 units to 401 units (50 unit increase) for a density of 36 units per acre. The increased density was a result of the increased number of units and the reduction of land area. The site area was originally 12.7 acres and has been reduced by 1.57 acres to 11.13 acres. The site reduction included 0.723 acres from the south and the 0.847-acre site designated on the Concept Plan as “Future Park”.

- Lack of retail/mixed use opportunities – The northern ground floor of Building A, and the ground floors of Buildings C and D that face Alma Road were designated and would be constructed as “retail-ready”, with a minimum floor height of twenty-one (21) feet.
- Lack of building height for an urban development – The building heights were increased to a maximum of four (4) stories, not to exceed sixty (60) feet. With the exception of Building E and the western portion of Buildings D, which were three-story in height, the balance of the buildings were 4-story buildings.
- Amount of surface parking – The number of surface parking was reduced from 380 to 306 spaces. The parking design included a 2-level parking garage, in addition to tuck under parking for all of the buildings.

At the November 12, 2012 Council meeting, ongoing concerns were discussed, including the location of the site within the East Arapaho/Collins enhancement/redevelopment study area and lack of density and its inability to support the retail portion of the development. The Council also raised concerns with the revised concept plan regarding the location and orientation of the parking deck along with the location and ownership of the proposed park. Since the November 12, 2012 Council meeting, the applicant has revised the concept plan to address some of Council’s issues including the following:

- Parking deck location/orientation – The proposed parking deck is located in the center of the property. The previous concept plan located the deck closer to Collins Boulevard and no building was located between the deck and Collins. The revised concept plan has provided an additional 4-story building, located on the north side of the parking deck which screens the view of the deck from Collins Boulevard. The other three (3) sides are also screened by Buildings C and D. Although the parking deck is not a typical wrapped parking garage, the buildings have been located directly adjacent to it to provide that style of parking structure.
- Retail portion of development – The previous plan depicted approximately 8,000 square feet of retail ready space along Alma Road and 6,000 square feet of retail ready space along Greenville Avenue. The revised concept has removed the retail ready space along Alma Road. There is only 6,000 square feet of retail ready space remaining which is to be located in the northern portion of Building A along Greenville Avenue.

The applicant has revised the proposed concept plan to address the City Council’s concerns and are attached as Exhibits “B”, “C-1”, and “C-2”. The original exhibits are attached for reference and are dated “09-24-12” and “11-12-12”.

## Applicant's Original and Revised Request

	<b>Original (9/24/12) Proposed Development (ZF 12-11)</b>	<b>Revised (1/28/13) Proposed Development (ZF 12-11)</b>
<b>Site Area</b>	12.7 acres	11.13 acres
<b>Current Zoning</b>	I-FP(2) Industrial	I-FP(2) Industrial
<b>Proposed Zoning</b>	PD Planned Development	PD Planned Development
<b>Number of Units</b>	351	408
<b>Number of Buildings</b>	Eight (8) multi-family buildings consisting of 1-bedroom and 2-bedroom units	Six (6) multi-family buildings consisting of 291, 1-bedroom and 117, 2-bedroom units
<b>Access</b>	Greenville Avenue and Alma Road, connected by a main spine road that serves as the major driveway through the property.	Greenville Avenue and Alma Road, connected by a main spine road that serves as the major driveway through the property.
<b>Governing Ordinance</b>	PD Planned Development Regulations.	PD Planned Development Regulations (attached as Exhibit "F").
<b>Misc. Requests</b>	<p>Variance from Chapter 21 (Subdivision and Development Code, Section 21-58):</p> <p>Consider the 351-unit complex as one apartment community</p> <p>Waive the requirement for physical separation between every 250 units.</p>	<p>Variance from Chapter 21 (Subdivision and Development Code, Section 21-58):</p> <p>Consider the 408-unit complex as one apartment community</p> <p>Waive the requirement for physical separation between every 250 units.</p> <p>Waive the requirement for a perimeter fencing around an apartment complex.</p>
<b>Other Features</b>	Multiple open space areas, including an open space/pool area located behind the leasing center/clubhouse in the southwest portion of the property. A 6-foot decorative metal fence surrounds the property and a 10-foot wide bike trail is provided along all three (3) street frontages.	Multiple open space areas including a dog park and an open space/pool area located behind the leasing center/clubhouse in the southwest portion of the property. A 10-foot wide bike trail is provided along all three (3) street frontages.

### Revised Proposed Development

- Lot Area: 11.13 acres / 484,739 square feet
- Number of Units/Density: 408 units / Maximum 37 dwelling units per acre.
- Building Area: Six (6) apartment buildings and leasing center/clubhouse totaling 499,176 square feet / approximately 37% lot coverage (including parking deck) (max 40% proposed)

- Setbacks:
  - Front: 40 feet along Greenville Avenue and Collins Boulevard / 25 feet along Alma Road.
  - Side: Side setbacks adjacent to the park shall be provided as shown on Exhibit “B”.
  - Rear: 25 feet along southern property line and adjacent to KCS Railroad except for accessory structures related to pool area/central open space may be located within five (5) feet of said property lines.
- Number of Parking Spaces: 612 required/614 proposed (proposed parking ratio of 1.5 spaces per dwelling unit).
- Building Height: Maximum four (4) stories, not to exceed sixty (60) feet, excluding architectural features in scale with the building. With the exception of the two (2) buildings labeled “Building B”, which are three-story structures, the balance of the buildings are 4-story buildings. Building A, a four (4) story building features units above the entry drive (Exhibit “D-4”).
- Building Materials: The buildings will be constructed of a combination of brick, stone, concrete block, 3-stage stucco, metal panels, and hardipanel. The minimum amount of masonry allowed will be 70% per elevation, which is utilized on the west elevation of Building A and the north elevation of Building E. The other building elevations will be between 74% and 77% masonry construction as noted on the attached elevations (Exhibits “C-1” and “C-2”).
- Landscaping: 30% proposed.

The following table provides a comparison between the A-950-M Apartment regulations, the original proposed development, the latest revised proposed development, and similar developments (Brick Row, The Venue and Eastside).

Regulation	A-950-M Apartment	Original Proposed Development (ZF 12-11)	Revised Proposed Development (ZF 12-11)	Brick Row	The Venue	Eastside
Min. Floor Area per Unit	700 s.f.	550 s.f.	600 s.f.  <b>INCREASE</b>	1-bedroom - 750 s.f. 2-bedroom -900 s.f.  5% of each unit type allowed to be reduced up 25% provided overall average per building is 800 s.f.	1-bedroom - 700 s.f. 2-bedroom -900 s.f.  5% of each unit type allowed to be reduced up 75% provided overall average is 750 s.f.	625 s.f., except up to 8 units may be a minimum 500 s.f.  Average floor area shall be 850 s.f.

<b>Number of Units Developed</b>	Max 18 units per acre allowed	351	408 <b>INCREASE</b>	577	285	436
<b>Building Materials</b>  (Masonry considered to be brick, concrete block, and stone)	Min. 75% masonry	Min. 50% masonry on each elevation (Elevations range from 50% to 77% masonry)	Min. 70% masonry on each elevation (Elevations range from 70% to 77% masonry)  <b>INCREASED MINIMUM PERCENTAGE</b>	100% masonry on ground floor / Overall min. 85% masonry for entire building  Reduced requirements for exterior walls of courtyard areas above 1 <sup>st</sup> floor	100% masonry on ground floor / Min. 50% above ground floor  Reduced requirements for exterior walls of courtyard areas above 1 <sup>st</sup> floor	Min. 50% masonry
<b>Parking Ratio</b>	2 spaces per unit (.5 garage or carport/unit)	1.5 spaces/unit (no requirement for garage/carport)  158 garage or tuck under spaces provided (approx. 0.45 spaces/unit)	1.5 spaces/unit (no requirement for garage/carport)  312 deck or tuck under spaces provided (approx. 0.76 spaces/unit)  <b>INCREASE</b>	1-BR – 1.5 spaces/unit 2-BR – 1.75 spaces/unit 3-BR – 2 spaces/unit  Multi-family projects required structure parking	1.5 spaces/unit  Required structured parking	1.5 spaces/unit
<b>Max. Height</b>	2-story (max. 40')	4-story (max. 50')	4-story (max. 60')  <b>INCREASE</b>	6-story (max. 100') / Developed at 4 stories (66'9" to top of architectural features)	Max. 151' / Developed at 87'10" to top of architectural features)	Max. 80'
<b>Max. Density</b>	18 units/acre	28 units/acre	37 units/acre  <b>INCREASE</b>	Max. 577 units allowed (no density requirement) Developed at approx. 52 units/acre on lots developed as multi-family)	90 units/acre / developed at approx. 64 units/acre	Max 450 units allowed (Allows max. 30.4 units/acre). Developed at 29.5 units/acre
<b>Recreational Areas</b>	For each 250 units a min. 900 s.f. playground for children under 10 years  For each 250 units a min. 70 recreational amenity points.	No playground being proposed  A minimum 100 amenity points shall be required per proposed PD regulations	No playground being proposed  A minimum 100 amenity points shall be required per proposed PD regulations	2 acres of public open space required within 30-acre Brick Row development  Additionally, a minimum 70 amenity points required (40 on-site) for each 350 units	No specific amenity points requirement. Approved as part of master plan and site plan approval.	Min. 900 s.f. in area of indoor or outdoor recreation space designed for use of residents.  Additionally, a minimum 70 points required for entire development

### **Proposed PD Standards:**

The proposed PD standards utilize the A-950-M Apartment District regulations as a base zoning district, but calls out all areas where deviations from the district are being requested. To accommodate the proposed development, much of the base zoning district regulations are being revised including building materials, height, setbacks, density, parking and recreational areas. The revisions are being requested because the applicant is proposing a product that is more urban in nature than what our typical A-950-M Apartment regulations would allow. Several of these deviations are discussed below:

Minimum Floor Area of Dwelling Unit – The applicant is proposing to allow the minimum floor area of a dwelling unit to be 600 square feet. They are proposing to construct 291, 1-bedroom/1-bathroom units and 117, 2-bedroom/2-bathroom units. The minimum floor area allowed within the A-950-M District is 700 square feet regardless of the number of bedrooms. The applicant is proposing a lower minimum because a large part of their market is single residents who do not want to maintain a large living space and prefer a smaller living space with a higher quality finish-out. The applicant has also stated that their 1-bedroom, 600-square foot unit is currently their fastest leasing unit in other projects.

Building Materials – The applicant proposed regulations require a minimum 70% masonry construction per elevation as depicted on the attached elevations (Exhibits “C-1” and “C-2”). They are proposing to include 3-stage stucco and metal panels as non-masonry materials that could be used for the remainder of the elevations in addition to hardipanel.

In the West Spring Valley Corridor PD, exterior walls at the ground floor level of buildings were required to be a minimum 50% masonry; however, the remaining 50% of the exterior walls were allowed to utilize 3-stage stucco or ventilated façade systems (type of metal panels). Although the typical A-950-M Apartment District regulations require a minimum 75% masonry construction, the applicant is proposing to utilize higher quality, non-masonry materials such as 3-stage stucco and metal panels that they feel will create a quality design.

Building Height/Density – The applicant is proposing that a maximum 4-story building, not exceeding sixty (60) feet, exclusive of architectural features respecting building scale, be allowed. Due to the increased proposed density and urban nature of the project, the proposed buildings will be four (4) stories in height. The proposed density of the project is a maximum thirty-seven (37) dwelling units per acre. In the A-950-M Apartment District, buildings are restricted to a maximum of two (2) stories, not to exceed forty (40) feet in height and density is limited to a maximum of eighteen (18) dwelling units per acre. The applicant is also proposing the addition of elevators to the 4-story buildings.

Building Setbacks – The A-950-M Apartment District requires a landscaped 30-foot front setback along streets. The proposed development has three (3) street frontages.

- Along Greenville and Collins, the applicant intends to provide a 40-foot building setback.
- Along Alma, the proposed building location is approximately seventy-five (75) feet back of the property line; however, they are requesting a minimum 25-foot setback.

Along the southern property line and KCS Railroad, the applicant is proposing a 25-foot setback that is consistent with the rear property line required in the A-950-M Apartment District; however, they are requesting that accessory structures related to the central open space/pool area at the southwest corner of the property be allowed to utilize a setback of five (5) feet.

Perimeter Landscape Setbacks – The A-950-M Apartment District requires that the 30-foot front setbacks shall be landscaped. The proposed development has three (3) street frontages.

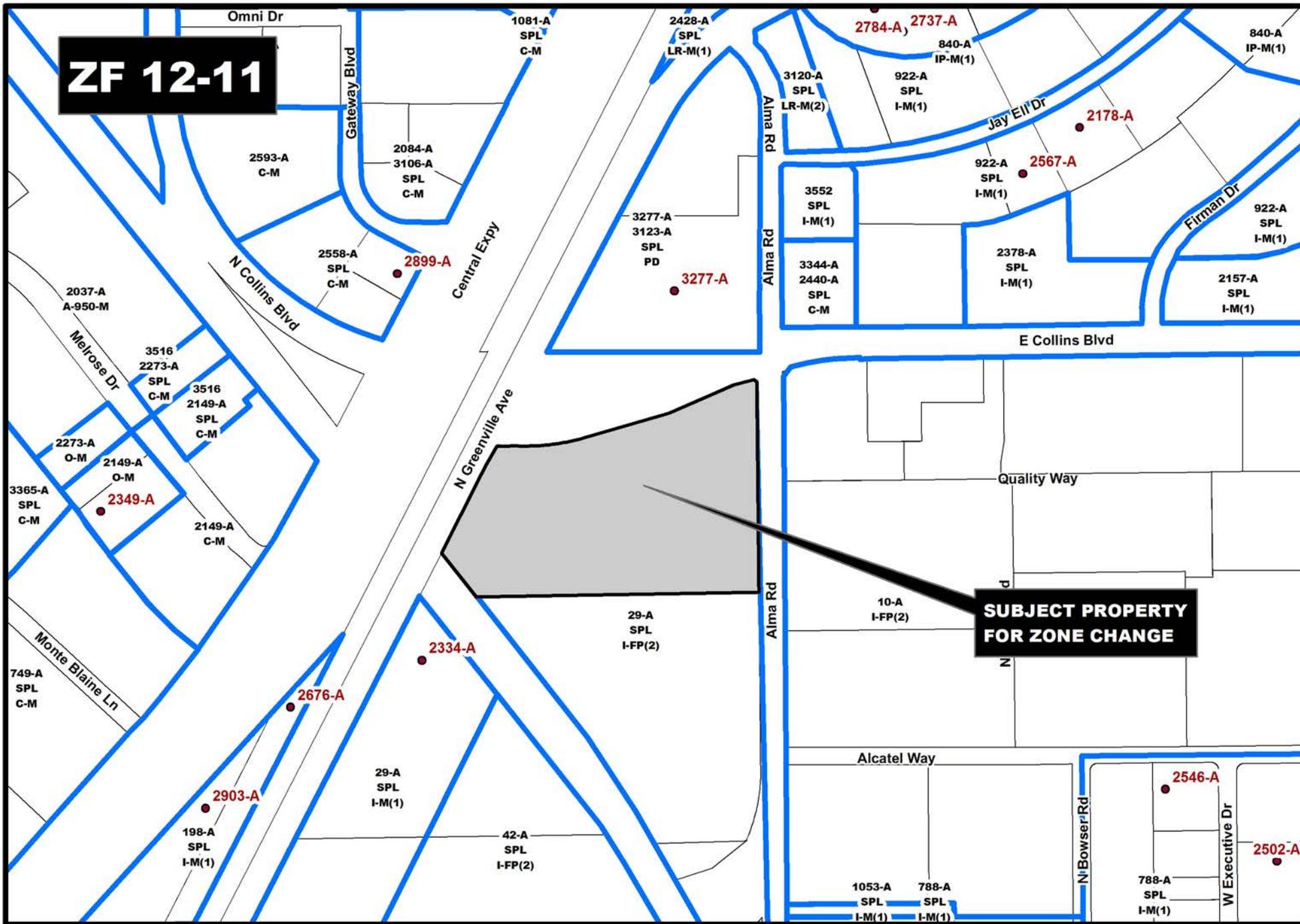
- Along Greenville, the applicant intends to provide a 30-landscape setback except for a small area where a portion of a driving aisle encroaches. The applicant intends to provide one (1) canopy tree and one (1) ornamental tree for every fifty (50) feet of lineal frontage as suggested in the City’s Landscaping Policy.
- Along Alma, the proposed landscape setback is a minimum of twenty (20) feet. This is due to the location of the building along Alma as stated above. The applicant intends to provide one (1) canopy tree and one (1) ornamental tree for every fifty (50) feet of lineal frontage as suggested in the City’s Landscaping Policy.
- Along Collins, a minimum 6-foot landscape setback is proposed. The landscape area is reduced to accommodate the driving aisle and parking spaces along the north side of the development. The applicant feels the reduced landscape setback is appropriate since most of the Collins frontage is along the retaining wall of the overpass so the need for the landscape buffer is not necessary. Along with the reduced landscape buffer, the applicant does not intend to provide the canopy and ornamental trees adjacent to the retaining wall.

Internal Landscaping – The applicant intends to comply with the City’s Landscape Ordinance and Policy for the internal site landscaping. In addition to the City’s typical landscaping policies which include minimum 10-foot wide landscape islands at the end of parking rows planted with canopy trees, the applicant has included a condition regarding proposed landscaping along the main east-west drive as noted on the concept plan (Exhibit “B”). Along both sides of the drive, the applicant proposes to place canopy trees in bulb-outs in between the parallel parking spaces. This placement of trees will provide a shaded area along the sidewalks that run adjacent to the east-west drive.

**Correspondence:** No correspondence has been received.

**Motion:** On September 4, 2012, the City Plan Commission recommended approval of the request on a vote of 4-3 (Commissioners Bouvier, Hand, and Linn opposed) subject to the following special conditions as presented (#1) and with an additional condition (#2) as listed below:

1. The subject site shall be zoned PD Planned Development for the A-950-M Apartment District and shall be developed in accordance with the attached “GreenVUE Planned Development District Proposed PD Conditions” (Exhibit “F”).
2. The gates located at the Greenville Avenue and Alma Road entrances may be removed. ***“The applicant’s revised Concept Plan does not include internal gates”.***



## ZF 12-11 Zoning Map

Updated By: shacklett, Update Date: July 13, 2012  
 File: DSI\Mapping\Cases\Z\2012\ZF1211\ZF1211 zoning.mxd

This product is for informational purposes and may not have been prepared for or be suitable for legal, engineering, or surveying purposes. It does not represent an on-the-ground survey and represents only the approximate relative location of property boundaries.





**ZF 12-11**

**SUBJECT PROPERTY  
FOR ZONE CHANGE**

## ZF 12-11 Aerial Map

Updated By: shacklett, Update Date: July 13, 2012  
File: DS\Mapping\Cases\Z\2012\ZF1211\ZF1211 ortho.mxd

This product is for informational purposes and may not have been prepared for or be suitable for legal, engineering, or surveying purposes. It does not represent an on-the-ground survey and represents only the approximate relative location of property boundaries.





Alma Rd

Collins Blvd

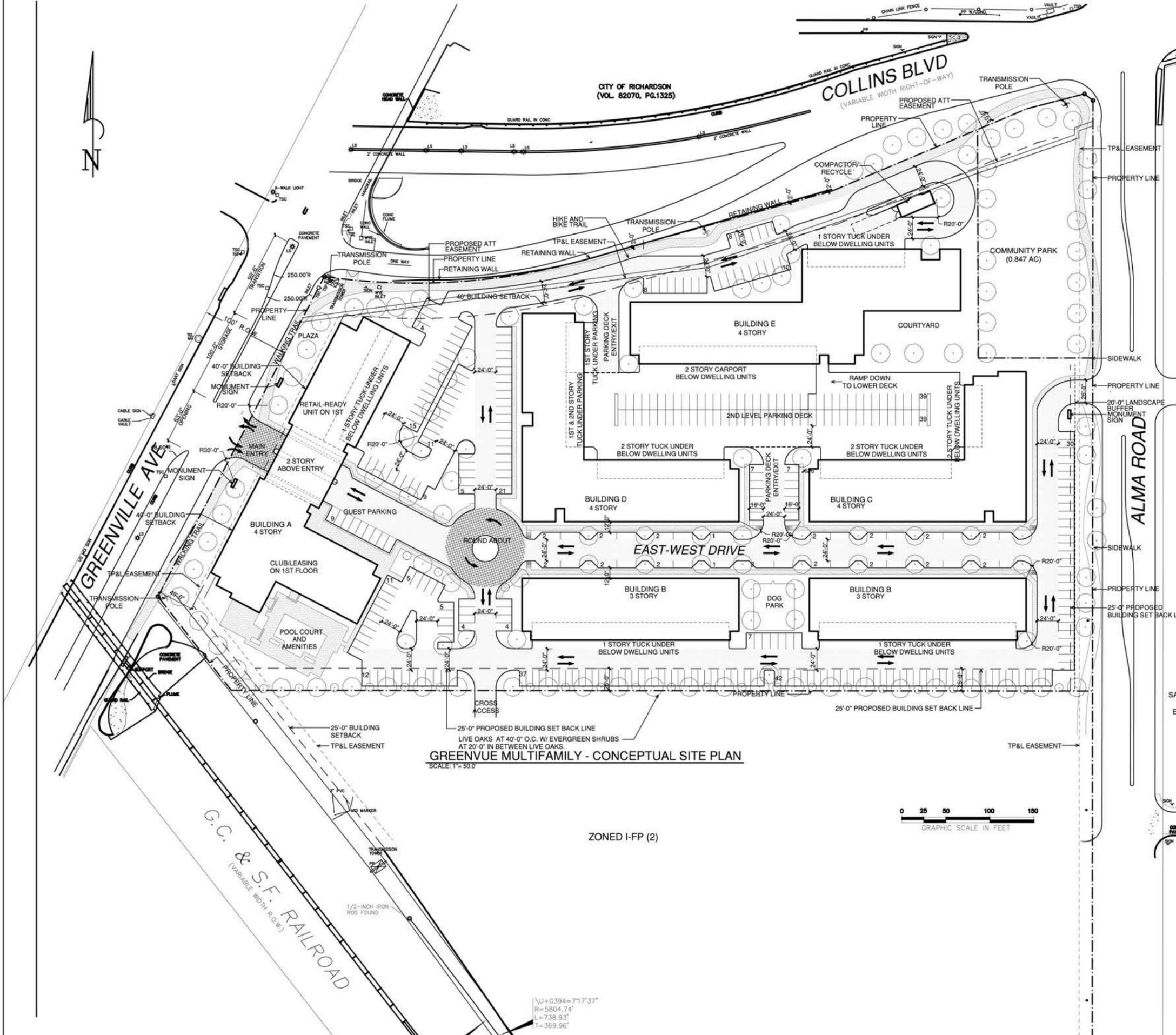
Greenville Ave

KCS RR

DART Light Rail

Oblique Aerial  
Looking East





**GREENVUE MULTIFAMILY - CONCEPTUAL SITE PLAN**  
SCALE: 1" = 50.0'

ZONED I-FP (2)



PROJECT SUMMARY		
	EXISTING	PROPOSED
ZONING	I-FP (2) INDUSTRIAL	PLANNED DEVELOPMENT FOR MULTIFAMILY RESIDENTIAL
USE	VACANT	
SETBACKS	40'-0" ALONG GREENVILLE AVE & COLLINS BLVD. 25'-0" ALONG ALMA ROAD 25'-0" REAR	
LOT AREA	11.13 ACRES OR 484,739.4 SF	
BUILDING SQ. FT. PER FLOOR	FIRST = 126,968 SF SECOND = 116,872 SF THIRD = 127,668 SF FOURTH = 127,668 SF TOTAL = 499,176 SF	
PARKING RATIO REQUIREMENT	1.50 SPACES PER UNIT	
PARKING	REQUIRED 612 - PROVIDED 614 TOTAL SPACES 302 SURFACE SPACES 169 TUCK UNDER PARKING SPACES PARKING DECK: UPPER = 65P LOWER = 78P	
LANDSCAPE %-SF	145,426.72 SF OR 30%	
F.A.R.	0.92 F.A.R. (TUCK UNDER PARKING AND PARKING DECKS ARE NOT CALCULATED WITHIN THIS F.A.R.)	
DENSITY	36.7 UNITS-PER ACRE	
LOT COVERAGE	179,511.2 SF/484,739.4 = 37.0% / 40% MAX. PROPOSED	
BUILDING HEIGHT	60'-0" (MEAN LEVEL BETWEEN EAVE AND RIDGE) / 4 STORY MAX	
TOTAL # OF UNITS	408 UNITS - 291 @ 1B/1B & 117 @ 2B/2B = TOTAL 525 BEDROOMS	

NOTES:  
1. THIS SITE DOES NOT LIE IN THE FLOODPLAIN, FLOODWAY, STREAM COURSE OR OTHER WATER FEATURE.  
2. THERE ARE NO LOADING DOCKS ON THIS SITE.  
3. THERE ARE NO OPEN STORAGE AREAS ON THIS SITE.  
4. TEMPORARY TRASH STORAGE WILL BE LOCATED WITHIN THE BUILDINGS.



**APPLICANT:**  
JOHN KIRK  
1020 N.E. LOOP 410  
SUITE 700  
SAN ANTONIO, TEXAS 78209  
REPRESENTING:  
EMBREY PARTNERS, LTD.

**LAND OWNER:**  
TOD FOBARE, REPRESENTING:  
SAF CTP, LLC.  
18111 PRESTON ROAD  
SUITE 1000  
DALLAS, TEXAS 75252

ZONING EXHIBIT  
EXHIBIT 'B' - PART OF ORDINANCE

REVISIONS  
01-23-2013

**GREENVUE**  
ROCKWALL SUBDIVISION LOT 4, BLOCK 3  
RICHARDSON, TEXAS  
EMBREY

**BGO**  
architects

4144 N. Central Expy.  
Suite 855  
Dallas, TX 75204  
214.520.8878  
bgoarchitects.com

DATE  
01-18-2013

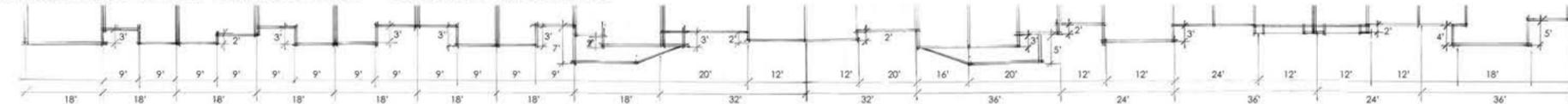
PROJECT  
12130

SHEET NUMBER

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**BUILDING A WEST ELEVATION- GREENVILLE AVE.**

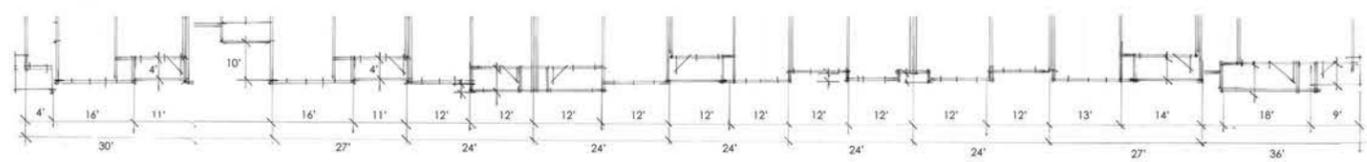


Metal Panel : 6%  
 Hardi Panel : 13%  
 3 Coat Stucco : 11%  
 Brick : 28%  
 Block : 4%  
 Stone : 38%  
 Masonry Total : 70%

Metal Panel : Blue  
 Hardi Panel : White, Gray  
 3 Coat Stucco : White  
 Metal Awning : Blue  
 Metal Canopy : Brown  
 Brick : Gray  
 Block : White  
 Stone : Tan



**BUILDING B SOUTH ELEVATION**



Hardi Panel : 15%  
 3 Coat Stucco : 9%  
 Brick : 40%  
 Block : 5%  
 Stone : 31%  
 Masonry Total : 76%

Hardi Panel : White, Blue, Gray  
 3 Coat Stucco : White  
 Metal Awning : Blue  
 Brick : Gray  
 Block : White  
 Stone : Tan

Hardi Panel : 16%  
 3 Coat Stucco : 10%  
 Brick : 28%  
 Block : 6%  
 Stone : 40%  
 Masonry Total : 74%

Hardi Panel : White, Blue, Gray  
 3 Coat Stucco : White  
 Metal Awning : Blue  
 Brick : Gray  
 Block : White  
 Stone : Tan

**TRASH ENCLOSURE SOUTH ELEVATION**



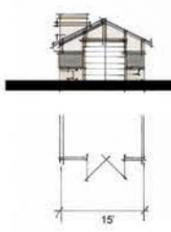
3 Coat Stucco : 36%  
 Brick : 39%  
 Stone : 25%  
 Masonry Total : 64%

**TRASH ENCLOSURE NORTH ELEVATION**



3 Coat Stucco : 15%  
 Brick : 62%  
 Stone : 23%  
 Masonry Total : 85%

**TRASH ENCLOSURE WEST ELEVATION**

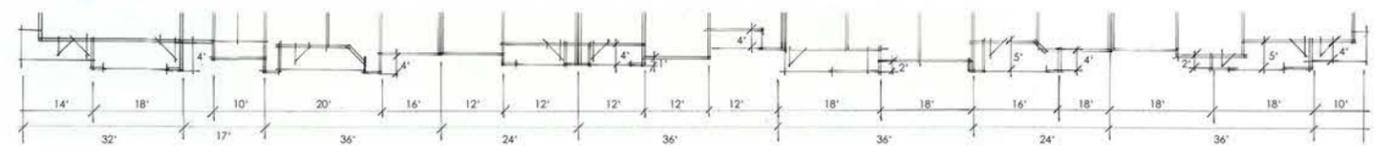


3 Coat Stucco : 5%  
 Brick : 72%  
 Stone : 23%  
 Masonry Total : 95%

3 Coat Stucco : White  
 Brick : Gray  
 Stone : Tan



**BUILDING B NORTH ELEVATION**



**GREENVUE DEVELOPMENT CONCEPTUAL ELEVATIONS  
 RICHARDSON, TX**

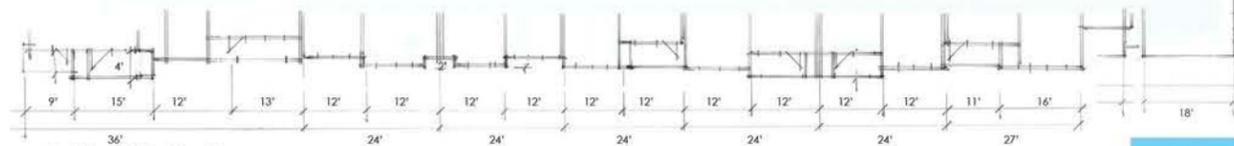
**Exhibit C-1 - Part of Ordinance**

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**BUILDING E NORTH ELEVATION**

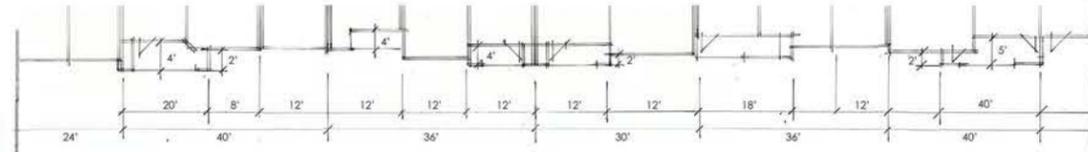


Hardi Panel : 11%  
 3 Coat Stucco : 19%  
 Brick : 29%  
 Block : 4%  
 Stone : 37%  
 Masonry Total : 70%

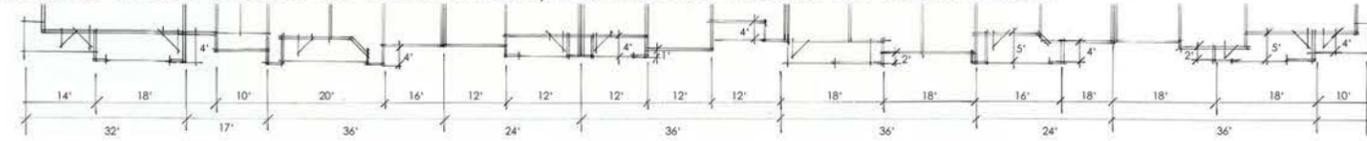
Hardi Panel: White, Blue, Gray  
 3 Coat Stucco : White  
 Metal Awning : Blue  
 Metal Canopy : Brown  
 Brick : Gray  
 Block : White  
 Stone : Tan

Hardi Panel : 15%  
 3 Coat Stucco : 11%  
 Brick : 29%  
 Block : 5%  
 Stone : 40%  
 Masonry Total : 74%

Hardi Panel: White, Blue, Gray  
 3 Coat Stucco : White  
 Metal Awning : Blue  
 Brick : Gray  
 Block : White  
 Stone : Tan



**BUILDING D WEST & SOUTH ELEVATION, BUILDING C SOUTH ELEVATION**

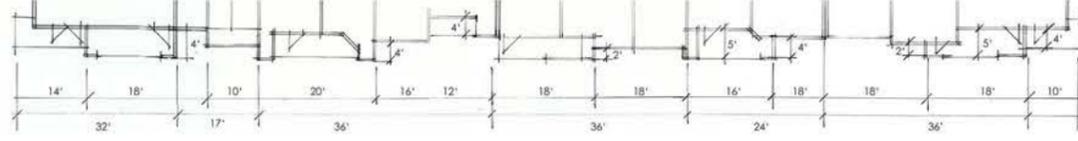


Hardi Panel : 15%  
 3 Coat Stucco : 11%  
 Brick : 29%  
 Block : 5%  
 Stone : 40%  
 Masonry Total : 74%

Hardi Panel: White, Blue, Gray  
 3 Coat Stucco : White  
 Metal Awning : Blue  
 Brick : Gray  
 Block : White  
 Stone : Tan

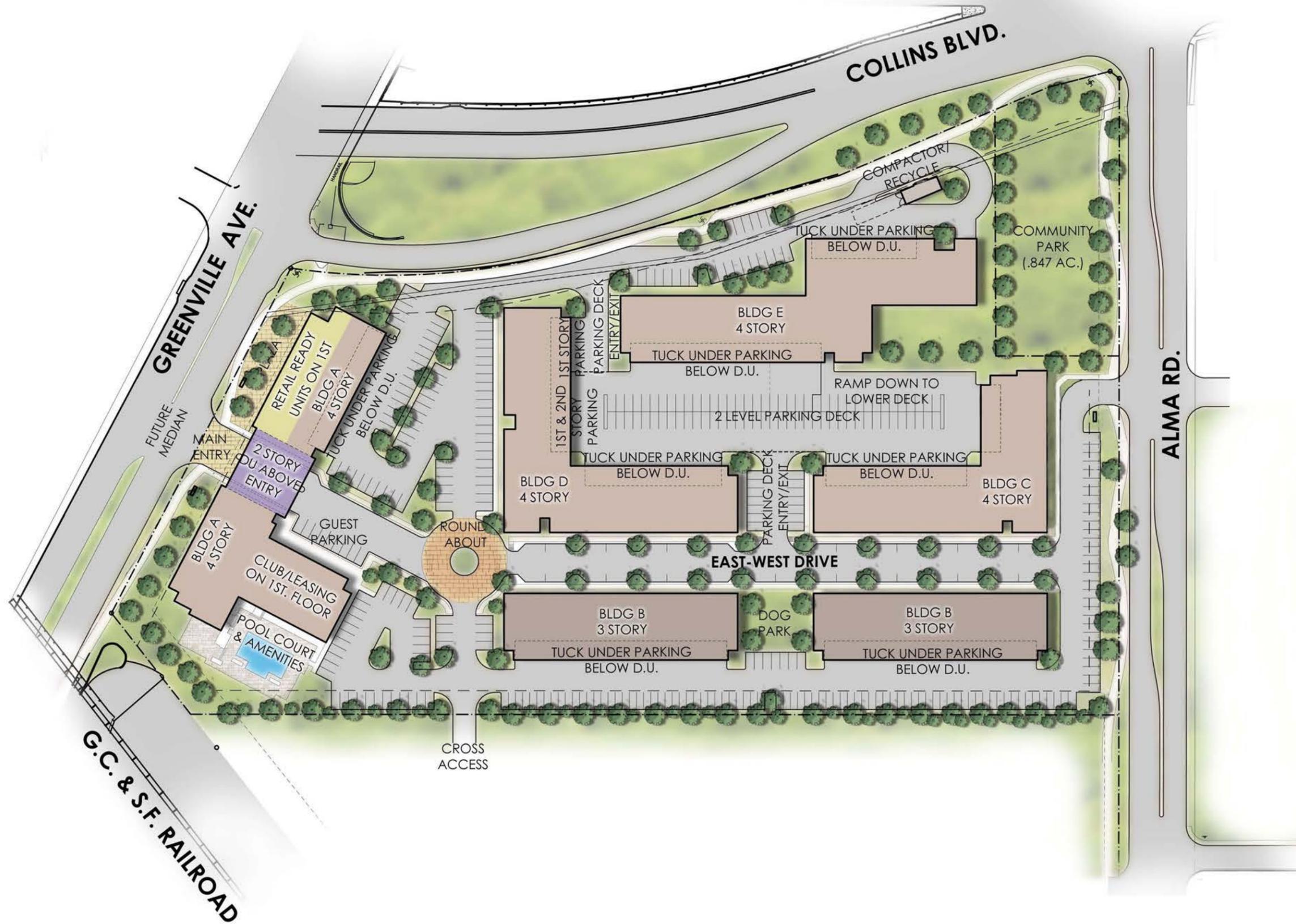


**BUILDING C EAST ELEVATION**



**GREENVUE DEVELOPMENT CONCEPTUAL ELEVATIONS**  
 RICHARDSON, TEXAS

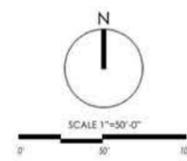
**Exhibit C-2 - Part of Ordinance**



**GREENVUE MULTI FAMILY SCHEMATIC SITE PLAN**

RICHARDSON, TEXAS

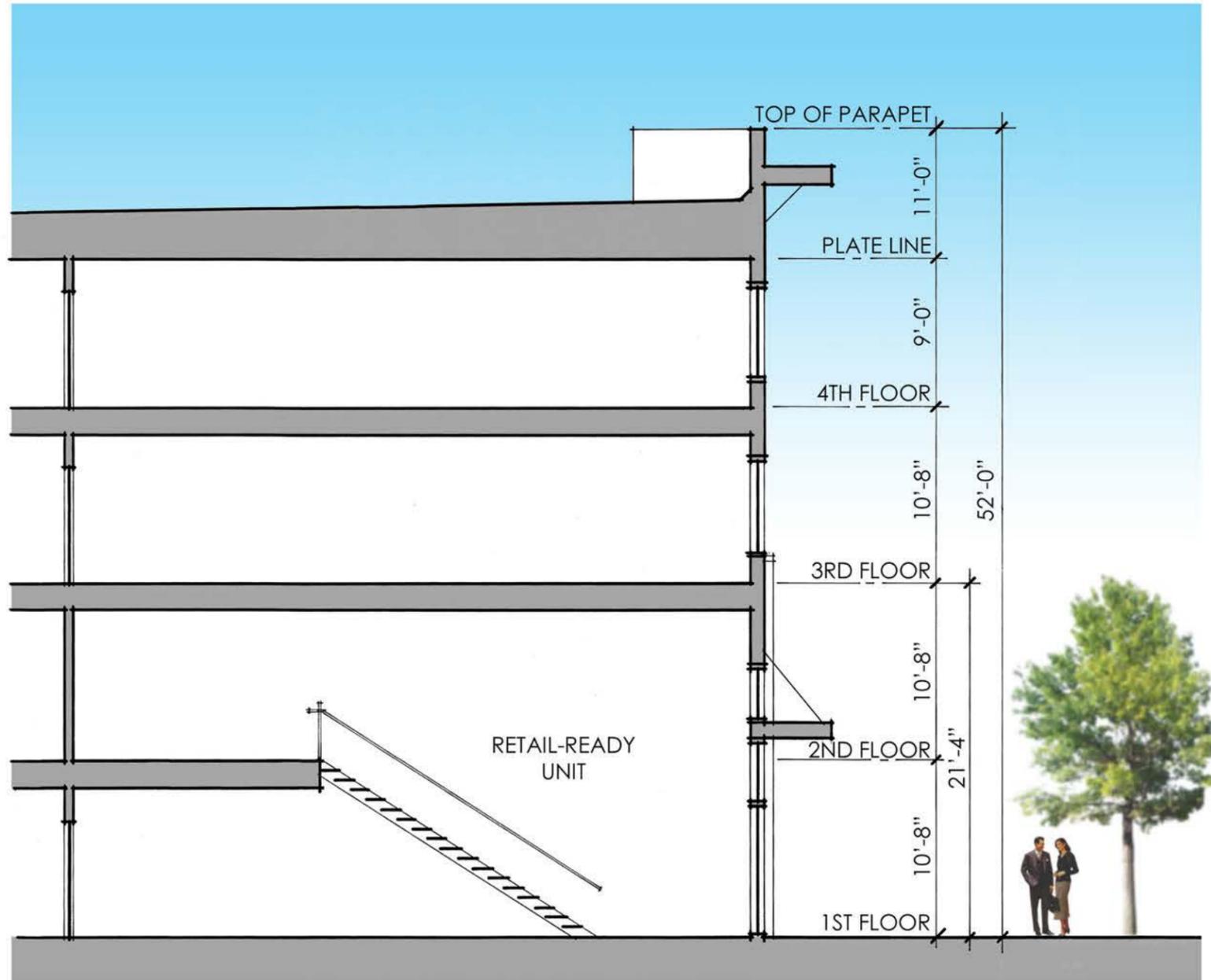
**Exhibit D-1**



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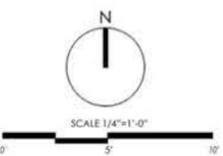


# 12130  
1.18.2013  
EMBREY



**GREENVUE SCHEMATIC BUILDING SECTION**  
 RICHARDSON, TEXAS

**Exhibit D-2**

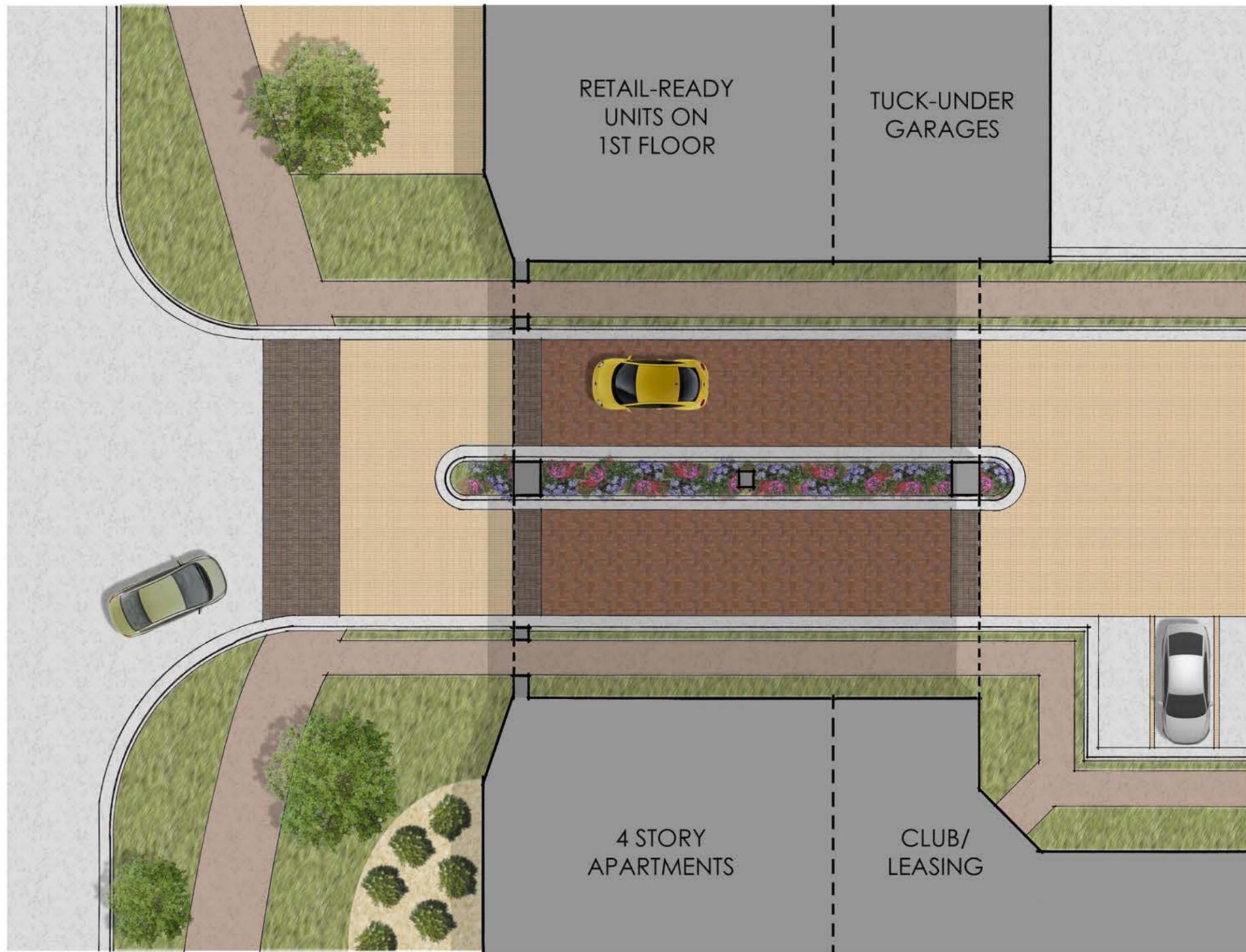




GREENVUE DEVELOPMENT CONCEPTUAL PERSPECTIVE GREENVILLE AVENUE  
RICHARDSON, TX

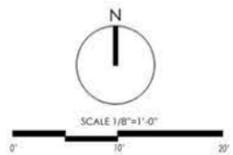
Exhibit D-3

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**GREENVUE ENLARGED ENTRY SITE PLAN**  
 RICHARDSON, TEXAS

**Exhibit D-4**





**SEC Greenville & Collins - August 2012**





(3)

**Looking Northeast  
from Greenville Ave**

**SEC Greenville & Collins - August 2012**



(4)

**Looking Northwest  
from Alma Road**

**Exhibit E-2**



**(5)**

**Looking Southwest at  
Subject Property  
from Alma Road**

**SEC Greenville & Collins - August 2012**



**(6)**

**Looking North along  
East Property Line (Alma Road)**

**Exhibit E-3**



(7)

Looking along  
Main Road (Similar to  
East-West Drive)

**Embrey Apt. Development - Carrollton - July 2012**



(8)

Open Space Area

Exhibit E-4

**GREENVUE PLANNED DEVELOPMENT DISTRICT  
PROPOSED PD CONDITIONS  
EXHIBIT “F”**

**Sec. 1. Intent.**

The purpose of the **GREENVUE PLANNED DEVELOPMENT DISTRICT** is to facilitate the development of a high-quality, transit-oriented multifamily residential community on a fairly difficult development tract. The physical attributes of the proposed development will demonstrate excellence in site planning and design. The proposed development will provide exceptional circulation from within the development to Alma Road and Greenville Avenue, while maintaining a high level of architectural design. Aesthetics from the streets and main driveway present a friendly and inviting perspective for pedestrians and motorists alike. The proposed buildings will be enhanced with vertical and horizontal articulation and will contain a combination of high-quality materials and a monochromatic color palette with selective accent colors.

**Sec. 2. Concept Plan.**

Development of the Property must generally comply with the concept plan attached hereto for all purposes as Exhibit “B”, (the “Concept Plan”).

**Sec. 3. Building regulations.**

(1) Except as otherwise provided herein, for multifamily uses, the building regulations of the A-950-M Apartment District shall apply .

(2) The following building regulations shall apply to apartment uses:

- a. *Minimum floor area per dwelling unit.* 600 square feet.
- b. *Type of Materials.*

(1) *Building A.*

- a) *Front and Side Facades.* Shall substantially conform to the architectural design and material mix set forth on “Building A West Elevation Greenville Ave.” in Exhibit “ ”, the Conceptual Elevations.
- b) *Rear Façade.* Shall substantially conform to the architectural design and material mix set forth on “Building B South Elevation” in Exhibit “ ”.

- (2) *Building B*
  - a) *Front and Side Facades.* Shall substantially conform to the architectural design and material mix set forth on “Building B North Elevation” in Exhibit “”.
  - b) *Rear Façade.* Shall substantially conform to the architectural design and material mix set forth on “Building B South Elevation” in Exhibit “”.
  
- (3) *Buildings C and D*
  - a) *Front and Side Facade.* Shall substantially conform to the architectural design and material mix set forth on “Building D West and & South Elevation , C South Elevation” in Exhibit “”.
  - b) *Rear Façade.* Shall substantially conform to the architectural design and material mix set forth on “Building B South Elevation” in Exhibit “\_”.
  
- (4) *Building E*
  - a) *Front Side Facade.* Shall substantially conform to the architectural design and material mix set forth on “Building E North Elevation” in Exhibit “”.
  - b) *Rear Façade.* Shall substantially conform to the architectural design and material mix set forth on “Building B South Elevation” in Exhibit “\_”.
  
- (5) *Trash Enclosure.*
  - a) *Front, Side and Rear Facades.* Shall substantially conform to the architectural design and material mix set forth on “Trash Enclosure North, South and West Elevation” in Exhibit “”.
  
- (6) The nonmasonry exterior walls may be constructed of materials such as metal, 3-coat stucco system, and cementitious siding.
  
- (7) For purposes of this planned development only, “masonry” includes brick, manmade or natural stone, cast stone, rock, marble, granite, curtain glass, glass block or any other similar materials approved by the building official.

**Sec. 4. Height regulations.**

(1) *Maximum height.* Except as otherwise provided herein, the maximum building height shall be limited to four stories not to exceed 60 feet in height. (2) *Architectural features.* Features that may exceed the maximum height include turrets, towers, and lighting features, in addition to other features set forth in the definition of “height” in the Zoning Ordinance, provided that such features respect the scale of the building, subject to Development Plan approval.

**Sec. 5. Area regulations.**

- (1) *Maximum Density:* Thirty-six (37) dwelling units per acre.
- (2) *Maximum Lot Coverage:* The principal building(s) and any accessory buildings (inclusive of parking structures) shall not exceed 40 percent of the total area of the lot.
- (3) *Perimeter Setbacks.*
  - (a) *Front.*
    - i. The minimum building setback from Greenville Avenue and from Collins Boulevard shall be forty (40) feet.
    - ii. The minimum building setback from Alma Road shall be twenty-five (25) feet.
    - iii. Balconies above first floors may encroach up to three (3) feet into the building setback and ten-foot landscape strip.
  - (b) *Rear.* The rear property line shall be defined as the southern property line and property line adjacent to the KCS Railroad. Except as otherwise provided herein, the minimum rear yard setback shall be twenty-five (25) feet. The minimum rear yard setback for amenities and accessory structures within the “Pool Court and Amenities” as identified on the Concept Plan, shall be five (5) feet.
  - (c) *Parking.* Driveways and surface parking will be allowed in any setback area.
  - (d) *Overhangs and fireplaces.* The minimum setback requirements shall apply in all cases, except that fireplaces, eaves, bays, balconies and fireproof outside stairways may extend to a maximum of 3 1/2 feet into the required front, side or rear yards.
- (4) *Landscaping.*
  - (a) Landscaping shall be provided at a minimum ratio of thirty percent (30%) of the gross land area of the Property.

- (b) Landscaping shall include enhanced paving areas, landscape islands, and open space areas including plaza areas as shown on the Concept Plan.
- (c) Entry features and signage may be provided as shown on the Concept Plan.
- (d) Within the front yard area, a landscape strip shall be provided as follows: (i) 30 feet along Greenville Avenue, except as provided herein, (ii) 20 feet along Alma Road, and (iii) 6 feet along Collins Boulevard. Within the 30-foot landscape strip along Greenville Avenue, driveways and driving aisles are permitted as shown on the Concept Plan. The Bike Trail may be located within the landscape strip as identified on the Concept Plan.
- (e) Within the required landscape strip, the developer shall install, and all subsequent owners shall maintain, plant materials as follows: One canopy tree for every 50 lineal feet of street frontage and one ornamental tree shall be required for every 50 lineal feet of street frontage; provided, however, that canopy and ornamental trees may be grouped. In addition, three evergreen shrubs, which shall reach a minimum height of 30 inches, shall be installed for each head-in parking space facing Collins Boulevard except as otherwise provided herein. No trees shall be required to be planted within the landscape strip along the retaining wall along Collins Boulevard
- (f) Berms measuring 30" in height with 3:1 slopes may be used in lieu of evergreen shrubs for a minimum of 1/3 of the lineal frontage of a perimeter street.
- (g) A minimum five-foot landscape strip with evergreen and live oak trees planted on alternating 40-foot centers shall be provided along the southern property line.
- (h) Along the "East-West Drive", from the entrance at Greenville Avenue to the entrance at Alma Road, canopy trees shall be in tree wells in accordance with the Concept Plan.
- (i) Trees that are planted within tree wells shall be planted within 8-foot tree wells which are constructed in accordance with City details. The tree well opening shall be covered with a 6-foot x 6-foot tree grate, also in accordance with City details.
- (j) Underground bubbler irrigation is required and shall be installed on a zone separate from other landscape areas. Irrigation must be designed to deliver the appropriate amount of water to each tree with minimum waste.
- (k) Drainage for the tree well must be provided in accordance with City details.
- (l) Parking is permitted within setbacks and landscape buffer areas as shown on the Concept Plan.

**Sec. 6. Parking.**

- (a) *Minimum Parking requirements for multifamily uses* 1.50 parking spaces per unit.

(b) *Carports.* Surface parking spaces serving multifamily residential uses are not required to have a covered carport.

**Sec. 7. Special requirements.**

(a) *Recreational areas.*

(1) Indoor or outdoor recreational amenities shall be required pursuant to this ordinance to meet the requirements of the residents in apartment units on the Property.

(2) For purposes of this ordinance, all apartments constructed on the Property are considered to be one community. The amenities listed below shall accrue points based on the values assigned. A minimum of 100 recreational amenity points must be accumulated for the PD.

a. Clubhouse/game room/multi-purpose room a minimum of 400 square feet in area. (Ten points.)

b. Equipment, such as pool tables, ping-pong tables, foosball tables, etc., in the clubhouse/game room/multi-purpose room; electronic videogames or pinball games shall not be eligible for points. The appropriateness of the equipment shall be determined by the city's director of parks and recreation. (One point for each piece of approved equipment.)

c. Outdoor multi-use sport court, tennis court, racquetball court or similar facility. (Five points/court.)

d. Indoor multi-use sport court, tennis court, racquetball court or similar facility. (Ten points/court.)

e. Indoor fitness center at least 400 square feet in area. (Ten points.)

f. Swimming pool, including wading area. Pools shall be fenced and secured according to the requirements of the City of Richardson. (Ten points)

g. Reinforced concrete jogging trail, bike path, sidewalks or combination thereof looping through or around the Property, a

minimum of ten (10) feet in width to be constructed solely by the developer. (Twenty points.)

h. Sidewalks, hiking, jogging, and/or bike trail connecting the development to public trail systems. (Five points)

i. Usable open space, at least 1,000 square feet in area, to include at least three of the following: cluster of trees, water features, seating areas, picnic tables, barbecue grills, gazebos or other elements as approved by the city's director of parks and recreation. The plaza areas shown on the Concept Plan shall qualify as usable open space for purposes of this planned development. (Ten points; maximum 30 points for the PD.)

j. Other recreational amenities as approved by the city's director of parks and recreation. (One through ten points, to be determined by the director of parks and recreation.)

k. Fenced dog park minimum 1,200 square feet (15 points).

(3) It shall be the responsibility of the director of parks and recreation of the city to review the proposed recreational amenities.

(4) Open space shall be disposed in such a manner as to ensure the safety and welfare of residents

(b) *Signage.* Signage identifying the proposed development may be provided as set forth on the Concept Plan and Elevations. Signage for the “retail ready” spaces shown on the Concept Plan may be provided for each separate space as such spaces are used for nonresidential uses. A signage package shall be approved prior the issuance of a certificate of occupancy for the first “retail ready” space converted to a nonresidential use.

## **Sec. 9. General Miscellaneous Regulations.**

(a) **Enhanced street paving.** Enhanced street paving shall be provided at appropriate locations throughout the development to emphasize pedestrian crossings, key intersections, and driveways entrances.

(1) For purposes of this planned development district, the multifamily development shown on the Concept Plan shall be considered as one “apartment community” in accordance with Chapter 21 and no physical separation shall be required.

(2) Loading docks, refuse storage containers, and utility accessories shall be screened to reduce their visual impact by screening these sites from adjoining properties and shall be located as set forth on the Concept Plan.

(b) **Minor modifications.** For purposes of this planned development, a minor modification shall be defined as (i) a change to a footprint of a building in which the proposed footprint remains within the building envelope shown on the Conceptual Site Plan, and (ii) except as otherwise provided in (i), a change which does not increase the building coverage, floor area ratio or residential density of the planned development, does not decrease any of the specified area regulations or enumerated parking ratios, nor substantially changes the access or circulation on or adjacent to the site.

(c) For purposes of this planned development district and the development and performance standards in the City's Comprehensive Zoning Ordinance, this planned development district shall not be considered a residential or apartment district.

## MEMORANDUM

Date: June 27, 2012  
To: John Kirk, Embrey Partners  
From: Scott Polikov, AICP, CNU, Gateway Planning  
Re: Land Use Analysis for GreenVUE Site, Richardson, Texas

Gateway Planning has been tasked with analyzing the existing and future development context around the 12.7 acre site located at the southeast corner of the intersection of Greenville Ave and Collins Blvd in Richardson, Texas, less than a ¼ mile from the DART Redline Arapaho Station. Gateway has also been tasked with making recommendations on the best potential land uses on the property based on this analysis of existing and future development options.

This memorandum provides a summary of our findings and recommendations based on our assessment of the local real estate market and an understanding of long-term redevelopment potential in areas that have seen relatively limited commercial redevelopment in close proximity to transit stations.

Based on the results of our assessment of the existing physical context, current plans for redevelopment around the DART station, and an analysis of current and future real estate market potential, we conclude the following:

- Lack of access from Collins Blvd and poor visibility from US 75 make this site challenging for any high-profile retail/commercial type development
- Existing development context of underutilized industrial and office uses and the general lack of market demand for office uses makes this site challenging for new office development
- There is no significant residential redevelopment activity within the ¼ mile radius around the existing Arapaho DART rail station as envisioned in the Arapaho Station Area Plan. Development/redevelopment around transit stations has been driven primarily by multi-family residential in the DART service area and across the country.
- The subject property is one of the few sites around the DART station that is currently vacant and thus becomes a good first candidate for residential development; thus with the potential to drive redevelopment of other adjoining underutilized properties.
- In addition, the site's proximity to the DART station –it is less within a 5-minute walking distance from the station—is attractive for the development of multi-family residential on the subject property would implement one of the City's significant policies of encouraging multi-family around DART stations.

Attached with this memorandum is a summary report with our detailed assessment and analysis and concluding recommendations. Please feel free to contact me if you have specific questions on any of the information included.

### Land Use Analysis for GreenVUE Site, Richardson, Texas

Gateway Planning has been tasked with undertaking a land use analysis for a 12.7 acre site (known as the GreenVUE Site) located at the intersection of Greenville Ave, Collins Blvd, and Alma Road in Richardson, Texas. This land use analysis evaluates not only the existing development context, but also existing plans and studies impacting the area, demographic and market dynamics, and other factors affecting development and redevelopment.

Based on this assessment and analysis and Gateway Planning’s experience in master planning and redevelopment in infill and TOD contexts, we were also charged with making future land use recommendations for the property.

This first section of this summary document provides an overview of the GreenVUE site, its surrounding development context including adjacent land uses. The second section provides a summary of all existing plans and studies as they pertain to the subject property and its vicinity. The third section provides the demographic and market analysis for Richardson. The next section establishes the land use analysis which provides the basis for the land use recommendations for the site.

#### 1. Surrounding Context and Site Analysis:

The GreenVUE site is 12.7 acres and is located at the intersection of Greenville Ave, Collins Blvd, and Alma Road in the City of Richardson, Texas. The site is bordered on the west by Greenville Avenue, on the north by Collins Street, on the east by Alma Road and on the south by an existing Kansas City Southern Railroad freight line and an existing flex/industrial building.



Figure 1 General Location of GreenVUE Site in the DFW Region

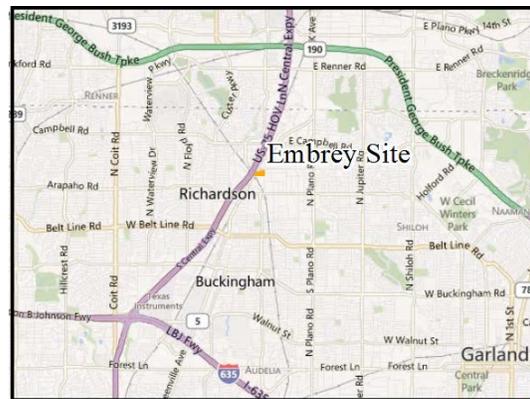


Figure 2 GreenVUE site in relation to the City of Richardson



Figure 3 Site Location and Vicinity

Site Photos:



Looking south along Greenville Avenue from the site with the Kansas City Southern Rail line in the foreground



Looking north along Greenville Avenue from the site



Looking at site from Greenville Avenue to the northeast



Looking north from site to the grade separated Collins Blvd.



*Looking southwest from the site showing the grade separated Kansas City Southern Rail line*



*Existing office use to the north of site*



*Existing Central Trail section crossing under the Kansas City Southern rail overpass*

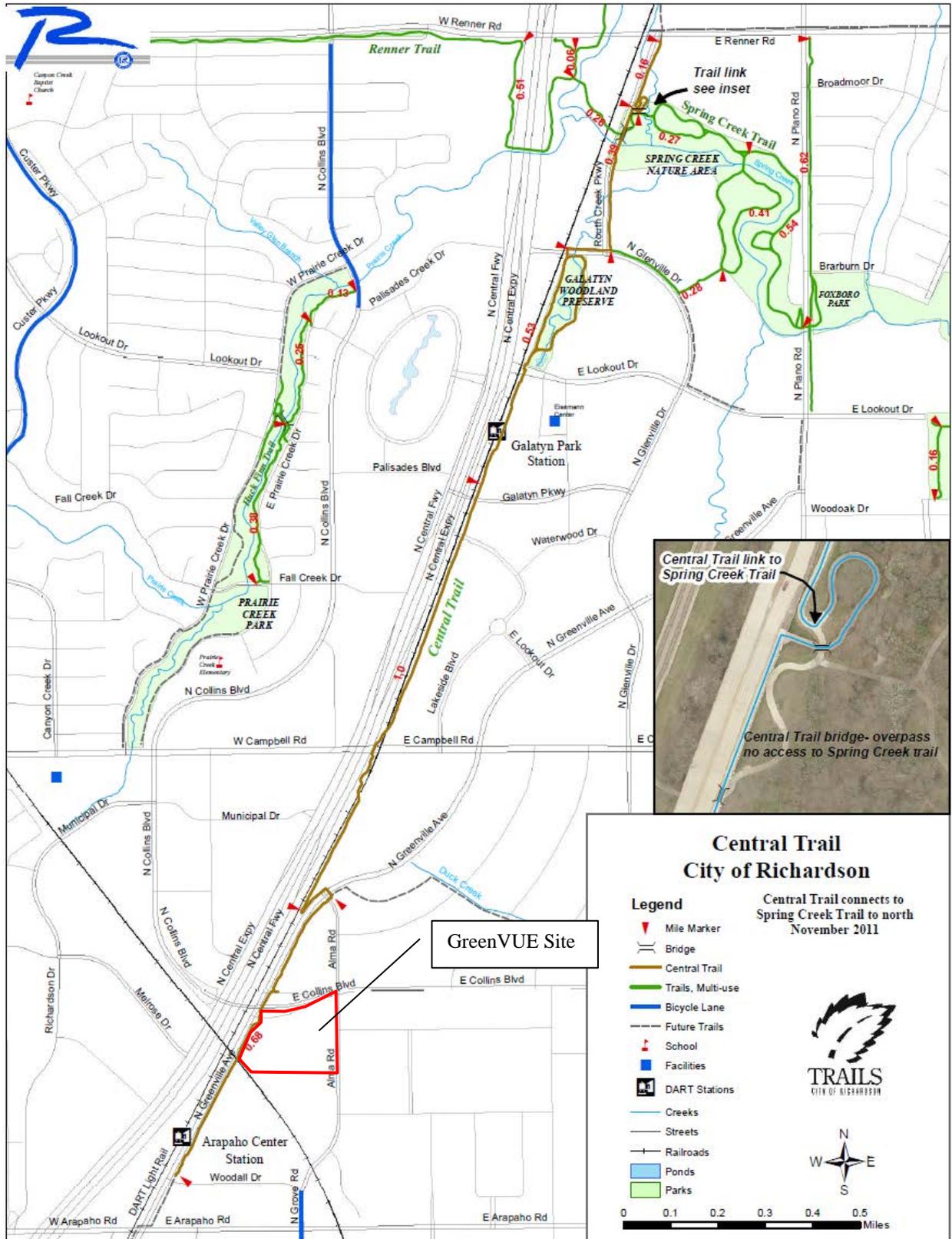


*Alma Rd. street section looking south with industrial uses to the east and existing site access*

**Site Access:**

The site's current main automobile access is from Alma Road. Collins Blvd is grade separated from Greenville Avenue and US 75 (main lanes and frontage roads) and comes down to grade at its intersection with Alma Road located at the northeast corner of the subject site. Alma Road is a 4-lane divided major collector with existing median openings at Quality Way and at the southern property line. Greenville Avenue is a 6-lane divided arterial running approximately parallel to US 75 at this location.

The Central Trail runs along the Greenville Road frontage of this property and provides a direct pedestrian link to the Arapaho Station located approximately 500 feet south of the site via a below grade crossing under Greenville Avenue. The Central Trail is a 12-foot wide multi-use trail facility that currently links the City of Richardson's trail system to two DART Stations – Arapaho and Galatyn Park stations to some of the largest employers along the Telecom Corridor (along US 75). The Central Trail connects with other trails in Richardson and beyond creating a regional trail and park system. It also connects to existing retail uses at Campbell and US 75.



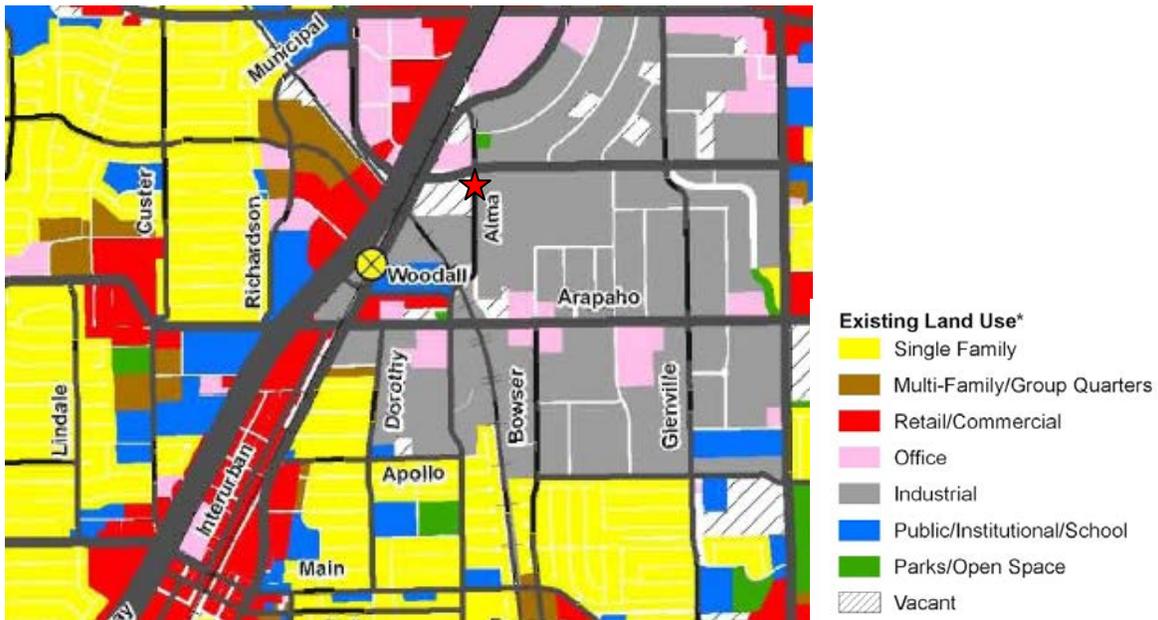
City of Richardson Central Trail Plan



Existing access around the site  
(Source: Arapaho Center Station Area Plan)

Existing Adjacent Land Uses:

The figure below shows the existing land uses in the vicinity of the subject site. Much of the land around the site is zoned for industrial use however the actual uses of surrounding properties varies, with much of the activity being technology related and therefore, has minimal impact on nearby properties. To the north across the grade separated Collins Blvd. is a midrise office tower and parking structure, to the east across Alma Road are two low rise data center buildings, to the south is a vacant lot and a low rise flex/industrial building and to the west is Greenville Ave, Central Trail, and US-75.



Existing land uses around the subject site  
(Source: Richardson Comprehensive Plan)

To the south of the Kansas City Southern Rail line, across from the DART Arapaho station, is a park and ride facility and a DART parking lot in conjunction with the DART transit transfer station.

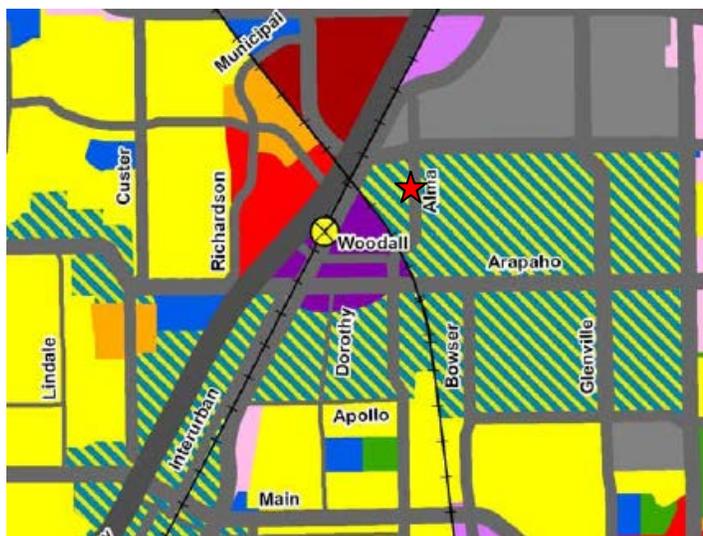
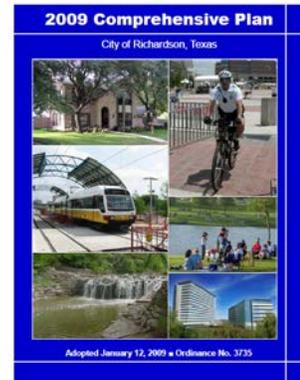
As identified in the 2009 Comprehensive Plan the City seeks to encourage redevelopment within this area to better utilize the land in an underperforming area of the City. The resulting higher intensity and a wider variety of uses and corresponding zoning can better respond to market conditions.

**2. Relevant Studies and Reports:**

In addition to the City’s Comprehensive Plan, which was updated in 2009, there are several other studies and reports that impact this subject property including the Arapaho Center Station Area Plan (2002), Richardson DART Station Area Market Analysis (2003), and the focus on more planning in the East Arapaho/Collins Redevelopment Area (2012).

City of Richardson Comprehensive Plan

The Richardson Comprehensive Plan was adopted in 2009 to provide a vision and guidance for the City’s development and redevelopment over the next 20 to 30 years. The plan consists of ten sections ranging from demographics to parks and recreation – urban design to community facilities. For the purposes of this memorandum, the most relevant components are the land use and transportation sections.



- Future Land Use**
- Yellow: Neighborhood Residential
  - Orange: Multi-Family Residential
  - Pink: Neighborhood Service
  - Red: Community Commercial
  - Dark Red: Regional Employment
  - Grey: Office/Industry
  - Purple: Neighborhood Mixed-Use
  - Dark Purple: Transit Village
  - Green: Parks & Open Space
  - Blue: Public/Semi-Public/School
  - Green with diagonal lines: Enhancement/Redevelopment

Future Land Use Plan  
(Source: Richardson Comprehensive Plan)

In the Land Use section, the Comprehensive Plan outlines “the best uses for the City’s remaining undeveloped land, acknowledges special areas within the City such as its anticipated transit villages and its original downtown area, and integrates land use assumptions with multi-modal transit/mobility options throughout the City.” The Comprehensive Plan acknowledges the opportunity for Transit-Oriented Development and a mixed-use district as shown in the Future Land Use Map below.

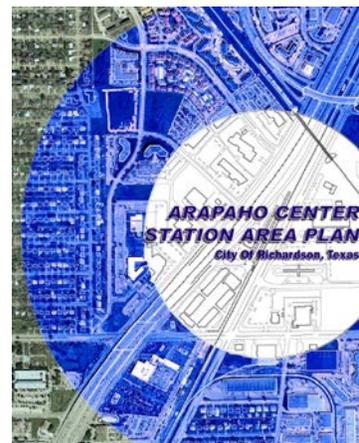
Richardson’s Comprehensive Plan explains each land use designation. It is telling that “nearly half of the City’s land area is utilized for residential uses” however multifamily developments account for only 5% of the City’s land area. The Comprehensive Plan emphasizes that “Most of the multi-family units recently approved have been at or near Richardson’s rail stations to take advantage of the adjacency to transit. They also tend to be within mixed-use developments, with retail and/or office uses within walking distance or even in the same building.”

Specific areas within Richardson called “Enhancement/Redevelopment Areas” have been identified for further study. One such area is the East Arapaho/Collins Enhancement/Redevelopment area. This area is currently slated for additional study in 2012-13 and some preliminary issues as they relate to this study are further described in the subsequent sections of this report.

The transportation section of the City’s Comprehensive Plan further emphasizes the importance of linkages to the DART Red Line and the need for bike and pedestrian facilities to the stations and the regional trail system. The Comprehensive Plan also recognizes that Transit-Oriented Developments (TODs) are critical economic development tools that can create sustainable development around the City’s rail stations and improve the overall economic health of Richardson.

### Arapaho Center Station Area Plan

In 2001 a station area plan was commissioned to build upon recommendations of a 2000 ULI Advisory Services Panel Report that outlined the opportunities for Transit Oriented Development along the Dart Red Line in Richardson. The panel recommended that the City be flexible in the types of land uses allowed near the Arapaho Station stating that the “Larger parcels could incorporate retail and residential uses, as well as office development...the panel recommended that these options be left open, to allow prospective developers to respond to market demands.”



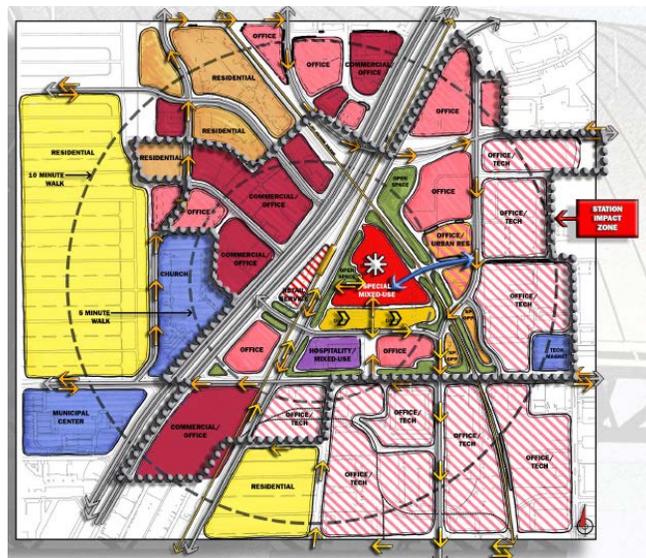
The Station Area Plan also describes the advantages of the station’s location based on its proximity to US 75, Arapaho Road, the Transit Park and Ride facility and the adjacency to large land parcels that allow for development opportunities. However, since this report was put together in 2001 there

have been several market shifts and the demands for office and residential have significantly changed as illustrated in the Market snapshot in the next section.

The plan seeks to transform the 1/2-mile area around the station on the east side of US-75 by not segregating land uses and ensuring that “interrelationships will exist both horizontally and vertically. These combinations of uses on the same site should make for an interesting and more functional development.” The plan also encourages a pedestrian connection underneath the Kansas City Southern Railroad so that “a mid-rise *urban* type residential development” could be appropriate for the area to the north east – in close proximity to the GreenVUE site.

The feasibility of this connection, however, needs to be evaluated by the City in conjunction with Kansas City Southern Railroad. In addition, the timing of this connection needs to be coordinated with the redevelopment of the property immediately south of the Kansas City Southern Rail line. The GreenVUE site does have direct connection to the Central Trail that provides the needed connectivity to the Arapaho Rail Station.

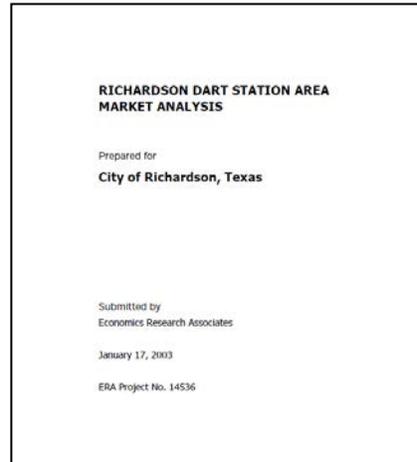
The Station Area plan below shows the zoning recommendations in relation to the station. Although the overall theme of mixed use within walking distance to the station is still applicable, the specific allocation of land uses and corresponding zoning needs to be re-evaluated due to changes in the market and the fact that there has not been any mixed use redevelopment as originally envisioned in the plan.



Arapaho Station Land Use Recommendations  
(Source: Arapaho Center Station Area Plan)

Richardson DART Station Area Market Analysis

This comprehensive market study was completed in 2003 and although it is exhaustive in its evaluation of the TOD areas and the regional economic outlook, nearly a decade and two incredible market shifts have occurred since its completion. Therefore, the application of these findings needs to be considered with these changes.



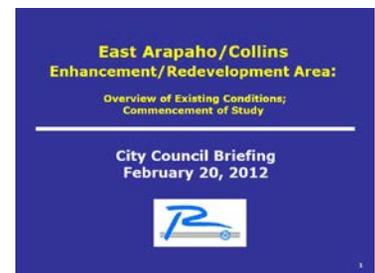
The report does however point out the transformative nature that TOD allows for and underscores the success that the Galatyn Park Station has had with its influx of development spurred by the Renaissance Hotel and Eisemann Center for Performing Arts. This report identifies that “Housing at the DART stations may be particularly appealing to young professionals interested in a walkable commute to work, or empty nesters desiring increased mobility without increased automobile dependence.”

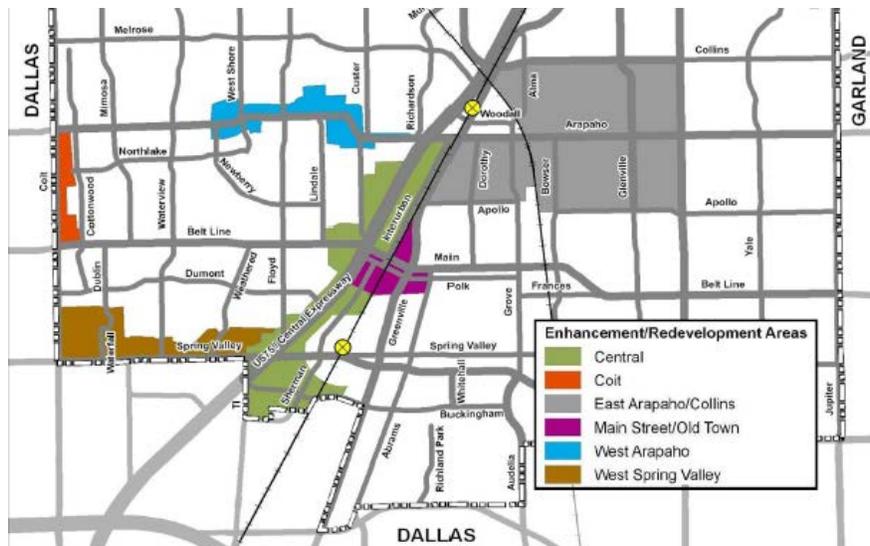
The report states that the Arapaho station features large developable/underdeveloped parcels surrounding it that have higher potential. The market has shifted since this report with limited market for retail and office development and higher potential market for multifamily residential. The recommendation for multifamily units was 250 Apartment and 150 Condominium/ Townhome Units, of which none have been built as part of the TOD.

This report also provides an extensive analysis of projected demographic changes for the City and region. In addition a potential contradiction is brought to light in which the City recognizes the benefits of multifamily development, particularly located near transit stations; yet there is also the burden that large numbers of residential units have placed on area schools in the past. However, the report recommends an allocation of a specific quota of multi-family for each TOD to alleviate this issue. In addition, demographics show that most multifamily dwellers at TODs are either single professionals or younger or older childless couples that do not over burden the school system.

East Arapaho/Collins Enhancement/ Redevelopment Area

As identified in the City’s Comprehensive Plan, the East Arapaho/Collins Enhancement/Redevelopment Area is an upcoming planning priority for the City. At a presentation to the City Council in February 2012, City Staff presented several salient issues about the redevelopment area which encompasses the GreenVue property.





City of Richardson Enhancement/Redevelopment Areas  
 (Source: East Arapaho/Collins Enhancement/ Redevelopment Area)

The aim of this preliminary report to Council is to demonstrate why Enhancement/Redevelopment study is relevant to the Arapaho Station area. Much of it has to do with the fact that Richardson has significant areas of aging development, infrastructure and uses no longer performing at their highest and best use. Public investment will be targeted to these Enhancement/ Redevelopment districts based on a sustainable, market sensitive plan.

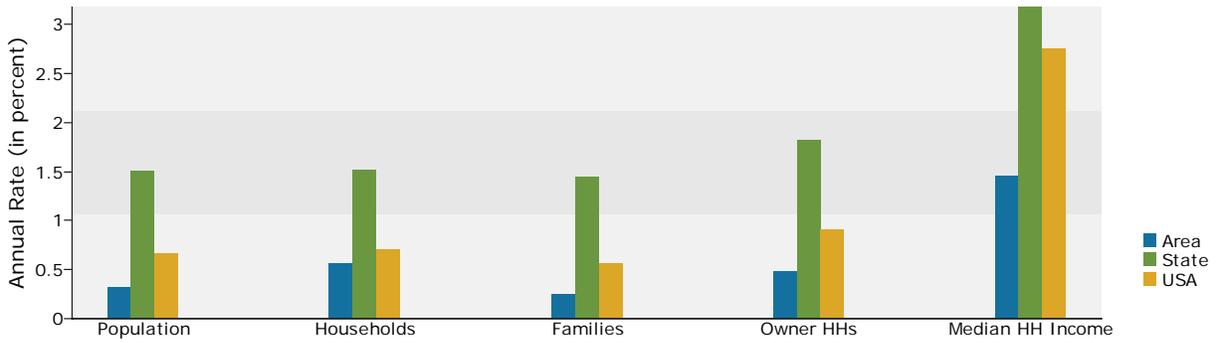
Currently the area is home to many of Richardson’s office and technology related businesses. However as the preliminary report states, “much of the area has been challenged in recent years by evolving markets, technology, and user requirements. Redevelopment, enhancement, and building format changes should be considered to address these changes.”

The presentation outlines how this redevelopment strategy is in line with the previously summarized comprehensive and station area plans and points to challenges and opportunities in the specified area. The study approach signifies the need to determine market viability, create a strategy and implementation plan for redevelopment and amend zoning if needed. The overall goal is how to reappoint underutilized land and flex space to better match the needs of the community and the realities of the market.

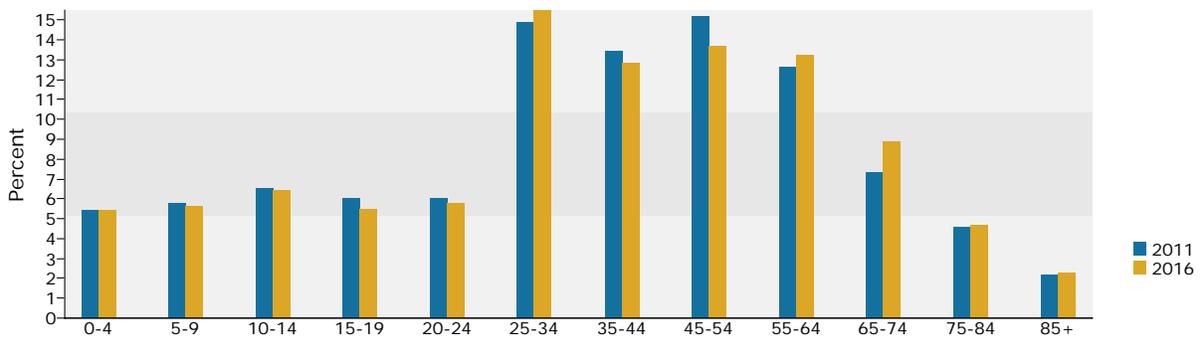
**3. Area Demographic & Market Analysis:**

The goal of this section is to evaluate the impacts that demographics of the area have on the potential land uses on the site. The following graphs below give a snapshot of the demographic and economic conditions within a 5 Minute drive time of the GreenVUE site. Compared to the national and state averages, the area has lower population, number of households and owner occupied households. Future population projections are for higher population in the 25-34 age range (typically single professionals) and the 55 – 74 age range (typically empty nesters).

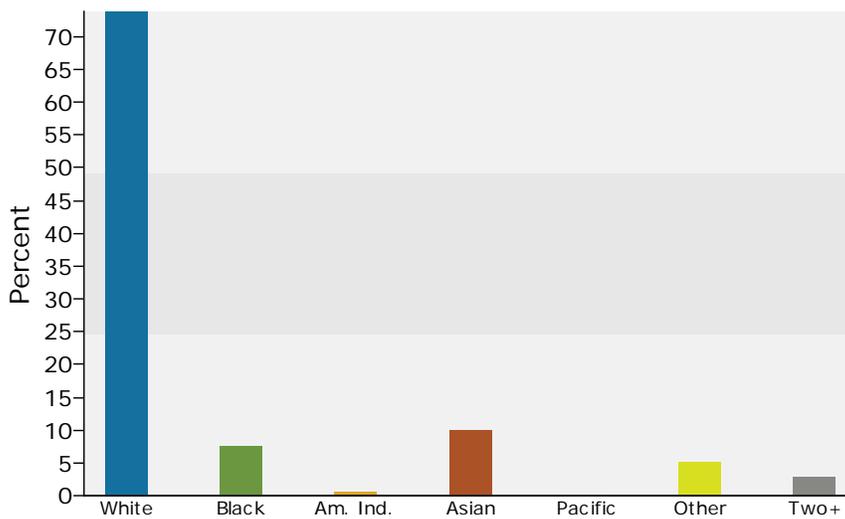
Trends 2011-2016



Population by Age



2011 Population by Race



**Market Assessment:**

Nationwide across all sectors development is less than robust, ULI in “Emerging Trends in Real Estate 2012” reports that “Except for multifamily, no markets or property sectors

offer sure-shot opportunities for big gains in 2012.” This is especially true in regions like DFW that have some factors attracting “surging numbers of gen-Y’ers, housing-bust refugees, and immigrants.” Apartments are attractive due to trends of “Living smaller, closer to work, and preferably near mass transit... as more people look to manage expenses wisely” which the Embrey site offers.

In many sectors development has slowed, particularly suburban office parks as “more companies concentrate in urban districts where sought-after generation-Y talent wants to locate in 24-hour environments.” The report also emphasizes the sentiment that “Retail will be terrible for years”; “no need for more office”; and “hotel is overbuilt, especially outside the major tourist and business cities.”

The data show that with an uptick in the economy apartment demand could “intensify further from people doubling up or young adults living at home but looking for their own space.” Multifamily is also the sector where financing is readily available and favorable deals can be made with contractors that need work. This notion is reflected in the National Association of Realtors May 2012 Commercial Real Estate Outlook in which they expect apartment rents to increase “4.0 percent this year and an additional 4.1 percent in 2013, putting apartment properties on the must-have list for many investors.”

In the DFW region the outlook is similar to nationwide trends with a slightly more positive position overall. The tables below are from the Texas Real Estate Center Market Report 2012. They show the higher rents and occupancies year over year as compared to the state average, especially for units constructed since 2000.

Apartment Statistics 2011			
	Dallas	Fort Worth	Texas Metro Average
Average rent per square foot	\$0.91	\$0.83	\$0.88
Average rent for units built since 2000	\$1.05	\$0.92	\$0.98
Average occupancy	94.0%	95.2%	93.7%
Average occupancy for units built since 2000	95.9%	96.9%	95.8%
Total units*	413,197	162,969	1,631,425
Total units built since 2000*	85,869	33,760	388,352

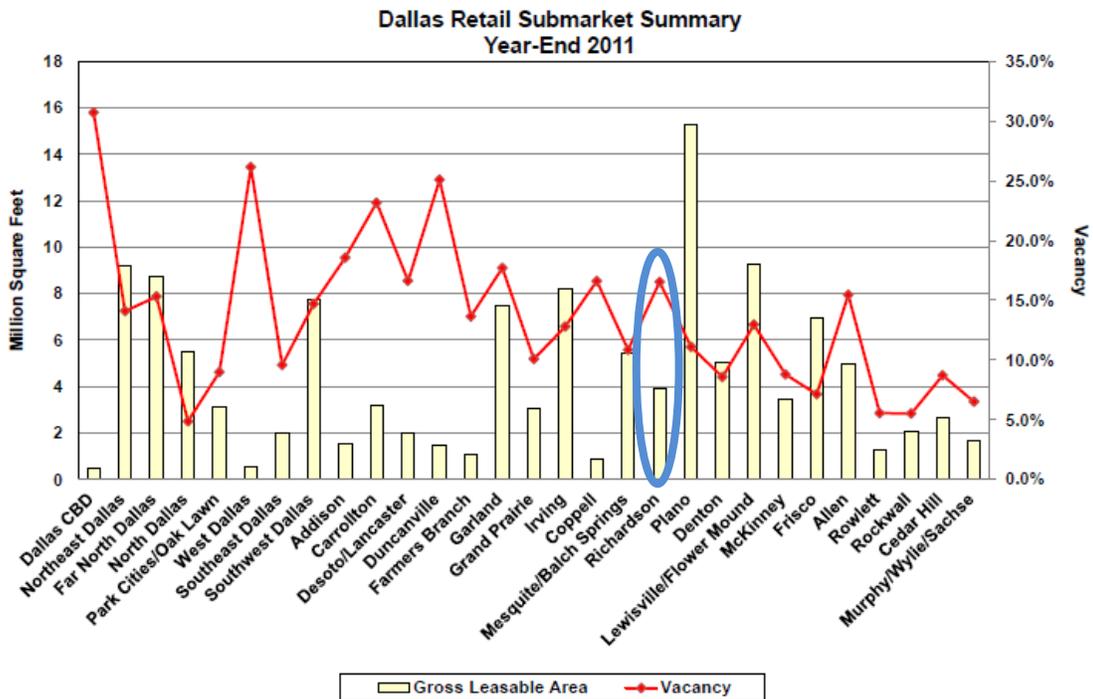
\*As of December 2011  
Source: Apartment MarketData Research

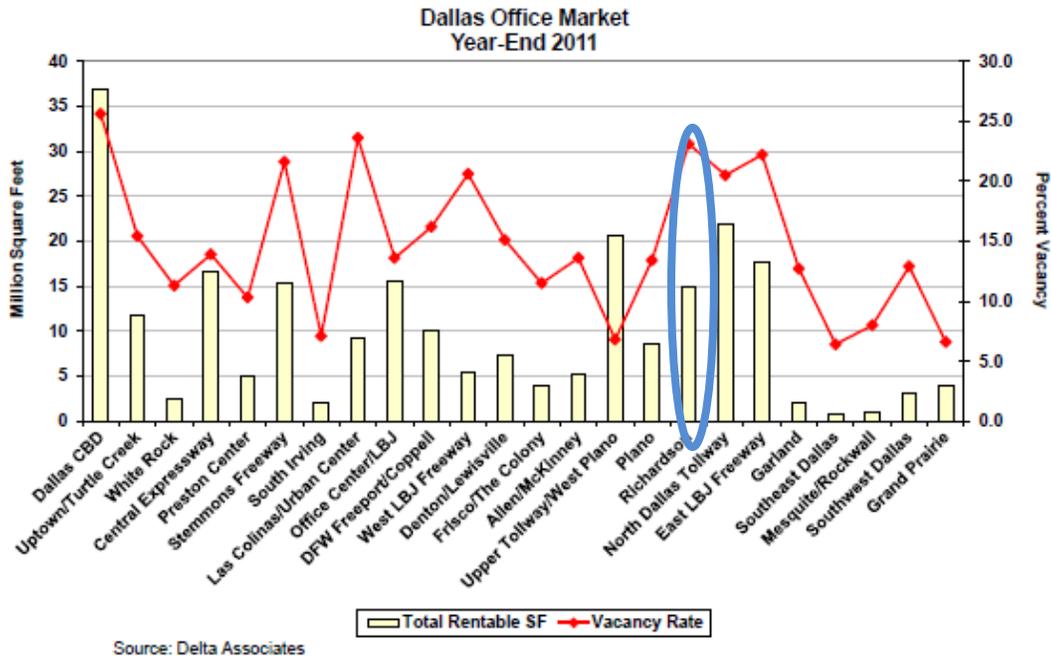
**Apartment Occupancy and Effective Rent\***  
Same Store\*\*

Metro	Occupancy			Effective Rent		
	Dec. 2010	Dec. 2011	Change	Dec. 2010	Dec. 2011	Change
Austin	93.7%	94.2%	0.5%	\$828	\$883	6.6%
<b>Dallas</b>	<b>90.9%</b>	<b>92.2%</b>	<b>1.4%</b>	<b>\$763</b>	<b>\$811</b>	<b>6.3%</b>
<b>Fort Worth</b>	<b>89.7%</b>	<b>91.3%</b>	<b>1.8%</b>	<b>\$670</b>	<b>\$700</b>	<b>4.4%</b>
Houston	87.4%	88.8%	1.7%	\$736	\$766	4.2%
San Antonio	90.8%	91.1%	0.3%	\$701	\$732	4.4%
Lubbock	92.0%	91.8%	-0.2%	\$647	\$664	2.5%
Amarillo	91.0%	90.8%	-0.2%	\$585	\$615	5.1%
Abilene	93.0%	90.7%	-2.5%	\$586	\$592	1.1%
Corpus Christi	92.3%	94.0%	1.9%	\$695	\$718	3.4%
Texas Average	89.9%	91.0%	1.3%	\$739	\$776	5.1%
Florida Average	90.2%	91.0%	0.9%	\$790	\$819	3.7%
Arizona Average	89.3%	90.8%	1.6%	\$662	\$669	1.1%

\*Statistics reflect only conventional, mid-rise and high-rise apartment communities. They do not include income restricted, student housing or senior independent housing.  
\*\*Same store is defined as stabilized properties, excluding lease-ups.  
Source: ALN Apartment Data

In the Richardson sub-market of the DFW region, the multifamily demand is substantial but the retail and office market is lackluster. The charts below show higher than average vacancy rates (over 15% for retail and over 20% for office) in both sectors as compared to the regional average.





Richardson Area Development:

This section evaluates the recent office, industrial and multi-family development trends in the City of Richardson. For multi-family residential uses, we evaluated existing, recently developed and approved multi-family developments in the City. The map on page 16 and corresponding Table 1 in the Appendix developed by the City summarizes all the existing and entitled multi-family zoning within the City. Table 2 in the appendix shows the aging nature of the multifamily properties throughout Richardson.

In summary, over the past 3 years, 409 multi-family units, 125,729 sqft. of office space, and 378,647 sq.ft. of industrial/flex office have been built in the City of Richardson. In addition, zoning for approximately 5,000 additional multifamily residential units was also approved, mainly in other TODs.

In addition to recently approved development, an analysis of the location of multifamily and its age provides some critical insight into potential land uses for the GreenVUE site. The tables below point to the lack of multifamily near the Arapaho station. It is evident that within the 5 minute drive time supply is far below the average of the 10 and 15 minute radii. The immediate area also is subject to aging housing stock disproportionately compared to the larger surrounding area.

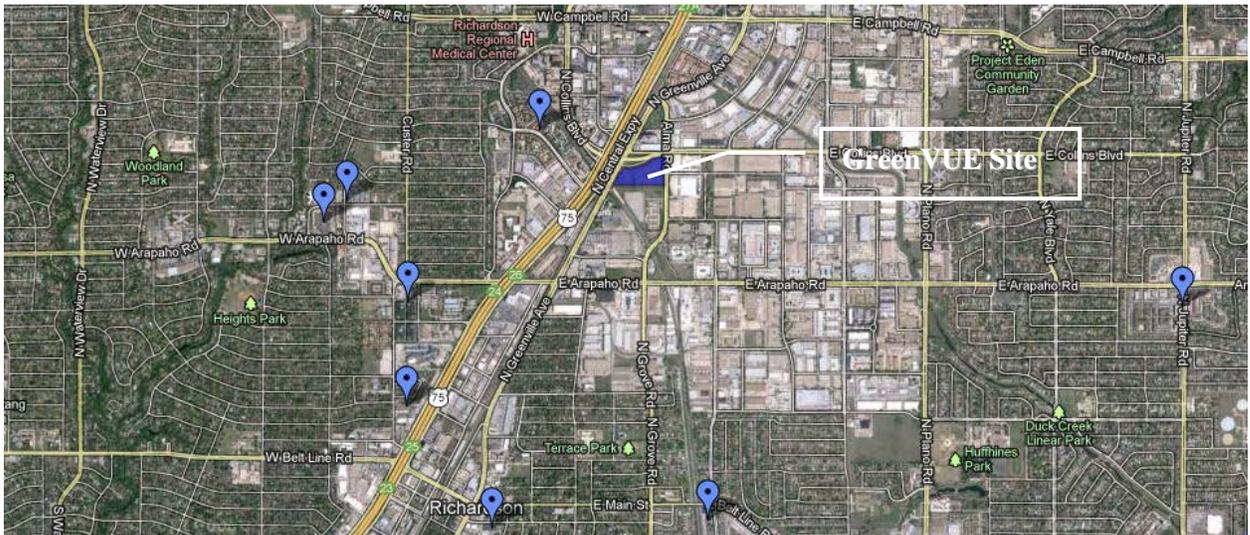
	5 Minutes	10 Minutes	15 Minutes
2010 Housing Units	11,348	117,955	372,426
Owner Occupied Housing Units	61.9%	45.3%	41.4%
Renter Occupied Housing Units	32.9%	47.4%	50.0%

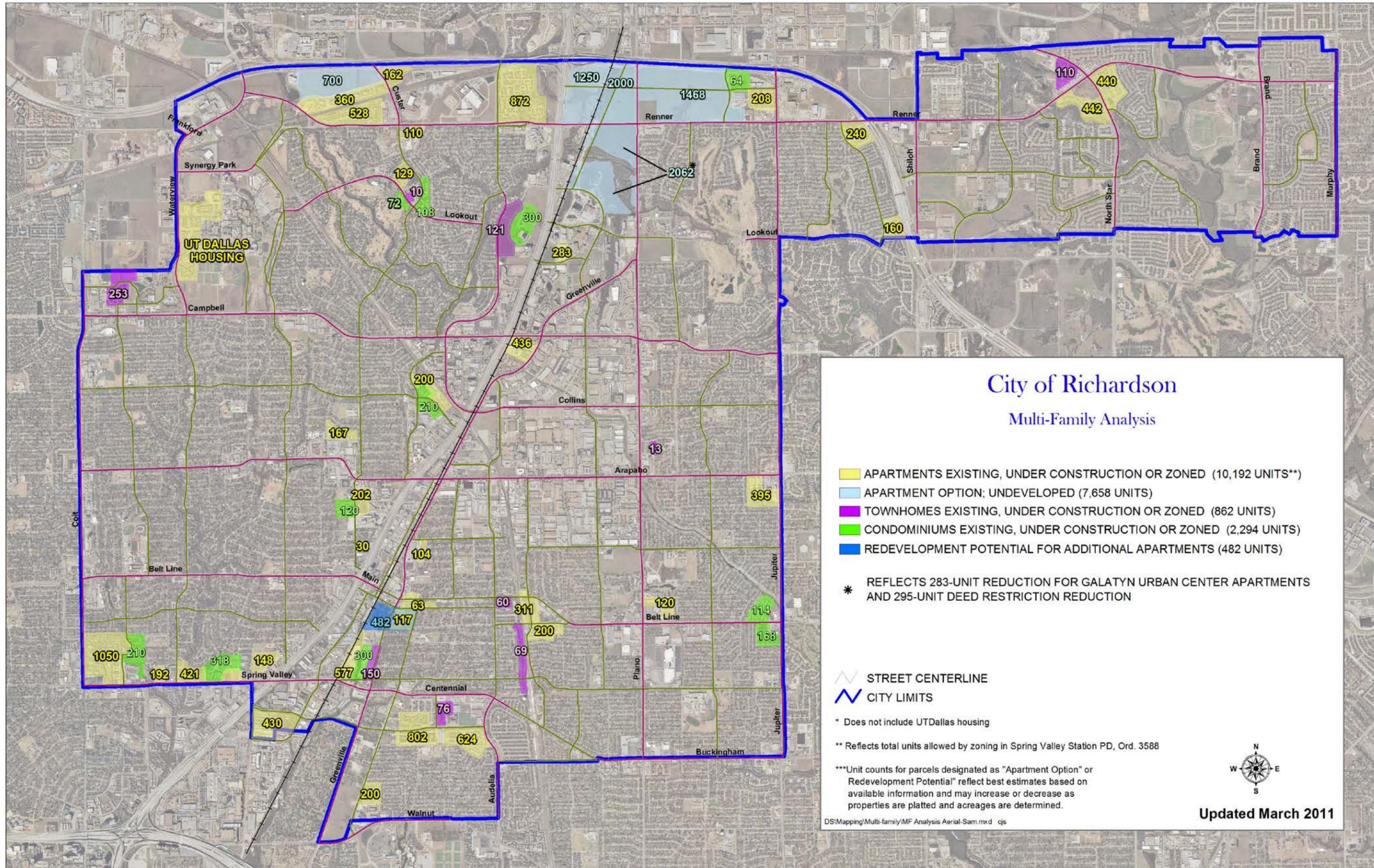
(Source: ESRI forecasts for 2010 and 2015; U.S. Bureau of the Census, 2000 Census of Population and Housing)

2000 Housing Units by Year Structure Built	5 Minutes	10 Minutes	15 Minutes
Total	10,145	107,610	335,696
1999 to March 2000	0.9%	2.2%	2.6%
1995 to 1998	7.7%	8.3%	10.2%
1990 to 1994	3.5%	5.8%	8.3%
1980 to 1989	13.1%	24.7%	29.2%
1970 to 1979	33.7%	33.1%	27.7%
1969 or Earlier	41.1%	25.9%	21.9%
Median Year Structure Built	1973	1977	1980

(Source: U.S. Bureau of the Census, 2000 Census of Population and Housing)

Currently there are very limited multifamily uses east of US 75 and none adjoining the Arapaho Station. The figure below demonstrates the proximity near the GreenVUE Site and the Arapaho Station.





#### 4. Area Land Use Analysis:

This section provides land use recommendations for the GreenVUE site based on several factors including the assessment of existing conditions, City's plans and policies, market opportunities, and future redevelopment potential at the Arapaho station area.

Based on our analysis, assessment, and understanding of redevelopment dynamics, we conclude the following:

- Access challenges and extremely poor visibility from grade separated Collins Blvd and US 75 make this site challenging for retail/commercial type development. In addition, the sluggish market demand and high vacancy rates for both office uses make this site challenging for new office development
- There is no significant multi-family residential redevelopment activity within the ¼ mile radius around the existing Arapaho DART rail station as envisioned in the Arapaho Station Area Plan. Development/redevelopment around other DART stations in metroplex has been driven primarily by multi-family residential uses. In order for any redevelopment and transformation in the Arapaho station area there needs to be a catalyst for change and a base of multi-family residential can be such a catalyst signaling to the market on the latent development opportunities at this DART station.
- Currently there is high demand for Multi Family and limited supply in the pipeline. With the surrounding uses being vacant or underutilized industrial land, an infusion of population in the area will be a beneficial catalyst for future development of complementary uses.
- The subject property is one of the few sites around the DART station that is currently vacant and thus becomes a good first candidate for residential development; thus with the potential to trigger redevelopment of other adjoining underutilized properties by changing the local real estate market dynamics. Multi Family use could encourage a mix of 24/7 uses throughout the station area rather than just the day time commuters and industrial uses that are currently present.
- The development of multi-family residential on the subject property would implement one of the City's significant policies of encouraging multi-family around DART stations.

Based on these conclusions we recommend multi-family residential uses on the GreenVUE site to take advantage of current real estate demand while creating the market dynamic needed to implement important City policies and possibly providing the impetus for redevelopment of other underutilized properties in the vicinity of the Arapaho rail station.

**Appendix**

**TABLE I:**

<b>Development Summary</b>						
<i>City of Richardson</i>						
<b>Multi-family, Office and Industrial (2009-2012)</b>						
<b>Development Plan Approvals</b>						
<b>Multi-Family</b>	<b>Location</b>		<b>Total Units</b>	<b>Acreage</b>	<b>Zoning</b>	<b>Date</b>
Alta Creekside	3650 Custer Pkwy		162	13.64	(PD) Planned Development	2012
Brick Row Buildings D & E	151 Brick Row		77	2.52	(PD) Planned Development	2012
Evergreen of Richardson	3551 North Star Road		170	5.91	(PD) Planned Development	2010
<b>Office</b>	<b>Location</b>	<b>Additional Building</b>	<b>Total Building</b>	<b>Acreage</b>	<b>Zoning</b>	<b>Date</b>
		<b>Square Footage</b>	<b>Square Footage</b>			
Glenville Office Park I	2500 Glenville Dr.	92,967	92,967	16.35	I-M(1) Industrial District	2012
Breckinridge Animal Hospital & Office Bldg.	4110 & 4112 E. Renner Rd.	9,425	9,425	1.26	(PD) Planned Development	2012
Society of Petroleum Engineers	222 Palisades Creek Blvd.	5,880	57,546	5	(TO-M) Technical Office District	2011
Tran Dental Office	327 W. Spring Valley Rd.	3,044	9,286	1.15	(C-M) Commercial District	2011
Arapaho Office Center	1150 E. Arapaho Rd	4,900	67,446	4.35	I-(M)1 Industrial District	2010
Doctor's Office	708 W. Spring Valley Rd	2,027	5,100	0.93	(PD) Planned Development	2009
Professional Office Building	2460 N. Central Expressway	7,486	7,486	1.27	(C-M) Commercial District	2009
<b>Industrial</b>	<b>Location</b>	<b>Additional Building</b>	<b>Total Building</b>	<b>Acreage</b>	<b>Zoning</b>	<b>Date</b>
		<b>Square Footage</b>	<b>Square Footage</b>			
Collins Technology Park	850 E. Collins Blvd.	121,368	121,368	7.17	I-FP(2) Industrial Fire Proof District	2012
Collins Technology Park	904 Quality Way	2,142	48,425	4.32	I-FP(2) Industrial Fire Proof District	2012
Stream Datacenter	1811 E. Renner Rd	73,927	73,927	7.09	(PD) Planned Development	2011
Bank of America Datacenter	3510 Wyndham Ln.	18,300	138,788	11.79	(PD) Planned Development	2011
Bank of America Datacenter	3000 Telecom Pkwy.	18,430	151,214	19.94	I-(M)1 Industrial District	2011
English Paint and Supply	820 Grove Rd.	19,280	19,280	1.70	I-FP(2) Industrial Fire Proof District	2011
Collins Technology Park	1215 Datacenter Blvd	112,000	112,000	6.96	I-FP(2) Industrial Fire Proof District	2011
Viawest Datacenter	3000 Waterview Pkwy.	13,200	312,620	16.02	(TO-M) Technical Office District	2011
<b>Zoning Approvals</b>						
<b>Case Name</b>	<b>Location</b>	<b>Case Details</b>		<b>Acreage</b>		<b>Date</b>
Brick Row	NEC Spring Valley at Green	Converted 77 condo units to apartments units		26.97		2011
West Spring Valley	N side of Spring Valley, between Coit Rd & Central Expswy.	(PD) Planned Development District for pedestrian-oriented, mixed-use district (permits office and multi-family development)		188		2011
GO Industries	420 N. Grove Rd.	Converted site from I-M(1) Industrial District to I-FP(2) Industrial Fire Proof District		1.79		2011
Bush Station - 75 Partners	North side of Renner Rd. between the DART Light Rail and Plano Rd.	2,207,000 SF of Office (Max) 3,756 Apartments (Max)		57.1		2010
Bush Station - Carruth	NEC & NWC Central and Renner Rd.	1,607,000 SF of Office (Max) 1,365 Multi-family units (Max)		85.9		2010

TABLE 2

Project Name	Address	Zip	Type	Year Built	Unit Count
ARBORETUM ESTATES	411 BUCKINGHAM	75081	Apartment	1997	342
ASHLEY PLACE	732 W SPRING VALLEY RD	75080	Apartment	Demolished	
BELLE GROVE	800 CUSTER RD	75080	Apartment	1968	101
BLOCK 24	2000 E ARAPAHO RD	75081	Apartment	2002	396
BRIARWOOD	330 E POLK ST	75081	Apartment	1972	30
BRICK ROW (CONDOS)	NWC SPRING VALLEY & GREENVILLE	75081	Condominium	Platted	
BRICK ROW (TOWNHOMES)	NWC SPRING VALLEY & GREENVILLE	75081	Townhome	Platted	
BRICK ROW URBAN VILLAGE	744 BRICK ROW	75081	Apartment	2009	247
BRISTOL AT BUCKINGHAM	535 BUCKINGHAM RD	75081	Apartment	2001	242
CAMDEN BUCKINGHAM	430 BUCKINGHAM RD	75081	Apartment	1996	464
CAMELOT	1212 HAMPSHIRE LN	75080	Apartment		
CENTRAL NORTH	340 CUSTER RD	75080	Apartment	1960	30
CENTRE SQUARE I CONDOMINIUMS	919 S WEATHERED DR	75080	Condominium	1976	86
CUTTER'S POINT	1111 ABRAMS RD	75081	Apartment	1978	196
FALLS ON CLEARWOOD	613 CLEARWOOD DR	75081	Apartment		
FRANCES WAY VILLAS	900 FRANCES WAY	75081	Apartment	1979	200
HEATHER GLEN	105 S BOWSER RD	75081	Apartment	1963	25
HILLSDALE GARDEN	800 W SPRING VALLEY RD	75080	Apartment	1969	72
HUNTINGTON TOWNHOMES	910 SPRING VALLEY PLZ	75080	Townhome	1963	73
LA MIRADA	1433 REGAL DRIVE	75080	Apartment		
LAKEFRONT VILLAS	900 W SPRING VALLEY RD	75080	Apartment	1968	105
LAKESIDE ON SPRING VALLEY	1000 W SPRING VALLEY RD	75080	Apartment	1968	81
LIFESCAPE VILLAS	821 DUBLIN DR	75080	Condominium	1981	74
MADISON ON MELROSE	1520 RICHARDSON DR	75080	Apartment	1995	200
MIRAMONTE	929 SAINT PAUL DR	75080	Apartment		
MISSION PRESTON WOOD	333 PRESTONWOOD DR	75081	Apartment	1979	194
NEW ORLEANS	925 S WATERVIEW DR	75080	Apartment		
OAKS AT SPRING VALLEY	740 W SPRING VALLEY RD	75080	Apartment	1965	56
SHENANDOAH	939 ALLEGHENY CT APT A	75080	Apartment	1969	192
SONTERRA AT BUCKINGHAM	530 BUCKINGHAM RD	75081	Apartment	1995	312
SWEETWATER RANCH	540 BUCKINGHAM RD	75081	Apartment	1994	312
TRADE WINDS	104 S BOWSER RD	75081	Apartment	1963	34
TRELLIS PLACE DUPLEXES	206 TRELLIS PL	75081	Apartment		
WATERFALL CROSSING CONDOS	SEC DUBLIN & ALLEGHENY	75080	Condominium		
WATERFORD VILLAS	SEC BELTLINE & GROVE RD	75081	Townhome		
WELLINGTON AT ARAPAHO	600 W ARAPAHO RD	75080	Apartment	2001	137
WINDHAM CHASE	1330 W SPRING VALLEY RD	75080	Apartment	1971	236
			Avg. Year Built	1980	

(Source: Dallas Central Appraisal District)

## Explanation and Description of Request

The proposed multifamily community provides a unique opportunity to stimulate development in close proximity to a key Richardson transit station. This site is approximately 12.7 acres of land, which has been vacant for many years, located at the southeast corner of Greenville Avenue and Collins Boulevard. The site is less than one quarter mile from the DART Red Line Arapaho Road station and is an excellent location for the proposed development which will contain approximately 350 multifamily dwelling units at 3 to 4 stories in height (approximately 30 units per acre).

Embrey Partners, Ltd. is proposing a high quality community on a fairly difficult development tract. The physical attributes of the proposed development demonstrate excellence in site planning and design. The proposed development provides exceptional circulation from within the development to Alma Drive and Greenville Avenue while maintaining a high level of architectural design. Aesthetics from the streets and main driveway present a friendly and inviting perspective for pedestrians and motorists alike. The proposed buildings are enhanced with vertical and horizontal articulation and contain a combination of interesting materials and colors.

Multifamily residential uses are ideal for this site based on the following considerations:

1. **ACCESS:** Access challenges from grade-separated Collins Boulevard and U.S. 75 make this site very challenging for retail or commercial development. Primary access will be from Alma Road and Greenville Avenue. There is no access directly from Collins Boulevard.
2. **VISIBILITY:** The site has inadequate visibility for retail development. The site is visually separated from US 75 by Greenville Avenue and somewhat concealed by the embankments for the Kansas City Southern Railroad track to the south and Collins Boulevard to the north.
3. **MARKET CONDITIONS:** The sluggish market demand and high vacancy rates for office use make this site challenging for new office development.
4. **DEMAND FOR MULTIFAMILY RESIDENTIAL AT THE ARAPAHO STATION:** There are no significant multifamily residential redevelopment uses within the one-quarter mile radius around the existing DART Arapaho rail station as envisioned in the Arapaho Station Area Plan. Development or redevelopment around DART stations has been driven primarily by multifamily residential uses, as is true nationwide. In order for redevelopment and transformation to take place in the Arapaho station area, there needs to be a catalyst for change. A base of multifamily residential can be such a catalyst, signaling to the market the latent development opportunities at this DART station.
5. **LOCAL DEMAND FOR MULTIFAMILY USES:** Currently, there is high demand for multifamily and limited supply in the pipeline. With the surrounding uses being vacant or under-utilized industrial land, an infusion of population in the area will be a beneficial catalyst for future development of complementary uses.
6. **DEVELOPMENT OPPORTUNITY:** The subject property is one of the few sites around the DART station that is currently vacant and is a good first candidate for residential development thus driving redevelopment of other adjoining under-utilized properties. Multifamily uses can encourage a mix of compatible uses throughout the station area, rather than just the daytime commuters and industrial uses that are currently present.
7. **IMPLEMENTATION OF CITY POLICY:** The development of multifamily residential on the subject property would implement one of the City of Richardson's significant policies of encouraging multifamily residential uses around DART stations. This site is within the recommended walking distance for transit-oriented residential uses.

Based on the foregoing, multifamily residential uses should be developed on the site to take advantage of current market opportunities while implementing important City of Richardson policies and positioning the entire DART Arapaho station area for redevelopment.



JACKSON WALKER L.L.P.



ATTORNEYS & COUNSELORS

November 7, 2012

**VIA ELECTRONIC MAIL**

Mr. Michael Spicer  
Development Services Department  
City of Richardson  
411 West Arapaho Road, Room 204  
Richardson, Texas 75080

RE: ***ZF 12-11***  
***Application for Change in Zoning from I-FP(2) to a new Planned Development District; 12.7 Acres, Southwest Corner of Greenville Avenue and Collins Boulevard.***

Dear Michael:

We are very excited to be submitting to you the revised plans and elevations for the zoning application referenced above. We believe that we have followed the direction the City Council provided at the last hearing and that the suggestions they provided greatly enhance the community we are developing.

The proposed development will be an extremely high quality, Class A community with superior design, finishes and amenities. As such, it will demand top of the market rents and be a landmark development for the City of Richardson. It will be the catalytic development that encourages further development around the Arapaho DART Station and set the standard for quality within this transit oriented development district.

Specifically, our site plan and elevations have been modified to include the following:

- a. Enhanced urban design with buildings pulled close to Greenville Avenue;
- b. Four story structures, up to 60' in height with elevators, that create the urban form on Greenville Avenue and Alma Road;
- c. Increased density from 28 to 36 units per acre;
- d. Structured parking;
- e. "Retail Ready" space fronting Greenville Avenue and Alma Road that will provide the option for mixed use when the market supports such a conversion; and
- f. A future community park that enhances the community "feel" and provides the required social interaction at the crossroads of significant City trails.

Accompanying this letter, please find the revised planned development conditions. The site plan, elevations and renderings will be delivered to you under separate cover.

We appreciate all of the meetings and correspondence we have had with you and other professionals at the City since the previous City Council hearing in addressing design and technical issues to make the site plan more effective. Again, with the direction of City Council and the assistance of the City staff, we are confident that we are providing a preeminent, yet pioneering, development true to Richardson and the its objectives for sound growth. Embrey is very proud to be a part of Richardson.

Thank you for your assistance with this request and do not hesitate to contact me if you have any questions.

Sincerely,

A handwritten signature in black ink, appearing to read "William S. Dahlstrom", written in a cursive style.

William S. Dahlstrom

CC: Dan Johnson  
Sam Chavez  
Chris Shacklett  
John Kirk  
Scott Polikov



# Notice of Public Hearing

## City Plan Commission • Richardson, Texas

An application has been received by the City of Richardson for a:

### PD PLANNED DEVELOPMENT

**File No./Name:** ZF 12-11 / GreenVUE Planned Development  
**Property Owner:** Leora Azoulay Lesh, VP / SAF CTP, LLC  
**Applicant:** John S. Kirk, Sr. VP / Embrey Partners, Ltd.  
**Location:** SEC Greenville Avenue & Collins Blvd. (See map on reverse side)  
**Current Zoning:** I-FP(2) Industrial  
**Request:** A request by John S. Kirk, representing Embrey Partners, Ltd., for a change in zoning from I-FP(2) Industrial with special conditions to PD Planned Development District for the development of a multi-family community.

The City Plan Commission will consider this request at a public hearing on:

**TUESDAY, AUGUST 21, 2012**  
**7:00 p.m.**  
**City Council Chambers**  
**Richardson City Hall, 411 W. Arapaho Road**  
**Richardson, Texas**

*This notice has been sent to all owners of real property within 200 feet of the request; as such ownership appears on the last approved city tax roll.*

**Process for Public Input:** A maximum of 15 minutes will be allocated to the applicant and to those in favor of the request for purposes of addressing the City Plan Commission. A maximum of 15 minutes will also be allocated to those in opposition to the request. Time required to respond to questions by the City Plan Commission is excluded from each 15 minute period.

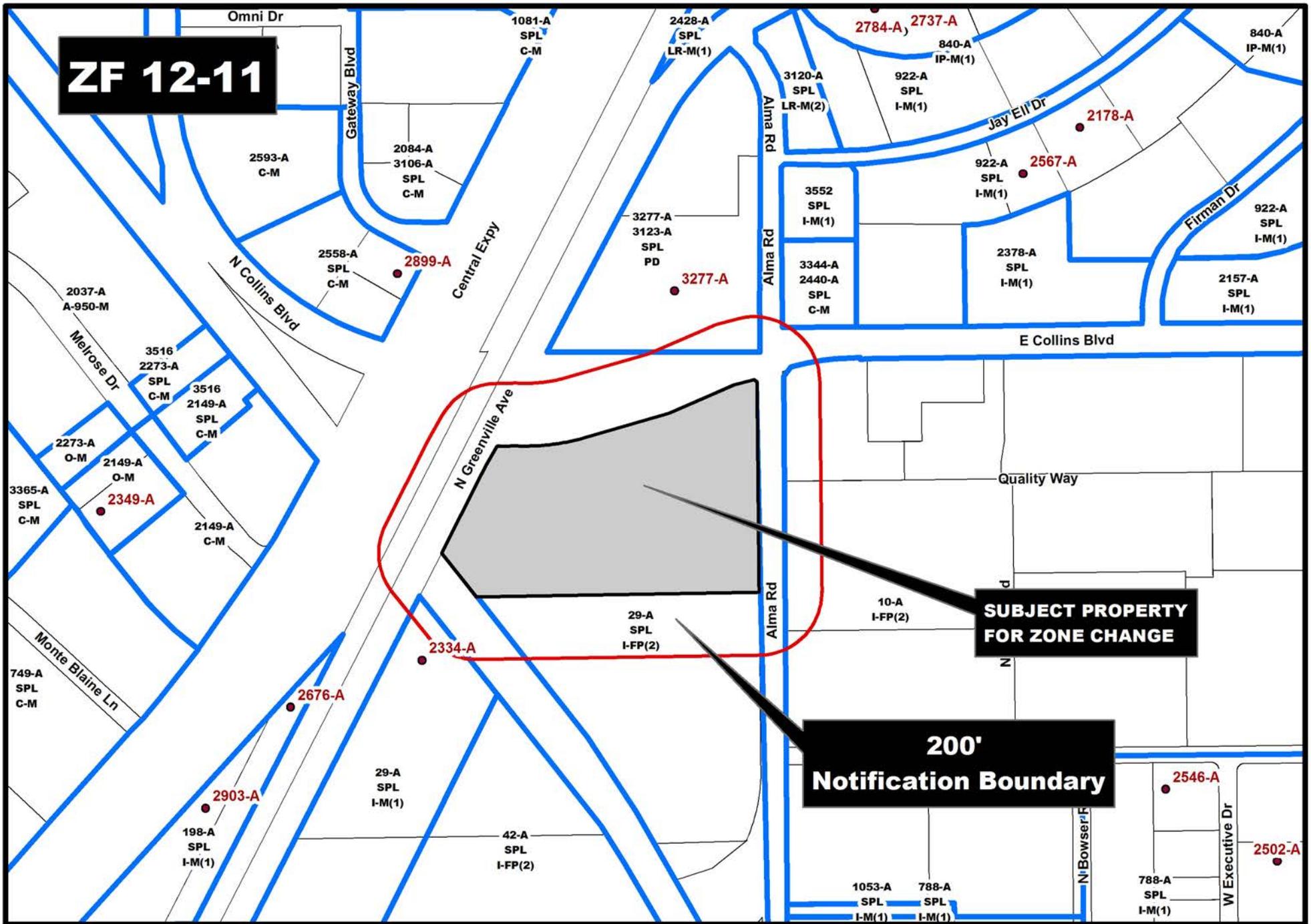
Persons who are unable to attend, but would like their views to be made a part of the public record, may send signed, written comments, referencing the file number above, prior to the date of the hearing to: Dept. of Development Services, PO Box 830309, Richardson, TX 75083.

*The City Plan Commission may recommend approval of the request as presented, recommend approval with additional conditions or recommend denial. Final approval of this application requires action by the City Council.*

**Agenda:** The City Plan Commission agenda for this meeting will be posted on the City of Richardson website the Saturday before the public hearing. For a copy of the agenda, please go to: <http://www.cor.net/DevelopmentServices.aspx?id=13682>.

For additional information, please contact the Dept. of Development Services at 972-744-4240 and reference Zoning File number ZF 12-11.

Date Posted and Mailed: 08/10/12



## ZF 12-11 Notification Map

Updated By: shacklett, Update Date: July 13, 2012  
 File: DSI\mapping\Cases\Z\2012\ZF1211\ZF1211 notification.mxd

This product is for informational purposes and may not have been prepared for or be suitable for legal, engineering, or surveying purposes. It does not represent an on-the-ground survey and represents only the approximate relative location of property boundaries.



DART  
PO BOX 660163  
DALLAS, TX 75266-0163

FSP COLLINS CROSSING LTD  
C/O FRANKLIN ST PPTIES CO  
401 EDGEWATER PL #200  
WAKEFIELD, MA 01880-6207

TESS PARTNERS LTD  
9023 CLAYCO DR  
DALLAS, TX 75243-6318

UNIVERSITY DRIVE LLC  
2550 INTERSTATE TOWER  
121 W TRADE ST  
CHARLOTTE, NC 28202-5399

KANSAS CITY SOUTHERN RR  
PO BOX 219335  
KANSAS CITY, MO 64121-9335

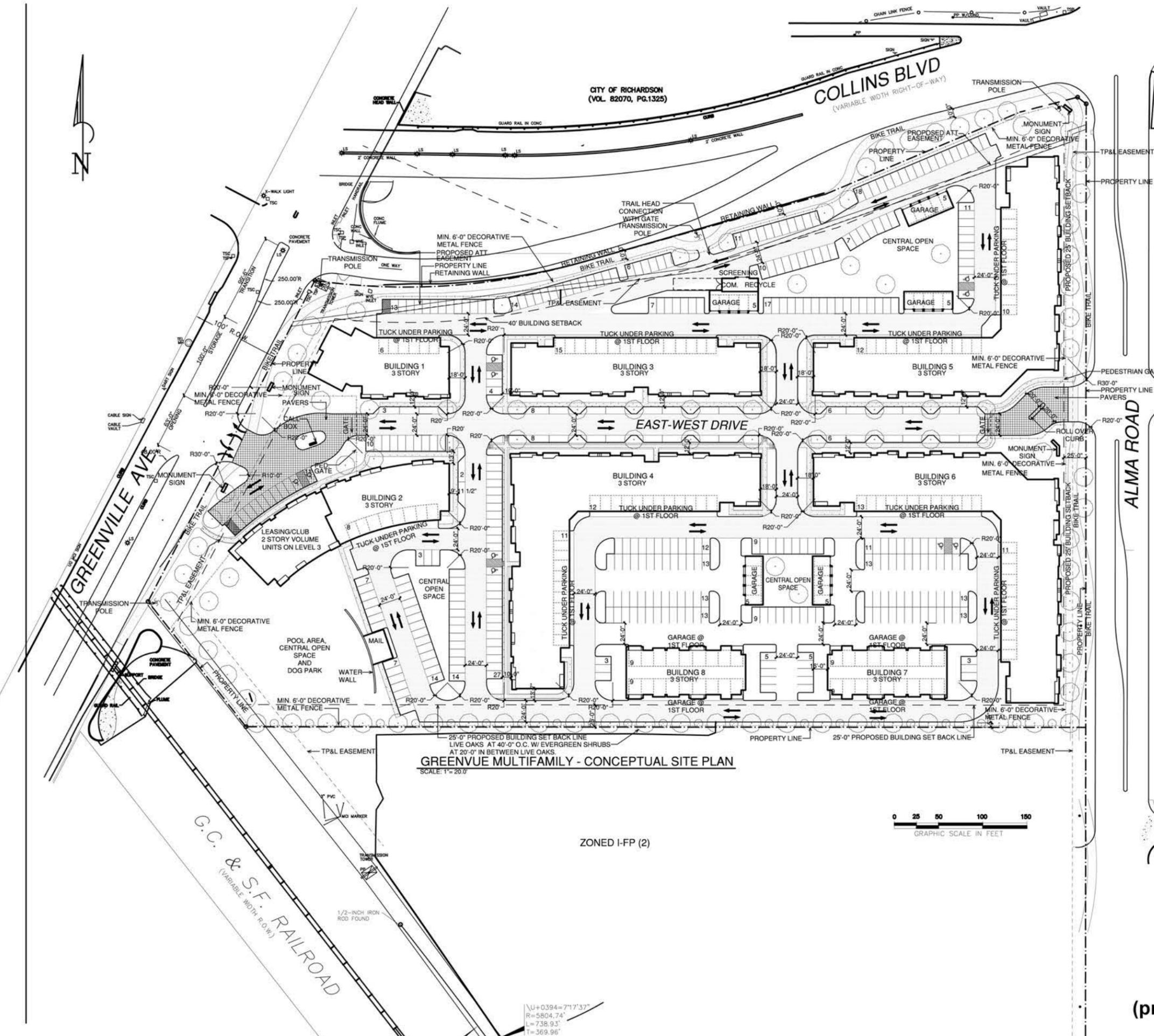
WOODALL JAMES KIRK  
% VENT A HOOD  
PO BOX 830426  
RICHARDSON, TX 75083-0426

COLLINS TECHNOLOGY PARK  
908 QUALITY WAY  
RICHARDSON, TX 75081-2277

**LEORE AZOULEY LESH, VP**  
**SAF COLLINS TECHNOLOGY PARK**  
**18111 PRESTON RD STE 1000**  
**DALLAS, TX 75252-6099**

**ZF 12-11**  
**Notification List**

**JOHN S. KIRK, EXECUTIVE VP**  
**EMBRY PARTNERS, LTD.**  
**1020 NE LOOP 410, SUITE 700**  
**SAN ANTONIO, TX 75209**



**GREENVUE MULTIFAMILY - CONCEPTUAL SITE PLAN**  
SCALE: 1" = 20.0'

ZONED I-FP (2)

PROJECT SUMMARY		
	EXISTING	PROPOSED
ZONING	I-FP (2) INDUSTRIAL	PLANNED DEVELOPMENT FOR MULTIFAMILY RESIDENTIAL
SETBACKS		40'-0" ALONG GREENVILLE AVE & COLLINS BLVD. 25'-0" ALONG ALMA ROAD 25'-0" REAR
LOT AREA	12.747 ACRES OR 655,259.32 SF	
BUILDING SQ. FT. PER FLOOR	FIRST = 153,950.85 SF	
	SECOND = 147,450.74 SF	
	THIRD = 143,708.74 SF	
	TOTAL = 445,110.33 SF	
PARKING RATIO	1.5/UNIT MIN. = 527 SPACES	
PARKING	PROVIDED 538 TOTAL SPACES 380 SURFACE SPACES 133 SINGLE CAR GARAGES/TUCK UNDER PARKING 25 DETACHED GARAGE SPACES	
LANDSCAPE % SF	173,101.74 SF OR 31%	
F.A.R.	0.80 F.A.R.	
DENSITY	28 UNITS-PER ACRE MAX.	
LOT COVERAGE	153,951.04 SF/555,259.32 = .277 = 28% / 30% MAX. PROPOSED	
BUILDING HEIGHT	50'-0" (MEAN LEVEL BETWEEN EAVE AND RIDGE) / 4 STORY MAX.	
TOTAL # OF UNITS	351 UNITS - 244 @ 1B/1B & 107 @ 2B/2B = TOTAL 458 BEDROOMS	

- NOTES:
1. THIS SITE DOES NOT LIE IN THE FLOODPLAIN, FLOODWAY, STREAM COURSE OR OTHER WATER FEATURE.
  2. THERE ARE NO LOADING DOCKS ON THIS SITE.
  3. THERE ARE NO OPEN STORAGE AREAS ON THIS SITE.
  4. TEMPORARY TRASH STORAGE WILL BE LOCATED WITHIN THE BUILDINGS.



**APPLICANT:**  
JOHN KIRK  
1020 N.E. LOOP 410  
SUITE 700  
SAN ANTONIO, TEXAS 78209  
REPRESENTING:  
EMBREY PARTNERS, LTD.

**LAND OWNER:**  
TOD FOBARE, REPRESENTING:  
SAF CTP, LLC.  
18111 PRESTON ROAD  
SUITE 1000  
DALLAS, TEXAS 75252

REVISIONS

NO.	DATE	DESCRIPTION

**GREENVUE**  
ROCKWALL SUBDIVISION LOT 4, BLOCK 3  
RICHARDSON, TEXAS  
EMBREY

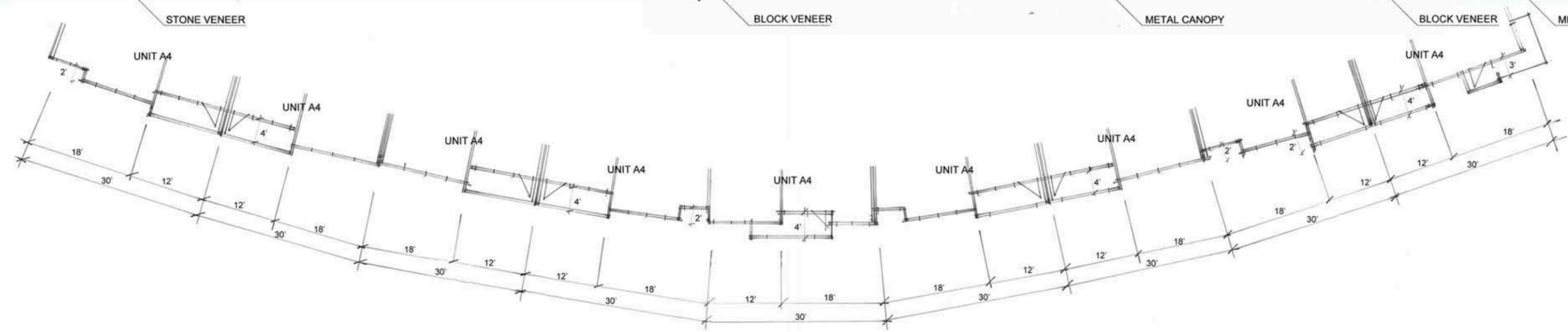


4144 N. Central Expy.  
Suite 855  
Dallas, TX 75204  
214.520.8878  
bgoarchitects.com

DATE	08-13-2012
PROJECT	12130
SHEET NUMBER	

Previous Zoning Exhibit  
(presented at 9-24-2012 CC Mtg)

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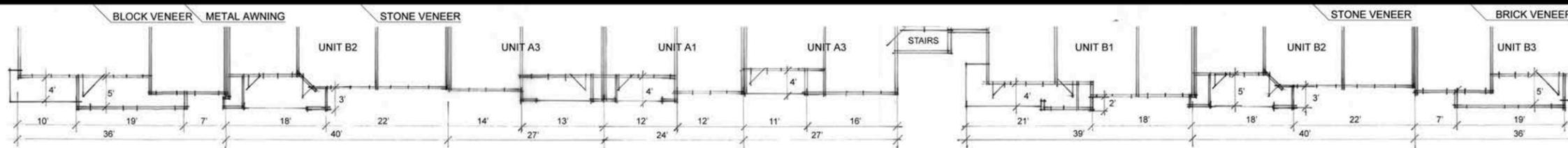


Metal Panel : 7%  
 Hardi Panel: 12%  
 3 Coat Stucco : 11%  
 Brick : 27%  
 Block : 5 %  
 Stone : 38%  
  
 Masonry Total : 70%  
  
**Typical Color selection**  
 Metal Panel : Blue  
 Hardi Panel: White, Gray  
 3 Coat Stucco : White  
 Metal Awning : Blue  
 Brick : Gray  
 Block : White  
 Stone :Tan

**BUILDING 2 NORTH ELEVATION**

**GREENVUE DEVELOPMENT CONCEPTUAL ELEVATIONS  
RICHARDSON, TX**

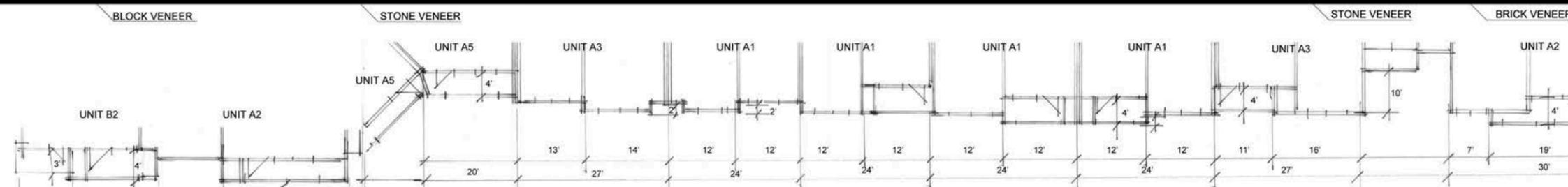
**Previous Elevations  
(presented at 9-24-2012 CC Mtg)**



Hardi Panel : 16%  
 3 Coat Stucco : 10%  
 Brick : 28%  
 Block : 6%  
 Stone : 40%  
 Masonry Total : 74%

**Typical Color selection**  
 Hardi Panel : White, Blue, Gray  
 3 Coat Stucco : White  
 Metal Awning : Blue  
 Brick : Gray  
 Block : White  
 Stone : Tan

**BUILDING 4 NORTH ELEVATION-BUILDING 3,5 AND 6 SIMILAR**



Hardi Panel : 15%  
 3 Coat Stucco : 8%  
 Brick : 32%  
 Block : 6%  
 Stone : 39%  
 Masonry Total : 77%

**Typical Color selection**  
 Hardi Panel : White, Blue, Gray  
 3 Coat Stucco : White  
 Metal Awning : Blue  
 Brick : Gray  
 Block : White  
 Stone : Tan

**BUILDING 4 SOUTH ELEVATION-BUILDING 3,5 AND 6 SIMILAR**

**GREENVUE DEVELOPMENT CONCEPTUAL ELEVATIONS**

**RICHARDSON, TX**

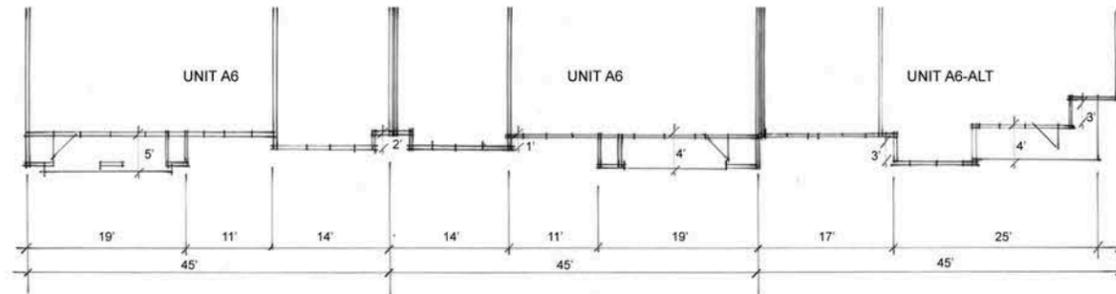
**Previous Elevations  
 (presented at 9-24-2012 CC Mtg)**



**BUILDING 7 & 8 NORTH AND SOUTH ELEVATION**



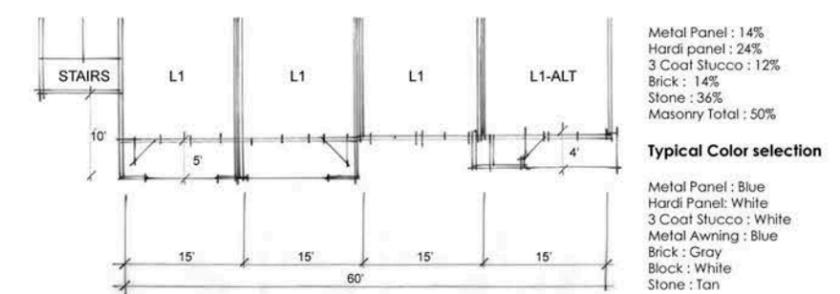
**BUILDING 1 WEST ELEVATION**



Hardi Panel : 24%  
 3 Coat Stucco : 20%  
 Brick : 21%  
 Block : 3%  
 Stone : 32%  
 Masonry Total : 56%

**Typical Color selection**

Metal Panel : Blue  
 Hardi Panel: White  
 3 Coat Stucco : White, Blue  
 Metal Garage Door : Brown  
 Brick : Gray  
 Block : White  
 Stone : Tan



Metal Panel : 14%  
 Hardi panel : 24%  
 3 Coat Stucco : 12%  
 Brick : 14%  
 Stone : 36%  
 Masonry Total : 50%

**Typical Color selection**

Metal Panel : Blue  
 Hardi Panel: White  
 3 Coat Stucco : White  
 Metal Awning : Blue  
 Brick : Gray  
 Block : White  
 Stone : Tan



**GARAGE & TRASH ENCLOSURE SOUTH ELEVATION**

**GARAGE & TRASH ENCLOSURE NORTH ELEVATION**

**GARAGE & TRASH ENCLOSURE WEST ELEVATION**

**GREENVUE DEVELOPMENT CONCEPTUAL ELEVATIONS**  
 RICHARDSON, TX

**Previous Elevations**  
 (presented at 9-24-2012 CC Mtg)



CITY OF RICHARDSON  
(VOL. 82070, PG.1325)

COLLINS BLVD  
(VARIABLE WIDTH RIGHT-OF-WAY)

PROJECT SUMMARY		
	EXISTING	PROPOSED
ZONING	I-FP (2) INDUSTRIAL	PLANNED DEVELOPMENT FOR MULTIFAMILY RESIDENTIAL
SETBACKS		40'-0" ALONG GREENVILLE AVE & COLLINS BLVD. 25'-0" ALONG ALMA ROAD 25'-0" REAR
LOT AREA	11.13 ACRES OR 484,739.4 SF	
BUILDING SQ. FT. PER FLOOR	FIRST = 136,271.26 SF	
	SECOND = 123,923.26 SF	
	THIRD = 132,671.26 SF	
	FOURTH = 101,075.41 SF	
	TOTAL = 493,941.19 SF	
PARKING RATIO REQUIREMENT	1.5/UNIT MIN. = 802 SPACES	
PARKING	PROVIDED 836 TOTAL SPACES 306 SURFACE SPACES 125 TUCK UNDER PARKING SPACES 205 PARKING GARAGE SPACES	
LANDSCAPE %-SF	139,101.72 SF OR 29%	
F.A.R.	1.08 F.A.R.	
DENSITY	38 UNITS-PER ACRE MAX.	
LOT COVERAGE	136,271.26 SF/484,739.4 = .281 = 28% / 30% MAX. PROPOSED	
BUILDING HEIGHT	60'-0" (MEAN LEVEL BETWEEN EAVE AND RIDGE) / 4 STORY MAX.	
TOTAL # OF UNITS	401 UNITS - 279 @ 1B/1B & 122 @ 2B/2B = TOTAL 523 BEDROOMS	

NOTES:  
1. THIS SITE DOES NOT LIE IN THE FLOODPLAIN, FLOODWAY, STREAM COURSE OR OTHER WATER FEATURE.  
2. THERE ARE NO LOADING DOCKS ON THIS SITE.  
3. THERE ARE NO OPEN STORAGE AREAS ON THIS SITE.  
4. TEMPORARY TRASH STORAGE WILL BE LOCATED WITHIN THE BUILDINGS.



APPLICANT:  
JOHN KIRK  
1020 N.E. LOOP 410  
SUITE 700  
SAN ANTONIO, TEXAS 78209  
REPRESENTING:  
EMBREY PARTNERS, LTD.

LAND OWNER:  
TOD FOBAE, REPRESENTING:  
SAF CTP, LLC.  
18111 PRESTON ROAD  
SUITE 1000  
DALLAS, TEXAS 75252

**GREENVUE MULTIFAMILY - CONCEPTUAL SITE PLAN**

SCALE: 1" = 50.0'



ZONED I-FP (2)

Previous Zoning Exhibit  
(presented at 11-12-2012 CC Mtg)

REVISIONS

**GREENVUE**  
ROCKWALL SUBDIVISION LOT 4, BLOCK 3  
RICHARDSON, TEXAS  
EMBREY

**BGO**  
architects

4144 N. Central Expy.  
Suite 855  
Dallas, TX 75204  
214.520.8878  
bgoarchitects.com

DATE

11-07-2012

PROJECT

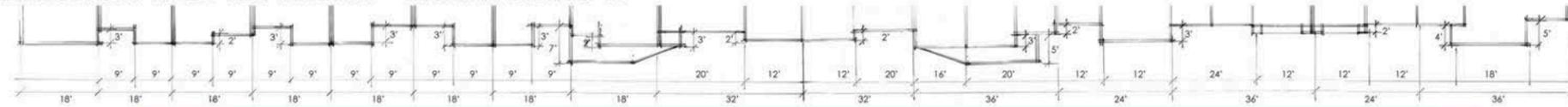
12130

SHEET NUMBER

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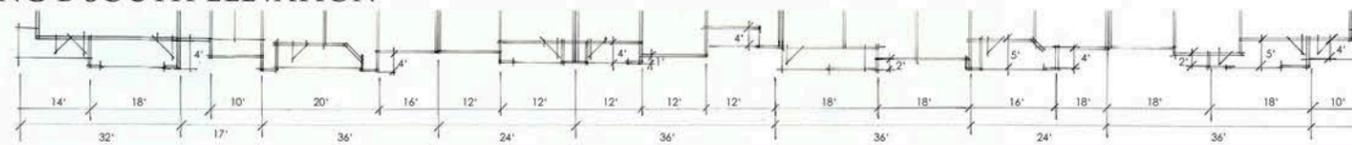
**BUILDING A WEST ELEVATION - GREENVILLE AVE.**



- Metal Panel : 6%
  - Hardi Panel : 13%
  - 3 Coat Stucco : 11%
  - Brick : 28%
  - Block : 4%
  - Stone : 38%
  - Masonry Total : 70%
- Metal Panel : Blue
  - Hardi Panel : White, Gray
  - 3 Coat Stucco : White
  - Metal Awning : Blue
  - Metal Canopy : Brown
  - Brick : Gray
  - Block : White
  - Stone : Tan



**BUILDING B SOUTH ELEVATION**

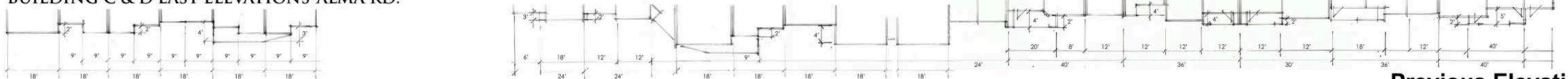


- Hardi Panel : 15%
  - 3 Coat Stucco : 11%
  - Brick : 29%
  - Block : 5%
  - Stone : 40%
  - Masonry Total : 74%
- Hardi Panel : White, Blue, Gray
  - 3 Coat Stucco : White
  - Metal Awning : Blue
  - Brick : Gray
  - Block : White
  - Stone : Tan

- BUILDING C:**
  - Hardi Panel : 10%
  - 3 Coat Stucco : 19%
  - Brick : 28%
  - Block : 4%
  - Stone : 39%
  - Masonry Total : 71%
- BUILDING D:**
  - Hardi Panel : 11%
  - 3 Coat Stucco : 19%
  - Brick : 29%
  - Block : 4%
  - Stone : 37%
  - Masonry Total : 70%
- Hardi Panel : White, Blue, Gray
  - 3 Coat Stucco : White
  - Metal Awning : Blue
  - Metal Canopy : Brown
  - Brick : Gray
  - Block : White
  - Stone : Tan



**BUILDING C & D EAST ELEVATIONS-ALMA RD.**



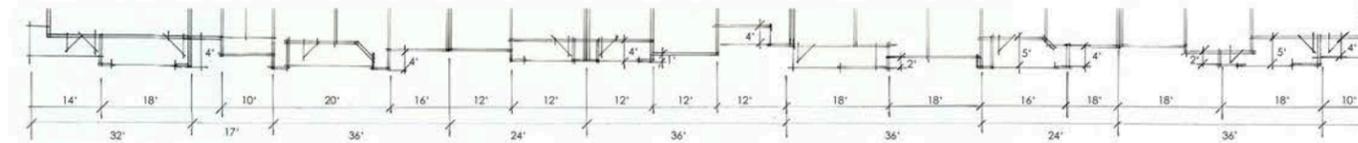
**GREENVUE DEVELOPMENT CONCEPTUAL ELEVATIONS**  
RICHARDSON, TX

**Previous Elevations**  
(presented at 11-12-2012 CC Mtg)

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**BUILDING E NORTH ELEVATION**

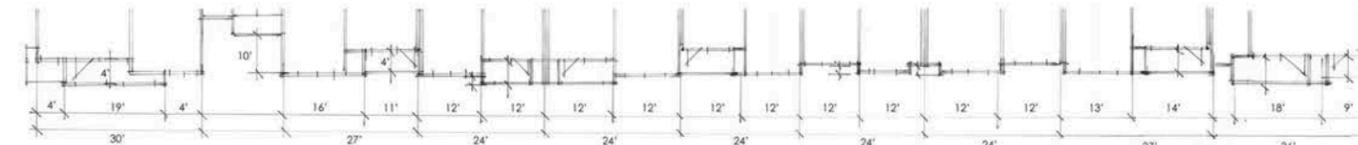


Hardi Panel : 16%  
 3 Coat Stucco : 10%  
 Brick : 28%  
 Block : 6%  
 Stone : 40%  
 Masonry Total : 74%

Hardi Panel: White, Blue, Gray  
 3 Coat Stucco : White  
 Metal Awning : Blue  
 Brick : Gray  
 Block : White  
 Stone : Tan



**BUILDING E SOUTH ELEVATION**



Hardi Panel : 15%  
 3 Coat Stucco : 9%  
 Brick : 40%  
 Block : 5%  
 Stone : 31%  
 Masonry Total : 76%

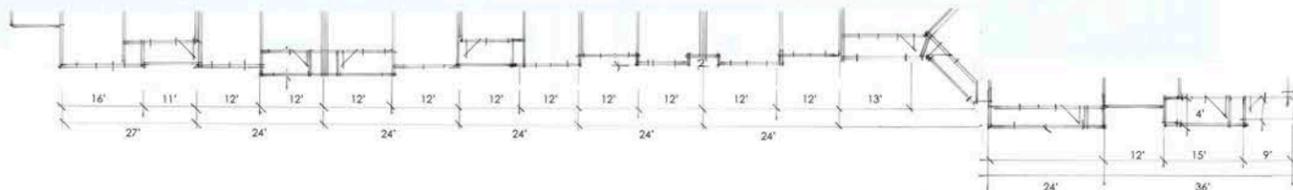
Hardi Panel: White, Blue, Gray  
 3 Coat Stucco : White  
 Metal Awning : Blue  
 Brick : Gray  
 Block : White  
 Stone : Tan

Hardi Panel : 15%  
 3 Coat Stucco : 8%  
 Brick : 39%  
 Block : 6%  
 Stone : 32%  
 Masonry Total : 77%

Hardi Panel: White, Blue, Gray  
 3 Coat Stucco : White  
 Metal Awning : Blue  
 Brick : Gray  
 Block : White  
 Stone : Tan



**BUILDING B NORTH ELEVATION**



**TRASH ENCLOSURE SOUTH ELEVATION**



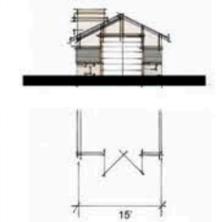
3 Coat Stucco : 36%  
 Brick : 39%  
 Stone : 25%  
 Masonry Total : 64%

**TRASH ENCLOSURE NORTH ELEVATION**



3 Coat Stucco : 15%  
 Brick : 62%  
 Stone : 23%  
 Masonry Total : 85%

**TRASH ENCLOSURE WEST ELEVATION**



3 Coat Stucco : 5%  
 Brick : 72%  
 Stone : 23%  
 Masonry Total : 95%

3 Coat Stucco : White  
 Brick : Gray  
 Stone : Tan

**GREENVUE DEVELOPMENT CONCEPTUAL ELEVATIONS**  
 RICHARDSON, TX

**Previous Elevations**  
 (presented at 11-12-2012 CC Mtg)



City of Richardson  
City Council Worksession  
Agenda Item Summary



**Worksession Meeting Date:** Monday, January 28, 2013

**Agenda Item:** Sign Control Board Case 13-01

**Staff Resource:** Don Magner, Assistant City Manager of Community Services

**Summary:** Consider the request of the Warming Hut for a variance to the City of Richardson Code of Ordinances, Chapter 18, Article III, Section 18-96(23)(d)(ii) to allow for a 18 foot reduction of the 30 foot setback requirement to allow for a 59.88 square foot pole sign, with an electronic messaging center to be 12 feet from the adjoining private line at the property located at 331 N Central Expressway; and take appropriate action.

**Board/Commission Action:** Sign Control Board approved the request unanimously.

**Action Proposed:** Approve, Approve with conditions, or Deny the request



City of Richardson  
City Council Worksession  
Agenda Item Summary



**Worksession Meeting Date:** Monday, January 28, 2013

**Agenda Item:** Sign Control Board Case 13-03

**Staff Resource:** Don Magner, Assistant City Manager of Community Services

**Summary:** Consider the request of Golf Cars of Dallas for variances to the City of Richardson Code of Ordinances, Chapter 18, Article III, Section 18-96(23)(b)(iii)(1) and Section 18-96(23)(c)(iii)(1) for a 20 foot increase in height and 52 square foot increase in sign area to allow for a 40 foot in height, 112 square foot multi-tenant pole sign at the property located at 2100 Alamo Road; and take appropriate action.

**Board/Commission Action:** Sign Control Board approved the request by a vote of 4-1.

**Action Proposed:** Approve, Approve with conditions, or Deny the request

**ORDINANCE NO. 3896**

**AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF RICHARDSON, TEXAS, CHANGING THE NAME OF “DATACENTER PARK BOULEVARD”, IN ITS ENTIRETY, TO “INTEGRITY DRIVE”; AND PROVIDING AN EFFECTIVE DATE.**

**WHEREAS**, the City Council of the City of Richardson, Texas, has deemed it appropriate to change the name of “Datacenter Park Boulevard” from its intersection with Security Row to its intersection with Quality Way, to “Integrity Drive”; and

**WHEREAS**, the City Council has determined that renaming “Datacenter Park Boulevard” from its intersection with Security Row to its intersection with Quality Way, to “Integrity Drive” is consistent with the ordinances of the City and in the best interest of the citizens.

**NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF RICHARDSON, TEXAS, THAT:**

**SECTION 1.** The name of “Datacenter Park Boulevard” from its intersection with Security Row its intersection with Quality Way shall be changed to “Integrity Drive” as shown in Exhibit “A” attached hereto. All maps and records of the City shall be corrected to reflect the change in street name described herein and appropriate signage shall be erected.

**SECTION 2.** Should any word, sentence, paragraph, subdivision, clause, phrase or section of this ordinance, be adjudged or held to be void or unconstitutional, the same shall not affect the validity of the remaining portions of said ordinance, which shall remain in full force and effect.

**SECTION 3.** All ordinances of the City of Richardson, Texas, in conflict with the provisions of this ordinance be, and same are hereby, repealed; provided, however, that all other provisions of said ordinances not in conflict with the provisions of this ordinance shall remain in full force and effect.

**SECTION 4.** This ordinance shall take effect immediately from and after its passage as the law and charter in such cases provide.

**DULY PASSED AND APPROVED** by the City Council of the City of Richardson, Texas, on this the \_\_\_\_\_ day of \_\_\_\_\_, 2013.

**APPROVED:**

---

**MAYOR**

**ATTEST:**

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**CITY SECRETARY**

**APPROVED AS TO FORM:**

---

**CITY ATTORNEY**

**RESOLUTION NO. 13-05**

**A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF RICHARDSON, TEXAS, ORDERING A GENERAL ELECTION TO BE HELD ON THE 11TH DAY OF MAY 2013, FOR THE PURPOSE OF ELECTING A MAYOR AND SIX (6) MEMBERS OF THE RICHARDSON CITY COUNCIL; DESIGNATING POLLING PLACES; ORDERING NOTICES OF ELECTION TO BE GIVEN; AUTHORIZING EXECUTION OF JOINT ELECTION CONTRACT; PROVIDING A REPEALING CLAUSE; PROVIDING A SEVERABILITY CLAUSE; AND PROVIDING FOR AN EFFECTIVE DATE.**

**WHEREAS**, in accordance with the general laws and Constitution of the State of Texas and the Charter of the City of Richardson, Texas, a Municipal Officers' election is to be held on the second Saturday in May; and

**WHEREAS**, it is necessary that the City Council of the City of Richardson order an election to be held on the 11th day of May 2013, 7:00 a.m. to 7:00 p.m., for the purpose of electing a Mayor (Place 7) and council members for Places 1, 2, 3, 4, 5, and 6; and

**WHEREAS**, the election shall be held as a Joint Election administered by the Dallas County Elections Administrator in accordance with the provisions of the Texas Election Code, the Charter of the City of Richardson, and a Joint Election Contract with the County of Dallas; and

**WHEREAS**, the City of Richardson accepts Dallas County Election Administration's use of the direct record and optical scan voting systems, which have been certified by the Secretary of State in accordance with the Texas Election Code and approved by the United States Department of Justice.

**NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF RICHARDSON, TEXAS:**

**SECTION 1:** That a Municipal Officers' election is hereby ordered for May 11, 2013, for the City of Richardson, Texas, for the purpose of electing a Mayor (Place 7) and council members for Places 1, 2, 3, 4, 5, and 6. Polling locations for the election will be determined in the Joint Election Contract to be entered into with Dallas County. The polling locations shall be open between the hours of 7:00 a.m. and 7:00 p.m. the date of the election. The election will be conducted in accordance with the Joint Election Contract by and between the City, the County of Dallas and other units of government and the Texas Election Code.

**SECTION 2:** That the City Secretary is hereby directed to cause notice of said election to be published at least once, not earlier than the 30th day nor later than the 10th day, before election day as provided in Section 4.003(a) (1) of the Texas Election Code; and shall be posted on the bulletin board used for posting notices of the City Council meetings not later than the 21st day before election day. A copy of the published notice that contains the name of the newspaper and the date of publication shall be retained as a record of such notice, and the person

posting the notice shall make a record at the time of posting stating the date and place of posting in accordance with Texas Election Code Section 4.005.

**SECTION 3.** That early voting by personal appearance by any qualified Dallas County or Collin County resident may be conducted at the Richardson Civic Center/City Hall, 411 W. Arapaho Road, or at any of the other Dallas County branch location established by the Joint Election Contract. Early voting by personal appearance for the May 11, 2013 election will be conducted by the Dallas County Elections Department beginning on Monday, April 29, 2013 and continue through Saturday, May 4, 2013, between 8:00 a.m. and 5:00 p.m.; Sunday, May 5, 2013 between 1:00 p.m. and 6:00 p.m.; and Monday, May 6, 2013 and Tuesday, May 7, 2013 between 7:00 a.m. and 7:00 p.m.

**SECTION 4.** The City shall be divided into twenty-two (22) Dallas County election precincts and six (6) Collin County election precincts for this election and the polling places designated for each election precinct are shown in Exhibit "A", which is attached hereto and incorporated herein by reference as a part hereof for all purposes. The City Secretary may change the places and times of the locations contained in Exhibit "A" without further City Council action or approval after consulting with the Dallas County Elections Administrator and determining that a change in the places or times is necessary.

**SECTION 5.** That applications for early voting ballot by mail shall be mailed to: Toni Pippins-Poole, Early Voting Clerk, Dallas County Elections Department, 2377 N. Stemmons Freeway, 8<sup>th</sup> Floor, Dallas, Texas 75207. Applications for early voting ballot by mail must be received no later than the close of business on May 3, 2013.

**SECTION 6.** That if a run-off election becomes necessary, the Dallas County Elections Administrator will conduct the run-off election to be held on Saturday, June 15, 2013.

**SECTION 7.** That in accordance with Section 123.001 of the Texas Election Code, the Direct Record and Optical Scan Voting Systems approved by the Secretary of State are hereby adopted for the election on May 11, 2013.

**SECTION 8.** That the City Manager is hereby authorized to execute a Joint Election Contract for the conduct of a joint election to be held on May 11, 2013, and to approve any amendments thereto.

**SECTION 9.** That the first day to file an application for a place on the ballot with the City Secretary is January 30, 2013 with the last day for filing to be March 1, 2013, at 5:00 p.m., in accordance with the Election Code Sections 143.006 and 143.007.

**SECTION 10.** That pursuant to the Joint Election Contract, the Dallas County Elections Administrator shall serve as Election Administrator for the election. Presiding Election Judges and Alternate Presiding Election Judges appointed to serve at said polling places shall be those election officials furnished by the Elections Administrator from the list of proposed election judges listed in an attachment to the Election Contract.

**SECTION 11.** That in compliance with Section 271.006 of the Texas Election Code, Toni Pippins-Poole, Dallas County Elections Administrator, will be appointed as Early Voting Clerk. Other deputy early voting clerks will be appointed as needed to process early voting mail and to conduct early voting by personal appearance at the branch locations.

**SECTION 12.** That an Early Voting Ballot Board shall be created to process early voting results in accordance with Section 87.007 of the Texas Election Code. The Early Voting Ballot Board shall be made up of members appointed in the manner stated in the Joint Election Contract and the Presiding Judge and Alternate Presiding Judge of the Early Voting Ballot Board shall be the election officials listed in the Joint Election Contract.

**SECTION 13.** That should any word, sentence, paragraph, subdivision, clause, phrase or section of this resolution be adjudged or held to be void or unconstitutional, the same shall not affect the validity of the remaining portions of said resolution, which shall remain in full force and effect.

**SECTION 14.** That this resolution shall become effective immediately from and after its passage and is accordingly so resolved.

**DULY RESOLVED AND ADOPTED** by the City Council of the City of Richardson, Texas on this the 28<sup>th</sup> day of January, 2013.

CITY OF RICHARDSON, TEXAS

\_\_\_\_\_  
MAYOR

ATTEST:

\_\_\_\_\_  
CITY SECRETARY

APPROVED AS TO FORM:

\_\_\_\_\_  
CITY ATTORNEY

*Resolution 13-05*  
*Exhibit A*

**CITY OF RICHARDSON**  
**MAY 11, 2013 PROPOSED ELECTION DAY VOTING LOCATIONS**

**PRECINCT**

**DALLAS COUNTY POLLING LOCATIONS**

2500	Greenwood Hills Elementary School, 1313 West Shore, Richardson 75080
2501	Canyon Creek Elementary School, 2100 Copper Ridge, Richardson 75080
2502	Prairie Creek Elementary School, 2120 E. Prairie Creek, Richardson 75080
2503	Mohawk Elementary School, 1500 Mimosa, Richardson 75080
2504	Richardson North Jr. High School, 1820 N. Floyd Road, Richardson 75080
2505	Northrich Elementary School, 1301 Custer, Richardson 75080
2506	Arapaho Classical Magnet School, 1300 Cypress, Richardson 75080
2507	Richardson Heights Elem. School, 101 N. Floyd Road, Richardson 75080
1500	Dover Elementary School, 700 Dover Drive, Richardson, 75080
1501	RISD Prof. Development Center, 701 W. Belt Line Road, Richardson 75080
2508	Richardson Terrace Elem. School, 300 N. Dorothy Drive, Richardson 75081
2509	Richardson East Church of Christ, 1504 E. Campbell Rd, Richardson 75081
2510	Yale Elementary School, 1900 E. Collins Blvd, Richardson 75081
2511 & 2512	Dartmouth Elementary School, 417 Dartmouth, Richardson 75081
2513	L. V. Berkner High School, 1600 E. Spring Valley, Richardson 75080
2514	Jess Harben Elementary School, 600 S. Glenville, Richardson 75081
1502	Math/Science/Technology Magnet School, 450 Abrams, Richardson 75081
1503 & 1504	Richland Elementary School, 550 Park Bend Drive, Richardson 75081
2701	Big Springs Baptist Church, 6538 N. Jupiter Rd, Garland 75044
2702	Miller Elementary School, 5651 Coventry Drive, Richardson 75082

**PRECINCT**

**COLLIN COUNTY POLLING LOCATIONS**

78 & 110	Richardson Police Substation, 2003 Renner Road, Richardson 75082
48 & 55	Aldridge Elementary School, 720 Pleasant Valley, Richardson 75080
94 & 125	Miller Elementary School, 5651 Coventry Drive, Richardson 75082

CITY OF RICHARDSON

TO: Dan Johnson - City Manager  
THRU: Kent Pfeil - Director of Finance  
FROM: Pam Kirkland - Purchasing Manager  
SUBJECT: Bid Initiation Request # 33-13  
DATE: January 23, 2013

Request Council approval to initiate bids for the following:

2013 Annual Requirement Contract for Street Rehabilitation

Proposed Council approval date: January 28, 2013  
Proposed advertising dates: January 30, 2013 & February 6, 2013  
Proposed bid due date: Thursday, February 14, 2013 – 3:00 p.m.  
Proposed bid opening date: Thursday, February 14, 2013 – 3:30 p.m.  
Engineer's estimated total cost: \$1,000,000  
Account: Various

  
\_\_\_\_\_  
Pam Kirkland, CPPO, CPPB  
Purchasing Manager

  
\_\_\_\_\_  
Kent Pfeil  
Director of Finance

  
\_\_\_\_\_  
Date

Approved: \_\_\_\_\_  
Dan Johnson  
City Manager

\_\_\_\_\_  
Date



## MEMO

TO: Dan Johnson, City Manager

THRU: Cliff Miller, Assistant City Manager *CM*

FROM: Steve Spanos, P.E., Director of Engineering *SS*

SUBJECT: Permission to Advertise Bid # 33-13  
2013 Annual Requirements Contract for Street Rehabilitation

DATE: January 18, 2013

### **BACKGROUND INFORMATION:**

The 2013 Annual Requirements Contract for Street Rehabilitation consists of citywide concrete replacement/rehab at various locations throughout the city. The contractor will be on-call for various concrete work including arterial streets. Work to be performed generally includes concrete pavement removal and replacement, concrete sidewalk/leadwalk replacement, 6" concrete curbs, barrier free ramps, sodding and other appurtenances related to performing the work. This contract contains an option to renew the contract for two (2) additional one year periods subject to the conditions, attached hereto.

It's the intent of the City to schedule approximately One Million Dollars (\$1,000,000) worth of work for the contractor during fiscal year 2013-2014. The City has the option to decrease or increase the amount of work during any contract period.

### **FUNDING:**

Funding is provided from the Street Rehabilitation Fund.

### **SCHEDULE:**

Capital Projects plans to begin construction for this project March 2013 and be completed by March 2014. An option to renew the contract may be exercised and could extend the schedule for up to two (2) additional one (1) year periods.

**NOTICE TO CONTRACTORS  
CITY OF RICHARDSON**

**2013 ANNUAL REQUIREMENTS CONTRACT  
FOR STREET REHABILITATION**

**BID 33-13**

Sealed bids addressed to the Purchasing Manager, of the City of Richardson, Texas, will be received at the Office of the City Purchasing Department, Suite 101, City Hall, 411 West Arapaho Road, Richardson, Texas, until **Thursday, at 3:00 p.m. on Thursday, February 14**, and will be opened and read aloud in the **Capital Projects Department, Room 206**, 30 minutes later that same day, for furnishing all labor, materials, tools and equipment, and performing all work required including all appurtenances for:

The 2013 Annual Requirements Contract for Street Rehabilitation consists of citywide concrete replacement/rehab at various locations throughout the city. The contractor will be on-call for various concrete work including arterial streets. Work to be performed generally includes concrete pavement removal and replacement, concrete sidewalk/leadwalk replacement, 6" concrete curbs, barrier free ramps, sodding and other appurtenances related to performing the work. This contract contains an option for two (2) one-year renewals subject to the conditions, attached hereto.

Proposals shall be accompanied by a certified or cashier's check on a state or national bank in an amount not less than five percent (5%) of the possible total of the bid submitted, payable without recourse to the City of Richardson, Texas, or an acceptable bid bond for the same amount from a reliable surety company as a guarantee that the bidder will enter into a contract and execute required Performance and Payment Bonds within ten (10) days after notice of award of contract. The notice of award of contract shall be given to the successful bidder within ninety (90) days following the opening of bids.

The successful bidder must furnish a Performance Bond upon the form provided in the amount of one hundred percent (100%) of the contract price, a material and labor Payment Bond upon the form provided in the amount of one hundred percent (100%) of the contract price, and a Maintenance Bond upon the form provided in the amount of one hundred percent (100%) of the contract price, from a surety authorized under the laws of the State of Texas to act as a surety on bonds for principals.

The right is reserved, as the interest of the Owner may require, to reject any and all bids, to waive any informality in the bids received, and to select bid best suited to the Owner's best interest.

**The contract will be for a period of 12 months beginning on the construction start date as indicated in the Notice to Proceed letter. The contract may be renewed at the option of the City for two (2) additional one (1) year periods under the same terms and conditions of the initial contract, if mutually agreed upon by both parties.**

One set of specifications and bid documents may be secured from the Office of the City Engineer, Capital Projects Department in Room 204, of the Richardson Civic Center/City Hall, 411 West Arapaho Road, Richardson, Texas, **beginning at 12:00 p.m. on Tuesday, January 29, 2013** upon a **NON-REFUNDABLE FEE OF FIFTY Dollars (\$50.00)** per set, payable to the City of Richardson, accompanied by the contractor's name, address, phone number, email address and FAX number.

**A pre-bid conference will be held Thursday, at 9:00 a.m. February 7, 2013, in the Capital Projects Department, Room 206, Richardson Civic Center/City Hall.**

By:/s/Bob Townsend, Mayor  
City of Richardson  
P. O. Box 830309  
Richardson, Texas 75083

**PROJECT SCHEDULE**  
**2013 ANNUAL REQUIREMENT CONTRACT FOR**  
**STREET REHABILITATION**

**BID # 33-13**

Agenda Paperwork to Advertise	Friday, January 18, 2013
Council Authorization to Advertise	Monday, January 28, 2013
Plans/Specs Available for Contractors	Tuesday, January 29, 2013
Advertise in Dallas Morning News	Wednesday, January 30, 2013
Advertise in Dallas Morning News	Wednesday, February 6, 2013
Pre Bid Meeting (Room 206 @ 9:00 am)	Thursday, February 7, 2013
Bids Received & Opened (Room 206 Opening @ 3:30 pm)	Thursday, February 14, 2013
Agenda Paperwork to Award Contract	Friday, February 15, 2013
Council to Award Contract	Monday, February 25, 2013
Pre-Construction Meeting	~ March 11, 2013
Project Start	~ March 18, 2013
Project Completed 365 Calendar Days	~ March 2014

*Project Manager: Henry Drexel  
Engineers Estimate: \$1,000,000  
Fund 353 Street Rehabilitation*



# MEMO

**DATE:** January 22, 2013

**TO:** Kent Pfeil – Director of Finance

**FROM:** Pam Kirkland – Purchasing Manager

**SUBJECT:** Award of Bid #15-13 for the Pavement/Drainage Rehabilitation (300 Block of Pittman, Wista Vista and Huffhines) to Quality Excavation in the amount of \$787,916.90

**Proposed Date of Award: January 28, 2013**

I concur with the recommendation of Steve Spanos – Director of Engineering, and request permission to award a contract to the low bidder, Quality Excavation, for the above referenced construction in the amount of \$787,916.90, as outlined in the attached memo.

Funding is provided from the G.O. Street Bonds and Water and Sewer C.O.'s.

The bid was advertised in *The Dallas Morning News* on November 28, 2012 & December 5, 2012 and was posted on Bidsync.com. A prebid conference was held on December 19, 2012 and eight bids were solicited and five bids were received.

Concur:

  
\_\_\_\_\_  
Kent Pfeil

Attachments

Xc: Dan Johnson  
David Morgan  
Cliff Miller  
Don Magner  
Shanna Sims-Bradish



## MEMO

TO: Dan Johnson, City Manager  
THROUGH: Cliff Miller, Assistant City Manager *cam*  
FROM: Steve Spanos, P.E., Director of Engineering *SS*  
SUBJECT: Award of Bid No. 15-13 for Pavement/Drainage Rehabilitation (300 Block of Pittman, Wista Vista and Huffhines) to Quality Excavation  
DATE: January 18, 2013

### **ACTION REQUESTED:**

Council to consider award of Bid No. 15-13 to Quality Excavation for the Pavement Rehabilitation in the amount of \$787,916.90.

### **BACKGROUND INFORMATION:**

On January 3, 2013 the Capital Projects Department opened bids for the subject project. The attached bid tabulation certifies the lowest bid was submitted by Quality Excavation in the amount of \$787,916.90.

Staff as well as the Finance Department have reviewed Quality Excavation company financials, bonding company, the insurance company and references, and recommend awarding the Pavement/Drainage Rehabilitation to Quality Excavation in the amount of \$787,916.90.

The asphalt pavement, along Pittman, Wista Vista and Huffhines between Greenville and Abrams will be rehabilitated. This project consists of recycling the existing asphalt roadway, re-grading the ditches, installing storm sewer pipe and replacing the driveway approaches. Recycling the roadway includes removing the top 2" of asphalt, remixing and compacting the remaining asphalt and base, and then overlaying the entire roadway with 2" of new asphalt.

### **FUNDING:**

Funding is provided from G.O. Street Bonds and Water and Sewer C.O.'s.

### **SCHEDULE:**

Construction is expected to begin February 2013 and be completed by June 2013.

**PAVEMENT REHABILITATION AND DRAINAGE IMPROVEMENTS**

**BID # 15-13**

**BID OPENING: JANUARY 3, 2013**

ITEM NO.	DESCRIPTION	EST QTY	UNIT	QUALITY EXCAVATION		CPS CIVIL, LLC.		NORTH TEXAS CONTRACTING		ADVANCED PAVING		CAMINO CONSTRUCTION, LP.		AVERAGES	
				UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT
1	MOBILIZATION/ ROW PREP	1	LS	\$35,250.00	\$35,250.00	\$45,000.00	\$45,000.00	\$13,000.00	\$13,000.00	\$28,000.00	\$28,000.00	\$120,318.00	\$120,318.00	\$48,313.60	\$48,313.60
2	UNCLASSIFIED EXCAVATION	483	CY	\$12.00	\$5,796.00	\$20.00	\$9,660.00	\$20.00	\$9,660.00	\$35.50	\$17,146.50	\$13.00	\$6,279.00	\$20.10	\$9,708.30
3	MILL, REM & DISP 2" ASPHALT	5822	SY	\$3.00	\$17,466.00	\$4.00	\$23,288.00	\$3.00	\$17,466.00	\$3.15	\$18,339.30	\$4.00	\$23,288.00	\$3.43	\$19,969.48
4	6" PRE-PULVERIZATION OF BASE	5958	SY	\$2.50	\$14,895.00	\$2.50	\$14,895.00	\$3.00	\$17,874.00	\$1.80	\$10,724.40	\$3.00	\$17,874.00	\$2.56	\$15,252.48
5	6" DEEP CEMENT APPLICATION	5958	SY	\$3.90	\$23,236.20	\$3.50	\$20,853.00	\$10.00	\$59,580.00	\$2.75	\$16,384.50	\$9.50	\$56,601.00	\$5.93	\$35,330.94
6	ROLLING AND MICRO-CRACKING	5958	SY	\$0.75	\$4,468.50	\$0.25	\$1,489.50	\$1.00	\$5,958.00	\$1.00	\$5,958.00	\$1.00	\$5,958.00	\$0.80	\$4,766.40
7	2" TYPE "D" ASPHALT	5461	SY	\$12.70	\$69,354.70	\$11.00	\$60,071.00	\$12.00	\$65,532.00	\$10.80	\$58,978.80	\$13.00	\$70,993.00	\$11.90	\$64,985.90
8	6" CLASS "C" CONC PAVEMENT	592	SY	\$70.00	\$41,440.00	\$40.00	\$23,680.00	\$80.00	\$35,520.00	\$53.25	\$31,524.00	\$47.00	\$27,824.00	\$54.05	\$31,997.60
9	6" CLASS "C" CONC CURB	390	LF	\$5.00	\$1,950.00	\$25.00	\$9,750.00	\$1.00	\$390.00	\$9.00	\$3,510.00	\$3.00	\$1,170.00	\$9.80	\$3,354.00
10	5" TYPE "D" HMAC DRIVEWAY	951	SY	\$32.00	\$30,432.00	\$32.00	\$30,432.00	\$35.00	\$33,285.00	\$23.25	\$22,110.75	\$36.00	\$34,236.00	\$31.65	\$30,099.15
11	5" CLASS "A" CONC DRIVEWAY	376	SY	\$55.00	\$20,880.00	\$45.00	\$16,920.00	\$60.00	\$22,560.00	\$52.75	\$19,834.00	\$47.00	\$17,672.00	\$51.95	\$19,533.20
12	4" CLASS "A" CONC SIDEWALK	640	SF	\$4.50	\$2,880.00	\$40.00	\$25,600.00	\$5.00	\$3,200.00	\$14.00	\$8,960.00	\$5.00	\$3,200.00	\$13.70	\$8,768.00
13	REM & DISP CONC DRIVEWAY	414	SY	\$18.50	\$7,659.00	\$10.00	\$4,140.00	\$15.00	\$6,210.00	\$15.70	\$6,499.80	\$8.00	\$3,312.00	\$13.44	\$5,564.16
14	PORTLAND CEMENT	134	TON	\$187.00	\$25,058.00	\$200.00	\$26,800.00	\$125.00	\$16,750.00	\$175.00	\$23,450.00	\$175.00	\$23,450.00	\$172.40	\$23,101.60
15	FULL DEPTH SAWCUT	310	LF	\$3.50	\$1,085.00	\$5.00	\$1,550.00	\$6.00	\$1,860.00	\$1.80	\$558.00	\$3.00	\$930.00	\$3.86	\$1,196.60
16	DOWEL TO EXIST PAVEMENT	295	LF	\$7.85	\$2,315.75	\$4.00	\$1,180.00	\$6.00	\$1,770.00	\$2.00	\$590.00	\$4.00	\$1,180.00	\$4.77	\$1,407.15
17	TYPE "F" ADA RAMP	8	EA	\$1,100.00	\$8,800.00	\$1,200.00	\$9,600.00	\$1,200.00	\$9,600.00	\$1,000.00	\$8,000.00	\$900.00	\$7,200.00	\$1,080.00	\$8,640.00
18	DITCH EXCAV & SHAPING	2640	CY	\$12.00	\$31,680.00	\$25.00	\$66,000.00	\$20.00	\$52,800.00	\$26.00	\$68,640.00	\$13.00	\$34,320.00	\$19.20	\$50,688.00
19	15" CLASS III RCP	9	LF	\$120.00	\$1,080.00	\$50.00	\$450.00	\$50.00	\$450.00	\$168.00	\$1,512.00	\$38.00	\$342.00	\$85.20	\$766.80
20	21" CLASS III RCP	41	LF	\$80.00	\$3,280.00	\$55.00	\$2,255.00	\$60.00	\$2,460.00	\$135.00	\$5,535.00	\$42.00	\$1,722.00	\$74.40	\$3,050.40
21	STD 6'-0" CURB INLET	1	EA	\$4,000.00	\$4,000.00	\$2,000.00	\$2,000.00	\$2,800.00	\$2,800.00	\$5,040.00	\$5,040.00	\$4,000.00	\$4,000.00	\$3,568.00	\$3,568.00
22	STD 10'-0" CURB INLET	1	EA	\$4,000.00	\$4,000.00	\$2,500.00	\$2,500.00	\$3,300.00	\$3,300.00	\$6,160.00	\$6,160.00	\$4,500.00	\$4,500.00	\$4,092.00	\$4,092.00
23	REM & DISP CURB INLET	2	EA	\$440.00	\$880.00	\$400.00	\$800.00	\$1,000.00	\$2,000.00	\$1,680.00	\$3,360.00	\$200.00	\$400.00	\$744.00	\$1,488.00
24	CONNECT EXIST 15" TO INLET	1	EA	\$800.00	\$800.00	\$500.00	\$500.00	\$800.00	\$800.00	\$1,680.00	\$1,680.00	\$250.00	\$250.00	\$806.00	\$806.00
25	CONNECT EXIST 21" TO INLET	1	EA	\$800.00	\$800.00	\$600.00	\$600.00	\$1,500.00	\$1,500.00	\$2,016.00	\$2,016.00	\$350.00	\$350.00	\$1,053.20	\$1,053.20
26	CONNECT PROPOSED 16" HDPE	1	EA	\$800.00	\$800.00	\$300.00	\$300.00	\$1,000.00	\$1,000.00	\$1,120.00	\$1,120.00	\$450.00	\$450.00	\$734.00	\$734.00
27	16" DIA GRATE INLET	37	EA	\$1,200.00	\$44,400.00	\$750.00	\$27,750.00	\$1,700.00	\$62,900.00	\$1,120.00	\$41,440.00	\$1,500.00	\$55,500.00	\$1,254.00	\$46,398.00
28	24" DIA GRATE INLET	10	EA	\$1,500.00	\$15,000.00	\$1,000.00	\$10,000.00	\$1,900.00	\$19,000.00	\$1,344.00	\$13,440.00	\$1,700.00	\$17,000.00	\$1,488.80	\$14,888.00
29	STANDARD CLEANOUT IN METER BOX	5	EA	\$700.00	\$3,500.00	\$300.00	\$1,500.00	\$2,500.00	\$12,500.00	\$896.00	\$4,480.00	\$600.00	\$3,000.00	\$999.20	\$4,996.00
30	REM & DISP EXIST 18" SAFETY HDWL	1	EA	\$400.00	\$400.00	\$400.00	\$400.00	\$500.00	\$500.00	\$896.00	\$896.00	\$300.00	\$300.00	\$499.20	\$499.20
31	REM & DISP EXIST 30" SAFETY HDWL	1	EA	\$400.00	\$400.00	\$400.00	\$400.00	\$1,000.00	\$1,000.00	\$896.00	\$896.00	\$450.00	\$450.00	\$629.20	\$629.20
32	INSTALL 2' X 2' "Y" INLET	1	EA	\$2,300.00	\$2,300.00	\$1,950.00	\$1,950.00	\$2,500.00	\$2,500.00	\$1,680.00	\$1,680.00	\$2,100.00	\$2,100.00	\$2,106.00	\$2,106.00

**PAVEMENT REHABILITATION AND DRAINAGE IMPROVEMENTS  
 BID # 15-13  
 BID OPENING: JANUARY 3, 2013**

ITEM NO.	DESCRIPTION	EST QTY	UNIT	QUALITY EXCAVATION		CPS CIVIL, LLC.		NORTH TEXAS CONTRACTING		ADVANCED PAVING		CAMINO CONSTRUCTION, LP.		AVERAGES	
				UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT
33	INSTALL 3' X 3' "Y" INLET	1	EA	\$2,500.00	\$2,500.00	\$2,600.00	\$2,600.00	\$3,000.00	\$3,000.00	\$2,240.00	\$2,240.00	\$2,700.00	\$2,700.00	\$2,608.00	\$2,608.00
34	8" HDPE PIPE	612	LF	\$22.00	\$13,464.00	\$35.00	\$21,420.00	\$43.00	\$26,316.00	\$28.00	\$17,136.00	\$28.00	\$15,912.00	\$30.80	\$18,849.60
35	10" HDPE PIPE	862	LF	\$23.00	\$19,826.00	\$40.00	\$34,480.00	\$48.00	\$39,652.00	\$40.00	\$34,480.00	\$28.00	\$24,136.00	\$35.40	\$30,514.80
36	12" HDPE PIPE	564	LF	\$27.00	\$15,228.00	\$43.00	\$24,252.00	\$48.00	\$27,072.00	\$50.00	\$28,200.00	\$30.00	\$16,920.00	\$39.80	\$22,334.40
37	15" HDPE PIPE	515	LF	\$30.00	\$15,450.00	\$45.00	\$23,175.00	\$53.00	\$27,295.00	\$54.00	\$27,810.00	\$35.00	\$18,025.00	\$43.40	\$22,351.00
38	18" HDPE PIPE	115	LF	\$36.00	\$4,140.00	\$52.00	\$5,980.00	\$58.00	\$6,670.00	\$62.00	\$7,130.00	\$39.00	\$4,485.00	\$49.40	\$5,681.00
39	21" HDPE PIPE	475	LF	\$40.00	\$19,000.00	\$55.00	\$26,125.00	\$67.00	\$31,825.00	\$73.00	\$34,675.00	\$43.00	\$20,425.00	\$55.80	\$28,410.00
40	24" HDPE PIPE	383	LF	\$40.00	\$15,320.00	\$58.00	\$22,214.00	\$69.00	\$26,427.00	\$79.00	\$30,257.00	\$46.00	\$17,618.00	\$58.40	\$22,367.20
41	8" PVC SANITARY SEWER MAIN	770	LF	\$33.00	\$25,410.00	\$40.00	\$30,800.00	\$50.00	\$38,500.00	\$73.00	\$56,210.00	\$44.00	\$33,880.00	\$48.00	\$36,960.00
42	STD 4'-0" DIA SANITARY MANHOLE	1	EA	\$3,500.00	\$3,500.00	\$2,600.00	\$2,600.00	\$3,000.00	\$3,000.00	\$4,592.00	\$4,592.00	\$3,300.00	\$3,300.00	\$3,398.40	\$3,398.40
43	STD SANITARY SEWER CLEANOUT	1	EA	\$1,300.00	\$1,300.00	\$200.00	\$200.00	\$600.00	\$600.00	\$580.00	\$560.00	\$700.00	\$700.00	\$672.00	\$672.00
44	CONNECT 8" SANITARY TO MANHOLE	1	EA	\$1,500.00	\$1,500.00	\$700.00	\$700.00	\$1,000.00	\$1,000.00	\$1,680.00	\$1,680.00	\$600.00	\$600.00	\$1,096.00	\$1,096.00
45	STD SANITARY SEWER SERVICE	46	EA	\$1,000.00	\$46,000.00	\$1,000.00	\$46,000.00	\$2.00	\$920.00	\$2,800.00	\$128,800.00	\$750.00	\$34,500.00	\$1,114.00	\$51,244.00
46	REM & DISP OF TREES	1	LS	\$1,700.00	\$1,700.00	\$2,000.00	\$2,000.00	\$1,000.00	\$1,000.00	\$3,000.00	\$3,000.00	\$1,800.00	\$1,800.00	\$1,900.00	\$1,900.00
47	REM & DISP OF CONG PAD	1	LS	\$2,300.00	\$2,300.00	\$860.00	\$860.00	\$500.00	\$500.00	\$750.00	\$750.00	\$1,000.00	\$1,000.00	\$1,042.00	\$1,042.00
48	BARRICADING	1	LS	\$10,500.00	\$10,500.00	\$10,000.00	\$10,000.00	\$6,000.00	\$6,000.00	\$2,500.00	\$2,500.00	\$8,000.00	\$8,000.00	\$7,400.00	\$7,400.00
49	SWPPP	1	LS	\$5,000.00	\$5,000.00	\$9,000.00	\$9,000.00	\$5,000.00	\$5,000.00	\$3,000.00	\$3,000.00	\$4,000.00	\$4,000.00	\$5,200.00	\$5,200.00
50	PROJECT SIGNAGE	3	EA	\$770.00	\$2,310.00	\$600.00	\$1,800.00	\$500.00	\$1,500.00	\$300.00	\$900.00	\$5,000.00	\$15,000.00	\$1,434.00	\$4,302.00
51	BLOCK SOD	61485	SF	\$0.55	\$33,816.75	\$0.80	\$36,891.00	\$0.30	\$18,445.50	\$0.70	\$43,039.50	\$0.60	\$36,891.00	\$0.55	\$33,816.75
52	ADJUST/REPLACE EXIST WATER SERV	67	EA	\$320.00	\$21,440.00	\$625.00	\$41,875.00	\$25.00	\$1,675.00	\$560.00	\$37,520.00	\$500.00	\$33,500.00	\$406.00	\$27,202.00
53	ADJUST EXIST WATER METER BOX	20	EA	\$250.00	\$5,000.00	\$75.00	\$1,500.00	\$100.00	\$2,000.00	\$168.00	\$3,360.00	\$100.00	\$2,000.00	\$138.60	\$2,772.00
54	REM/REP WATER METER BOX	20	EA	\$300.00	\$6,000.00	\$230.00	\$4,600.00	\$150.00	\$3,000.00	\$450.00	\$9,000.00	\$400.00	\$8,000.00	\$306.00	\$6,120.00
55	REPAIR WATER/SEWER LINE	6	EA	\$4,000.00	\$24,000.00	\$660.00	\$3,960.00	\$500.00	\$3,000.00	\$8,980.00	\$53,780.00	\$1,700.00	\$10,200.00	\$3,164.00	\$18,984.00
56	1 1/2" TYPE "D" HMAC	56	SF	\$59.00	\$3,304.00	\$2.00	\$112.00	\$3.00	\$168.00	\$4.00	\$224.00	\$12.00	\$672.00	\$16.00	\$896.00
57	8" CLASS "C" PAVEMENT	56	SF	\$42.00	\$2,352.00	\$9.00	\$504.00	\$30.00	\$1,680.00	\$7.00	\$392.00	\$9.00	\$504.00	\$19.40	\$1,086.40
58	FLOWABLE FILL	12	CY	\$185.00	\$2,220.00	\$100.00	\$1,200.00	\$150.00	\$1,800.00	\$150.00	\$1,800.00	\$70.00	\$840.00	\$131.00	\$1,572.00
59	ROCK BASE OR CRUSHED CONCRETE	50	CY	\$64.00	\$3,200.00	\$35.00	\$1,750.00	\$20.00	\$1,000.00	\$70.00	\$3,500.00	\$25.00	\$1,250.00	\$42.80	\$2,140.00
60	MISC CLASS "C" REINF CONCRETE	50	CY	\$211.00	\$10,550.00	\$140.00	\$7,000.00	\$75.00	\$3,750.00	\$200.00	\$10,000.00	\$240.00	\$12,000.00	\$173.20	\$8,660.00
61	STRAW WATTLE WITH "Z" WIRE MESH	100	LF	\$15.00	\$1,500.00	\$3.00	\$300.00	\$6.00	\$600.00	\$6.50	\$650.00	\$10.00	\$1,000.00	\$8.10	\$810.00
62	IMPORTED SELECT FILL	50	CY	\$80.00	\$4,000.00	\$20.00	\$1,000.00	\$20.00	\$1,000.00	\$0.00	\$0.00	\$15.00	\$750.00	\$27.00	\$1,350.00
63	CONSTRUCTION CONTINGENCY	1	LS	\$40,000.00	\$40,000.00	\$40,000.00	\$40,000.00	\$40,000.00	\$40,000.00	\$40,000.00	\$40,000.00	\$40,000.00	\$40,000.00	\$40,000.00	\$40,000.00
<b>TOTAL BASE BID CONTRACTOR'S BID</b>					\$787,916.90 SAME		\$847,011.50 SAME		\$810,120.50 SAME	NO ADDENDUM	\$1,025,628.55 SAME		\$916,777.00 SAME		\$877,490.89

ENGINEERS ESTIMATE FOR BASE BID:  
\$549,000

CERTIFIED BY:



Steve Spanos, P.E., Director of Engineering



**HUFFHINES, WISTA VISTA, & PITTMAN  
FALL 2011**





## MEMO

**DATE:** January 22, 2013

**TO:** Kent Pfeil – Director of Finance

**FROM:** Pam Kirkland – Purchasing Manager 

**SUBJECT:** Award of Bid #19-13 for the Cottonwood Heights Bridge and Culvert Railing to Jeske Construction Company in the amount of \$505,222

**Proposed Date of Award: January 28, 2013**

I concur with the recommendation of Steve Spanos – Director of Engineering, and request permission to award a contract to the low bidder, Jeske Construction Company, for the above referenced construction in the amount of \$505,222, as outlined in the attached memo.

Funding is provided from Neighborhood Vitality G.O. Bonds.

The bid was advertised in *The Dallas Morning News* on December 12 & 19, 2012 and was posted on Bidsync.com. A prebid conference was held on December 20, 2012 and four bids were received.

Concur:

  
Kent Pfeil

Attachments

Xc: Dan Johnson  
David Morgan  
Cliff Miller  
Don Magner  
Shanna Sims-Bradish



## MEMO

TO: Dan Johnson, City Manager  
THROUGH: Cliff Miller, Assistant City Manager *CM*  
FROM: Steve Spanos, P.E., Director of Engineering *SS*  
SUBJECT: Award of Bid No. 19-13 for Cottonwood Heights Bridge and Culvert Railing  
Jeske Construction Co.  
DATE: January 18, 2013

### ACTION REQUESTED:

Council to consider award of Bid No. 19-13 to Jeske Construction Co., for the Cottonwood Heights Bridge and Culvert Railing in the amount of \$505,222.

### BACKGROUND INFORMATION:

On January 10, 2013 the Capital Projects Department opened bids for the subject project. The attached bid tabulation certifies the lowest bid was submitted by Jeske Construction Co., in the amount of \$505,222.

Staff as well as the Finance Department have reviewed Jeske Construction Co., company financials, bonding company, the insurance company and references, and recommend awarding the Cottonwood Heights Bridge and Culvert Railing to Jeske Construction Co., in the amount of \$505,222.

The 2010 Neighborhood Vitality Bond Project Bridge Enhancements Project consists of bridge enhancements at three locations in the Cottonwood Heights Neighborhoods. The scope includes new railing, installing a stone fascia and a stainless steel feature, constructing new end columns and replacing the sidewalks at the bridges located on Dumont Drive at Cottonwood Creek, Dublin Drive at Hunt Branch, and at Weatherred Drive near Blue Lakes.

### FUNDING:

Funding is provided from Neighborhood Vitality G.O Bonds.

### SCHEDULE:

Construction is expected to begin March 2013 and be completed by July 2013.

Cc: Henry Drexel, P.E., Senior Project Engineer *HD*  
CP/Office/AR/AI-Jan2013/Cottonwoodbridge.doc

**COTTONWOOD HEIGHTS BRIDGE AND CULVERT RAILING**

BID # 19-13

BID OPENING: JANUARY 10, 2013

ITEM NO.	DESCRIPTION	EST QTY	UNIT	JESKE CONSTRUCTION CO.		JIM BOWMAN		TEXAS STANDARD CONSTRUCTION		REBCON, INC.		AVERAGES	
				UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT
1	REMOVAL OF CURB & GUTTER	261	LF	\$7.00	\$1,827.00	\$15.00	\$3,915.00	\$8.00	\$2,088.00	\$19.00	\$4,959.00	\$12.25	\$3,197.25
2	REMOVAL CONC STREET/DRWY	823	SY	\$11.00	\$9,053.00	\$15.00	\$12,345.00	\$17.00	\$13,991.00	\$16.00	\$13,168.00	\$14.75	\$12,139.25
3	REMOVAL OF SIDEWALK	2088	SF	\$1.00	\$2,088.00	\$1.25	\$2,610.00	\$1.75	\$3,654.00	\$2.40	\$5,011.20	\$1.80	\$3,340.80
4	REMOVAL OF STORM SEWER PIPE	70	LF	\$12.00	\$840.00	\$58.00	\$4,060.00	\$14.50	\$1,015.00	\$220.00	\$15,400.00	\$76.13	\$5,328.75
5	REMOVAL 10' INLET	7	EA	\$2,800.00	\$18,200.00	\$2,600.00	\$18,200.00	\$2,250.00	\$15,750.00	\$4,700.00	\$32,900.00	\$3,037.50	\$21,262.50
6	REMOVAL OF VARIOUS SIZE INLET	6	EA	\$300.00	\$1,800.00	\$850.00	\$5,100.00	\$800.00	\$3,600.00	\$1,500.00	\$9,000.00	\$812.50	\$4,875.00
7	REM/REPL 4' CHAIN FENCE	6	LF	\$30.00	\$180.00	\$80.00	\$360.00	\$60.00	\$360.00	\$37.00	\$222.00	\$46.75	\$280.50
8	REM/REINSTALL STEEL FENCE	15	LF	\$30.00	\$450.00	\$60.00	\$900.00	\$85.00	\$1,275.00	\$33.00	\$495.00	\$52.00	\$780.00
9	REMOVE 6' STEEL FENCE	32	LF	\$5.00	\$160.00	\$15.00	\$480.00	\$10.00	\$320.00	\$17.00	\$544.00	\$11.75	\$376.00
10	REMOVE EXISTING PED RAIL	210	LF	\$12.00	\$2,520.00	\$10.00	\$2,100.00	\$7.50	\$1,575.00	\$13.00	\$2,730.00	\$10.83	\$2,231.25
11	REM NORTH BRIDGE RAIL	91	LF	\$18.00	\$1,638.00	\$23.00	\$2,093.00	\$30.00	\$2,730.00	\$7.00	\$637.00	\$19.50	\$1,774.50
12	REMOVE SOUTH BRIDGE RAIL	91	LF	\$12.00	\$1,092.00	\$12.00	\$1,092.00	\$30.00	\$2,730.00	\$7.00	\$637.00	\$15.25	\$1,387.75
13	REMOVE CONC FLUME	8	SY	\$15.00	\$120.00	\$27.00	\$216.00	\$30.00	\$240.00	\$45.00	\$360.00	\$29.25	\$234.00
14	REMOVE EXIST CULVERT HEADWALL	382	SF	\$6.00	\$2,292.00	\$21.00	\$8,022.00	\$30.00	\$11,460.00	\$85.00	\$32,470.00	\$35.50	\$13,561.00
15	REMOVE CONCE WINGWALL	1	EA	\$1,200.00	\$1,200.00	\$6,000.00	\$6,000.00	\$8,500.00	\$8,500.00	\$35,000.00	\$35,000.00	\$12,675.00	\$12,675.00
16	4" TOPSOIL	239	SY	\$2.00	\$478.00	\$4.00	\$956.00	\$9.00	\$2,151.00	\$4.00	\$956.00	\$4.75	\$1,135.25
17	DELIVERED FILL	50	CY	\$16.00	\$800.00	\$25.00	\$1,250.00	\$45.00	\$2,250.00	\$25.00	\$1,250.00	\$27.75	\$1,387.50
18	RECOMPACT STREET SUBGRADE	729	SY	\$3.00	\$2,187.00	\$2.00	\$1,458.00	\$9.00	\$6,561.00	\$12.00	\$8,748.00	\$6.50	\$4,738.50
19	SODDING	239	SY	\$6.00	\$1,434.00	\$5.00	\$1,195.00	\$3.00	\$717.00	\$6.00	\$1,434.00	\$5.00	\$1,195.00
20	CONSTRUCT SIDEWALK	3710	SF	\$6.00	\$22,260.00	\$5.00	\$18,550.00	\$5.20	\$19,292.00	\$10.00	\$37,100.00	\$6.55	\$24,300.50
21	CONSTRUCT DRIVEWAY	52	SY	\$53.00	\$2,756.00	\$50.00	\$2,600.00	\$59.00	\$3,068.00	\$130.00	\$6,760.00	\$73.00	\$3,796.00
22	CONSTRUCT VARIABLE HEIGHT CURB	71	LF	\$31.00	\$2,201.00	\$30.00	\$2,130.00	\$30.00	\$2,130.00	\$55.00	\$3,905.00	\$36.50	\$2,591.50
23	CONSTRUCT 11" CURB	155	LF	\$38.00	\$5,890.00	\$35.00	\$5,425.00	\$37.00	\$5,735.00	\$55.00	\$8,525.00	\$41.25	\$6,393.75
24	CONSTRUCT 8" STREET PAVEMENT	575	SY	\$53.00	\$30,475.00	\$57.65	\$33,148.75	\$49.00	\$28,175.00	\$117.00	\$67,275.00	\$69.16	\$39,768.44
25	CONSTRUCT 8" APPROACH SLAB	102	SY	\$82.00	\$8,364.00	\$108.00	\$11,016.00	\$75.00	\$7,650.00	\$235.00	\$23,970.00	\$125.00	\$12,750.00
26	CONSTRUCT BUTT JOINT	114	LF	\$7.00	\$798.00	\$4.50	\$513.00	\$19.00	\$2,166.00	\$18.00	\$2,052.00	\$12.13	\$1,382.25
27	21" RCP	29	LF	\$105.00	\$3,045.00	\$135.00	\$3,915.00	\$83.00	\$1,827.00	\$125.00	\$3,625.00	\$107.00	\$3,103.00
28	30" RCP	34	LF	\$120.00	\$4,080.00	\$150.00	\$5,100.00	\$75.00	\$2,550.00	\$127.00	\$4,318.00	\$118.00	\$4,012.00
29	CONSTR 10' STANDARD CURB INLET	2	EA	\$3,700.00	\$7,400.00	\$2,915.00	\$5,830.00	\$3,400.00	\$6,800.00	\$4,000.00	\$8,000.00	\$3,503.75	\$7,007.50
30	CONSTR 20' STANDARD CURB INLET	2	EA	\$5,900.00	\$11,800.00	\$5,170.00	\$10,340.00	\$6,700.00	\$13,400.00	\$6,000.00	\$12,000.00	\$5,942.50	\$11,885.00
31	INSTALL CONCRETE PIPE COLLAR	3	EA	\$400.00	\$1,200.00	\$300.00	\$900.00	\$300.00	\$900.00	\$400.00	\$1,200.00	\$350.00	\$1,050.00

**COTTONWOOD HEIGHTS BRIDGE AND CULVERT RAILING**

BID # 19-13

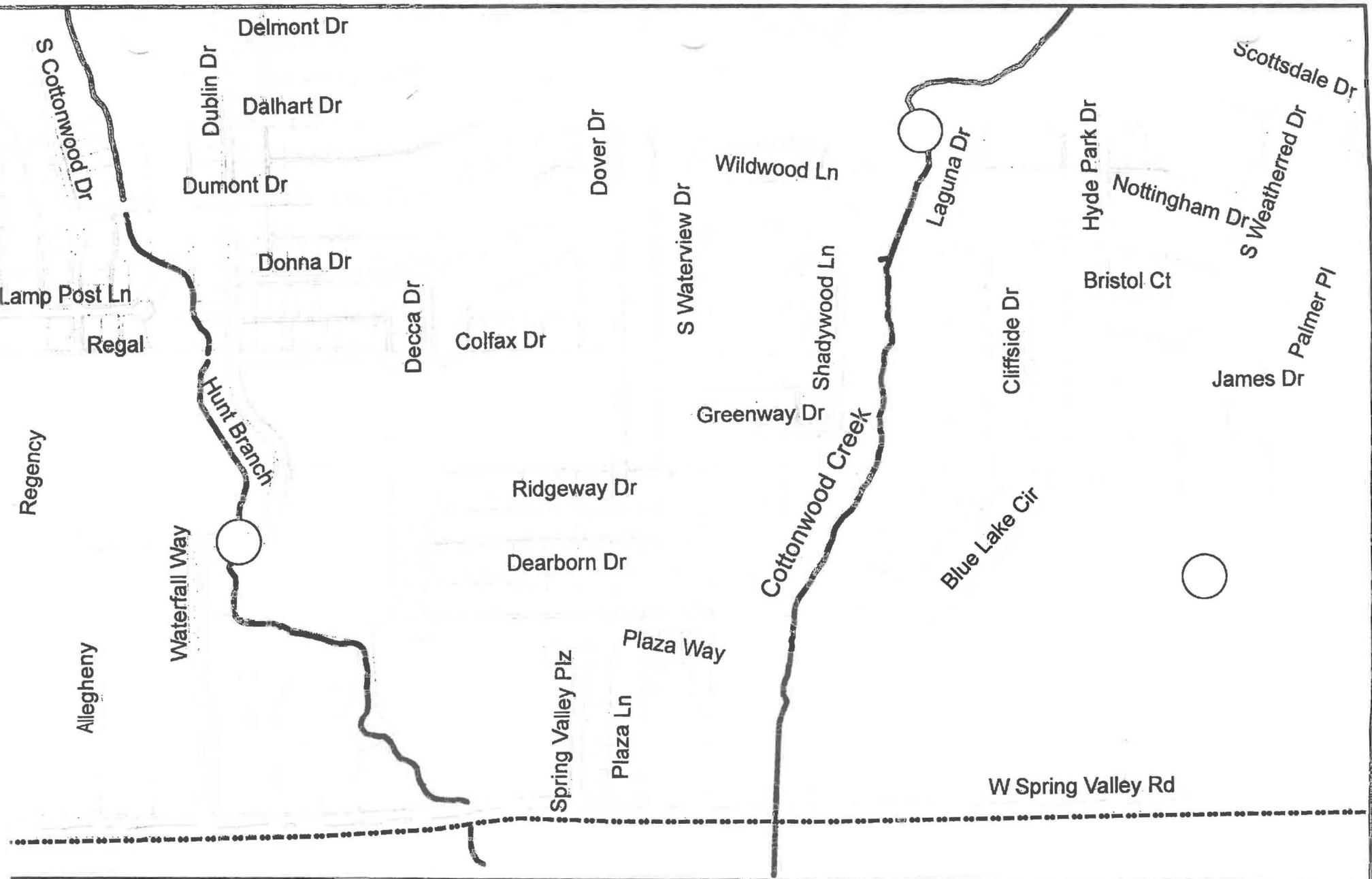
BID OPENING: JANUARY 10, 2013

ITEM NO.	DESCRIPTION	EST QTY	UNIT	JESKE CONSTRUCTION CO.		JIM BOWMAN		TEXAS STANDARD CONSTRUCTION		REBCON, INC.		AVERAGES	
				UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT	UNIT PRICE	AMOUNT
32	INSTALL PIPE PLUG	2	EA	\$800.00	\$1,200.00	\$250.00	\$500.00	\$475.00	\$950.00	\$350.00	\$700.00	\$418.75	\$837.50
33	CONSTR CONC CLOSURE WALL	1.5	CY	\$1,400.00	\$2,100.00	\$1,720.00	\$2,580.00	\$1,000.00	\$1,500.00	\$1,300.00	\$1,950.00	\$1,355.00	\$2,032.50
34	CONSTR RETAINING WALL	1.3	CY	\$1,500.00	\$1,950.00	\$1,925.00	\$2,502.50	\$2,000.00	\$2,600.00	\$1,500.00	\$1,950.00	\$1,731.25	\$2,250.63
35	CONSTR WINGWALL	19	CY	\$1,200.00	\$22,800.00	\$770.00	\$14,630.00	\$910.00	\$17,290.00	\$1,200.00	\$22,800.00	\$1,020.00	\$19,380.00
36	TRENCH SAFETY	1	LS	\$1,600.00	\$1,600.00	\$700.00	\$700.00	\$800.00	\$800.00	\$550.00	\$550.00	\$912.50	\$912.50
37	TRENCH SHORING	63	LF	\$8.00	\$504.00	\$10.00	\$630.00	\$30.00	\$1,890.00	\$8.00	\$504.00	\$14.00	\$882.00
38	INSTALL STEEL HANDRAIL	301	LF	\$80.00	\$24,080.00	\$66.00	\$19,866.00	\$210.00	\$63,210.00	\$70.00	\$21,070.00	\$106.50	\$32,056.50
39	INSTALL LIMESTONE CLADDING	1568	SF	\$40.00	\$62,720.00	\$21.50	\$33,712.00	\$22.00	\$34,496.00	\$15.00	\$23,520.00	\$24.63	\$38,612.00
40	INSTALL SS MONUMENT	444	SF	\$130.00	\$57,720.00	\$112.00	\$49,728.00	\$200.00	\$88,800.00	\$113.00	\$50,172.00	\$138.75	\$61,605.00
41	INSTALL CAST STONE MONUMENT	200	CY	\$93.00	\$18,600.00	\$92.00	\$18,400.00	\$80.00	\$16,000.00	\$121.00	\$24,200.00	\$96.50	\$19,300.00
42	CONSTR CONC RAIL POST/WALL	40	CY	\$800.00	\$32,000.00	\$1,600.00	\$64,000.00	\$1,700.00	\$68,000.00	\$1,700.00	\$68,000.00	\$1,450.00	\$58,000.00
43	CONSTR BRIDGE OVERLAY SIDEWALK	25	CY	\$350.00	\$8,750.00	\$725.00	\$18,125.00	\$920.00	\$23,000.00	\$750.00	\$18,750.00	\$686.25	\$17,156.25
44	CONSTR CONC RAIL / MONUMENT SUPP	29	CY	\$950.00	\$27,550.00	\$1,700.00	\$49,300.00	\$2,250.00	\$65,250.00	\$2,200.00	\$63,800.00	\$1,775.00	\$51,475.00
45	INSTALL 6' STEEL FENCE	32	LF	\$170.00	\$5,440.00	\$50.00	\$1,600.00	\$150.00	\$4,800.00	\$50.00	\$1,600.00	\$105.00	\$3,360.00
46	REPAINT 6' FENCE	177	LF	\$40.00	\$7,080.00	\$28.00	\$4,956.00	\$26.50	\$4,690.50	\$12.00	\$2,124.00	\$26.63	\$4,712.63
47	SWPPP	1	LS	\$3,500.00	\$3,500.00	\$2,000.00	\$2,000.00	\$5,000.00	\$5,000.00	\$19,000.00	\$19,000.00	\$7,375.00	\$7,375.00
48	BARRICADING	1	LS	\$13,000.00	\$13,000.00	\$12,500.00	\$12,500.00	\$27,500.00	\$27,500.00	\$35,000.00	\$35,000.00	\$22,000.00	\$22,000.00
49	PROJECT SIGNAGE	3	EA	\$700.00	\$2,100.00	\$500.00	\$1,500.00	\$375.00	\$1,125.00	\$500.00	\$1,500.00	\$518.75	\$1,556.25
50	MOBILIZATION	1	LS	\$43,900.00	\$43,900.00	\$57,000.00	\$57,000.00	\$20,000.00	\$20,000.00	\$32,000.00	\$32,000.00	\$38,225.00	\$38,225.00
51	CONSTRUCTION CONTINGENCY	1	LS	\$20,000.00	\$20,000.00	\$20,000.00	\$20,000.00	\$20,000.00	\$20,000.00	\$20,000.00	\$20,000.00	\$20,000.00	\$20,000.00
<b>TOTAL BASE BID CONTRACTOR'S BID</b>					\$505,222.00 SAME		\$546,049.25 SAME		\$641,561.50 SAME		\$753,841.20 SAME		\$611,668.49

CERTIFIED BY:

*Steve Spanos*  
Steve Spanos, P.E., Director of Engineering

ENGINEERS ESTIMATE FOR BASE BID:  
\$440,000



**Bridge Rail Enhancement**  
**Cottonwood Heights Bridges**



12-10-12



## MEMO

**DATE:** January 22, 2013

**TO:** Kent Pfeil – Director of Finance

**FROM:** Pam Kirkland – Purchasing Manager

**SUBJECT:** Award of Bid #30-13 for the cooperative annual requirements contract for the 2013 Trenchless Sewer Rehabilitation Project to Insituform Technologies, Inc. pursuant to unit prices bid through the Local Government Purchasing Cooperative (Buyboard) Contract #354-10

**Proposed Date of Award: January 28, 2013**

I concur with the recommendation of Jerry Ortega – Director of Public Services, and request permission to issue a purchase order to Insituform Technologies, Inc. for the above referenced contract in an amount not to exceed \$400,000, as per the project description and unit prices in the attached quotation and contained within Contract #354-10, if needed. Insituform Technologies, Inc. is the contract vendor for trenchless sewer repair through the Local Government Purchasing Cooperative (Buyboard) Contract #354-10.

The city is not obligated to pay for or use a minimum or maximum amount of repair services, payment will be rendered according to the amount of work that is done and pursuant to the unit prices bid.

The City of Richardson is a member of the Local Government Purchasing Cooperative through our existing interlocal agreement for cooperative purchasing pursuant to Texas Government Code, Chapter 791.025 and Texas Local Government Code, Subchapter F, Section 271.102. This agreement automatically renews annually unless either party gives prior notice of termination.

Funding is provided from 2011 and 2013 Water & Sewer C.O.'s.

Concur:

  
\_\_\_\_\_  
Kent Pfeil

### Attachments

Xc: Dan Johnson  
David Morgan  
Cliff Miller  
Don Magner  
Shanna Sims-Bradish



## MEMO

TO: Dan Johnson, City Manager

THROUGH: Don Magner, Assistant City Manager

FROM: Jerry Ortega, Director of Public Services  
Joe Travers, Assistant Director of Public Services

SUBJECT: Award of Contract – Bid #30-13  
Insituform Technologies, Inc. - 2013 Trenchless Sewer Rehabilitation Project

DATE: January 18, 2013

### ACTION REQUESTED

Council to consider award of a contract to Insituform Technologies, Inc., for the 2013 Trenchless Sewer Rehabilitation Project in an amount not to exceed \$400,000.

### BACKGROUND INFORMATION

Insituform Technologies, Inc., uses a "trenchless" sewer pipe rehabilitation method. This method "re-lines" and "reinforces" the sewer pipe and makes it water-tight. This is done to adhere to TCEQ's requirement to minimize or eliminate inflow and infiltration of waste water. The process is intended to extend the life of the pipe for at least 30 years.

Insituform Technologies is considered for this construction method through the Texas Local Government Statewide Purchasing Cooperative Contract #354-10 administered through the Buy board.

The locations for this capital improvement in-line reconstruction are the 300 Block of Centennial, 100 Block of Hyde Park, and 800 Block of W. Belt Line Road. Manhole rehab work will address various manholes within the Cottonwood and Floyd Branch drainage basins. The base proposal for this work is \$398,528.50. Pursuant to Buy Board pricing, a contingency amount of \$1,471.50 is included for other unforeseen manhole rehab areas, for a total award of \$400,000.

### FUNDING

Funding for the 2013 Trenchless Sewer Rehabilitation Project will be provided from 2011 and 2013 Water & Sewer C.O.'s.

### SCHEDULE:

This construction contract is expected to be complete by April 1, 2013.



1103 Postwood Dr.  
Corinth, TX 76210  
www.insituform.com

Name: Tim Peterie  
Phone: 214-317-0950

Fax: 940-498-0265  
Email: tpeterie@insituform.com

September 21, 2012

Mr. Joe Travers  
Assistant Director Public Services – Field Operations  
City of Richardson  
411 W. Arapaho Rd.  
Richardson, TX US 75080-4551

## Proposal

Project Name: **City of Richardson, TX  
'12 In-Line Sanitary Sewer & Manhole Rehabilitation**

INSITUFORM TECHNOLOGIES, LLC herein proposes to furnish all labor, materials, equipment, and services necessary to reconstruct the referenced project (as detailed in the project location maps presented by the City of Richardson) utilizing the Texas Statewide Cooperative Purchasing Contract #354-10 administered through the BuyBoard.

### ASSUMPTIONS AND QUALIFICATIONS

Insituform™ Design. We have based this proposal on a nominal wall thickness for the Insitubest™, which is based on the best available information at the time of this proposal. Existing pipe deterioration in excess of the conditions assumed, ground water loads in excess of those assumed, or other loads or conditions may increase the recommended thickness for all or portions of the work. Final recommendations may be submitted to you following the completion of the preliminary TV phase of the project. Stated prices are subject to adjustment if design changes are agreed upon.

Laterals. During TV inspection all side sewers are verified, using best practical efforts, to determine if each is an active hook up. Normal practice only reinstates those that are active. You may direct us to reinstate all, or specific laterals, as you desire. This proposal, unless otherwise stated, assumes that all laterals will be reconnected, and all will be internally reconnected using the Insitucutter™. Specific service connections will not be reconnected until written directions are received from the Owner. The Owner will indemnify and hold INSITUFORM TECHNOLOGIES, LLC harmless from all claims arising from backups and other effects of such actions or inaction's.

The pricing in this proposal assumes that all Technical Specifications set forth by the BuyBoard will be strictly adhered to.

INSITUFORM TECHNOLOGIES, LLC will supply the City of Richardson the necessary Maintenance, Payment, and Performance Bonds as required by the City, following acceptance of this proposal.

- Special:**
1. Pricing for point repairs is based on a length of 6 feet. Any point repairs found to be necessary for lengths over 6 feet will be addressed on an as-needed basis and priced accordingly.
  2. Richardson to allow looped soft bypass during the day of installation.
  3. Water shall be provided at no cost to Insituform Technologies, LLC for all construction phases of this project. Insituform Technologies, LLC will follow all required deposit, backflow prevention, and metering procedures.
  4. A price is included to expose MH811. If Richardson exposes the manhole prior to lining, then this item will not be charged.
  5. Insituform estimates that this project should be completed 90 days following the Notice to Proceed from the City of Richardson.

### PROPOSAL TERMS AND CONDITIONS

Terms and Conditions from the Texas Statewide Cooperative Purchasing Contract are available upon request from the BuyBoard. Any changes to these conditions must be noted and agreed upon by both parties.

**PROPOSAL PRICING**

**Richardson Defined Scope '12 In-Line Rehab**

Description	Quantity	Unit of Measure	BuyBoard Price	Adjusted Price	Extended Total
10" x 6.0mm CIPP	1,872	LF	\$32.00	\$32.00	\$59,904.00
12" x 6.0mm CIPP	645	LF	\$43.00	\$43.00	\$27,735.00
21" x 9.0mm CIPP	550	LF	\$84.00	\$84.00	\$46,200.00
24" x 10.5mm CIPP	1,940	LF	\$102.00	\$102.00	\$85,680.00
21" & 24" Additional 1.5mm	1,940	LF	\$16.00	\$9.00	\$17,460.00
10" Clean and TV sanitary sewer	1,872	LF	\$6.00	\$4.00	\$7,488.00
12" Clean and TV sanitary sewer	645	LF	\$6.00	\$4.00	\$2,580.00
21" Clean and TV sanitary sewer	550	LF	\$13.00	\$10.00	\$5,500.00
24" Clean and TV sanitary sewer	840	LF	\$14.00	\$10.00	\$8,400.00
Internal reconnects	9	EA	\$275.00	\$250.00	\$2,250.00
Set up 4" pump (per pump)	7	EA	\$500.00	\$350.00	\$2,450.00
Set up 4" piping	2,517	LF	\$5.00	\$2.50	\$6,292.50
Operate 4" pumping system	7	Day	\$100.00	\$100.00	\$700.00
Set up 6" pump (per pump)	4	EA	\$1,250.00	\$1,250.00	\$5,000.00
Set up 6" piping	1,390	LF	\$15.00	\$10.00	\$13,900.00
Operate 6" pumping system	4	Day	\$950.00	\$950.00	\$3,800.00
10'-12" Point repair (0'-8' deep)	2	EA	\$3,000.00	\$3,000.00	\$6,000.00
10'-12" Point repair (8'-12' deep)	1	EA	\$3,500.00	\$3,500.00	\$3,500.00
21"-24" Point repair (8'-12' deep)	1	EA	\$3,500.00	\$3,500.00	\$3,500.00
Install new 4' DIA manhole 0'-6' deep	1	EA	\$5,000.00	\$5,000.00	\$5,000.00
Extra depth 4' DIA manhole over 6'	7	VF	\$450.00	\$450.00	\$3,150.00
Premium manhole frame/cover (30")	1	EA	\$1,000.00	\$1,000.00	\$1,000.00
Trench safety	60	LF	\$10.00	\$10.00	\$600.00
Repair/Rehab 8" concrete pavement	60	SY	\$150.00	\$150.00	\$9,000.00
Repair/Rehab 8" flex base	60	SY	\$50.00	\$50.00	\$3,000.00
Traffic control	10	Day	\$1,000.00	\$850.00	\$8,500.00
Manhole rehabilitation -- modified polymer Spectrashield (30 manholes)	3,467	SF	\$21.00	\$17.00	\$58,939.00
Expose buried manhole (MH811)	1	EA	N/A	\$1,000.00	\$1,000.00
<b>TOTAL</b>					<b>\$398,528.50</b>

Note: It is understood that the point repairs and associated items may not be necessary until verified during the pre-tv/clean phase. These items will only be billed if utilized.

**OFFERED BY**

**INSITUFORM TECHNOLOGIES, LLC**



**TIMOTHY R. PETERIE  
BUSINESS DEVELOPMENT MANAGER**

**REVIEWED BY:  
ANDY OZMENT  
AREA MANAGER**

cc: Donnie Davis (City of Richardson)  
Josh Awalt  
Ben Hawkins



P.O. Box 400  
Austin, TX 78767-0400  
800.695.2919 | 512.467.0222 | Fax: 800.211.5454  
buyboard.com

September 10, 2012

Sent via Email to:tpeterie@insituform.com

Tim Peterie  
Insituform Technologies, LLC  
17988 Edison Avenue  
Chesterfield MO 63005

Re: Cured In Place Pipe for Pipeline Rehabilitation (CIPP), Contract 354-10

The Cured In Place Pipe for Pipeline Rehabilitation (CIPP) Proposal, # 354-10, submitted by your company will expire September 30, 2012. At this time, we are extending this proposal, in accordance with proposal documents, through September 30, 2013. This will be the final extension of this contract.

All prices, discounts, terms, and conditions will remain the same. If you agree with the extension, there is nothing you need to do. However, if you do not approve of this extension, you must notify me immediately via email at [connie.burkett@tasb.org](mailto:connie.burkett@tasb.org).

If you have questions or comments concerning this extension, please contact me as soon as possible at [connie.burkett@tasb.org](mailto:connie.burkett@tasb.org). We appreciate your interest and participation in The Local Government Purchasing Cooperative.

Sincerely,

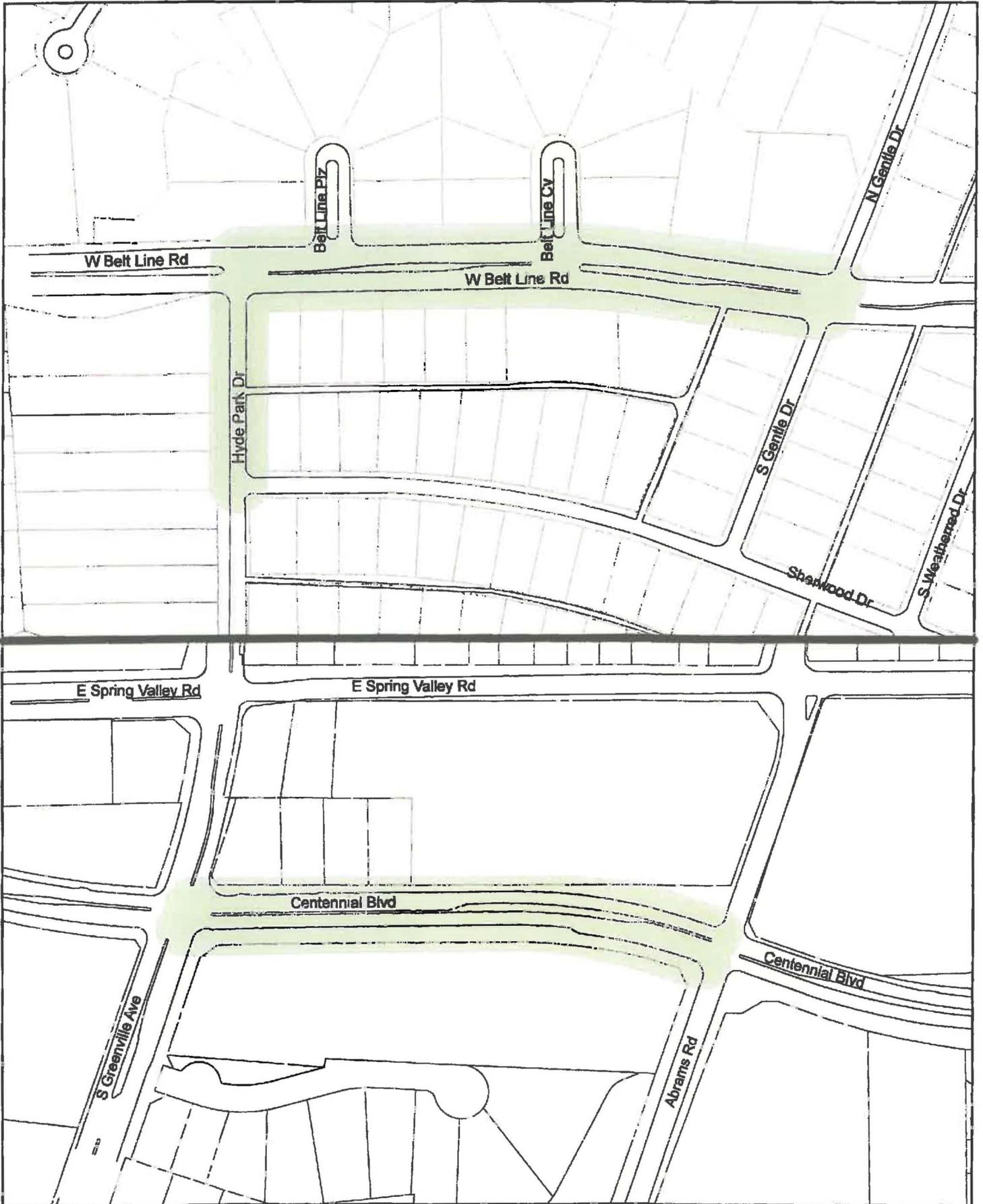
Connie W Burkett  
Contract Administrator



The Local Government Purchasing Cooperative is endorsed by the Texas Association of School Boards, Texas Municipal League, Texas Association of Counties, and the Texas Association of School Administrators.

**12-13 In Line Reconstruction**

Project Number	Alley Street	Street Name	From	To	Location	Drainage Basin	Priority	Break Down	Unit	Total	Pipe Size	CST FCT	Cost
561098258	Easement	300 Centennial	Centennial	Plant	Easement	Flyd/Branch	1	CIPP	LNFT	539/837	21"/24"	\$110	\$150,150
								CL/TV	LNFT	1365	21"	\$13	\$17,745
								Manhole Install	EACH				\$0
								R/R Concrete Pavement	SQ YDS				\$0
								R/R Flex Base	SQ YDS	20		\$150	\$3,000
								Bypass Setup	EACH	1	8"	\$37,650	\$37,650
								Bypass Operation Services	DAY	2		\$950	\$1,900
								Excavation Spot Repair	EACH	2		\$3,000	\$6,000
561098269	Street	100 Hyde Park	Beltline	Downing	Street	Cottonwood	2	CIPP	LNFT	624	10"	\$32	\$19,968
								CL/TV	LNFT	624	10"	\$6	\$3,744
								Manhole Install	EACH				\$0
								R/R Concrete Pavement	SQ YDS				\$0
								R/R Flex Base	SQ YDS	20		\$150	\$3,000
								Bypass Setup	EACH	1		\$3,620	\$3,620
								Bypass Operation Services	EACH	1		\$100	\$100
								Excavation Spot Repair	EACH	9		\$275	\$2,475
561098267	Street	800 W. Beltline	Hyde Park	Weatherred	Street	Cottonwood	3	CIPP	LNFT	1800	10"	\$32	\$57,600
								CL/TV	LNFT	1800	10"	\$6	\$10,800
								Manhole Install	EACH				\$0
								R/R Concrete Pavement	SQ YDS				\$0
								R/R Flex Base	SQ YDS	20		\$150	\$3,000
								Bypass Setup	EACH	1		\$9,500	\$9,500
								Bypass Operation Services	EACH	2		\$100	\$200
								Excavation Spot Repair	EACH	2		\$3,000	\$6,000
56111256	Various					Cottonwood Flyd/Branch		Manhole Rehab-Epoxy					\$0
								4' Diameter	30			\$0	
								Linear Feet Depth	246			\$0	
								Square Feet	3091			\$20	\$61,820
			246'	X	(4x3.1416)	Equals	3091.33				\$20	\$61,820	
			30	X	12.5664	Equals	377				\$20	\$7,540	
								Traffic Control	Day	5		\$1,000	\$5,000
								Trench Safety		6		\$100	\$600
								Total Estimate					\$417,412



**Trenchless Sewer Repair Project**  
**300 Blk. Centennial, 100 Blk. Hyde Park & 800 Blk. W Belt Line Rd**  
**January 2013**





# MEMO

**DATE:** January 22, 2013  
**TO:** Kent Pfeil – Director of Finance  
**FROM:** Pam Kirkland – Purchasing Manager *Pam*  
**SUBJECT:** Award of Bid #34-13 for a cooperative annual contract for traffic signal controller hardware with Traffic Highway Products, Ltd. pursuant to unit prices through the City of Fort Worth Invitation to Bid #12-0135

**Proposed Date of Award: January 28, 2013**

I concur with the recommendation of Robert Saylor – Traffic Engineering and Operations Manager, and request permission to issue an annual contract for traffic signal controller hardware with Traffic Highway Products, Ltd. pursuant to unit prices, as outlined in Mr. Saylor's attached memo.

The City of Fort Worth, acting as the lead agency, competitively bid a cooperative annual contract for the attached traffic signal controller hardware on Bid #12-0135, which was awarded by their City Council on September 26, 2012. The term of the contract is twelve (12) months beginning on September 26, 2012 through September 25, 2013 with options to renew up to four (4) additional twelve month periods, if agreeable to both parties.

The award of this contract allows the city to purchase traffic signal controller hardware as the requirements and needs of the city arise on an annual basis and during any subsequent renewal period(s). Since the city is not obligated to pay for or use a minimum or maximum amount of product, payment will be rendered pursuant to the unit prices bid.

The City of Richardson and the City of Fort Worth have an existing interlocal agreement for cooperative purchasing as provided by Texas Government Code, Chapter 791.025 and Texas Local Government Code, Subchapter F, Section 271.102. This agreement automatically renews annually unless either party gives prior notice of termination.

Concur:

*Kent Pfeil*  
\_\_\_\_\_  
Kent Pfeil

Attachments

Xc: Dan Johnson  
David Morgan  
Cliff Miller  
Don Magner  
Shanna Sims-Braddish



## MEMO

TO: Pam Kirkland, Director of Purchasing  
FROM: Robert Saylor, P.E., PTOE – Traffic Engineering and Operations Manager  
DATE: January 10, 2013

SUBJECT: Annual Contract for Traffic Signal Controller Hardware

I recommend that the City award a contract for Traffic Signal Controller Hardware to Traffic Highway Products, Ltd. Through a piggy-back on the recently awarded City of Fort Worth contract. I recommend that this be awarded as an annual contract with options to renew for four (4) additional years.

Expenditures are estimated to be \$490,000.00 in the first year, then significantly less annually after that and will come from a number of Bond Fund and General Fund accounts, however, most of the activity will be paid from Bond Account 378-8702-585-75-24 SD1027, and General Fund Account 011-2071-531-4331.

Thanks,  
Robert Saylor, P.E., PTOE  
Traffic Engineering and Operations Manager  
City of Richardson  
(972) 744-4324

# CITY COUNCIL AGENDA




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## COUNCIL ACTION: Approved on 9/25/2012

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<b>DATE:</b>	9/25/2012	<b>REFERENCE NO.:</b>	P-11435	<b>LOG NAME:</b>	13P12-0135 ATC LSJ
<b>CODE:</b>	P	<b>TYPE:</b>	NON-CONSENT	<b>PUBLIC HEARING:</b>	NO
<b>SUBJECT:</b>	Authorize Purchase Agreement with Texas Highway Products, LTD, for Advanced Traffic Controllers for the Transportation and Public Works Department in the Amount Up to \$1,459,917.00 for the First Year (ALL COUNCIL DISTRICTS)				

---

### RECOMMENDATION:

It is recommended that the City Council authorize purchase Agreement with Texas Highway Products, LTD, for advanced traffic controllers for the Transportation and Public Works Department in the amount up to \$1,459,917.00 for the first year.

### DISCUSSION:

The Transportation and Public Works Department (TPW) will use this Agreement to purchase traffic controllers that operate signalized intersections around the City. The controllers will be installed at intersections for projects involving new traffic signals or equipment modernization efforts. The controllers include new features to operate intersections more efficiently, control complex intersections and provide better methods to connect to the central Traffic Management Center.

Staff collaborated with the cities of Dallas and Richardson to evaluate the latest transportation industry specifications for these new modern controller units. As a result of this evaluation, the cities specified standardized equipment with modern processors and ample memory and also specified additional USB connectors which exceed industry standards so the controller would better meet long term needs. Staff collaboration resulted in a Request for Proposals that meets the needs of all three cities and will lead to the development of a cooperative contract. The cooperative effort of the three cities improves the purchasing power of all cities, and the cities of Dallas and Richardson will procure modern controller units using Fort Worth's cooperative purchasing Agreement.

The Invitation to Bid (ITB) consisted of detailed specifications describing requirements, quantities, materials and workmanship for traffic signal controllers to be supplied to the City on an as-needed basis. The ITB also required the equipment to be in compliance with the Advanced Transportation Controller (ATC) Standard Version 5.2b published jointly by the American Association of State Highways and Transportation Officials (AASHTO), the Institute on Transportation Engineers (ITE) and the National Electrical Manufacturers Association (NEMA). Staff certified that the recommended bid meets specifications.

**COOPERATIVE PURCHASING** - Governmental entities utilizing Inter-Governmental Contracts with the City of Fort Worth will be eligible, but not obligated, to purchase material/services under the Agreement awarded as a result of this solicitation. All purchases by governmental entities other than the City of Fort Worth will be billed directly to that governmental entity and paid by that governmental entity. The City of Fort Worth will not be responsible for another governmental entity's debts. Each governmental entity will order its own material and services as needed.

**PRICE ANALYSIS** - The City has no recent purchase history for this type of equipment. No guarantee has been made for a specific amount of goods or services to be purchased. The department

anticipates spending in the amount up to \$1,459,917.00 for the traffic signal controllers for the first year, \$496,000.00 of that amount will be deployed using capital improvement project and grant funds.

BID ADVERTISEMENT - This bid was advertised in the Fort Worth Star-Telegram on March 28, 2012 and April 4, 2012. Sixty-six vendors from the purchasing database system were notified of the bid, two bids were received.

BID TABULATION - See attached bid tabulation.

M/WBE - A waiver of the goal for M/WBE for subcontracting requirements was requested by the Purchasing Division and approved by the M/WBE Office because the purchase of goods or services is from sources where subcontracting or supplier opportunities are negligible.

ADMINISTRATIVE CHANGE ORDER - An Administrative Change Order or increase may be made by the City Manager in the amount up to \$50,000.00 and does not require specific City Council approval as long as sufficient funds have been appropriated.

AGREEMENT TERM - Upon City Council's approval, the Agreement will begin September 26, 2012 and expire September 25, 2013.

RENEWAL OPTIONS - This Agreement may be renewed for up to four additional one-year terms at the City's option. This action does not require specific City Council approval provided that the City Council has appropriated sufficient funds to satisfy the City's obligations during the renewal term. Staff anticipates that the costs for the renewal years will be the same as for the first year.

**FISCAL INFORMATION/CERTIFICATION:**

The Financial Management Services Director certifies that funds are available in the current operating budget, as appropriated, of the General Fund.

BQN\12-0135\LSJ

**TO Fund/Account/Centers**

**FROM Fund/Account/Centers**

**Submitted for City Manager's Office by:**

Susan Alanis (8180)

**Originating Department Head:**

Lena Ellis (8517)

**Additional Information Contact:**

Jack Dale (8357)

LaKita Slack-Johnson (8314)

**ATTACHMENTS**

**12-0135 Advanced Traffic Controllers Bid Tabulation.pdf**

				VENDORS			
Invitation to Bid 12-0135 Advanced Traffic Signal Controllers				1		2	
Tabulated By: LaKita Slack - Buyer				Texas Highway Products		McCain Inc	
				1301 Clark Street		1302 Hwy 3 S	
				Roundrock, Texas 78681		League City, Texas 77573	
				512 255-7633		281 332-8822	
ITEM	UNIT	DESCRIPTION	QTY	UNIT PRICE	TOTAL PRICE	UNIT PRICE	TOTAL PRICE
1	Each	TYPE 2070-LC-CONTROLLER UNIT, INCLUDES TYPE 2070 UNIT CHASSIS, TYPE 2070-1C CPU MODULE, 2070-2A FIELD I/O MODULE, 2070-3D FRONT PANEL ASSEMBLY AND 2070-4A POWER SUPPLY.	640	\$ 1,709.00	\$ 1,093,760.00	\$ 1,825.00	\$ 1,168,000.00
2	Each	TYPE 2070-LC-USB CONTROLLER UNIT WITH 3D USB FRONT PANEL ASSEMBLY. THIS ITEM MAY BE SUBSTITUTED FOR ITEM 1	1	\$ 1,840.00	\$ 1,840.00		\$ -
3	Each	TYPE 2070-LCS CONTROLLER UNIT, WHICH INCLUDES TYPE 2070 UNIT CHASSIS, TYPE 2070-1C CPU MODULE, TYPE 2070-2B FIELD I/O MODULE, TYPE 2070-3D FRONT PANELL ASSEMBLY AND TYPE 2070-4A POWER SUPPLY	10	\$ 1,409.00	\$ 14,090.00	\$ 1,890.00	\$ 18,900.00
4	Each	TYPE 2070-LCS-USB CONTROLLER UNIT WITH 3D USB FRONT PANEL. NOTE: THIS ITE	1	\$ 1,540.00	\$ 1,540.00	\$ -	\$ -
5	Each	TYPE 2070-NIC CONTROLLER UNIT, INCLUDES THE FOLLOWING TYPE 2070 UNIT CHASSIS, TYPE 2070-1C CPU MODULE, TYPE 2070-2B FIELD I/O MODULE, TYPE 2070-3D FRONT PANEL ASSEMBLY, TYPE 2070-4AN POWER MODULE AND TYPE 2070-8 NEMA MODULE	75	\$ 2,327.00	\$ 174,525.00	\$ 2,365.00	\$ 177,375.00
6	Each	TYPE 2070-NIC-USB CONTROLLER UNIT WITH 3D USB FRONT PANELL ASSEMBLY. NO	1	\$ 2,458.00	\$ 2,458.00	\$ -	\$ -
7	Each	TYPE 2070-N2C-CONTROLLER UNIT INCLUDING TYPE 2070 UNIT CHASSIS, TYPE 2070	75	\$ 1,567.00	\$ 117,525.00	\$ 2,120.00	\$ 159,000.00
8	Each	TYPE 2070-N2C-USB CONTROLLER UNIT WITH 3D USB FRONT PANEL ASSEMBLY. THI	1	\$ 1,698.00	\$ 1,698.00		\$ -
9	Each	TYPE 2070-UNIT CHASSIS	1	\$ 224.00	\$ 224.00	\$ 271.00	\$ 271.00
10	Each	TYPE 2070-1C CPU MODULE	30	\$ 680.00	\$ 20,400.00	\$ 1,009.00	\$ 30,270.00
11	Each	TYPE 2070-2A FIELD I/O MODULE	30	\$ 494.00	\$ 14,820.00	\$ 637.00	\$ 19,110.00
12	Each	TYPE 2070-2B FIELD I/O MODULE	1	\$ 131.00	\$ 131.00	\$ 198.00	\$ 198.00
13	Each	TYPE 2070-2N FIELD I/O MODULE	1	\$ 307.00	\$ 307.00	\$ 420.00	\$ 420.00
14	Each	TYPE 2070-3D FRONT PANEL ASSEMBLY	30	\$ 252.00	\$ 7,560.00	\$ 474.00	\$ 14,220.00
15	Each	TYPE 2070-3D USB FRONT PANEL ASSEMBLY	1	\$ 393.00	\$ 393.00	\$ 614.00	\$ 614.00
16	Each	TYPE 2070-4A POWER SUPPLY	30	\$ 225.00	\$ 6,750.00	\$ 665.00	\$ 19,950.00
17	Each	TYPE 2070-4AN POWER SUPPLY	1	\$ 233.00	\$ 233.00	\$ 671.00	\$ 671.00
18	Each	TYPE 2070-8NEMA MODULE	1	\$ 998.00	\$ 998.00	\$ 1,437.00	\$ 1,437.00
19	Each	DISCOUNT PERCENTAGE (%) FROM CATALOG OR LIST PRICE FOR ALL RELATED ITEMS REQUIRED TO MAINTAIN OR REPAIR ANY ITEM LISTED IN THE BID.	1	5% DISCOUNT		\$ -	
20	Each	TYPE 2070-6B COMM. MODULE	1	\$ 562.00	\$ 562.00	\$ 966.00	\$ 966.00
21	Each	TYPE 2070-7A COMM. MODULE	1	\$ 103.00	\$ 103.00	\$ 278.00	\$ 278.00
GRAND TOTAL					\$ 1,459,917.00		1,611,680.00



City of Richardson  
City Council Work Session  
Agenda Item Summary



**Work Session Meeting Date:** Monday, January 28, 2013

**Agenda Item:** Review and Discuss Item Listed on the City Council Meeting Agenda

**Staff Resource:** Dan Johnson, City Manager

**Summary:** The City Council will have an opportunity to preview and discuss with City Staff the agenda items that will be voted on at the City Council Meeting immediately following the Work Session.

**Board/Commission Action:** Various, if applicable.

**Action Proposed:** No action will be taken.



City of Richardson  
City Council Worksession  
Agenda Item Summary



**Worksession Meeting Date:** Monday, January 28, 2013

**Agenda Item:** Review and Discuss Part 2 of the Final Report and Recommendation, Main Street/Central Expressway Corridor Study - Implementation

**Staff Resource:** Monica Heid, Community Projects Manager

**Summary:** On Monday, staff and the consulting team will present a briefing on the implementation plan for the Main Street/Central Expressway Corridor Study. The briefing will begin with a short recap of the study and then focus on various aspects of a successful implementation plan, including a set of high-priority actions for the next phase of the project.

**Board/Commission Action:** None required

**Action Proposed:** Review and Discuss



City of Richardson  
City Council Worksession  
Agenda Item Summary



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**Worksession Meeting Date:** Monday, January 28, 2013

**Agenda Item:** Review and Discuss the 2012 Year End Crime Statistics and Program Updates

**Staff Resource:** Jim Spivey, Police Chief

**Summary:** Chief Spivey will provide an end of the year review of the Police Department which will include crime statistics, department outreach activities, and other general updates.

**Board/Commission Action:** N/A

**Action Proposed:** N/A

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City of Richardson  
City Council Work Session  
Agenda Item Summary



**Work Session Meeting Date:** Monday, January 28, 2013

**Agenda Item:** Items of Community Interest

**Staff Resource:** Dan Johnson, City Manager

**Summary:** The City Council will have an opportunity to address items of community interest, including:

Expressions of thanks, congratulations, or condolence; information regarding holiday schedules; an honorary or salutary recognition of a public official, public employee, or other citizen; a reminder about an upcoming event organized or sponsored by the City of Richardson; information regarding a social, ceremonial, or community event organized or sponsored by an entity other than the City of Richardson that was attended or is scheduled to be attended by a member of the City of Richardson or an official or employee of the City of Richardson; and announcements involving an imminent threat to the public health and safety of people in the City of Richardson that has arisen after the posting of the agenda.

**Board/Commission Action:** NA

**Action Proposed:** No action will be taken.



City of Richardson  
City Council Work Session/City Council Meeting  
Agenda Item Summary



**Work Session Meeting Date:** Monday, January 28, 2013

**Agenda Item:** Executive Session

**Staff Resource:** Dan Johnson, City Manager

**Summary:** The Council will convene into a closed session in compliance with Texas Government Code Section 551.087 – Deliberation Regarding Economic Development Negotiations: Commercial Development –Glenville Drive/Campbell Road area; Retail Development –U.S. 75/ Arapaho Road area

**Board/Commission Action:** N/A

**Action Proposed:** Council will reconvene into open session to take any action, if any, on matters discussed in Executive Session.

