

RICHARDSON CITY COUNCIL
JANUARY 17, 2011
6:00 P.M.
CIVIC CENTER/CITY HALL, 411 W. ARAPAHO, RICHARDSON, TX

1. **INVOCATION**
 2. **PLEDGE OF ALLEGIANCE: U.S. AND TEXAS FLAGS**
 3. **MINUTES OF THE DECEMBER 6, 2010, DECEMBER 20, 2010, AND JANUARY 10, 2011 MEETINGS**
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4. VISITORS.

5. PUBLIC HEARING, ZONING FILE 10-21: A REQUEST BY SCOTT POLIKOV, GATEWAY PLANNING GROUP, INC., REPRESENTING BUSH/75 PARTNERS LP, FOR A PD PLANNED DEVELOPMENT FOR APPROXIMATELY 57 ACRES LOCATED ON THE NORTH SIDE OF RENNER ROAD BETWEEN THE DART LIGHT RAIL AND PLANO ROAD.

ACTION TAKEN:

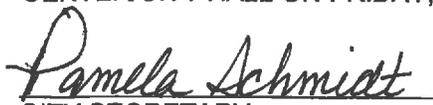
6. PUBLIC HEARING, ZONING FILE 10-20: A REQUEST BY BRIAN E. MOORE, GOOD FULTON & FARRELL PLANNING AND THE CITY OF RICHARDSON, REPRESENTING THE ESTATE OF W.W. CARUTH, JR., US TRUST, BANK OF AMERICA AND THE CITY OF PLANO, FOR A PD PLANNED DEVELOPMENT FOR APPROXIMATELY 86 ACRES LOCATED AT THE NORTHWEST AND NORTHEAST CORNERS OF US HWY 75 AND RENNER ROAD.

ACTION TAKEN:

7. REPORT ON ITEMS OF COMMUNITY INTEREST.
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THE RICHARDSON CITY COUNCIL WILL MEET AT 5:30 P.M. ON MONDAY, JANUARY 17, 2011, IN THE RICHARDSON ROOM OF THE CIVIC CENTER/CITY HALL, 411 W. ARAPAHO, RICHARDSON, TEXAS. AS AUTHORIZED BY SECTION 551.071(2) OF THE TEXAS GOVERNMENT CODE, THIS MEETING MAY BE CONVENED INTO CLOSED EXECUTIVE SESSION FOR THE PURPOSE OF SEEKING CONFIDENTIAL LEGAL ADVICE FROM THE CITY ATTORNEY ON ANY AGENDA ITEM LISTED HEREIN. THIS BUILDING IS WHEELCHAIR ACCESSIBLE. ANY REQUESTS FOR SIGN INTERPRETIVE SERVICES MUST BE MADE 48 HOURS AHEAD OF THE MEETING. TO MAKE ARRANGEMENTS, CALL 972-744-4000 VIA TDD OR CALL 1-800-735-2989 TO REACH 972-744-4000.

I CERTIFY THE ABOVE AGENDA WAS POSTED ON THE BULLETIN BOARD AT THE CIVIC CENTER/CITY HALL ON FRIDAY, JANUARY 14, 2011, BY 5:00 P.M.


CITY SECRETARY

**MINUTES OF A SPECIAL MEETING OF THE CITY COUNCIL
December 6, 2010
City of Richardson, Texas**

A Regular Meeting of the City Council was held at 7:30 p.m., Monday, December 6, 2010 with a quorum of said Council present, to-wit:

Gary Slagel	Mayor
Bob Townsend	Mayor Pro Tem
Mark Solomon	Council member
John Murphy	Council member
Bob Macy	Council member
Steve Mitchell	Council member
Amir Omar	Council member

City staff present:

Bill Keffler	City Manager
Dan Johnson	Deputy City Manager
Michelle Thames	Assistant City Manager Administrative Services
David Morgan	Assistant City Manager Community Services
Cliff Miller	Assistant City Manager Development Services
EA Hoppe	Assistant to the City Manager
Pamela Schmidt	City Secretary
Peter G. Smith	City Attorney
Monica Heid	Community Projects Manager

Mayor Slagel called the meeting to order at 7:35 p.m.

- **Rescheduled Public Hearing and consideration of Ordinance No. 3800, on first reading, adopting and imposing a moratorium pursuant to Section 212.1352 of the Texas Local Government Code on commercial property development in the area known as the West Spring Valley Road Corridor, generally described as approximately 188 acres bounded on the west by the east right-of-way line of Coit Road; on the south by the north right-of-way line of West Spring Valley Road; on the east by the west right-of-way line of US 75/Central Expressway; and on the north by: the south right-of-way line of Dumont Drive to its intersection with the alley east of Nottingham Drive; the south boundary of Richardson Heights #2; a portion of the east and south boundaries of Richardson Heights #5; the south boundaries of Centre Court Place, Richardson Heights #11 and Richardson Heights #7, Section 5; the south and west boundaries of Richardson Heights #7, Section 4; a portion of the west boundary of Richardson Heights #7, Section 3, to its intersection with Colfax Drive; the south boundary of Shadow Oaks; the south boundary of an unplatted parcel on the west side of Dublin Drive west of Shadow Oaks; and the south boundary of Northwood Estates.**

Mr. Keffler referred to the Council's work over the past 1½ years to develop a reinvestment strategy for the West Spring Valley Corridor. He noted the recently received study results and implementation strategy. He asked Monica Heid, Community Projects Manager, to further brief the Council.

Ms. Heid stated that the West Spring Valley Corridor consists of approximately 188 acres bounded on the west by Coit Road, on the east by Central Expressway, on the south by Spring Valley Road and on the north by a single family neighborhood. She reviewed the purpose of the moratorium as well as the findings listed within proposed Ordinance No. 3800. She advised that the moratorium would apply to commercial property development only defined as property zoned for, or otherwise authorized for, use other than single-family use, multifamily use, heavy industrial use or use as a quarry. She stated that among the many objectives for the future of the Corridor is a change in the market perception based upon future potentials in order to attract new development and diversify retail offerings as well as more residential uses in mixed use settings. She stated that during the period of the temporary moratorium, a municipality may stop accepting permits, authorizations, and approvals necessary for the subdivision of, site planning of, or construction on real property. Ms. Heid advised that the public hearing held by the Cit Plan Commission earlier and this hearing were advertised in the Dallas Morning News on December 2, 2010. She informed the Council that the City Plan Commission recommended approval of the moratorium. If approved by Council, the moratorium would go into effect on December 9 and the second reading and formal adoption of the ordinance would be scheduled for December 13. She advised that the 90 day period would expire on March 13, 2010.

Mayor Slagel opened the public hearing.

Andrew Laska, 502 Hyde Park, advised that he represented the Richardson Heights Neighborhood Association and referred to a presentation made by several representatives of several neighborhood associates in 2009 proposing the Heights Plan for Excellence with five areas of concern; one of which was the need for a moratorium for the West Spring Valley Corridor. He felt the moratorium was an important step in the process. He stated the urgency for the moratorium was that the area would decay without any change or revitalization. He felt there was also urgency to avoid inappropriate uses moving into the area. He spoke in favor of the moratorium and felt it was an appropriate time to move forward.

Mr. Solomon moved to close the public hearing; second by Mr. Macy and the motion was approved with a unanimous vote.

Mr. Mitchell felt the moratorium and strategy for the West Spring Valley Corridor was huge step in the right direction for the City. With regard to the question of urgency, he stated the deterioration of the area has been a concern for a lot of years, and felt this was a bold move by the Council to move the City in the right direction to address the issues on Spring Valley.

ACTION TAKEN: Mr. Murphy moved approval on the first reading of Ordinance No. 3800, an ordinance of the City of Richardson, Texas, adopting and imposing a moratorium pursuant to Chapter 212, Texas Local Government Code, on property development, including but not limited to the construction, reconstruction, or other alteration or improvement of all commercial property within property in the City of Richardson, Texas described in Exhibit "A", known as the West Spring Valley Corridor, for the purpose of the establishment of new commercial uses and development regulations pending the review and amendment of the Code of Ordinances and the Comprehensive Zoning Ordinance of the City of Richardson, Texas; providing for the temporary suspension of the acceptance, review and approval of plats, permits, zoning applications and other permits and approvals for the construction, reconstruction or other alteration of new commercial uses or the expansion of existing commercial use establishments; providing for an appeal process; providing for a severability clause; and providing for an effective date; second by Mr. Townsend and the motion was approved with a unanimous vote.

Mayor Slagel adjourned the Special Meeting at 7:49 p.m. and announced that Council would reconvene the Work Session and continue the discussion regarding Neighborhood Vitality projects.

MAYOR

ATTEST:

CITY SECRETARY

MINUTES OF A WORK SESSION OF THE CITY COUNCIL
December 20, 2010
City of Richardson, Texas

A Work Session of the City Council was convened at 6:00 p.m., Monday, December 20, 2010 with a quorum of said Council present, to-wit:

Gary Slagel	Mayor
Bob Townsend	Mayor Pro Tem
Mark Solomon	Council member
John Murphy	Council member
Bob Macy	Council member
Steve Mitchell	Council member
Amir Omar	Council member

City staff present:

Bill Keffler	City Manager
Dan Johnson	Deputy City Manager
Michelle Thames	Assistant City Manager Administrative Services
David Morgan	Assistant City Manager Community Services
Cliff Miller	Assistant City Manager Development Services
EA Hoppe	Assistant to the City Manager
Pamela Schmidt	City Secretary

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- **Call to Order** – Mayor Slagel called the meeting to order at 6:00 p.m.

A. Visitors

Ed Curran, 639 Williams Way, asked the Council to look into the matter that Channel 16 is no longer aired on the analog channel for those who do not have digital cable. He noted his appreciation for the dedication of the council and staff on behalf of the City and citizens.

B. Review and Discuss the Time Warner Cable Franchise

Mr. Keffler introduced the item and asked Brian Davis to brief the Council.

Mr. Davis granted a cable franchise effective December 31, 2000 for a term of ten years. In 2005, the Texas Legislature passed SB 5 pre-empting local authorities' ability to franchise cable providers, and establishing a State Issued Certificate of Franchise Authority (SICFA) to regulate the provision of cable services. He reported that Time Warner has provided notice that they intend to move to SICFA effective January 1, 2011. He described the expected impact of the change with regard franchise fees, services and police power. He advised that no action is required at this time. He stated he would follow-up with Mr. Curran regarding Channel 16.

C. Review and Discuss the Community Garden Partnership Program

Mr. Keffler stated this was the first of several briefing that would be brought to the Council. He asked EA Hoppe to brief the Council. Mr. Hoppe recognized Chris Halicki, member of the Environmental Advisory Commission and the Reverend from the Church of the Epiphany. He listed some of the benefits of a community garden including the social aspect, local produce for

area food banks and charities, provides a multi-generational educational opportunity, enhanced air quality and an option for a community amenity with limited taxpayer resources required. He noted the successful programs throughout the Metroplex with most requiring a user agreement, a one-time or annual fee and a requirement for donation to a food bank or local charity. He described typical garden user agreements stating it typically refers to the fee and number of plots per family. He provided information on the three gardens located in Richardson. Mr. Hoppe explained the program reviewed and approved by the EAC on October 28, 2010 and described the ordinance change that would be needed and the items that would be addressed by resolution. Other points of consideration were spacing between gardens and the maximum number of partnerships.

Mr. Omar stated he was excited about being able to address a need and desire of the community as voiced in the Parks and Recreation Master Plan. With regard to a user fee, Mr. Townsend felt it would be an incentive for ownership by the participant and Mr. Hoppe stated that the Commission did not recommend a particular fee and felt the non-profit organization should set the fee. Mr. Murphy felt it must be open to all members of the community and asked about use. Mr. Hoppe stated the City would not be involved with assigning plots but would be able to provide information about available areas. Mr. Murphy also felt that a standard fee should be set for all gardens. He also felt that the partnership should not be limited to faith based organizations. Mr. Mitchell felt that the City did not need to control or regulate the user fee and also felt there was a need to set a spacing limit. He felt it was a good use of taxpayer dollars and a program that would be welcomed by the citizens. In response to Mr. Mitchell, Mr. Hoppe stated it was his understanding that the other entities did not regulate the fees. Mr. Solomon stated that Mark Twain Elementary have started their garden and asked if it would meet the major thoroughfare definition and Mr. Hoppe felt it would. Mr. Solomon felt it was a great family activity and would be well received. Mr. Macy recommended that the City have a way to help those who want to volunteer their time. Mayor Slagel felt the staff could not take on the responsibility and the organization that brings the idea forward should take on the responsibility of managing the garden. Mr. Macy suggested provided a checklist. Mr. Hoppe stated staff would provide a "best practices" document to provide to applicants. Mr. Omar stated the point for the various plots is to include the community rather than one person. Mr. Solomon asked if a drip system would be authorized and Mr. Hoppe responded that hand-watering only would be allowed. Mr. Murphy encouraged moving forward as soon as possible.

D. Report on Items of Community Interest

Mr. Keffler reported that the NTMWD Board gave authority for Jim Parks to execute the non-binding document discussed and approved on November 22, 2010. He noted that other signatures of the homeowner association representatives had also been obtained and the City Attorney and NTMWD attorney will begin developing the final legal document.

Mr. Mitchell reported that Mayor Slagel and John Murphy attended a NCTCOG meeting where it was felt that the Cottonbelt rail line seemed like it would be moving forward and is a real possibility. Others are starting to see the value it will bring to this region and the overall meeting was very productive.

Mayor Slagel announced that Council would convene in Executive Session and recessed the meeting at 7:03 p.m.

EXECUTIVE SESSION

- In compliance with Section 551.074 of the Texas Government Code, Council convened into a closed session at 7:10 pm to discuss the following:
 - Personnel
 - Evaluation of the City Manager
- Council reconvened into open session at 10:31 pm to take action, if any, on matters discussed in executive session.

ACTION TAKEN: Mr. Mitchell moved that the current compensation and benefits of 2009-2010 will be paid to the City Manager for the 2010-2011 fiscal year; second by Mr. Townsend and the motion was approved with a 7-0 vote.

There being no further business, Mayor Slagel adjourned the meeting at 10:32 p.m.

MAYOR

ATTEST:

CITY SECRETARY

MINUTES OF A SPECIAL MEETING OF THE CITY COUNCIL
January 10, 2011
City of Richardson, Texas

A Special Meeting of the City Council was held at 6:00 p.m., Monday, January 10, 2011 with a quorum of said Council present, to-wit:

Gary Slagel	Mayor
Bob Townsend	Mayor Pro Tem
Mark Solomon (absent)	Council member
John Murphy	Council member
Bob Macy	Council member
Steve Mitchell	Council member
Amir Omar	Council member

City staff present:

Bill Keffler	City Manager
Dan Johnson	Deputy City Manager
Michelle Thames	Assistant City Manager Administrative Services
David Morgan	Assistant City Manager Community Services
Cliff Miller	Assistant City Manager Development Services
EA Hoppe	Assistant to the City Manager
Pamela Schmidt	City Secretary
Monica Heid	Community Projects Manager

1. **INVOCATION**
2. **PLEDGE OF ALLEGIANCE: U.S. AND TEXAS FLAGS**
3. **MINUTES OF THE DECEMBER 13, 2010 MEETING**

ACTION TAKEN: Upon a motion made by Mr. Murphy and seconded by Mr. Townsend, the December 13th minutes were approved and no changes were made.

4. **VISITORS. NONE**
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5. PUBLIC HEARING AND REVIEW AND CONSIDER A REQUEST BY DR. DIEP TRUONG FOR A WAIVER FROM THE MORATORIUM ON COMMERCIAL LAND DEVELOPMENT IN THE WEST SPRING VALLEY CORRIDOR, 722 W. SPRING VALLEY ROAD.

Mr. Keffler announced that the item before the Council is a request from Dr. Truong relative to a request of a waiver of the moratorium in regard to Ordinance No. 3800. Mr. Keffler called on Ms. Heid to brief the Council.

Ms. Heid read the provision of Ordinance No. 3800 providing for a request for a waiver and advised that the request submitted by Dr. Diep Truong was submitted on Tuesday, January 4, 2011. Ms. Heid provided a location map along with photos, the proposed site plan, elevations

and floor plan. She provided the criteria for consideration and noted the options before Council in consideration of the request. Mayor Slagel asked the applicant to present the request.

Dr. Diep Truong advised that she, her husband and architect were available to answer questions by Council and did not offer any further presentation. Dr. Truong thanked the council for reviewing their request for consideration.

Mr. Omar asked if she had an opportunity to review the planning and other things that have been done in the Spring Valley Corridor and Dr. Truong replied that she participated in meetings regarding the West Spring Valley Corridor strategy. She stated that she made sure the property in question was zoned properly for a dental office and expressed her hope that her proposed renovations could be part of the project. She remarked that no one mentioned the moratorium at any time. Mr. Omar asked if she was familiar with the catalyst concept as it relates to this project. Dr. Truong said that it is her hope to be part of that catalyst and join in the beautification process.

Mr. Mitchell asked Dr. Truong to talk about her practice and research regarding purchase of the site. Dr. Truong stated her current practice is in Oak Cliff and she chose Richardson as a second location because the community is very diverse and she felt a need to bring about a fresh approach to the community. She stated that she was raised in Richardson noting that she and her sisters attended school here. She stated she would like to give back to Richardson and noted that her partner lives in Richardson. She stated that she chose the site because of its visibility and because of the needs of the community. Mr. Mitchell asked if she looked at sites that already have other types of medical fields and Dr. Truong stated the visibility factor was a primary factor for selecting this property.

Mr. Townsend asked Dr. Truong if the purchase of the property was completed or if it was dependent on the request and Dr. Truong replied that the purchase had been finalized.

Mayor Slagel explained that the moratorium is to define the area. He stated the goal is not to eliminate good uses; the goal is to change the architecture and the image of the area. He asked if there was a reason to move forward now rather than after the moratorium was completed. Dr. Truong stated it is important to move forward now due to financing reasons to secure a low interest loan for remodeling the building. She advised also that there is a verbal agreement with Pollo Fiesta to buy out their lease. Mayor Slagel noted that the idea of the West Spring Valley Corridor strategy is to change look of the area and the proposed building would still look like the existing restaurant and stated he would like to see the dental business located here, but the proposed configuration would not change enough from the current building. She stated that they did know they more modifications were needed.

Mr. Murphy asked for clarification about the financing issue and Dr. Truong explained that the financing was for the renovations of the site. Mr. Murphy asked Dr. Truong if she had any concerns regarding how the surrounding property could change due to the moratorium that might make the site less attractive for a dental business. Dr. Truong stated that had she known there would be a moratorium, she would not have moved forward with the purchase of the property and reiterated that she was not aware that there would be a moratorium. Mayor Slagel noted that staff did not mention it because a moratorium was not discussed until the very end of the process. Mr. Murphy noted his understanding of doctor's offices tending to cluster and asked Dr. Truong if she had thought about the fact that the future of the area could change in a way that may not be conducive to a successful business. Dr. Truong stated they did their own

research and would not have moved forward had she know there was the possibility of zoning changes.

Mr. Townsend asked if she would be moving her business or expanding and Dr. Truong stated she would be expanding the business.

Mayor Slagel emphasized that the visioning process was a very public process and felt that it was obvious that change in the area was expected. He stated he sympathizes with her that she may not have realized what was happening, but was not sure that the proposal was in the best interest of what the City was trying to accomplish.

Mr. Mitchell referred to the second criteria of Ordinance No. 3800 and felt that although a dental office may serve the community, he did not see that it is compatible with what is there or what will be there. He felt that granting the waiver would be contrary to the spirit of the ordinance and would not be in the best interest of the community. Mr. Mitchell stated the Council was not at the point of knowing the types of businesses that would be beneficial.

Mayor Slagel opened the public hearing.

David Knepper, 101 Shadywood Lane, spoke opposed to the request for a waiver. He felt she has good intentions and would probably a good dentist office, but felt it may not fit in the future. He felt at the present time, it probably was not the best business to have there.

Andrew Laksa, President of Richardson Heights Neighborhood Association, 502 Hyde Park, reported that the Board voted to oppose the waiver request. He stated they have no objections to the applicant's business, but object to the continued form of the property and any enhancements that extend the life of the out-of-date property. The proposed building does not fit the vision for the Spring Valley Corridor. He provided various points for denying the request for a waiver. He felt it would be a minor reuse of the property and felt it would disrupt a potential catalyst project.

Barry Hand, 710 Laguna, President of Cottonwood Heights Neighborhood Association, stated the Board of nine members, while sympathetic to the doctor's position, felt it was not in line with the highest use and vision for the Corridor. He stated he would love to see the doctor's practice in Richardson but felt the location in an old restaurant would not be suitable for this area. He stated that the Board would also encourage further study to ultimately decide what the appropriate land uses are for the area.

Mr. Townsend moved to close the public hearing; second by Mr. Macy and the motion was approved with a 6-0 vote.

While directing his comments to the applicant, Mr. Omar stated that it's hard to listen to the her explanation and not be rooting for her, and noted that an office of her caliber and the vision that she has described is exactly what would be desired in Richardson, but the building design proposed seems to be a hold-over from the old one. He stated he would like Dr. Truong to be in Richardson and encouraged her to find a way to be a part of the solution.

Mr. Murphy noted the original purpose of the moratorium was to halt situations like we are facing here and encouraged Dr. Truong to come back to Richardson, just not in this location or at least not with this configuration. He stated this is not what was envisioned for the area and would not support the request.

Mr. Townsend felt the proposal is a significant upgrade to what is presently there, but felt it would not be compatible with the future plans and although he sympathizes with Dr. Truong, would not support the request.

Mr. Mitchell expressed his appreciation to Dr. Truong for bringing forward the request and her desire to locate her business in Richardson. He encouraged her to have a part in transforming Spring Valley and hoped she would at other locations.

Mr. Macy stated he appreciates her initiative and liked her willingness to invest in Richardson. He noted that conversions of buildings happen and stated his regret that the waiver would not be supported.

Mayor Slagel clarified that Dr. Truong has the ability to come back to the Council with a new plan for the site that is reconfigured to fit in with the vision for the area and what we are trying to accomplish. There is an opportunity to do something larger here.

ACTION TAKEN: Mr. Mitchell moved that the request by Dr. Truong for a waiver from the West Spring Valley Corridor commercial development moratorium to allow the conversion of the existing Pollo Fiesta Restaurant located at 722 W. Spring Valley Road, to a dental office be denied; second by Mr. Townsend and the motion was approved with a vote of 5-1, Mr. Macy opposed.

6. REPORT ON ITEMS OF COMMUNITY INTEREST.

Mr. Omar reported that he had the opportunity to run the Super Bowl 5K race Saturday morning at Galatyn Park and stated it was a beautiful venue and a great experience at Galatyn.

Mayor Slagel adjourned the meeting at 6:55 p.m.

MAYOR

ATTEST:

CITY SECRETARY



**City of Richardson
City Council Meeting
Agenda Item Summary**



Meeting Date: Monday, January 17, 2011

Agenda Item: Visitors

Staff Resource: Pamela Schmidt, City Secretary

Summary: Members of the public are welcome to address the City Council about items of interest. Speaker Appearance Cards should be submitted to the City Secretary. Speakers are limited to 5 minutes and should avoid personal attacks, accusations, and characterizations.

In accordance with the Texas Open Meetings Act, the City Council cannot take action on items not listed on the agenda. However your concerns will be addressed by City staff, may be placed on a future agenda, or by some other course of resolution.

Board/Commission Action: N/A

Action Proposed: Receive comments by visitors.





**City of Richardson
City Council Meeting
Agenda Item Summary**



City Council Meeting Date: Monday, January 17, 2011

Agenda Item: ZF 10-21 – Bush Central Station -PD

Staff Resource: Sam Chavez, Assistant Director of Development Services *SC*

Summary: The applicant's request is to rezone approximately 57.1 acres of land to a Planned Development District with modified development standards under a Form Based Code.

Numerous citizens spoke in opposition to the proposed request.

Board/Commission Action: The request was considered by the City Plan Commission on December 7, 2010. The Commission voted 5-2 to recommend approval of the request with amendments.

Action Proposed: The City Council may approve the request as presented, approve with conditions, or deny the request.

Exhibit A

BACKGROUND:

Historical Context

Since 1997, much of the vacant property in that area has been the subject of various land use studies designed to see what type of development would be appropriate for the site. The studies followed Dallas Area Rapid Transit's accelerated plan to bring light rail into Richardson. The 1997 update of the Comprehensive Planning Guide noted the location, for what is now the Bush Turnpike Station.

As the arrival of multi-modal transportation options approached, the City's 2000 Comprehensive Planning Guide, following community input and ratification by the City Plan Commission and City Council, designated the area for mixed-use Transit Oriented Development (TOD).

In 2000, the City started the process of determining future development along the light rail corridor by commissioning an Urban Land Institute (ULI) panel study. The study, published in early 2001, laid out growth priorities for the property and reinforced using the property for transit-oriented development. The study indicated that the limited number of landowners would increase the likelihood that the development would use high-quality, master-planned development strategies, rather than short-term profit maximization strategies, which are often associated with the development of small parcels. The site should allow for a mix of land uses, so that the development community can respond to potential future demand for residential and/or retail development.

The 2009 update of the City's Comprehensive Plan designated the area around all current and potential rail transit stations as the Transit Village future land use classification. Transit Villages are nodes of mixed or multiple land uses within a development and/or a single building, often in a vertical or "stacked" format, built around small-scale pedestrian-friendly blocks. Uses were to include medium- to high-density residential (townhomes and multi-family), retail, entertainment, hospitality and offices. The intensity of development within Transit Villages can range from medium to high based on the proximity of the rail transit facility, the adjacent roadway infrastructure, and surrounding land uses.

Form Based Codes

A Form Based Code (FBC) is a zoning technique that emphasizes form, as opposed to conventional zoning whose emphasis is on the separation of uses. Form-based codes foster predictable built results and a high-quality public realm by using physical form (rather than separation of uses) as the organizing principle for the code. They are regulations, not guidelines.

Form-based codes address the relationship between building facades and the public realm, the form and mass of buildings in relation to one another, and the scale and types of streets and blocks. The regulations and standards in form-based codes, presented in both text and graphics, are keyed to a Regulating Plan that designates the appropriate form and scale (and therefore, character) of development rather than only distinctions in land-use types. Form-based codes are drafted to achieve a community vision based on time-tested forms of urbanism.

A Form-Based Code commonly includes the following elements:

- *Regulating Plan.* A plan or map of the regulated area designating the locations where different building form standards apply based on clear community intentions regarding the physical character of the area being code.
- *Public Space Standards.* Specifications for the elements within the public realm (e.g., sidewalks, travel lanes, on-street parking, street trees, street furniture, etc.).
- *Building Form Standards.* Regulations controlling the configuration, features, and functions of buildings that define and shape the public realm.
- *Administration.* A clearly defined application and project review process.
- *Definitions.* A glossary to ensure the precise use of technical terms.

Form-based codes also sometimes include the following elements:

- *Architectural Standards.* Regulations controlling external architectural materials and quality.
- *Landscaping Standards.* Regulations controlling landscape design and plant materials on private property as they impact public spaces (e.g. regulations about parking lot screening and shading, maintaining sight lines, insuring unobstructed pedestrian movements, etc.).
- *Signage Standards.* Regulations controlling allowable signage sizes, materials, illumination, and placement.

A well-crafted form-based code can be an effective form of development regulation for shaping pedestrian-scaled, mixed use and fine-grained urbanism. To determine if a proposed development regulation is a well-crafted form-based code, the following should apply:

- The code's focus is primarily on regulating urban form and less on land use.
- The code is regulatory rather than advisory.
- The code emphasizes standards and parameters for form with predictable physical outcomes (build-to lines, frontage type requirements, etc.) rather than relying on numerical parameters (FAR, density, etc.) whose outcomes are impossible to predict.
- The code requires private buildings to shape public space through the use of building form standards with specific requirements for building placement.
- The code promotes an interconnected street network and pedestrian-scaled blocks.
- Regulations and standards are keyed to specific locations on a regulating plan.
- The diagrams in the code are unambiguous, clearly labeled, and accurate in their presentation of spatial configurations.

A form-based code, if effective, should receive affirmative answers to the following questions:

Is the code enforceable?

- Does the code implement a plan that reflects specific community intentions?
- Are the procedures for code administration clearly described?
- Is the form-based code effectively coordinated with other applicable policies and regulations that control development the property?

Is the code easy to use?

- Is the overall format and structure of the code readily discernable so that users can easily find what is pertinent to their interest?
- Can users readily understand and execute the physical form intended by the code?
- Are the intentions of each regulation clearly described and apparent even to planning staff and citizens who did not participate in its preparation?
- Are technical terms used in the code defined in a clear and understandable manner?
- Does the code format lend itself to convenient public distribution and use?

Will the code produce functional and vital urbanism?

- Will the code shape the public realm to invite pedestrian use and social interaction?
- Will the code produce walkable, identifiable neighborhoods that provide for daily needs?
- Is the code based on a sufficiently detailed physical plan and/or other clear community vision that directs development and aids implementation?
- Are parking requirements compatible with pedestrian-scaled urbanism?

Exhibit B

TRAFFIC IMPACT:

As part of the application, a Traffic Impact Analysis (TIA) was submitted by the applicants. The TIA was conducted by Kimley-Horn & Associates, Inc., a traffic engineering and planning consulting firm hired by the applicants to evaluate the combined impact of the developments on the roadway system. As part of the review process, the City hired an independent traffic engineering consultant, Lee Engineering, to perform a comprehensive evaluation of the TIA, its assumptions, and results.

The TIA examined both the Bush Central Station and Caruth TOPD requests in a single analysis. The applicant's consultant utilized the City's standard TIA guidelines and prior Kimley-Horn studies conducted in Richardson. As part of the study, new traffic counts were conducted at signalized intersections and on all surrounding arterial roadway and frontage roads.

The following scenarios were initially analyzed per the City TIA guidelines:

- 2020 Background Traffic (existing 2010 plus typical growth)
- 2020 Background Traffic plus Full Site build-out Traffic
- 2035 Background Traffic (existing 2010 plus typical growth)
- 2035 Background Traffic plus Full Site build-out Traffic

At the December 9, 2010 City Planning Commission meeting, members of the commission requested additional information pertaining to the existing traffic conditions of the roadway network so the public could better understand how the proposed development would potentially impact the roadway network. Following the CPC meeting and subsequent meetings with the public, City staff requested that Kimely-Horn conduct analysis of additional scenarios including the following:

- 2010 Existing Traffic
- 2010 Existing Traffic plus Full Site build-out Traffic
- 2035 Traffic plus Full Site build-out without any of the applicant's proposed mitigation
- 2035 Traffic with other potential intersection improvements at the Renner and US75 intersections which could be entertained by the City and TxDOT to combat the congestion generated by background traffic and future background traffic growth not associated with the development.
- 2035 Traffic plus Full Site build-out with additional background improvements and the developer's mitigation at the Renner and US75 intersections

The results of all these additional studies will be presented at the City Council meeting.

The existing 2010 traffic data collected by Kimley-Horn and the historical daily traffic volume counts conducted by the City are attached to this exhibit for review. The bar graph shows historical and current traffic volume counts for Renner Road and Plano

Road in the area of the development, and includes traffic volume counts for other arterial roadways in Richardson for comparison purposes.

To assess traffic impacts associated with the proposed development on the adjacent roadway network, basic land use assumptions were established by the applicants. The table below depicts land use intensity assumptions utilized in the TIA for the proposed zoning, as well as currently allowed land use intensities allowed by the existing zoning.

TIA Land Use Assumptions and Existing vs Proposed Zoning Comparison

All Tracts Combined		Units	Existing	Proposed	Difference
General Office	SF		5,172,584	2,207,000	(2,965,584)
Shopping Center/Retail	SF		293,403	386,000	92,597
Hotel	Rooms		0	400	400
Apartment/Multifamily	DU		426	3,765	3,339
Townhome	DU		0	175	175
Bush Central Station Tract		Units	Existing	Proposed	Difference
General Office	SF		2,640,632	600,000	(2,040,632)
Shopping Center/Retail	SF		293,403	200,000	(93,403)
Hotel	Rooms		0	400	400
Apartment/Multifamily	DU		426	2,400	1,974
Townhome	DU		0	100	100
East Caruth Tract		Units	Existing	Proposed	Difference
General Office	SF		1,507,006	767,500	(739,506)
Shopping Center/Retail	SF		0	161,000	161,000
Apartment/Multifamily	DU		0	1,365	1,365
West Caruth Tract		Units	Existing	Proposed	Difference
General Office	SF		1,024,946	839,500	(185,446)
Shopping Center/Retail	SF		0	25,000	25,000
Townhome	DU		0	75	75

In summary, the amount of square footage for non-residential uses anticipated in the proposed zoning scenario is reduced by a total of 2,965,584 square feet and the number of multi-family units is increased by 3,339 units over what is currently allowed in the existing zoning entitlements. In addition, 400 hotel room and 175 townhome units are being added.

The table below depicts new automobile trip generations associated with the proposed development and trip generations associated with the currently allowed land uses.

Trip Generation Table (existing and proposed zoning)

New Auto Trips - Proposed Zoning

Proposed Tract	Daily Total	AM Peak Hour			PM Peak Hour		
		IN	OUT	TOTAL	IN	OUT	TOTAL
Bush Central Station	28,255	1,066	1,103	2,169	1,363	1,435	2,798
Caruth - East	19,237	966	634	1,600	866	1,232	2,098
Caruth - West	9,082	898	166	1,064	287	917	1,204
Totals:	56,574	2,930	1,903	4,833	2,516	3,584	6,100

New Auto Trips - Existing Zoning

Proposed Tract	Daily Total	AM Peak Hour			PM Peak Hour		
		IN	OUT	TOTAL	IN	OUT	TOTAL
Bush Central Station	27,004	2,484	592	3,076	1,321	3,275	4,596
Caruth - East	9,696	1,301	177	1,478	270	1,320	1,590
Caruth - West	7,606	1,009	138	1,147	199	967	1,166
Totals:	44,306	4,794	907	5,701	1,790	5,562	7,352

28% Increase -39% Decrease 110% Increase -15% Decrease 41% Increase -36% Decrease -17%² Decrease

Based on the findings of the TIA, the following roadway improvements were recommended as mitigation measures to be implemented by the developer:

- Renner Road:
 1. Westbound Auxiliary Lane – Plano Road to Routh Creek Parkway
 2. Westbound Deceleration Lanes at All Other Driveways
 3. Westbound to Northbound free-flow right-turn lane and auxiliary lane at US75 leading north at least to the first driveway
 4. Median Opening with Turn Lanes at Drive P2 (proposed street designated as “Park Avenue” on the Bush Central Station Regulating Plan, located between Plano Road and proposed Routh Creek Parkway)
 5. Traffic Signal at Drive P2
 6. Eastbound Left Turn to Routh Creek Parkway

- Plano Road:
 1. Southbound Right Turn bays / Auxiliary Lane – Bush Turnpike to Renner Road
 2. Northbound Left Turn Lanes at Infocom and Drive P5
 3. Traffic Signals at Infocom Drive and Drive P5 (proposed street designated as “TOD Street” on the Bush Central Station Regulating Plan, located on Plano Road, between Infocom and Renner Road)
 4. Improved Right Turn Lane to Renner Road with free-flow into auxiliary lane on Renner Road

- US-75 Frontage Roads
 1. Deceleration Lanes at All Driveways (All driveways and right turn bays to satisfy TxDOT Access Management Requirements)

- Other Transportation Recommendations:
 1. Cotton Belt Transit Line Connection to Bush Turnpike Station
 2. Plano Road at Renner Road Intersection to remain as an At-Grade Intersection in the City’s Master Transportation Plan.

While the TIA identifies the measures which should be the responsibility of the developer to return traffic operations to the background conditions, the following modifications are also recommended if the City desires to improve the background operating conditions shown in the 2035 background scenarios. With these measures in place, there are no LOS F conditions in 2035. These recommendations would not be the responsibility of the developers

Renner Road at US 75 Frontage Roads

1. Modify the existing five-lane SBFR approach to Renner Road to provide two left-turn lanes (one shared with the U-turn lane), two through lanes, and one right-turn lane.
2. Modify the existing five-lane NBFR approach to Renner Road to provide two left-turn lanes (one shared with the U-turn lane), two through lanes, and one right-turn lane.

3. Convert the existing northbound right-turn lane to be a free right turn by the addition of an eastbound auxiliary lane on Renner Road between the NBFR and the eastbound right-turn bay at Routh Creek Parkway.

Renner Road at Plano Road

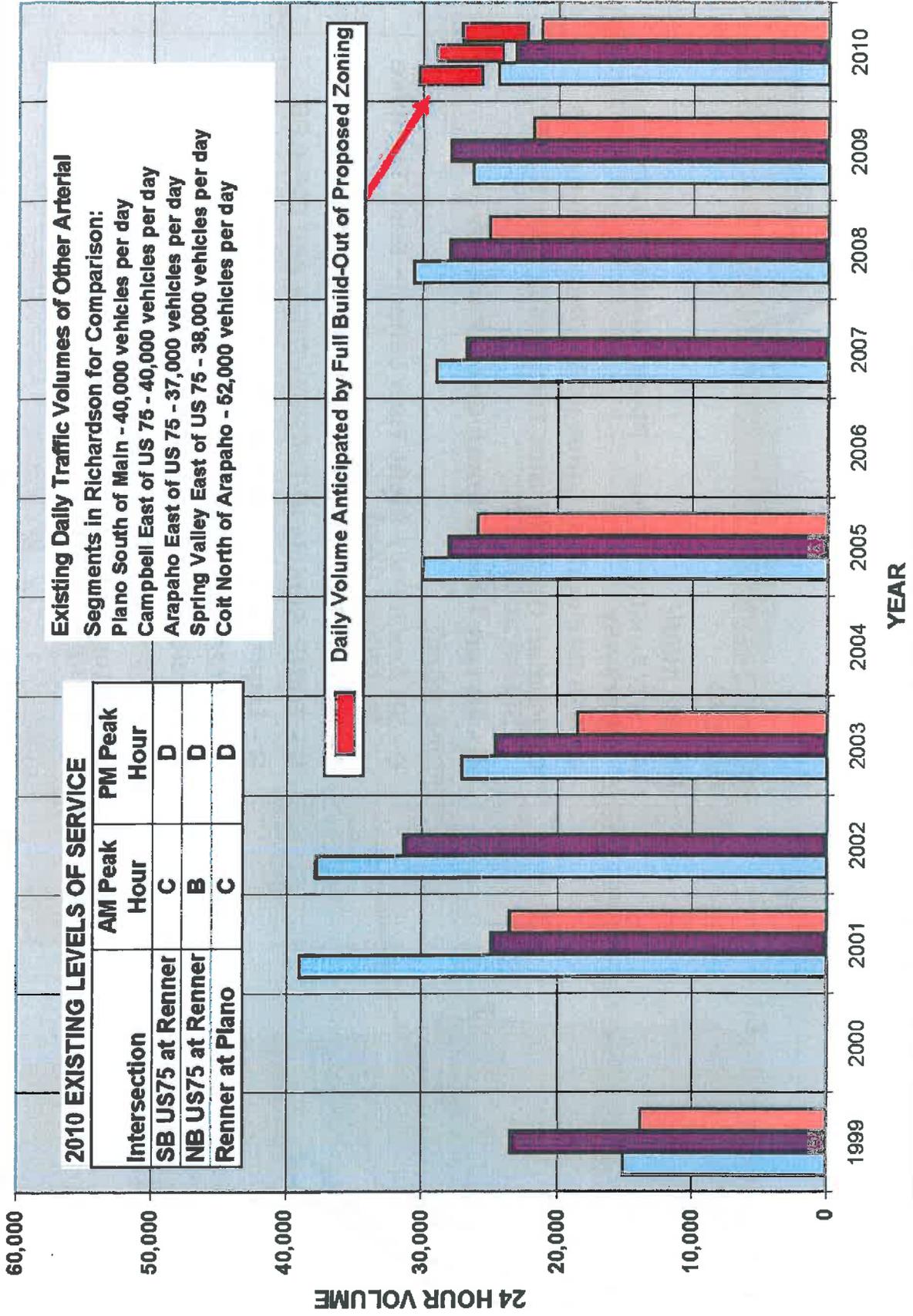
1. Convert the existing northbound right-turn lane to be a free right turn by the addition of an acceleration lane on eastbound Renner Road.

After a thorough review of the TIA and its recommendations, City staff and staff's traffic consultant (Lee Engineering) were able to conclude the following:

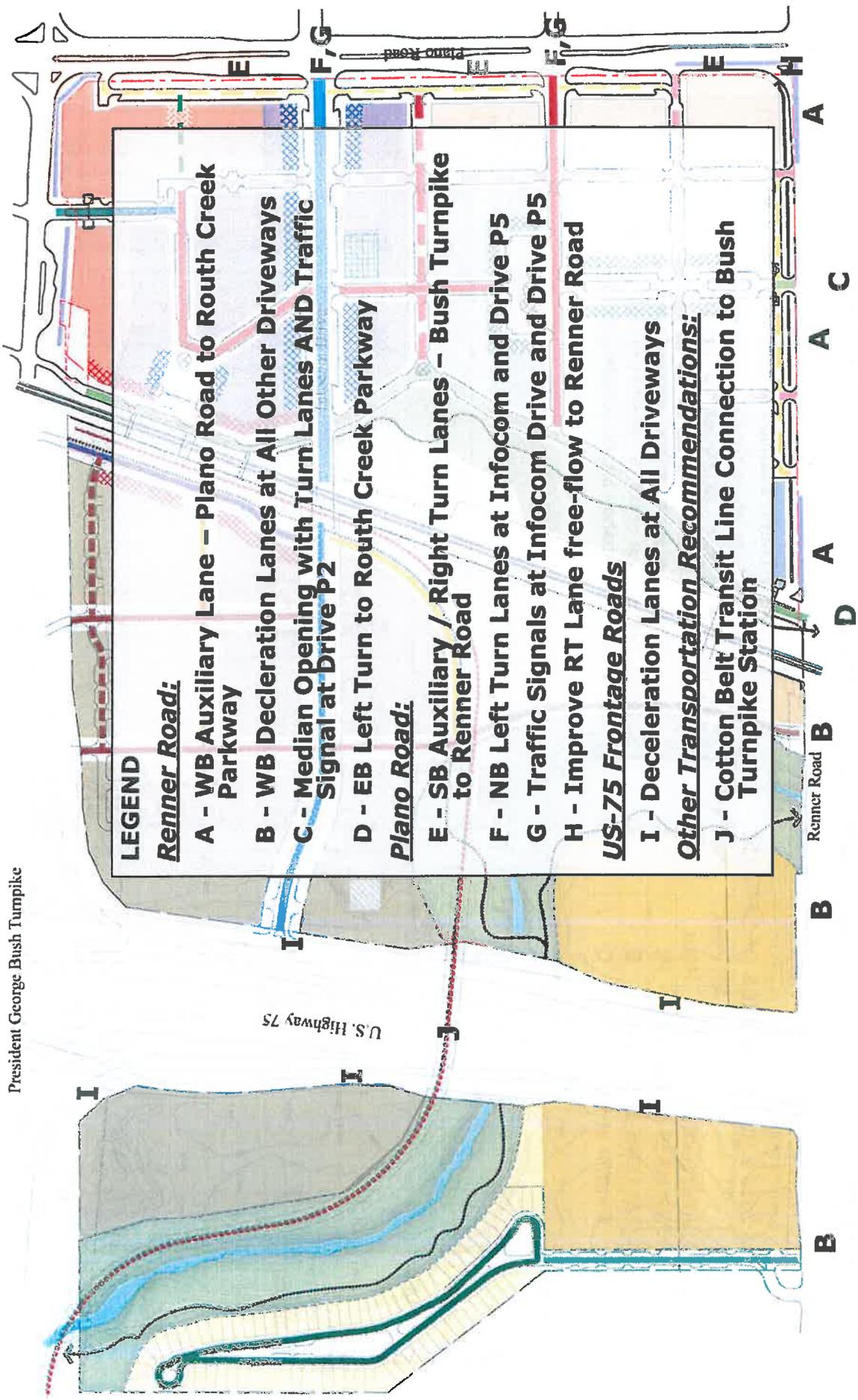
- The proposed TOD Mixed Use Zoning results in more daily traffic than the existing zoning entitlements, but it is more spread out over the entire day due to the mixture of proposed uses and the Peak Hour traffic volumes will be lower than the existing zoning allows with a better distribution of inbound and outbound traffic.
- Adequate provision of access and circulation drives will evenly distribute the traffic to the Arterial and Freeway frontage road system minimizing the impact to any specific link on the roadway network.
- The Cotton Belt Transit Line Connection to Bush Turnpike Station will greatly benefit the development and reduce the impact on traffic even more than depicted in the results of the TIA because staff limited the study's assumption of Transit usage to only a 10% reduction of trip capture rate.
- Significant levels of roadway capacity enhancements including additional turn bays and auxiliary lanes on the US 75 Frontage Roads, Plano Road and Renner Road are proposed by the applicant as part of the regulating plan to maximize the efficiency of the roadway network.
- The Plano Road at Renner Road Intersection can remain as an At-Grade Intersection. The Thoroughfare Plan will need to be amended to reflect this change at a future time.

The above conclusions were based on the land use assumptions utilized in the TIA; therefore, if a significant increase in land use intensity or a major modification to the proposed mix of land use are proposed in the future, staff recommends that a revised TIA should be required prior to any future PD amendment similar to the process in which other major modifications must be submitted through the City Council.

2010 Traffic Analysis and Development Traffic with Historical Data



Traffic Impact Analysis Recommended Mitigation Measures



Staff Updates

Applicant Code Revisions

As of January 13, 2011, the following revisions have been incorporated in the Code as a result of the Commission's December 21, 2010 meeting.

1. Added an appeals procedure to the City Managers decisions on minor modifications (Page 5, Section 3.8.6) and modified the associated Development Review Process flow chart accordingly (Page 60).
2. Changed the building material requirements along Plano and Renner Road to be the same as for a Type A Street (Page 31, Section 8.1.7, i. and ii. and Page 33 iii.).
3. Changed the masonry requirement along Type A streets to 85% from 80% and accent material to 15% from 20% (Page 32, Section 8.17, I and ii)
4. Added "curtain" before glass under building materials (Page 32, Section 8.1.7, i).
5. Added Plano, Renner and PGBT access road building frontages within specific character zones to require buildings to be built to Retail Ready Standards (Page 18, Section 7.1.4, Page 21, Section 7.2.4 and Page 27, Section 7.4.4).

December 7, 2010, Commission Requested Information

Applicant and Neighborhood Meeting

The applicants and interested area residents met on Monday, December 13, 2010 at 8 PM at the Renner Road police substation to discuss the applicant's zoning proposals. Approximately 75 people were in attendance that evening. The purpose of the meeting was to educate and answer any questions from those in attendance.

At staff's request, a meeting was held with area representatives on Thursday, December 16, 2010, to review and discuss the findings and recommendations of the Traffic Impact Analysis.

Adopted Area Form Based Codes Qualitative Responses

City of Roanoke, Tx

Oak Street - Form Based Code

Jimmy Stathatos, City Manager

- Adopted in 2008
- Attracting new businesses
- No residential to date
- Increased pedestrian traffic
- Success for property owners, businesses and City
- Flexible
- Extend Form Based Code to other areas of the City
- Recipient of 2010 Public Improvement Project by Texas Downtown Association

City of Duncanville, Tx

Downtown Duncanville District – Form Based Code
Mike Bromley, Assistant Director Public Works

- Adopted in 2008
- No private development projects to date
- Completed infrastructure project
- Issues appear to be related to retrofitting existing developed conditions with Code requirements (existing right-of-way, building setbacks)

City of McKinney, Tx

Regional Employment Center (REC) and Craig Ranch – Hybrid Form Based Code (more New Urbanism than Form Base Code)
John Kessell, Director of Development Services

- Flexible
- Shifts with the market
- No concerns with lack of density or intensity (allows development to reach its full potential)
- Most successful aspects has been found in vertical use developments
- \$51 advalorem tax return on \$1 City investment
- Vertical use developments more successful
- Pattern book (design standards) controlled by Craig Ranch, not the City

City of North Richland Hills, Tx

Transit Oriented Development Code – Form Based Code
John Pitstick, Planning Director

- Adopted in 2009
- Applied to 180 acres of land which contains existing development
- Positive reception from property owners, business owners, homeowners
- Code laid out well, easily administered by staff

General Information

The following codes were awarded the 2010 Driehaus Award and were recognized in May of 2010 at CNU 18 in Atlanta.

The Denver Commons

The Denver Commons is a “legacy code”, adopted in 1997, and now showing extensive built results in a critical redevelopment area of the center city. The code has shown remarkable ability to guide development according to the city's initial vision for a former “brownfield”. Its key characteristics:

- An early, groundbreaking form-based code with excellent built results.
- Visioning and intent statements are so well crafted that they make a convincing case for public and private support of the code. Area by area, the intent is clearly outlined.

- The code's public realm requirements were provided in great detail. A distinction was made between the required standards and additional guidelines for each plan component.
- While this code was adopted as a PUD using “design guidelines” language, its essential elements are replicable today as a form-based zoning district.
- This code could be emulated at any scale.

The Heart of Peoria

The Heart of Peoria Form-Based Code "form-districts" incorporate a form-based code into specific areas. These special FBC districts, which target small areas offering high potential for revitalization, fit within a conventional zoning rewrite for the balance of the city. Key characteristics:

- The code counters the misconception that a form-based code can only be undertaken citywide. In Peoria the entire city code was updated in a conventional fashion, while the form-based portions of the code were targeted to specific revitalization areas.
- The visioning process for the FBC districts gave a clear basis for the regulations applied to those districts.
- The street type organization of the code handles corner buildings particularly well.

Miami 21

Miami 21 took on – and very successfully met – the challenge of a new code for the entire City of Miami, setting up walkable urbanism as the default setting for the entire city and offering a highly stream-lined approval process for projects that conform to the code. Notable aspects:

- The code is ground-breaking in that city officials completely replaced the existing zoning code.
- The code sets up walkable urbanism as the city-wide default pattern for development and redevelopment.
- The entire document is carefully considered and well written. The table of contents and structure are very clear.
- Restricted, limited and open categories affect density and use for each zone; there are essentially 18 carefully calibrated zones for the entire city.
- If an applicant complies with the regulations, the project is approved without layers of time-consuming and unpredictable discretionary reviews.
- Miami 21 has all the provisions that would be found in a typical zoning ordinance but goes further, addressing such important aspects as sustainability, view corridors, and solar access. Preservation of historic districts is included. A public benefits program gives developers additional density for affordable housing and green building standards, while a contribution is required to a trust fund for open space.



Staff Report

TO: City Council
FROM: Sam Chavez, Assistant Director of Development Services **SC**
DATE: January 17, 2011
RE: **Zoning File 10-21: Bush Central Station - PD**

REQUEST:

To rezone approximately 57.1 acres of land to a Planned Development District with modified development standards under a Form Based Code.

The Commission requested additional information to be submitted for their December 21, 2010 public hearing which also provided for the applicant to meet with area interested parties. The requested information can be found under a separate attachment labeled "Staff Update". The "Staff Update" also includes amendments to the Code as a result of the Commission's December 21, 2010 meeting.

APPLICANT:

Scott Polikov / Gateway Planning Group, Inc.

PROPERTY OWNER:

Joe Altemore / Bush/75 Partners LP

TRACT SIZE AND LOCATION:

Approximately 57.1 acres, north side of Renner Rd. between the DART Light Rail and Plano Rd.

EXISTING DEVELOPMENT:

The subject tract is undeveloped.

ADJACENT ROADWAYS:

PGBT: Freeway/Turnpike; 69,000 vehicles per day on all lanes, eastbound and westbound, at the Shiloh Road Toll Plaza; 155,000 vehicles per day on all lanes, eastbound and westbound at the Coit Road toll Plaza (December 2009).

Renner Road: Six-lane, divided arterial; 27,900 vehicles on all lanes, eastbound and westbound between Plano Road and DART Light Rail (March 2009).

Plano Road: Six-lane, divided arterial; 26,300 vehicles per day on all lanes, northbound and southbound between PGBT and Renner Road (March 2009).

SURROUNDING LAND USE AND ZONING:

North: City of Plano
South: Undeveloped; I-M(1) Industrial
East: Undeveloped; PD Planned Development and C-M Commercial
West: Undeveloped and existing Bush Turnpike Station; R-1500-M Temp and TO-M Technical Office

FUTURE LAND USE PLAN:

Transit Village and Regional Employment

Mixed or multiple land uses built around small-scale pedestrian blocks located at the City's rail stations. Uses include medium- to high-density residential, retail, entertainment, hospitality and offices.

Higher density development is appropriate with the primary use being high-rise office. Secondary uses include retail centers and entertainment venues.

Future Land Uses of Surrounding Area:

North: City of Plano
South: Regional Employment
East: Regional Employment
West: Transit Village and Regional Employment

EXISTING ZONING:

Planned Development and TO-M Technical Office District (Ord. No. 2588-A, February 1987) and I-M(1) Industrial (Ord. No. 1044-A, October 1976)

APPLICANT'S STATEMENT

(Please refer to the complete Applicant's Statement.)

BACKGROUND INFORMATION

Please see Exhibit A

TRAFFIC IMPACTS:

Please see Exhibit B

STAFF COMMENTS:

Applicant’s Proposal:

The applicant’s request is to rezone approximately 57.1 acres of land to a Planned Development District with modified development standards under a Form Based Code.

The table below represents the current zoning entitlements for the subject property and the land use assumptions used for the purpose of analyzing traffic impacts associated with the request:

Current Zoning Entitlement Rights		TIA Land Use Assumptions		Difference
General Office	2,640,632 SF	General Office	600,000 SF	(2,040,632 SF)
Shopping Center/Retail	293,403 SF	Shopping Center/Retail	200,000 SF	(93,403 SF)
Hotel	0 Rooms	Hotel	400 Rooms	400 Room
Multi-Family	426 units	Multi-Family	2,400 Units	1,974 Units
Townhome	0 units	Townhome	100 Units	100 Units

In summary, the amount of square footage for non-residential uses anticipated in the subject request is reduced by a 2,134,035 square feet, and the number of multi-family units is increased by 1,974 units over what is currently allowed in the current zoning entitlements. In addition, 400 hotel rooms and 100 townhome units are being added.

The Traffic Impact Analysis (TIA) submitted by the applicant examined the subject request and the Caruth TODPD application (Zoning File 10-20) in a single analysis. The table below represents the current zoning entitlements for the subject properties and land use assumptions for both requests.

All Tracts Combined	Unit	Current Entitlement	TIA Land Use Assumptions	Difference
General Office	SF	5,172,584	2,207,000	(2,965,584)
Shopping Center/Retail	SF	293,403	386,000	92,597
Hotel	Rooms	0	400	400
Multi-Family	Units	426	3,765	3,339
Townhome	Units	0	175	175
Bush Central Station				
General Office	SF	2,640,632	600,000	(2,040,632)
Shopping Center/Retail	SF	293,403	200,000	(93,403)
Hotel	Rooms	0	400	400
Multi-Family	Units	426	2,400	1,974
Townhome	Units	0	100	100
East Caruth Tract				
General Office	SF	1,507,006	767,500	(739,506)
Shopping Center/Retail	SF	0	161,000	161,000
Multi-Family	Units	0	1,365	1,365
West Caruth Tract				
General Office	SF	1,024,946	839,500	(185,446)
Shopping Center/Retail	SF	0	25,000	25,000
Townhome	Units	0	75	75

In summary, the amount of square footage for non-residential uses anticipated in the proposed overall zoning scenarios is reduced by a total of 2,872,987 square feet and the number of multi-family units is increased by 3,340 units over what is currently allowed in the existing zoning entitlements. In addition, 400 hotel rooms and 175 townhome units are being added.

The proposed FBC includes the following elements:

- Administration
- Definitions
- Regulating Plan
- Schedule of Permitted Uses
- Building Form Standards
- Architectural Standards
- Street and Streetscape Design Standards
- Signage Standards
- Civic Space Standards
- Appendix A: Regulating Plan
- Appendix B: Planting List
- Appendix C: Development Process Flow Chart

Administration (Section 3) establishes the provisions for reviewing and approving development applications to ensure that all development is consistent with the Code. *In addition to the Code requirements, all development plans are also subject to all other applicable City of Richardson codes and ordinances, such as the Building Code, Fire Code, Engineering Design Standards, Thoroughfare Plan, Subdivision and Development Code and Sign Code.*

The Code outlines the basic steps for review of a development application. The outline serves as a checklist to ensure that the development plan complies with the applicable code requirements. The following steps are to be taken:

- Locate the property on the Regulating Plan
- Identify
 - Character Zone
 - Street Type
 - Special Frontage Standards
- Review the Schedule of Uses by Character Zone
- Examine the Building Form and Development Standards
- Refer to Building Design Standards
- Refer to Street Type and Streetscape Standards

If the development plan does not comply with the Code, the plan is deferred to the City Manager or designee for interpretation, for review as a minor modification as prescribed in the Code, if appealed-forwarded to City Council for consideration or forwarded to the City Plan Commission and City Council for consideration as a Special Development Plan.

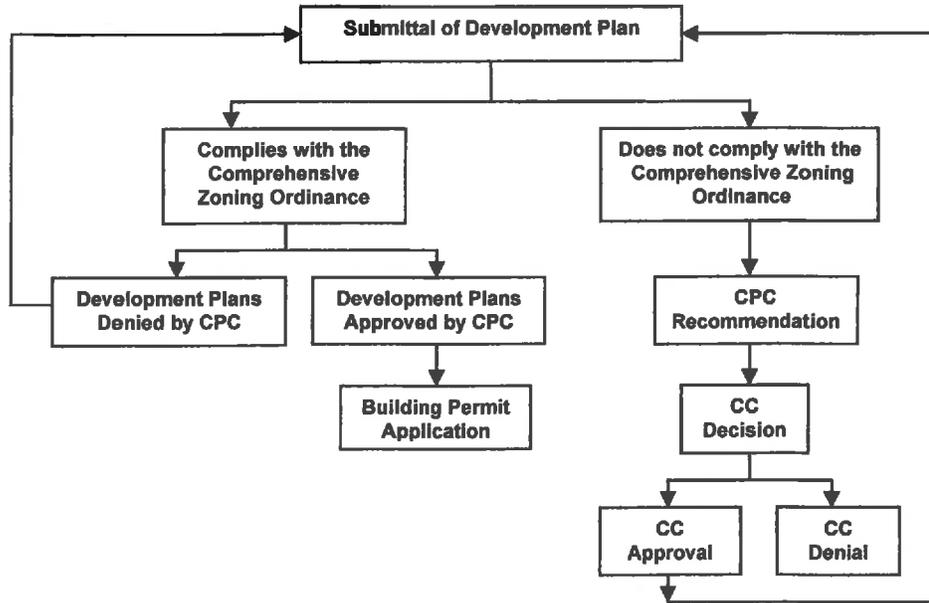
Minor Modifications (Subsection 3.8) designates the approving authority with regard to minor modifications to the Code and establishes thresholds (Table 3.1) for specific allowed modifications, including an appeal process for uses not listed in the Code.

- Minor Modifications (approving authority City Manager or designee)
 - Does not materially change the circulation and building location
 - Does not increase the building area permitted
 - Does not change the relationship between the building and streets
 - Does not allow a prohibited use
 - Does not increase building height or reduce required parking
 - Limited changes to a street cross section and Street and Streetscape Design Standards
 - Uses not listed, but are substantially similar (appeals are forwarded directly to the City Council)

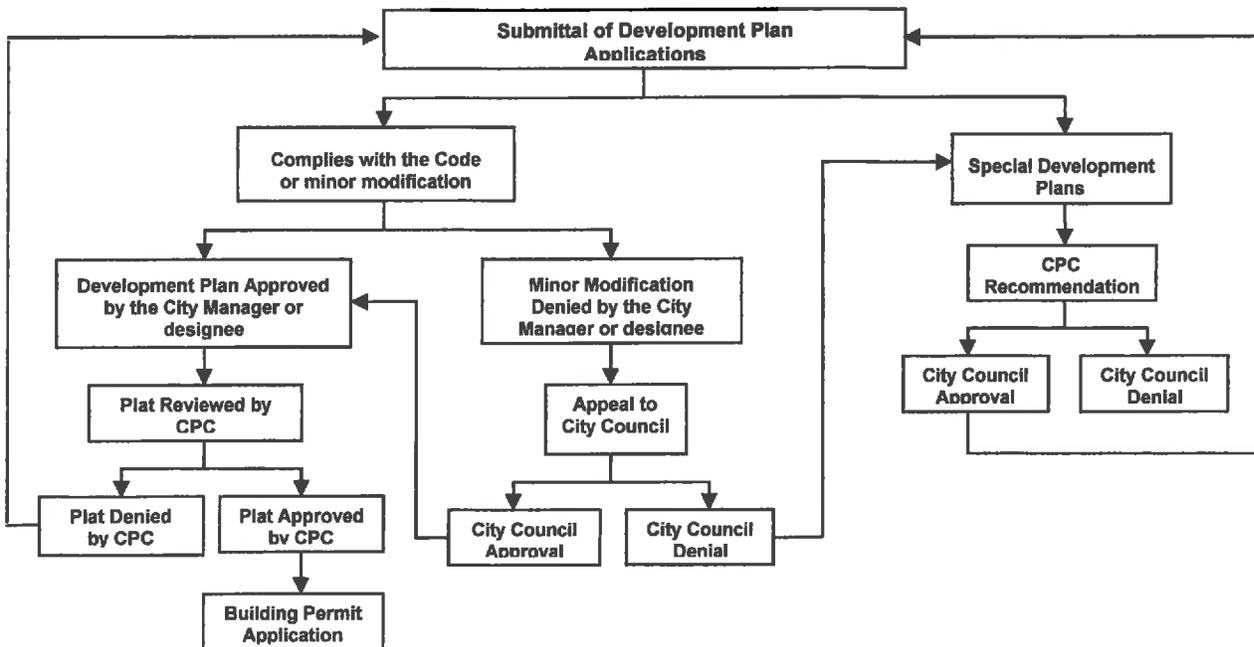
Special Development Plans (Subsection 3.7) establishes the approving authority for any proposed modification to the Code; other than those permitted as minor modification, and are consider major modifications and thus treated as Special Development Plans. Special Development Plans require the request to be reviewed and considered by the City Plan Commission and City Council.

The flow chart below depicts the City’s current development review process as a comparison with what is proposed by the applicant.

City of Richardson Development Review Process



Proposed Development Review Process



The proposed review/approval process defers from the City's current process in that site plans, building elevations, landscape plans, and civil engineering will be approved at staff level if the development application conforms with the Code, while development plats will continue to be approved by the Commission.

The Code

The main elements of a Form Based Code are the Regulating Plan and the Development Standards.

Regulating Plan (Appendix A) represents the zoning map for the subject site and creates distinct character zones which are different from other areas within the subject site. Other elements on the Regulating Plan include Street Designations, Special Frontages, Civic/Open Space and Other Destinations. The four (4) Character Zones and a general description of each and allowable uses are identified below. For a detailed list of proposed allowed uses in each Character Zone please refer to the Schedule of Permitted Uses (Section 6, Table 6.1) in the Code.

- **Character Zones**

- TOD Core (High Pedestrian Activity, Highest Density, Greatest Variety of Uses)
- TOD Mixed Use (High Intensity Commercial and Residential with supporting retail uses)
- Arterial Mixed Use (Transition to Regional Streets for Auto-Oriented Sites, Gateway to Development)
- Highway Mixed Use (Transition from PGBT and US 75, High Intensity Development)

General Allowable Uses (by Character Zone)

TOD Core, TOD Mixed, Arterial Mixed, Highway Mixed

- Retail Sales or Service, Office, Research, Food Service
- Arts, Entertainment, and Recreation
- Education, Public Administration, Health Care, Institutions
- Home Occupation, Multi-Family, Residential Lofts, Live-Work Units
- Hotel, Surface Parking (accessory), Structured Parking, Sales from Kiosk, Community Garden, Roof-Mounted Antenna, Special Event, Equipment (utility, wind, solar, rain harvesting)

Additional Uses (by Character Zone)

TOD Mixed, Arterial Mixed

- Drive-Thru
- Townhome

Arterial Mixed, Highway Mixed

- Auto-Related Sales and Service
- Surface Parking (primary)

Highway Mixed

- Drive-Through

- **Street Designations** (Mandatory and Non-Mandatory Streets) illustrates the design, configuration and standards of all streets and include the following street designations:

- TOD Main Street
- Parkway

- TOD Avenue
- Park Avenue
- TOD Street Type A
- TOD Street Type B
- Court Street
- Plano/Renner Slip Road
- Commercial Alley
- **Special Frontages** (Mandatory and Non-Mandatory) are applied to certain blocks to address specific requirement and transitions, and include the following designations:
 - Station Platform
 - Main Street
 - Neighborhood (Non-Mandatory) Only applicable if residential is developed
 - Special Designation
- **Civic/Open Space** (Mandatory and Non-Mandatory) creates a network of open spaces that provide passive and active recreational opportunities, and include the following designations:
 - Mandatory Park
 - Mandatory and Non-Mandatory Plazas
 - Mandatory Green
 - Mandatory Square
- **Other Destinations** (Mandatory and Non-Mandatory) indicates an area for a specific use with applicable standards, and include the following designations:
 - Mandatory Multi-Use Trail
 - Non-Mandatory Alley, Pedestrian Passage, Existing Trees, Gateway Element and Vista Terminus

The **Development Standards** are established in text and graphic form for each Character Zone, and include Building Form and Development Standards, Building Design Standards, Street & Streetscape Standards, Signage, Civic/Open Space and related standards for all streets, public and private development. All Development plans will be reviewed for compliance with each standard.

Building Form and Development Standards (Section 7) are established in text and graphic form for each Character Zone and include the following standards:

- Building Placement
 - Street-Setback Line
 - Build-To Zone
 - Setback
 - Building Frontage
- Block Standards
 - Block Face Dimension
 - Block Perimeter

- Building Height
 - TOD Core (maximum 300 feet)
 - TOD Mixed Use (maximum 225 feet)
 - Arterial Mixed Use (maximum 100 feet)
 - Highway Mixed Use (maximum 300 feet)
 - Special Destination Area (maximum 2 stories)
- Commercial Frontage Requirements
 - TOD Core (Ground floors of all buildings fronting on Type ‘A’ Streets and Plano Road built to Retail Ready Standards)
 - TOD Mixed Use (Ground floors of all buildings fronting on Type ‘A’ Streets and Renner Road built to Retail Ready Standards)
 - Arterial Mixed Use (Ground floors of all buildings fronting on Type ‘A’ Streets, Plano Road and Renner Road built to Retail Ready Standards)
 - Highway Mixed Use (Ground floors of all buildings fronting on PBPT service road and Plano Road built to Retail Ready Standards)
- Special Frontage Requirements
 - TOD Core (Main Street Frontage and Station Frontage requirements)
 - TOD Mixed Use (Neighborhood Frontage requirements)
 - Arterial Mixed Use (None)
 - Highway Mixed Use (Station Frontage requirements)
- Parking & Service Access
 - Parking Location
 - Off-Street Parking Standards
 - Driveways and Services
- Encroachments
 - Allowed for specific elements, require 8’ vertical clearance
- Applicability
 - Additional development standards

Building Design Standards (Section 8) are used to establish a coherent urban character, and includes the following standards:

- Building Orientation
- Design of Parking Structure
- Design of Automobile Related Building Site Elements
- Roof Form
- Façade Composition
- Windows and Doors
- Commercial and Mixed Use Building Materials
- Standards Specific to the Urban Neighborhood

The tables below depict the proposed building materials for the proposed development.

TOD Core, TOD Mixed Use, Arterial Mixed Use, Highway Mixed Use Building Materials

Proposed Building Material	Permitted by City Code	Bush Central Station
Ground Floor (Base)		
Masonry (brick, stone, stucco, cast stone, rock, marble, granite, curtain glass, glass block,)	Min. 85% of entire façade area (min. 80% per elevation) <ul style="list-style-type: none"> Proposed materials + split-face concrete block, pre-cast concrete panel EIFS not permitted below a height of 8' 	<ul style="list-style-type: none"> Min. 85% along a Type 'A' Street, Plano & Renner (+ stucco) Masonry or Accent when not along a Type 'A' Street, Plano & Renner EIFS not permitted along Type 'A' Street, Plano or Renner
Accent material (wood, architect metal, split-face concrete block, pre-cast concrete panel)	Max. 15% of entire façade area (max. 20% per elevation) <ul style="list-style-type: none"> Proposed materials EIFS not permitted below a height of 8' 	<ul style="list-style-type: none"> Max. 15% along a Type 'A' Street, Plano & Renner (+ architectural metal panel) EIFS not permitted along Type 'A' Street, Plano or Renner
Upper Floors (Middle and Cap)		
Masonry (brick, stone, stucco, cast stone, rock, marble, granite, curtain glass, glass block,)	Min. 85% of entire façade area (min. 80% per elevation) <ul style="list-style-type: none"> Proposed materials 	<ul style="list-style-type: none"> Permitted along streets and alleys EIFS limited to 10% on Type "B" Streets and alleys
Accent material (wood, architect metal, split-face concrete block pre-cast panel, cementitious-fiber clapboard with 50 year warranty and EIFS)	Max. 15% of entire façade area (max. 20% per elevation) <ul style="list-style-type: none"> Proposed materials EIFS not permitted below a height of 8' 	<ul style="list-style-type: none"> Permitted along streets and alleys EIFS limited to 10% on Type "B" Streets and alleys, not allowed on ground floors Clapboard limited to 15% along any street and alley, not allowed on ground floors
Roof material (copper, standing seam metal, slate, synthetic slate or similar materials)	Class 'C' Fire Classification (asphalt shingles, copper, standing seam metal roof, slate, synthetic slate or similar materials)	<ul style="list-style-type: none"> When visible from streets and alley (- asphalt shingles)

Neighborhood Frontage Building Materials (Residential)

Proposed Building Material	Permitted by City Code	Bush Central Station
Building Façade		
Masonry (brick, stone, man-made stone, stucco, cementitious-fiber clapboard with 50 year warranty)	Min. 85% of entire façade area (min. 80% per elevation) <ul style="list-style-type: none"> brick, stone, man-made stone, rock, marble, granite, glass block, split-face concrete block, 	<ul style="list-style-type: none"> Min. 85% along Type 'A' Streets EIFS not allowed on Type 'A' Streets or on ground floors
Accent material (architectural metal, rock, glass block, tile)	Max. 15% of entire façade area (max. 20% per elevation) <ul style="list-style-type: none"> Proposed + wood EIFS not permitted below a height of 8' 	<ul style="list-style-type: none"> Max. 15% architect metal panels or similar along streets EIFS not permitted along Type 'A' Streets or on ground floor Max. 10% EIFS on upper floors of side or rear façades
Roof material (asphalt shingles, copper, standing seam metal roof, slate, synthetic slate, terra cotta)	Class 'C' Fire Classification (asphalt shingles, copper, standing seam metal roof, slate, synthetic slate or similar materials)	<ul style="list-style-type: none"> When visible from streets and alley

- ❖ The sides and rear facades in the Urban Neighborhood are to be of finished quality and the same color that blend with the public street facades of the building.

Street & Streetscape Design Standards (Section 9) specify typical street configurations for the development in text and graphic form. The standards include specifications for:

- Right-of-way widths
- Vehicular lane width
- Parkway widths
- Number of travel lanes
- On-street parking
- Pedestrian accommodations

Signage Standards (Section 10) are established to ensure compatibility throughout the development. Except as specially listed in the proposed Sign Standards section of the Code, all other signage shall comply with the City's Sign Code. For a detail list, please refer to Table 10.1 of the proposed Code. In general the following types of sign are proposed:

- Wall (Building)
- Monument
- Window
- Building Blade
- Tenant Blade
- Marquee
- For Sale/For lease
- Address
- Temporary
- Banner
- Sandwich Board
- Light Pole Banner
- Directory
- LED Signage (requires lens covers or diffuser)

Civic/Opens Space (Section 11) creates a network of open spaces and standards that recognizes the natural qualities of the area while providing a range of both passive and active recreational opportunities.

- Park
- Green
- Square
- Plaza
- Pedestrian Passage
- Multi-Use Trail
- Playground
- Ancillary Structure

On November, 7, 2010, the Commission and City Council met at a Joint Work Session to receive a presentation from staff and the applicant's on the proposed project. As a result of the work session, the following items were discussed which needed to be considered by the applicant:

The following have been resolved and the Code has been amended accordingly.

- Require major modifications to be reviewed by the City Council (CC), after a City Plan Commission (CPC) recommendation...*The Code and Development Process Flow Chart been amended to reflect required action.*
- Prohibit EIFS as an allowed building material...*The code has been amended to prohibit along Type 'A' streets and ground floors and limit EIFS to 10% on upper floors.*
- Permit LED lighting for lighting and signage...*The code has been amended to require LED's to be covered by a lens or diffuser.*
- "Supergraphics" allowance...*The City's Sign Ordinance which does not allow "supergraphics" will supersede.*

The following were discussed with the applicant; however, they remain as items for discussion:

- Provide caps on density (FAR), or limits/phasing restrictions on residential development.
- Require minor modification to be approved by CPC
- Incorporate "green" design standards. (*Response: appropriate for CC&Rs*)
- Incorporate iconic elements/branding for the development. (*Response: appropriate for CC&Rs*)
- Incorporate additional architectural/design controls. (*Response: appropriate for CC&Rs*)

The following elements have been identified by staff that may require further refinement:

- Development Rights as they relate to the TIA
The Regulating Plan and the Code do not designate density and/or intensity values, which is common for a Form Based Code not to include. However, the appropriateness of the roadway network for the development is based on the land use assumption used in the TIA's findings. Establishing maximum number of units and maximum non-residential square footage could be considered as assumed in the applicant's TIA.
- Establishing minimum building heights, especially in the TOD Core Character Zone.
Although maximum building heights are indicated on the Regulating Plan and Code, there are no assurances that buildings will not be constructed at similar heights throughout the development. Establishing minimum building heights could be considered.
- Civic/Open Space Standards - Park Standards
The applicant intends to utilize the area located adjacent to and along the DART rail line; designated on the Regulating Plan as "Park", to meet the need of the residents of the development for active recreation opportunities. The subject site, with the exception of a multi-use trail that will be construction on the site is designated to be preserved. In addition, the Regulating Plan designates a 0.38-0.75 acre "Mandatory Green" located north of Renner Road, which may be impacted with the preservation of mature trees.

Throughout the development passive recreational opportunities will be provided, which will be in the forms of urban plazas/opens space. The details of either active or passive recreational areas will be further defined at the time of development.

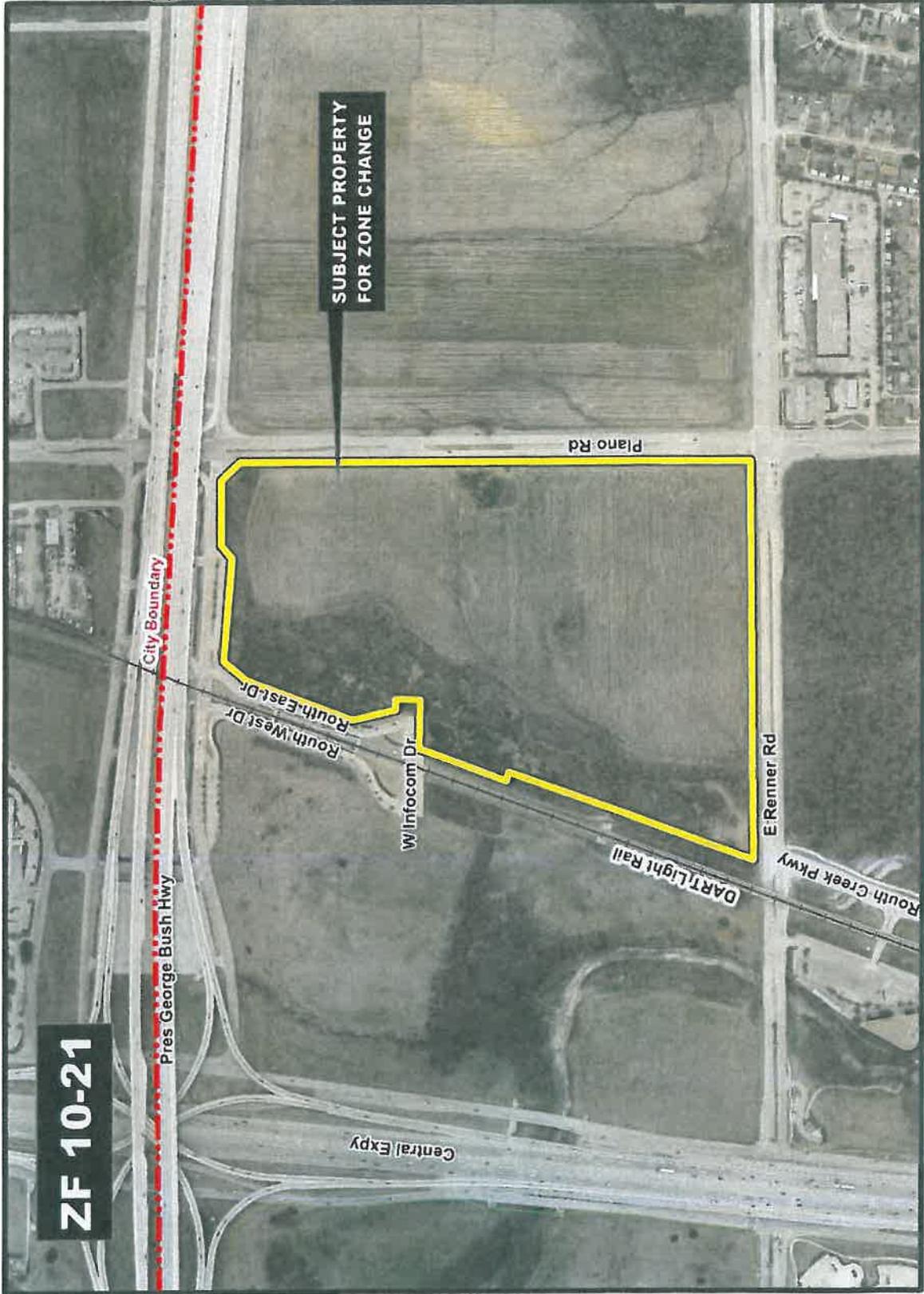
Conclusion

Based on staff's review of the applicant's request, the proposal of a Form Based Code appears to be appropriate for the site. The code achieves a predictable community vision through its regulatory nature. It achieves a predicable physical result by its concentration on the visual aspect of the development through building heights, façade treatment and the relationship of the building to the street (pedestrian friendly) through compact, walkable urbanism. If applied appropriately, it could provide the community with the opportunity to respond to market demands in an expedient and predictable manner.

Correspondence: See attached correspondence.

Motion: On December 21, 2010, the Commission on a vote of 5-2 recommended approval of the request, subject to the following special conditions:

1. The subject site shall be zoned PD Planned Development and shall be developed in accordance with the Bush Central Station Planned Development Code attached hereto as Exhibit "D".
2. The minimum percentage of masonry shall be 85% and maximum percentage of accent material shall be 15%.



ZF 10-21

**SUBJECT PROPERTY
FOR ZONE CHANGE**



ZF 10-21 Aerial Map

Updated By: shahzadeh.ghassemi, Updated Date: December 17, 2019
File: C:\sw\app\p\cas\zf\2019\ZF 10-21\ZF 10-21 callout.mxd



President George Bush Tpk

DART Light Rail

Plano Rd

Renner Rd

South Creek Pkwy

Subject Property

Subject Property
Looking Northwest

Explanation and Description of Request

The purpose of the Bush Central Station and the corresponding Planned Development Code is to support development of the area into a pedestrian-oriented, mixed-use urban development environment, with convenient access to rail transit, shopping, employment, housing, and regional retail services.

The Bush Central Station will foster the development of a major regional employment center with significant regional retail and residential uses within convenient walking distance from the existing transit station and potential transfer station to the Cotton Belt rail line. Development within this area would be of high intensity, accommodating large scale office and retail users while providing for moderate scale mixed use and residential within portions of the Bush Central Station.

The Bush Central Station and Planned Development Code are created to support economic development, sustainable tax base, and job creation by establishing adjacency predictability of private development that supports and leverages investment in and around the existing transit stop. The standards established provide a high level of prescription on the building form and design with flexibility on the mix of uses.

Bush Central Station implements the vision for the transit-oriented development in the City's Comprehensive Plan and the Regulating Plan (Appendix A). The Regulating Plan provides guidance to property owners, developers, and the City on the form, character, and intensity of future development within Bush Central Station. Creation of different Character Zones within Bush Central Station enables specific site and locational standards to be enumerated and applied. Clear graphic standards are provided for location, height, and building elements. Such standards promote sustainability, public welfare, walkable mixed use development, housing variety and transportation choice.

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Section 1. Purpose and Intent:

The purpose of the Bush Central Station Planned Development Code, hereafter known as the BCS-PD Code, is to support development of the Bush Central Station into a pedestrian-oriented, mixed-use urban development environment, with convenient access to rail transit, shopping, employment, housing, and regional retail services. The goal of the Bush Central Station is to promote an efficient, compact land use pattern; encourage pedestrian activity; reduce the reliance on private automobiles; and provide a more functional and attractive community through the use of recognized principles of urban design.

- 1.1 Economic Development – The Bush Central Station and corresponding standards are created to support economic development, sustainable tax base, and job creation by establishing adjacency predictability of private development that supports and leverages investment in and around the Bush Central Station.
- 1.2 Implement the Design Goals of the Bush Central Station – The objective of the Bush Central Station is to foster a major regional employment center with significant regional retail and residential uses within convenient walking distance from the existing transit station and potential transfer station to the Cotton Belt rail line. The existing transit station provides a connection to 12 member cities of Dallas Area Rapid Transit (DART) including Dallas. Development within this area would be of high intensity, accommodating large scale office and retail users while providing for moderate scale mixed use and residential within portions of the Bush Central Station.
- 1.3 Establish Specific Development Standards – The BCS-PD Code implements the vision for the Bush Central Station as established in the City’s Comprehensive Plan and the Bush Central Station Regulating Plan, hereafter known as the Regulating Plan (Appendix A). The Regulating Plan shall provide guidance to property owners, developers, and the City on the form, character, and intensity of future development in the Bush Central Station. Creation of different Character Zones within Bush Central Station enables specific site and locational standards to be enumerated and applied. Clear graphic standards are provided for location, height, and building elements. Such standards promote sustainability, public welfare, walkable mixed use development, housing variety and transportation choice.

Section 2. Components of the Code:

- 2.1 This BCS-PD Code shall apply to the Bush Central Station unless otherwise specified in this Code. Development of property within the Bush Central Station shall comply with all applicable development standards set forth in this Code. The components of this BCS-PD Code consist of:
 - 2.1.1 Bush Central Station Regulating Plan: The Bush Central Station Regulating Plan, hereafter known as the “Regulating Plan”, is its official zoning map. It identifies the applicable character zones within the Bush Central Station including:
 - i. Character Zones – The Bush Central Station is divided into different “Character Zones”. A Character Zone creates a distinct urban form within that Zone which is different from urban forms in other Character Zones. Each Character Zone shall establish use and development standards including height, bulk, building and parking location, and functional design. The Regulating Plan classifies all lots within the Bush Central Station into one of four Character Zones.
 - ii. Special Frontage Standards – The Special Frontage Standards establish exceptions and special conditions for all buildings along designated frontages. Special Frontage Standards may be Mandatory or Non-Mandatory. Mandatory Special Frontage Standards shall apply in addition to the underlying Character Zone standards. Non-Mandatory Special Frontage Standards may be applied at the election of the developer and are not required.

- iii. Street Designations by Street Type– The Street Designations illustrate the design, configurations, and development context for all streets within the Bush Central Station. The street classification addresses vehicular lane widths, number of lanes, pedestrian accommodation, street tree requirements, on-street parking, and parkway and median standards (streetscape standards). In addition, streets are distinguished by the appropriate development context by denoting them on the Regulating Plan as Type ‘A’ or Type ‘B’ Streets.
- iv. Mandatory Street Network – The Mandatory Street Network specifies the future streets needed to implement the Bush Central Station Regulating Plan. The Mandatory Streets shall be required and shall generally meet the locational and connectivity goals of the Regulating Plan. Their design shall be guided by the Street Type Specifications.
- v. Mandatory Civic/Open Space – The Mandatory Civic/Open Space areas shown on the Regulating Plan designate the locations of proposed civic/open spaces (including parks, plazas, greens, and squares).
- vi. Non-Mandatory Street Network – The Non-Mandatory Street Network indicates locations of suggested, but not required streets within the Bush Central Station Regulating Plan. These streets are not mandatory, but at the election of the developer and their design shall be guided by the Street Type Specifications.
- vii. Non-Mandatory Civic/Open Space – The Non-Mandatory Civic/Open Space designation indicates the locations of desired, but not required civic/open spaces (including parks, plazas, greens, and squares) to implement the Regulating Plan. Non-Mandatory Civic/Open Spaces are not required, but at the election of the developer.
- viii. Special Destination – The Special Destination indicates an area on the Regulating Plan identified for specific uses with applicable standards as established in Section 5.7 of this Code.

2.1.2 Development Standards: The BCS-PD Code (the text portion of this Code) enumerates the development standards with text and graphics for Character Zones, Special Frontage, building form, civic open space, landscape, building design, signage, lighting, and all related standards for all streets, public and private development.

Section 3. Administration

This section sets forth the provisions for reviewing and approving development applications within the Bush Central Station. The intent is to ensure that all development is consistent with the provisions of this Code. All sections of this Code shall be applied during the review process.

3.1 The development standards under Articles XIII-A thru XVI and Article XXI-C of the City of Richardson Comprehensive Zoning Ordinance, as amended, shall not apply to the Bush Central Station except as specifically referenced herein. Development standards not addressed in this ordinance shall be governed by the City of Richardson Comprehensive Zoning Ordinance to the extent they are not in conflict with the intent or text of the BCS-PD Code.

3.2 Sign Standards under Chapter 18, as amended, of the City of Richardson Code of Ordinances, shall not apply to the Bush Central Station except as specifically referenced herein.

3.3 Using this Code:

The following basic steps should be followed to determine the uses and development standards applicable on property within the Bush Central Station:

3.3.1 Locate the subject property on the Bush Central Station Regulating Plan.

3.3.2 Identify:

- i. the Character Zone in which the property is located;
 - ii. the Street Type designation along all its street frontages; and,
 - iii. any Special Frontage Requirements that may be applicable to the subject property.
- 3.3.3 Review the Schedule of Uses by Character Zone as listed in Table 6.1 to determine allowed uses.
- 3.3.4 Examine the corresponding zone standards in the Building Form and Development Standards in Section 7 to determine the applicable development standards.
- 3.3.5 Refer to Section 5.5 and 5.6 for Special Frontage Standards.
- 3.3.6 Refer to Section 8 for Building Design Standards.
- 3.3.7 Refer to Section 9 for Street Type and Streetscape Standards.

The information from the above listed steps explains where the building will sit on the lot, the limits on its three dimensional form, the range of uses, and the palette of materials that will cover it.

- 3.4 Development within the Bush Central Station that complies with the provisions of this Code shall follow the City's development process as outlined in Chapter 21, Article II of the City of Richardson's Code of Ordinances and shall be approved by the City Manager or designee (see Appendix C for flow chart of the review process). In addition to complying with applicable City regulations that are not in conflict with this Code, the applicant shall provide the information required to adequately show compliance with this Code.
- 3.5 Standard for Approval of development plans: If a development plan conforms to the standards set forth in this Code and applicable City regulations not in conflict with this Code, the development plan shall be approved. Upon request by an applicant, the authority charged with approving the development plan shall certify the reason(s) for an action taken on a development plan.
- 3.6 The City Manager or designee shall be responsible for the following:
- 3.6.1 Reviewing development plan applications for compliance with the requirements of BCS-PD Code.
 - 3.6.2 Approving development plan applications that are in compliance with the requirements of the BCS-PD Code.
 - 3.6.3 Making determinations on the applications and interpretations of standards in this Code.
 - 3.6.4 Approving revisions to previously approved development plans that comply with this Code and all applicable city ordinances.
 - 3.6.5 Approving any minor modifications to the approved Regulating Plan per Section 3.8.
 - 3.6.6 Recommendations on any SDP applications to the City Plan Commission (CPC) and City Council.
- 3.7 Special Development Plans: A request for a modification to any of the standards of this Code other than minor modifications permitted under Sections 3.8 shall be reviewed and processed as Special Development Plans.
- 3.7.1 Special Development Plans (SDP) are intended to allow applicants development flexibility to address specific market opportunities and/or contexts. An application for a Special Development Plan shall be processed as an amendment to the zoning ordinance under Article XXIX of the City of Richardson Zoning Ordinance and may only be considered by the City Council (CC) after the CPC has made a recommendation. The City Manager or designee shall review, make recommendations on any SDPs, and shall forward all SDP applications to the CPC. In evaluating an SDP, CC may consider the extent to which the application meets any of the following:
- i. provides an alternative "Master Plan" approach by consolidating multiple properties to create a predictable, market responsive development for the area; or

ii. fits the adjoining context by providing appropriate transitions.

3.8 Minor Modifications to the BCS-PD Code:

The City Manager or designee shall have the authority to approve a request for minor modifications to BCS-PD Code that:

- 3.8.1 Does not materially change the circulation and building location on the site;
- 3.8.2 Does not increase the building area permitted under this Code;
- 3.8.3 Does not change the relationship between the buildings and the street;
- 3.8.4 Does not allow a use not otherwise authorized in this Code;
- 3.8.5 Does not allow greater height of any building or reduction of any parking requirement established in this Code; or
- 3.8.6 Changes to established street cross sections per Table 3.1 below and Section 9 of this Code.

The City Manager or designee shall also have the authority to approve minor modifications outlined in Table 3.1. Any appeals to the decisions of the City Manager on minor modifications shall be heard by the City Council. Any City Council denials of minor modifications or any changes beyond those that meet the criteria above, the thresholds established in Table 3.1, shall be processed as an amendment to this Code under Article XXIX of the City of Richardson Comprehensive Zoning Ordinance.

Table 3.1 Minor Modifications Allowed

<i>Standard</i>	<i>Minor Modification Allowed</i>	<i>Comments</i>
Area/boundary of Character Zones (including any Mandatory Civic/Open Spaces)	No more than a 15% change (increase or decrease) in the area of any Character Zone (aggregate or per block)	<ul style="list-style-type: none"> • Shall not eliminate any Character Zone • 15% measurement shall be based on the total area of that specific Character Zone within the entire Bush Central Station • For the purposes of this code, any increase in any Character Zone that results from the elimination of a non-mandatory street shall not count towards the 15% threshold.
Location of any Mandatory Street	Location shall not move more than 100' in any direction	<ul style="list-style-type: none"> • Shall maintain the connectivity intended by the Regulating Plan
Building Form and Development Standards		
<ul style="list-style-type: none"> • Street Setback Line 	No more than a 10% change in the SSL as established in the corresponding Character Zone Building Form and Development Standards	<ul style="list-style-type: none"> • Changes to the Street Setback Lines may only be due to any changes to the street cross sections or change in the width of a sidewalk • In no case shall the sidewalk be less than 6 feet in width.
<ul style="list-style-type: none"> • Build to zones/setbacks 	No more than a 20% change in the maximum or minimum setback.	
<ul style="list-style-type: none"> • Building Frontage 	No more than a 15% reduction in the required building frontage along each block of a Type 'A' Street	<ul style="list-style-type: none"> • Any reduction in the required building frontage shall be to accommodate Porte-cocheres for drop-off and pick-up.
<ul style="list-style-type: none"> • Street screen 	Waiver of Street screen requirement along a Type 'B' Street	<ul style="list-style-type: none"> • Requirement for a street screen may only be waived along a Type 'B' Street along the frontage of any interim surface parking lot (off-street) that is intended to be in-filled with a parking structure. • In no case shall any portion of the surface parking have frontage along a Type 'A' Street without a required street screen • In no case shall the (off-street) surface parking lot be located at a street intersection for a minimum depth of 30' along each street (regardless of the Street Type).
Street Cross Sections	Cross sections of new streets may be adjusted with respect to number of lanes, lane widths, on-street parking configuration, pedestrian accommodation, and street tree planting	<ul style="list-style-type: none"> • Any changes in the street cross sections shall be based on specific development context such as vegetation, natural features, drainage, and fire access and is subject to approval by the City.

Section 4. Definitions

In addition to Definitions in Article I of the City of Richardson Comprehensive Zoning Ordinance, the following terms shall have the corresponding interpretations.

Arcade means a portion of the main façade of the building that is at or near the Street-Setback Line and a colonnade supports the upper floors of the building. Arcades are intended for buildings with ground floor commercial or retail uses and the arcade may be one or two stories.

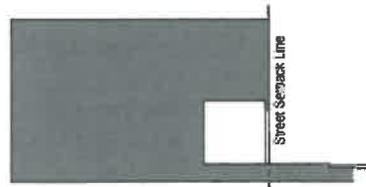


Image of an arcade

Attics/Mezzanines means the interior part of a building contained within a pitched roof structure or a partial story between two main stories of a building.

Auto-Related Sales and Service Uses means establishments that provide retail sales and services related to automobiles including, but not limited to, cars, tires, batteries, gasoline, etc.

Block Face Dimensions means the linear dimension of a block along one of its street frontages.

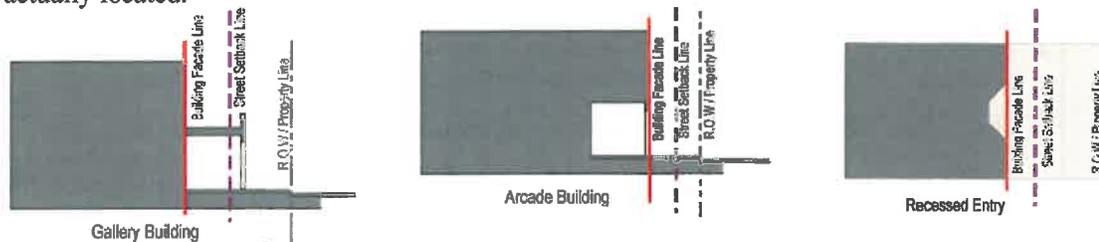
Block Perimeter means the aggregate dimension of a block along all of its street frontages.

Block means the aggregate of lots, pedestrian passages and rear alleys, circumscribed on all sides by streets.

Build-to Line means the line at which the principal building's front façade shall be built.

Build-to Zone (BTZ) means the area within which the principal building's front façade is to be built.

Building Façade Line means the vertical plane along a lot where the building's front façade is actually located.



Building Façade Line Illustrations

Building Form Standards means the standards established for each Character Zone that specify the height, bulk, orientation, and elements for all new construction and redevelopment.

Building Frontage means the percentage of the building's front façade that is required to be located at the front Build-to Line or Zone as a proportion of the block frontage along that public street. Parks, plazas, squares, improved forecourts, and pedestrian breezeway frontages shall be considered as buildings for the calculation of building frontage.

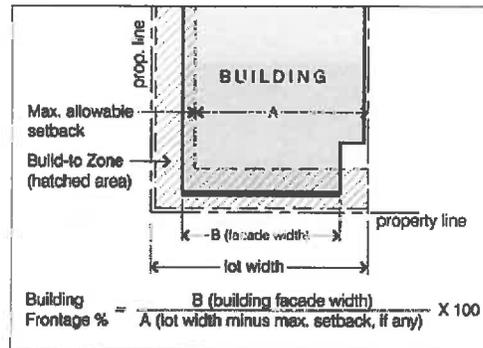


Image showing how a lot's building frontage is calculated.

Character Zone means an area within the Bush Central Station that creates a distinct urban form different from other areas within the Bush Central Station. Character Zones are identified in the Regulating Plan.

City Manager means the City Manager of the City of Richardson or his/her designee.

Civic/Open Space means publicly accessible open space in the form of parks, courtyards, forecourts, plazas, greens, pocket parks, playgrounds, etc. They may be privately or publicly owned. For all residential uses, privately accessible open spaces such as courtyards, porches, and balconies may also be considered as Civic/Open Space for the purposes of this ordinance. Building façades facing a Civic/Open Space shall be treated as a Type 'A' Street frontage.

Green means a civic/open space intended for unstructured recreation, spatially defined by landscaping rather than building frontages.

Park means a civic/open space that is a preserve largely available for unstructured recreation.

Plaza means a primarily hardscaped civic/open space with formal landscaping, available for civic purposes and commercial activities. A plaza shall be spatially defined by buildings.

Playground is a civic/open space designed and equipped for children's recreation. A playground may be fenced and may include an open shelter. Playgrounds may be located within residential areas and may be placed within a block. They may be included in other civic/open spaces.

Comprehensive Plan means the City of Richardson Comprehensive Plan that establishes the blueprint for the long-term growth and development of the City as adopted on the effective date of this Code.

Development Plan means any submittal as defined by Chapter 21, Article II of the City of Richardson's Code of Ordinances.

Encroachment means any structural or non-structural element such as a sign, awning, canopy, terrace, or balcony that breaks the plane of a vertical or horizontal regulatory limit, extending into a Setback, into the Public R.O.W, or above a height limit.

Gallery means an extension of the main façade of the building that is at or near the front property line and the gallery may overlap the public sidewalk.

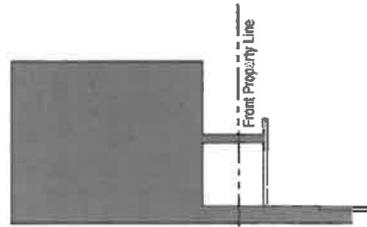


Image of a Gallery

Kiosk means a small temporary or permanent structure often open on one or more sides used for sales in civic/open spaces.

Live-Work Unit means a dwelling unit that is also used for work purposes, provided that the ‘work’ component is restricted to the uses of professional office, artist’s workshop, studio, or other similar uses and is located on the street level and constructed as separate units under a condominium regime or as a single unit. The ‘live’ component may be located on the street level (behind the work component) or any other level of the building. Live-work unit is distinguished from a home occupation otherwise defined by this ordinance in that the work use is not required to be incidental to the dwelling unit, non-resident employees may be present on the premises and customers may be served on site.

Living Screen means a Street Screen composed of landscaping in the form of vegetation.

Main Street Frontage means the special frontage requirement along identified Type A Street frontages as indicated in the Regulating Plan.

Master Sign Plan means a unique sign plan to implement a specific vision for a portion or all of the development that meets Section 10.2 of this Code.

Minor Modification means any changes to the BCS-PD Code that meet the threshold criteria established in Section 3.8 and Table 3.1.

Non-Mandatory Civic/Open Space means plaza, green, square, or park area designated on the Regulating Plan which is shown as a suggested feature within the Bush Central Station. The Non-Mandatory Plaza is not required and is at the election of the developer.

Non-Mandatory Neighborhood Frontage means the special frontage option to maintain a neighborhood compatible frontage as indicated in the Regulating Plan. The Non-Mandatory Neighborhood Frontage is not required and is at the election of the developer.

Non-Mandatory Pedestrian Passage means a pedestrian access area designated on the Regulating Plan which is shown as a suggested feature within the Bush Central Station. The Non-Mandatory Pedestrian Passage is not required and is at the election of the developer.

Non-Mandatory Street means a street that is designated on the Regulating Plan which is shown as a suggested street within the Bush Central Station. The Non-Mandatory Street is not required and is at the election of the developer.

Pedestrian Easement means a grant of use of private property for pedestrian access and use.

Regulating Plan means the Zoning Map attached hereto as Appendix A that shows the Character Zones, Civic Spaces, location of Special Frontages, Streets, and other Special Requirements applicable to the Bush Central Station subject to the standards in this Code. For the purposes of this Code, the Regulating Plan shall also be the Concept Plan for the Bush Central Station.

Residential Loft means a residential unit that is built to Retail Ready standards and adapted for residential uses.

Retail Ready means space constructed at a minimum interior height of 14 feet which may be used for noncommercial uses and can be converted into retail/commercial use. Prior to the issuance of a certificate of occupancy for a retail/commercial use in a Retail-Ready space, the space must comply with all building and construction codes for that use. The intent of Retail-Ready space is to provide the flexibility of occupying a space in accordance with market demand and allowing the use in such space to change to retail/commercial uses accordingly.

Retail Sales Retail establishments are the final step in the distribution of merchandise. They are organized to sell in small quantities to many customers. Establishments in stores operate as fixed point-of-sale locations, which are designed to attract walk-in customers. Retail establishments often have displays of merchandise and sell to the general public for personal or household consumption, though they may also serve businesses and institutions. Some establishments may further provide after-sales services, such as repair and installation. Included in, but not limited to this category, are durable consumer goods sales and service, consumer goods, other grocery, food, specialty food, beverage, dairy, etc, and health and personal services.

Service Uses means a category for limited personal service establishments which offer a range of personal services that include (but not limited to) clothing alterations, shoe repair, dry cleaners, laundry, health and beauty spas, tanning and nail salons, hair care, etc.

Sign, Building Blade means a pedestrian-oriented sign that is affixed perpendicular to the corner of a building or to the front façade of a building above the ground floor to provide identification for the whole building.



Image of a Building Blade Sign

Sign, Marquee means a sign structure placed over the entrance to a theatre or other public gathering venue. It has signage stating either the name of the establishment or the name of the event, artist, and other details of the event appearing at that venue. The marquee is often identifiable by a surrounding cache of light bulbs, usually yellow or white, that flash intermittently or as chasing lights. Marquee signs may often be combined with Building Blade signs.



Image of a Marquee sign with a Building Blade Sign

Sign, Monument means any sign which is connected to the ground and which has no clear space for the full width of the sign between the bottom of the sign and the surface of the ground. A monument sign may include a sign face and sign structure, and may also include a sign base and sign cap.



Image of a Monument Sign

Sign, Sandwich Board means a portable sign consisting of two panels of equal size, which are hinged at the top or one panel with a support and placed on the ground or pavement so as to be self-supporting.



Images of sandwich board signs.

Sign, Tenant Blade means a smaller pedestrian-oriented sign that is affixed perpendicular to the building façade under a canopy or awning or immediately over a tenant space and provides identification for individual tenants within a building.



Image of a Tenant Blade Signs

Slip Road means a drive that extends within the property and parallel to either Renner Road or Plano Road. The Slip Road is generally located within close proximity to these main roads as identified on the Regulating Plan.

Slip Road Frontage means the frontage along a Slip Road.

Special Development Plan means a development application that meets Section 3.7 of this Code.

Special Frontage Requirements means standards applied to certain blocks as indicated in the Regulating Plan in order to address specific requirements and transitions based on street frontage and adjacency in addition to the underlying Character Zone standards.

Station Platform Frontage means the special frontage requirement on blocks with frontage along the rail station platform as indicated in the Regulating Plan.

Street-Setback Line means the imaginary line located at the outside edge of the sidewalk and measured from the center line of the street from which all build-to lines and build-to zones are measured from.

Street Screen means a freestanding wall or living screen built along the BTZ or in line with the building façade line along the street. It may mask a parking lot or a loading/service area from view or provide privacy to a side yard and/or strengthen the spatial definition of the public realm.



Image of a combination masonry and living street screen

Street Type means a specific designation for streets in the Bush Central Station that establishes a certain character and cross-sections to improve walkability within the Bush Central Station.

Street Network means the Mandatory and Non-Mandatory network for new and existing streets within the Bush Central Station as established in the Regulating Plan.

Transition Zones means the areas with specific adjacencies within which certain limitations on building heights apply as established for each Character Zone.

Tree Well means an unpaved area around the trunk of a tree within the sidewalk area that is either landscaped with ground cover or covered with a tree grate.



Example of a tree well with a tree grate Example of a tree well with landscaping

Type 'A' Street means the streets identified as such on the Regulating Plan. Type 'A' Streets are the primary pedestrian streets and buildings along Type 'A' Streets shall be held to the highest standard of pedestrian-oriented design.

Type 'B' Street means the streets identified as such on the Regulating Plan. Type 'B' Streets are intended to primarily accommodate access to parking, service, and other auto-related functions.

Section 5. The Regulating Plan

5.1 The Regulating Plan (Appendix A) is hereby adopted as the official zoning map for the Bush Central Station.

5.2 Character Zones Established – the following Character Zones are established. The boundaries of the specific Character Zones shall be established in the Regulating Plan.

5.2.1 TOD Core: The TOD Core provides the most opportunity for the highest intensity development. It is the area that has significant development impact and the highest pedestrian activity due to its adjacency to the transit station. The TOD Core consists of the highest density and height, with the greatest variety of uses. Development within the TOD Core Zone shall meet the Building Form and Development Standards in Section 7.1 of this Code.

5.2.2 TOD Mixed Use: The TOD Mixed Use is the area adjacent to the TOD Core that is intended for high intensity commercial and residential uses in addition to supporting retail and restaurant uses. Development within the TOD Mixed Use Zone shall meet the Building Form and Development Standards in Section 7.2 of this Code.

5.2.3 Arterial Mixed Use: Arterial Mixed Use is intended to provide appropriate transitions to major regional roadways while taking advantage of the arterial frontage for limited auto-oriented sites. The Arterial Mixed Use frontage also acts as a gateway into the Bush Central Station by providing an attractive street frontage for the overall development. Development within the Arterial Mixed Use Zone shall meet the Building Form and Development Standards in Section 7.3 of this Code.

5.2.4 Highway Mixed Use: Highway Mixed Use is intended to provide an appropriate transition into the Bush Central Station from the President George Bush Turnpike (SH 190) access road. This area is also intended for high intensity development. Development within the Highway Mixed Use Zone shall meet the Building Form and Development Standards in Section 7.4 of this Code.

- 5.3 Street Designations By Street Type Established – The Regulating Plan shall establish the following Street Designations.
- 5.3.1 Type ‘A’ Streets Established – Type ‘A’ Streets are intended to be the primary pedestrian streets and buildings along Type ‘A’ Streets shall be held to the highest standard of pedestrian-oriented design. The Type ‘A’ Streets are TOD Main Street, TOD Street Type A, Parkway, Park Avenue and Court Street as identified in the Regulating Plan.
- 5.3.2 Type ‘B’ Streets Established – Type ‘B’ Streets are intended to balance pedestrian orientation with automobile orientation. Buildings along Type ‘B’ Streets may be permitted to accommodate some service and auto-related functions. The Type ‘B’ Streets are TOD Avenue and TOD Street Type B as identified in the Regulating Plan. In addition, Slip Roads provide a functional transition from the suburban arterials that bound the Bush Central Station to the urban street network offered within the development. The Slip Roads are designated along Plano Road and Renner Road and shall be considered as Type ‘B’ Streets for the purpose of development frontage standards.
- 5.4 Mandatory Streets by Street Type – The Street Network indicates Mandatory and Non-Mandatory streets needed to implement the Bush Central Station Regulating Plan. The Regulating Plan designates the type, classification, and location of streets. All new streets in the Bush Central Station shall meet the street design standards established in Section 9 herein.
- 5.5 Mandatory Special Frontage Requirements – In order to address specific requirements and transitions based on street frontage and adjacency, the following Special Frontage Requirements as established in the Regulating Plan shall apply:
- 5.5.1 Main Street Frontage: Ground floors of all buildings designated as Main Street Frontage on the Regulating Plan shall not be occupied by residential units and/or lodging rooms in hotels to a minimum depth of 30 feet as measured from the front building line.
- 5.5.2 Station Platform Frontage: Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.
- 5.6 Non-Mandatory Neighborhood Frontage Requirements – In order to allow the development of a unique neighborhood enclave of brownstones, townhomes and live-work units within the Bush Central Station, the blocks along the Park Avenue have an optional Neighborhood Frontage designation for a minimum depth of 30 feet from the front building façade line. All standards of the underlying Character Zone shall apply to the blocks designated as Neighborhood Frontage with the exception of height which shall be limited to a maximum of sixty (60) feet along this frontage. If the Non-Mandatory Neighborhood Frontage is developed on any block or portion of a block designated as such, it shall then become Mandatory for those blocks that face the developed blocks and designated as Neighborhood Frontage on the Regulating Plan.
- 5.7 Special Destination – The following building and site standards shall apply to areas designated as Special Destination on the Regulating Plan:
- 5.7.1 Maximum permitted building height shall be two (2) stories.
- 5.7.2 Only “Destination” uses shall be permitted. Destination uses shall include restaurants, cafes, entertainment venues and accessory retail with no drive through service.
- 5.7.3 Off-street parking shall not be permitted between the building and any Type ‘A’ Street. However, driveway access to parking may be permitted off Type ‘A’ or ‘B’ Streets.
- 5.7.4 Building pad and surface parking shall be located to maximize the preservation of trees.
- 5.7.5 Off-street parking may be met through a combination of on-site surface parking and shared parking within the Bush Central Station.
- 5.7.8 Valet parking circulation drives may be permitted from Type ‘A’ Streets.

- 5.7.9 Service areas shall not have frontage along Type ‘A’ Streets. Service areas shall be located along the back or side of the principal building.
- 5.7.10 All restaurant uses shall provide outdoor seating/patio.
- 5.7.11 The area occupied by any impervious surfaces (buildings or parking) shall not exceed the area identified on the regulating plan as Special Destination.
- 5.7.12 Uses permitted within the Special Destination area are:
 1. Accessory Use only - Retail Sales or Service with no drive through facility (includes alcohol sales which shall meet Chapter 4, Alcoholic Beverages of the City of Richardson Code of Ordinances). Excluded from this category are Auto-Retail Sales and Service Uses (see Section 4 of the Code for Definition of Retail, Service uses, and Auto-related Sales and Service).
 2. Food Service Uses such as full-service restaurants, cafeterias, bakeries and snack bars with no drive through facilities Included in this category is café seating within a public or private sidewalk area with no obstruction of pedestrian circulation. Also included in this category is the sale of alcoholic beverages which shall meet Chapter 4, Alcoholic Beverages of the City of Richardson Code of Ordinances.
 3. Theater, cinema, dance, music or other entertainment establishment
 4. Sales from kiosks
 5. Incidental Outdoor Display (subject to standards in Section 7 of the Code)
 6. Special Events

5.8 Civic/Open Space – The Regulating Plan indicates Mandatory and Non-Mandatory Civic/Open Spaces. The specific standards for Civic/Open Space are established in Section 11.

Section 6. Schedule of Permitted Uses

6.1 Generally: Due to the emphasis on urban form over land uses in the Station Areas, general use categories have been identified by character zones. Uses not listed in the following schedule, but are substantially similar, may be permitted upon the approval of the City Manager or designee, subject to appeal directly to the City Council.

6.2 Schedule of Uses:

Table 6.1

Character Zone	TOD Core	TOD Mixed Use	Arterial Mixed Use & Highway Mixed Use
Land Use			
Commercial Uses (Office, Retail, Sales & Service Uses)			
Retail Sales or Service with <u>no drive through facility</u> (includes alcohol sales which shall meet Chapter 4, Alcoholic Beverages of the City of Richardson Code of Ordinances). Excluded from this category are Auto-Retail Sales and Service Uses (see Section 4 of the Code for Definition of Retail, Service uses, and Auto-related Sales and Service)	P	P	P
Finance, Insurance, and Real Estate establishments including banks, credit unions, real estate, and property management services, <u>with no drive through facility</u>	P	P	P
Offices for business, professional, and technical uses such as accountants, architects, lawyers, doctors, etc.	P	P	P
Research laboratory headquarters, laboratories and associated facilities	P	P	P
Food Service Uses such as full-service restaurants, cafeterias, bakeries and snack bars with <u>no drive through facilities</u> Included in this category is café seating within a public or private sidewalk area with no obstruction of pedestrian circulation. Also included in this category is the sale of alcoholic	P	P	P

Character Zone	TOD Core	TOD Mixed Use	Arterial Mixed Use & Highway Mixed Use
beverages which shall meet Chapter 4, Alcoholic Beverages of the City of Richardson Code of Ordinances.			
Any use with a drive through facility	P/C	P/C	P/C
Auto-related Sales and Service	NP	NP	P/C
Arts, Entertainment, and Recreation Uses			
Art galleries	P	P	P
Art, antique, furniture or electronics studio (retail, repair or fabrication; excludes auto electronics sales or service)	P	P	P
Games arcade establishments	P	P	P
Theater, cinema, dance, music or other entertainment establishment	P	P	P
Museums and other special purpose recreational institutions	P	P	P
Fitness, recreational sports, gym, or athletic club	P	P	P
Parks, greens, plazas, squares, and playgrounds	P	P	P
Educational, Public Administration, Health Care and Other Institutional Uses			
Business associations and professional membership organizations	P	P	P
Child day care and preschools	P	P	P
Schools, libraries, and community halls	P	P	P
Universities and Colleges	P/C	P/C	P
Hospital	P	P	P
Civic uses	P	P	P
Social and fraternal organizations	P	P	P
Social services and philanthropic organizations	P	P	P
Religious Institutions	P	P	P
Funeral homes	P	P	P
Residential Uses			
Home Occupations	P/A	P/A	P/A
Multi-family residential			
Ground Floor	P/C	P	P/C
Upper Floors	P	P	P
Residential Lofts	P	P	P
Single-family residential attached dwelling unit (Townhomes)	P/C	P/C	P/C
Live-work unit	P	P	P
Other Uses			
Model homes for sales and promotion**	P	P	P
Hotels	P	P	P
Parking, surface (primary use of property)	NP	NP	P
Parking, surface (accessory use of property)	P	P	P
Parking, structured	P	P	P
Private attached garage	NP	NP	NP
Private detached garage	NP	NP	NP
Sales from kiosks	P	P	NP
Veterinary clinic	NP	P	P
Community garden	P	P	P
Incidental Outdoor Display (subject to standards in Section 7 of the Code)	P/A	P/A	P/A
Antennas including cell, accessory, and mounted on top of buildings.	P/A/C	P/A/C	P/A/C
Wind energy equipment	P/A	P/A	P/A
Solar energy equipment	P/A	P/A	P/A
Special Event	P	P	P
Rain harvesting equipment	P/A/C	P/A/C	P/A/C
Utility equipment (includes electrical transformers, gas meters, etc)	P/A/C	P/A/C	P/A/C

** Model homes are limited to a time period until all the homes are sold in the neighborhood.

P= Permitted by right NP= Not Permitted P.C= Permitted with design criteria per Table 6.2 P/A = Permitted Accessory Use NA= Not applicable
A = Accessory use to not exceed 25% of the primary use building square footage

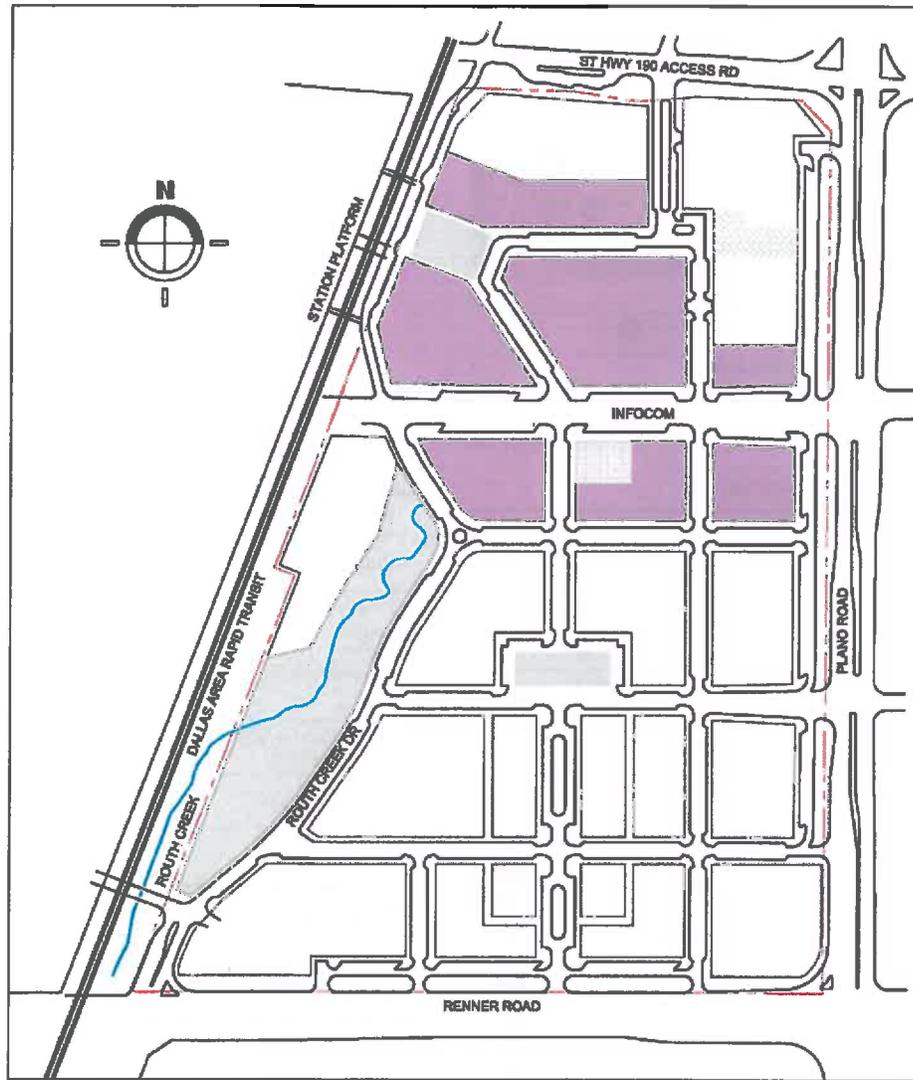
6.3 Use Criteria: All uses listed as P/C in Table 6.1 shall also meet the following standards in Table 6.2

Table 6.2 – Use Criteria		
<i>Use</i>	<i>Zone</i>	<i>Location & Design Criteria</i>
Non-Residential Uses		
Any permitted use with a drive through facility	TOD Core, TOD Mixed Use & Arterial Mixed Use	<ul style="list-style-type: none"> All drive through access (driveways) shall be from Type ‘B’ Streets (including Slip Roads). Drive through lanes and/or canopies shall not have frontage along on or be located along any Type ‘A’ Streets. Drive through areas screened by a 4’ high Street Screen.
Universities and Colleges	TOD Core & TOD Mixed Use	<ul style="list-style-type: none"> Shall be required to provide structured parking as part of the build-out for the university/college campus
Auto-related Sales and Service	Arterial Mixed Use & Highway Mixed Use	<ul style="list-style-type: none"> Gas pumps, canopies, and/or service bays shall not be located along any Type ‘A’ Street frontage. No more than 50% of a block’s frontage along a Type ‘B’ Street shall be occupied by gas pumps, canopies, and/or service bays. Any buildings associated with the use shall also have a pedestrian entrance at a Type ‘A’ Street. No outdoor storage of vehicles or other products sold shall be permitted. All auto-related sales display shall be inside storefronts.
Residential Uses		
Multi-family residential Ground Floor	TOD Core and Arterial Mixed Use	<ul style="list-style-type: none"> All ground floors along all Type ‘A’ Streets and Slip Road frontages shall be built to Retail Ready standards. Ground floors may be occupied by residential uses unless designated as Main Street Frontage. Ground floors of all buildings designated as Main Street Frontage on the Regulating Plan shall not be occupied by residential units and lodging rooms to a minimum depth of 30 feet as measured from the front building line.
Single-family residential attached dwelling unit (Townhomes)	TOD Core, TOD Mixed Use, Arterial Mixed Use	<ul style="list-style-type: none"> Shall be permitted along Type ‘B’ Streets or along Type ‘A’ Streets which are also designated for Neighborhood Frontage on the Regulating Plan Frontages along street intersections shall be built to Retail Ready standards for a minimum of 30’ along each street or the width of the lot, whichever is less.
Other Uses		
Antennas including cell, accessory and mounted (Excluded from this category are freestanding and commercial antennas and equipment buildings)	All Zones	<ul style="list-style-type: none"> Antennas shall be permitted on rooftops. Antennas shall be screened entirely with a screen of same color as the principal building. Antennas shall not be visible from adjacent Type ‘A’ Street.
Rain water harvesting equipment	All Zones	<ul style="list-style-type: none"> Rain water harvesting equipment may not be installed along Type ‘A’ Streets. On all other frontages, they shall be screened with a Street Screen at least as high as the equipment being screened.
Utility equipment (includes electrical transformers, gas meters, etc)	All Zones	<ul style="list-style-type: none"> Utility equipment shall not be installed with frontage on Type ‘A’ Streets. On all other frontages, they shall be screened with a Street Screen at least as high as the equipment being screened.

Section 7. Building Form and Development Standards

The following section establishes the Building Form and Development Standards for all Character Zones within the Bush Central Station. Diagrams and reference letters are used for illustrations purposes only. Reference letters may not be in continuous sequence.

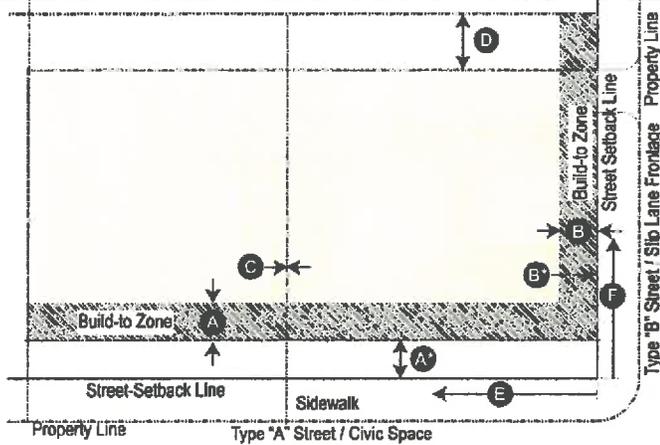
7.1 TOD Core Zone



TOD Core Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements

7.1.1 Building Placement



Legend

----- Property Line	----- Setback Line	----- Street-Setback Line
Building Area	Build-to Zone	

Street-Setback Line (SSL)
(Distance from center line of street to edge of the BTZ)

TOD Main Street	61 feet
TOD Street Type A	35 feet
TOD Street Type B	31 feet
TOD Avenue	44 feet
Parkway	36 feet
Slip Road Frontage	85 feet (measured from the R.O.W of Plano Road)

Build-To Zone (BTZ)
(Distance from edge of SSL to edge of the BTZ)

Front (Type 'A' Street and Civic Space)	5 – 10 feet	A
Front (Type 'B' Street and Slip Road Frontage)	0 – 10 feet (see #1)	B

Setback

Front (Type 'A' Street and Civic Space – distance from SSL)	5 feet (min.) – 10 feet (max)	A*
Front (Type 'B' Street and Slip Road Frontage – distance from SSL)	0 feet (min.) – 10 feet (max.)	B*
Side (from property line)	0 feet (see #2)	C
Rear (from property line)	5 feet	D

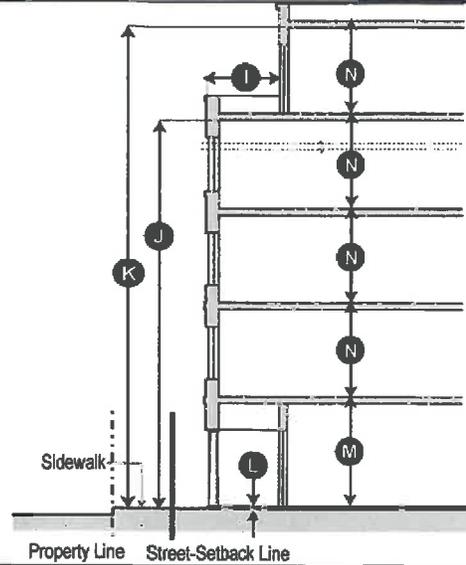
Building Frontage

Building Frontage required along Type 'A' Street/civic space BTZ	90% (min.) (see #3 and #7)	E
Building Frontage required along Type 'B' Street BTZ	0% (min.) (see #3 and #7)	F

7.1.2 Block Standards

Block face dimensions	250 – 400 feet (see #4)
Block perimeter	1400 feet (maximum) (see #4)

7.1.3 Building Height



Principal Building Standards

Building maximum	300 feet (see #8)	K
First floor to floor height	15 feet (min.) (see #5)	M
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)	L
Upper floor(s) height (floor-to-floor)	10 feet min.	N
Stepback height	Maximum 6 stories then stepback (see #13)	J
Stepback distance	10 feet min. (see #13)	I

7.1.4 Commercial Frontage Requirements

Ground floors of all buildings fronting on Type 'A' Streets and Plano Road shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

7.1.5 Special Frontage Requirements

Requirements Specific to Main Street Frontage

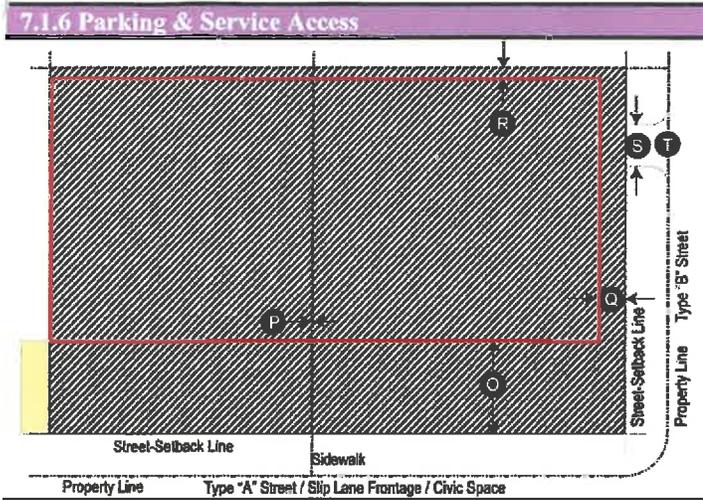
Ground floors of all buildings designated as Main Street Frontage on the Regulating Plan shall not be occupied by residential units and lodging rooms to a minimum depth of 30 feet as measured from the front building line.

Requirements Specific To Station Frontage

Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.

Notes

- #1 – The area between the building and the edge of the BTZ at the public sidewalk shall be paved flush with the sidewalk.
- #2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.



Legend

Property Line	Building Footprint
Surface Parking Area	Street-Setback Line
Above Grade Parking Area	

(i) Parking Location

Surface/At Grade Parking

Type 'A' Street, Slip Road Frontage and Civic Space setback	Shall be located behind the principal building	O
Type 'B' Street setback	Min. of 3 feet behind the building facade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R

Above Grade Parking

Setback along Type 'A' Street, Type 'B' Street, Slip Road Frontage and Civic Space (distance from SSL)	0 feet min.	O
Side and rear setbacks (distance from property line)	0 feet min.	
Upper Floors	May be built up to the building line	

(ii) Required Off-Street Parking Spaces (see #11 and #12)

Non-residential uses	1 space/300 square feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	20 feet max. (except when drives may need to be wider to address service access or fire lane standards)	S
Driveways and off-street loading and unloading shall not be located on a Type 'A' Streets.		T
Porte cocheres may be permitted on Type 'A' Streets to provide drop-off and valet service.		
Shared driveways and cross access easements are encouraged between lots to minimize curb cuts.		
If driveway and/or off-street service loading and unloading access is provided from a Type 'A' Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.		

7.1.7 Encroachments

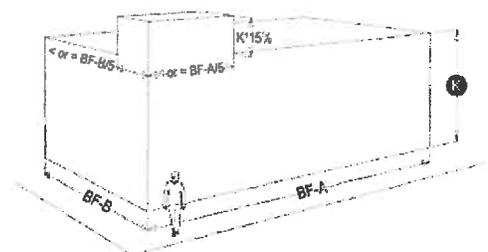
Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.1.8 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

Notes

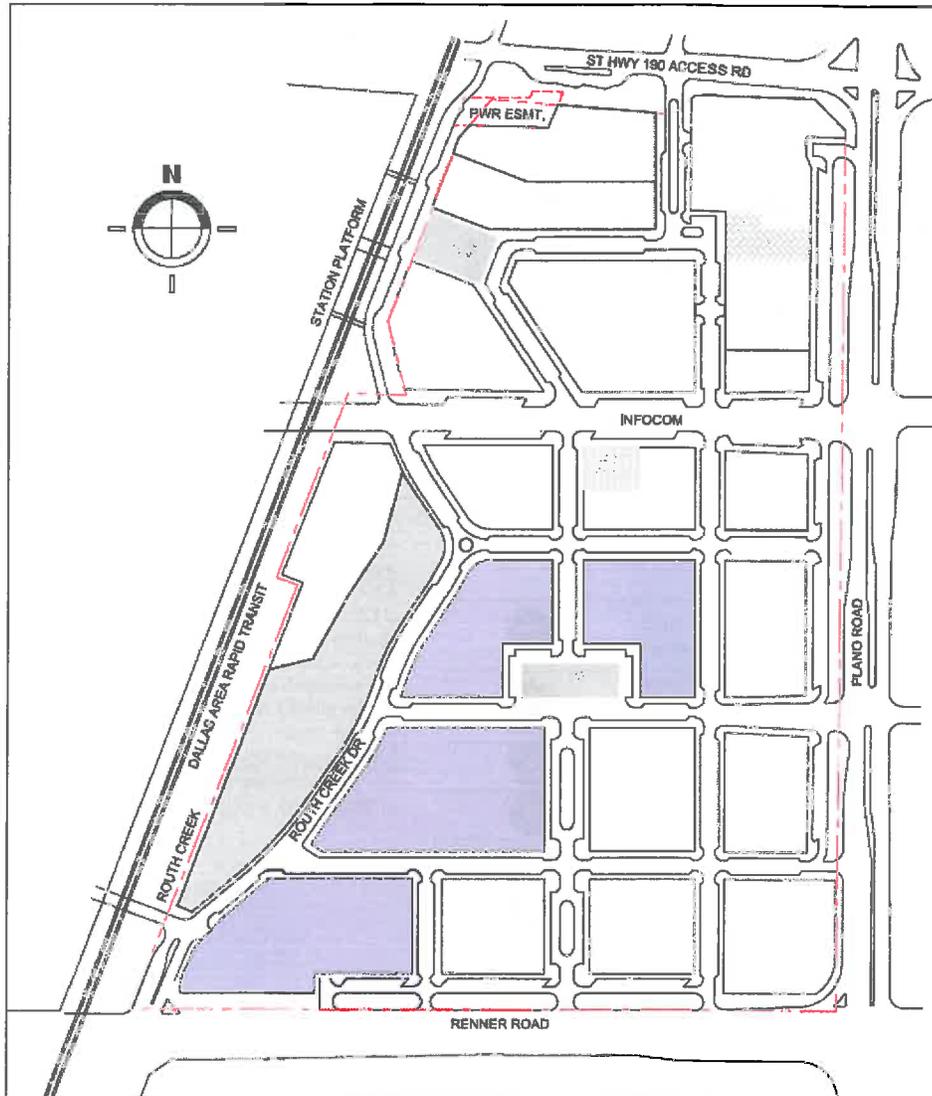
- #3 – Corner building street facades shall be built to the BTZ for a minimum of 30' from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to zone standards.
- #4 – Blocks exceeding the maximum block face and perimeter dimensions may be permitted as follows:
 - (i) No more than two adjacent blocks may be combined based on the Regulating Plan.
 - (ii) An increase in block dimensions shall not eliminate or significantly move a required street or required civic/open space. Required streets and required civic/open spaces may move a maximum of 100 feet.
 - (iii) The block shall maintain a continuous Type 'A' Street frontage with adjoining blocks.
 - (iv) Ground floor internal pedestrian connectivity shall be provided through the block. Connectivity may be provided inside the building, through a parking garage or outside by way of a pedestrian breezeway.
- #5 – First floor heights shall not apply to parking structures.
- #6 – All buildings in the TOD Core Zone shall meet the Building Design Standards in Section 8.
- #7 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen that is at least as high as the service equipment being screened. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Bush Central Station Planting List in Appendix B of this ordinance. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #8 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



#9 - Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.

- #10 – Setbacks and build-to lines on recessed entries and arcade buildings shall be measured from the building façade line.
- #11 – Required parking may be provided anywhere within the Bush Central Station.
- #12 – Article III, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas.
- #13 – Stepback requirement shall not apply to any perimeter streets of the BCS-PD including S.H. 190 access road, Plano Road, Renner Road, Routh Creek Parkway, and DART R.O.W frontage.

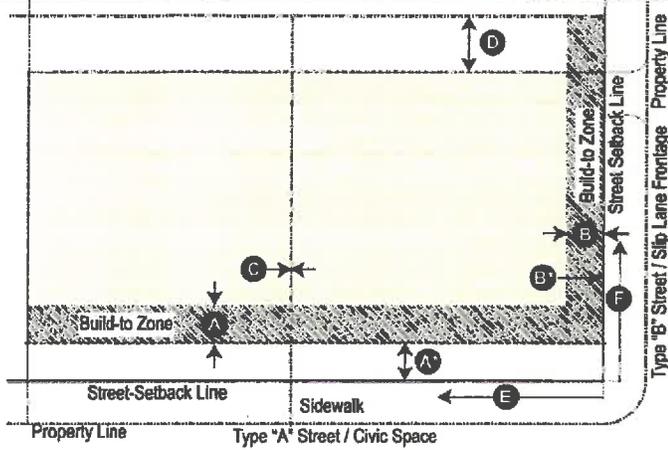
7.2 TOD Mixed Use Zone



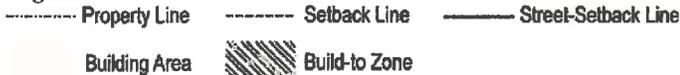
TOD Mixed Use Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements

7.2.1 Building Placement



Legend



**Street-Setback Line (SSL)
(Distance from center line of street cross section to edge of the BTZ)**

TOD Street Type A	35 feet
TOD Street Type B	31 feet
Parkway	36 feet
Court Street	26 feet
Slip Road Frontage	85 feet (measured from the R.O.W of Renner Road)

**Build-To Zone (BTZ)
(Distance from edge of SSL to edge of the BTZ)**

Front (Pedestrian Priority 'A' Street, Pedestrian Priority 'B' and Civic Space)		
Front (Type 'A' Street and Civic Space)	5 – 10 feet	A
Front (Type 'B' Street and Slip Road Frontage)	0 – 10 feet (see #1)	B

Setback

Front (Type 'A' Street and Civic Space – distance from SSL)	5 feet (min.) – 10 feet (max)	A*
Front (Type 'B' Street and Slip Road Frontage – distance from SSL)	0 feet (min.) – 10 feet (max)	B*
Side (distance from property line)	0 feet (see #2)	C
Rear (distance from property line)	0 feet (see #2)	

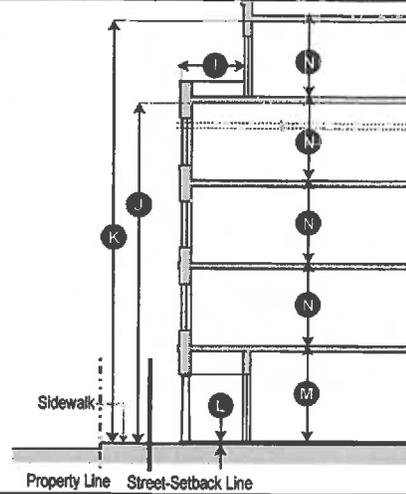
Building Frontage

Building Frontage required along Type 'A' Street/civic space BTZ	80% (min.) (see #3 and #7)	E
Building Frontage required along Type 'B' Street BTZ	0% (min.) (see #3 and #7)	F

7.2.2 Block Standards

Block face dimensions	250 – 400 feet (see #4)
Block perimeter	1500 feet (maximum) (see #4)

7.2.3 Building Height



Principal Building Standards

Building maximum	225 feet (see #8)	K
First floor to floor height	15 feet min. (see #5)	M
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)	L
Upper floor(s) height (floor-to-floor)	10 feet min.	N
Stepback height	Maximum 6 stories then stepback (see #13)	J
Stepback distance	10 feet min (see #13)	I

7.2.4 Commercial Frontage Requirements

Ground floors of all buildings fronting on Type 'A' Streets and Renner Road shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

7.2.5 Special Frontage Requirements

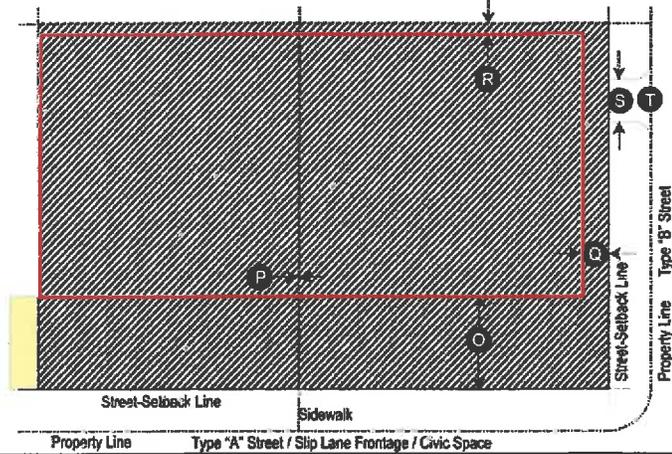
Requirements Specific to Neighborhood Frontage

All buildings designated Neighborhood Frontage on the Regulating Plan shall have a building height maximum of 4 stories to a minimum depth of 30 feet.

Notes

- #1 – The area between the building and the edge of the BTZ at the public sidewalk shall be paved flush with the sidewalk.
- #2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #3 – Corner building street facades shall be built to the BTZ for a minimum of 30 feet from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to zone standards.
- #4 – Blocks exceeding the maximum block face and perimeter dimensions may be permitted as follows:
 - i. No more than two adjacent blocks may be combined based on the Regulating Plan.
 - ii. An increase in block dimensions shall not eliminate or significantly move a required street or required civic/open space. Required streets and required civic/open spaces may move a maximum of 100 feet.
 - iii. The block shall maintain a continuous Type 'A' Street frontage with adjoining blocks.
 - iv. Ground floor internal pedestrian connectivity shall be provided through the block. Connectivity may be provided inside the building, through a parking garage or outside by way of a pedestrian breezeway.

7.2.6 Parking & Service Access



Legend

Property Line	Building Footprint
Surface Parking Area	Street-Setback Line
Above Grade Parking Area	

(i) Parking Location

Surface/At Grade Parking

Type 'A' Street, Slip Road Frontage and Civic Space setback	Shall be located behind the principal building	O
Type 'B' Street setback	Min. of 3 feet behind the building facade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R

Above Grade Parking

Setback along Type 'A' Street, Type 'B' Street, Slip Road Frontage and Civic Space (distance from SSL)	0 feet min.	O
Side and rear setbacks (distance from property line)	0 feet min.	
Upper Floors	May be built up to the building line	

(ii) Required Off-Street Parking Spaces (see #10 and #11)

Non-residential uses	1 space/300 square feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway	20 feet max. (except when drives may need to be wider to address service access or fire lane standards))	S
Driveways and off-street loading and unloading shall not be located on a Type 'A' Streets. Porte cocheres may be permitted on Type 'A' Streets to provide drop-off and valet service. Shared driveways and cross access easements are encouraged between lots to minimize curb cuts. If driveway and/or off-street service loading and unloading access is provided from a Type 'A' Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.		T

7.2.7 Encroachments

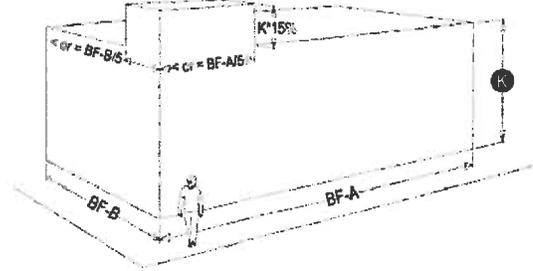
Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.2.8 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

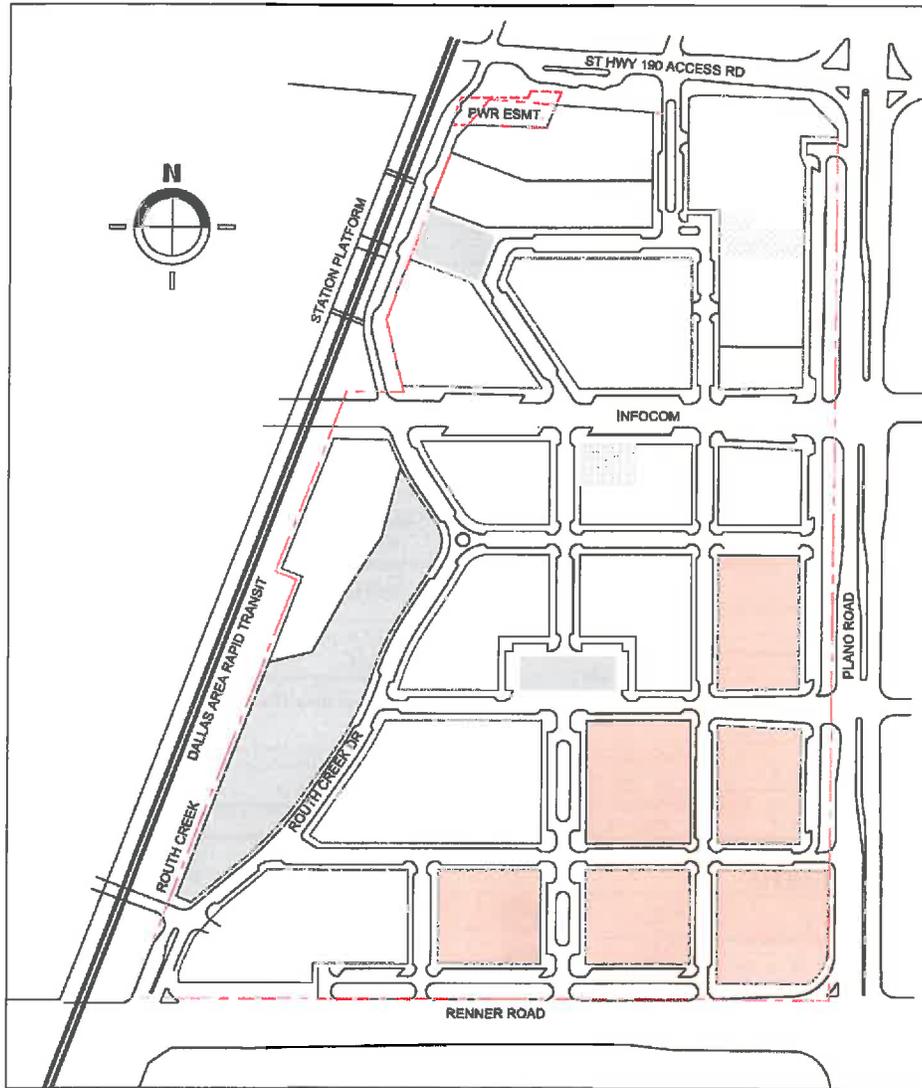
Notes

- #5 – First floor heights shall not apply to parking structures.
- #6 – All buildings in the TOD Mixed Use Zone shall meet the Building Design Standards in Section 8.
- #7 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen that is at least as high as the service equipment being screened. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Bush Central Station Planting List in Appendix B of this ordinance. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #8 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



- #9 - Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.
- #10 – Setbacks and build-to lines on recessed entries and arcade buildings shall be measured from the building façade line.
- #11 – Required parking may be provided anywhere within the Bush Central Station.
- #12 – Article III, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas.
- #13 – Stepback requirement shall not apply to any perimeter streets of the BCS-PD including S.H. 190 access road, Plano Road, Renner Road, Routh Creek Parkway, and DART R.O.W frontage.

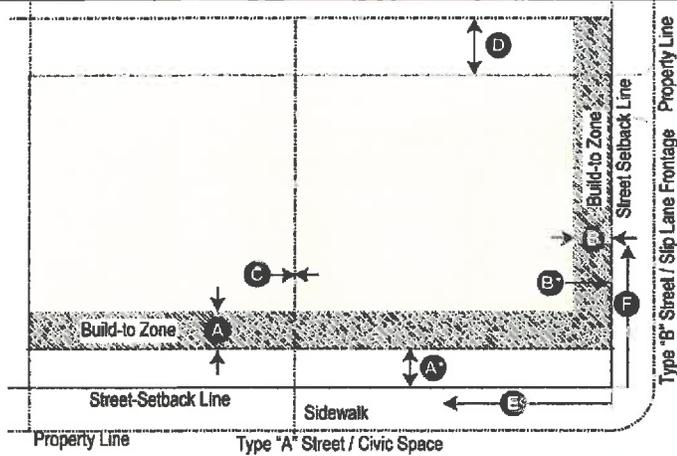
7.3 Arterial Mixed Use Zone



Arterial Mixed Use Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements

7.3.1 Building Placement



Legend

----- Property Line	----- Setback Line	----- Street-Setback Line
Building Area	Build-to Zone	

Street-Setback Line (SSL)
(Distance from center line of street cross section to edge of the BTZ)

TOD Street Type A	35 feet
TOD Street Type B	31 feet
TOD Main Street	61 feet
Park Avenue	51 feet
Slip Road Frontage (Plano Road)	85 feet (measured from the R.O.W of Plano Road)
Slip Road Frontage (Renner Road)	85 feet (measured from the R.O.W of Renner Road)

Build-To Zone (BTZ)
(Distance from SSL to edge of the BTZ)

Front (Type 'A' Street and Civic Space)	5 – 10 feet	A
Front (Type 'B' Street and Slip Road Frontage)	0 – 10 feet (see #1)	B

Setback

Front (Type 'A' Street and Civic Space – distance from SSL)	5 feet (min.) – 10 feet (max)	A*
Front (Type 'B' Street and Slip Road Frontage – distance from SSL)	0 feet (min.) – 10 feet (max)	B*
Side (distance from property line)	0 feet (see #2)	C
Rear (distance from property line)	0 feet	D

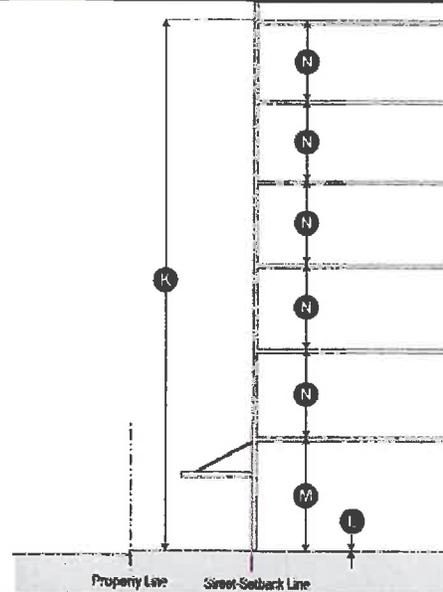
Building Frontage

Building Frontage required along Type 'A' Street/civic space BTZ	50% (min.) (see #3 and #7)	E
Building Frontage required along Type 'B' Street BTZ	0% (min.) (see #3 and #7)	F

7.3.2 Block Standards

Block face dimensions	250 – 400 feet (maximum) (see #4)
Block perimeter	1100 feet (maximum) (see #4)

7.3.3 Building Height



Principal Building Standards

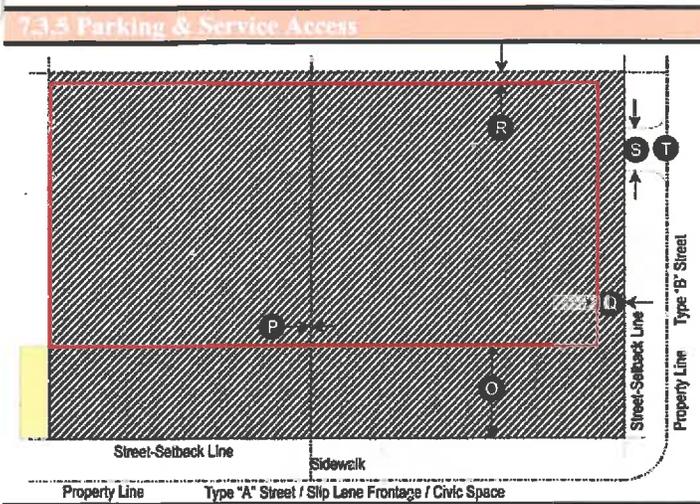
Building maximum	100 feet (see #8)	K
First floor to floor height	15 feet (min.) (see #5)	M
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)	L
Upper floor(s) height (floor-to-floor)	10 feet min.	N

7.3.4 Commercial Frontage Requirements

Ground floors of all buildings fronting on Type 'A' Streets, Plano Road and Renner Road shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

Notes

- #1 – The area between the building and the edge of the BTZ at the public sidewalk shall be paved flush with the sidewalk.
- #2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #3 – Corner building street facades shall be built to the BTZ for a minimum of 15' from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to zone standards.
- #4 – Blocks exceeding the maximum block face and perimeter dimensions may be permitted as follows:
 - i. Combined blocks shall be adjacent to one another based on the Regulating Plan.
 - ii. An increase in block dimensions shall not eliminate or significantly move a required street or required civic/open space. Required streets and required civic/open spaces may move a maximum of 100 feet.
 - iii. The block shall maintain a continuous Type 'A' Street frontage with adjoining blocks.
 - iv. Ground floor internal pedestrian connectivity shall be provided through the block. Connectivity may be provided inside the building, through a parking garage or outside by way of a pedestrian breezeway.



Legend

Property Line	Building Footprint
Surface Parking Area	Street-Setback Line
Above Grade Parking Area	

(i) Parking Location

Surface/At Grade Parking

Type 'A' Street, Slip Road Frontage and Civic Space setback	Shall be located behind the principal building	O
Type 'B' Street setback	Min. of 3 feet behind the building facade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R

Above and Below Grade Parking (Distance from SSL)

Setback along Type 'A' Street, Type 'B' Street, Slip Road Frontage and Civic Space (distance from SSL)	0 feet min.	O
Side and rear setbacks (distance from property line)	0 feet min.	

Upper Floors May be built up to the building line

(ii) Required Off-Street Parking Spaces (see #11 and #12)

Non-residential uses	1 space/300 square feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	City standards on Arterial Roadways and 24 feet max. on all Bush Central Station Streets (except when drives may need to be wider to address service access or fire lane standards)	S
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Driveways and off-street loading and unloading shall not be located on a Type 'A' Streets. **T**

Porte cocheres may be permitted on Type 'A' Streets to provide drop-off and valet service.

Shared driveways and cross access easements are encouraged between lots to minimize curb cuts.

If driveway and/or off-street service loading and unloading access is provided from a Type 'A' Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.

7.3.6 Encroachments

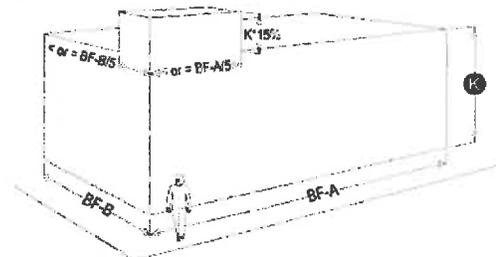
Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.3.7 Applicability

Building Form and Development Standards in this Section shall apply to all development in this Character Zone.

Notes

- #5 – First floor heights shall not apply to parking structures.
- #6 – All buildings in the Arterial Mixed Use Zone shall meet the Building Design Standards in Section 8.
- #7 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen that is at least as high as the service equipment being screened. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Bush Central Station Planting List in Appendix B of this ordinance. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #8 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



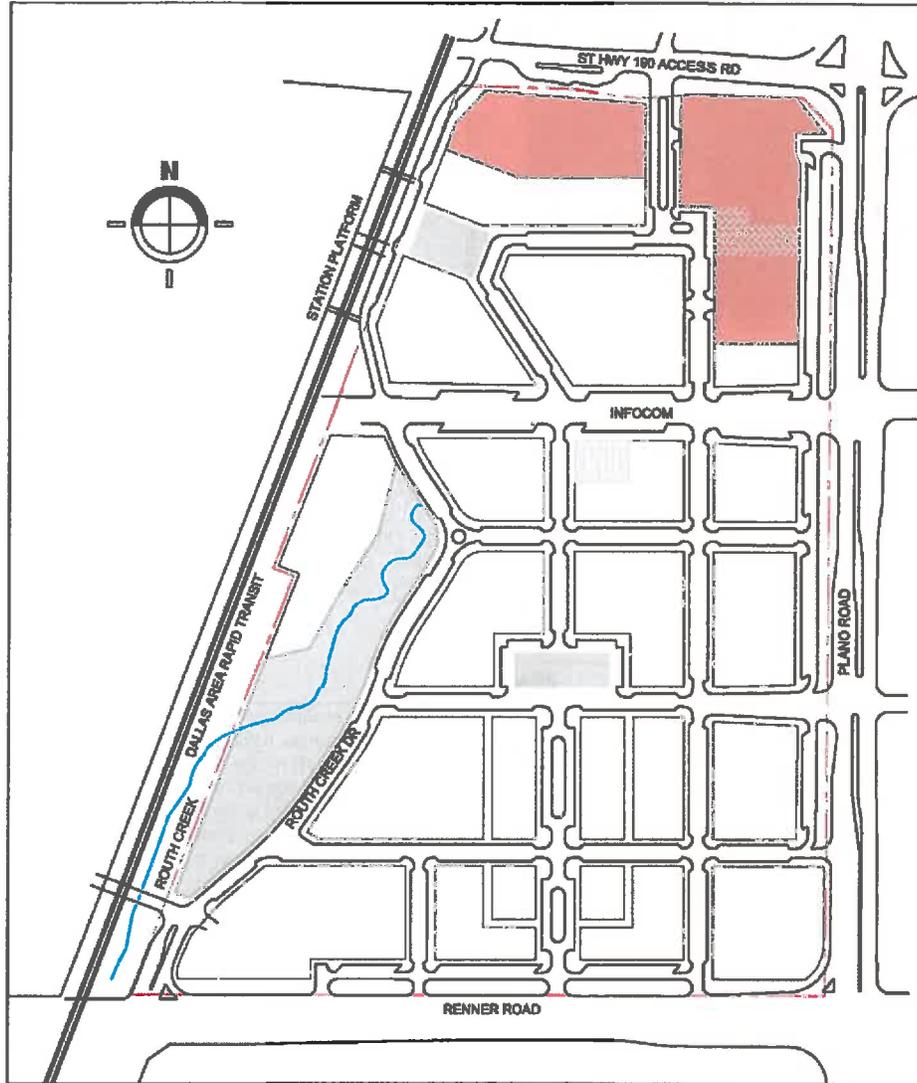
#9 - Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.

#10 – Setbacks and build-to lines on recessed entries and arcade buildings shall be measured from the building façade line.

#11 – Required parking may be provided anywhere within the Bush Central Station.

#12 – Article III, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas.

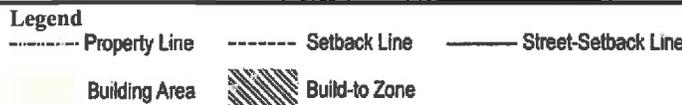
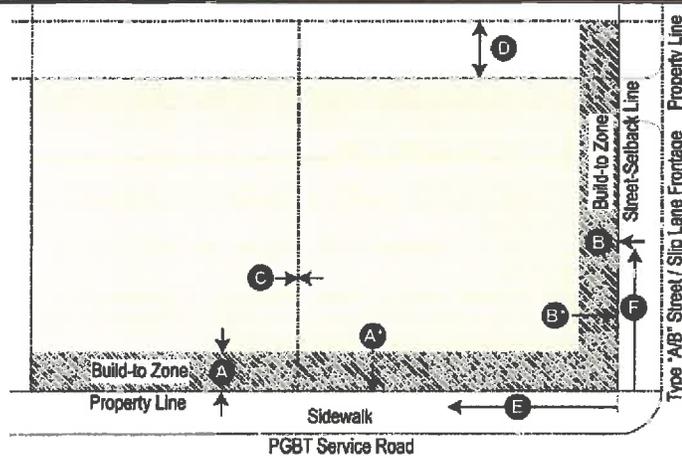
7.4 Highway Mixed Use



Highway Mixed Use Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements

7.4.1 Building Placement

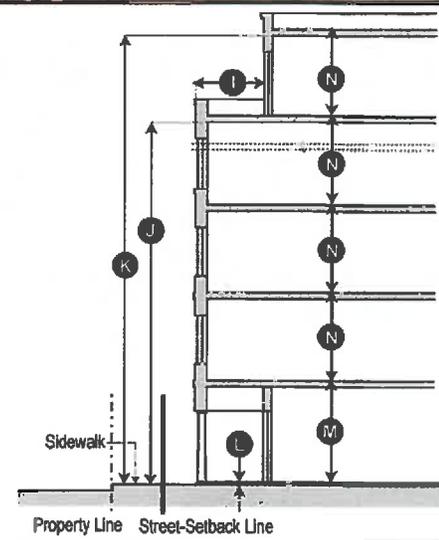


Street-Setback Zone (SSL) (Distance from center line of street cross section to edge of the BTZ)		
TOD Avenue	44 feet	
TOD Street Type B	31 feet	
Slip Road Frontage	85 feet (measured from the R.O.W of Plano Road)	
Build-To Zone (BTZ) (Distance from Property Line to edge of the BTZ)		
Front (PGBT Service Road)	None	A
(Distance from SSL to edge of the BTZ)		
Front (Type "A/B" Street and Slip Road Frontage)	0 – 10 feet (see #1)	B
Setback		
Front (PGBT Service Road – distance from property line or edge of utility easement)	15 feet (min.)	A*
Front (Type "A/B" Street and Slip Road Frontage – distance from SSL)	0 feet (min) – 10 feet (max)	B*
Side (distance from property line)	0 feet (see #2)	C
Rear (distance from property line)	0 feet	D
Building Frontage		
Building Frontage required along Type 'A' Street BTZ	90% (min.) (see #3 and #7)	
Building Frontage required along PGBT Service Road	0% (min.) (see #3 and #7)	E
Building Frontage required along Type 'B' Street BTZ	0% (min.) (see #3 and #7)	F

7.4.2 Block Standards

Block face dimensions	250 – 400 feet (maximum) (see #4)
Block perimeter	1400 feet (maximum) (see #4)

7.4.3 Building Height



Principal Building Standards

Building maximum	300 feet (see #8)	K
First floor to floor height	15 feet min. (see #5)	M
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)	L
Upper floor(s) height (floor-to-floor)	10 feet min.	N
Stepback height	Maximum 10 stories then stepback (see #13)	J
Stepback distance	10 feet min.	I

7.4.4 Commercial Frontage Requirements

Ground floors of all buildings fronting on President George Bush Turnpike (PGBT) service road and Plano Road shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

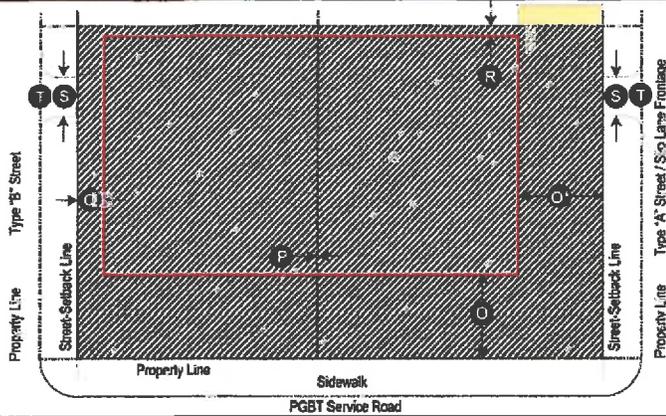
7.4.5 Special Frontage Requirements

Requirements Specific To Station Frontage
Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.

Notes

- #1 – The area between the building and the edge of the BTZ at the public sidewalk shall be paved flush with the sidewalk.
- #2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #3 – Corner building street facades shall be built to the BTZ for a minimum of 15 feet from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to zone standards.

7.4.6 Parking & Service Access



Legend

Property Line	Building Footprint
Surface Parking Area	Street-Setback Line
Above Grade Parking Area	

(i) Parking Location

Surface/At Grade Parking

PGBT Service Road setback	Shall be located behind the principal building	O
Type 'A' Street and Slip Road Frontage	Shall be located behind the principal building	O
Type 'B' Street setback	Min. of 3 feet behind the building façade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R

Above Grade Parking

Setback along Type 'A' Street, Type 'B' Street, Slip Road Frontage and Civic Space (distance from SSL)	0 feet min.	O
PGBT Service Road, side and rear setbacks (distance from property line)	0 feet min.	

Upper Floors May be built up to the building line

(ii) Required Off-Street Parking Spaces (see #11 and #12)

Non-residential uses	1 space/300 square feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	TXDOT standards on service road and 24 feet max on all Bush Central Station Streets (except when drives may need to be wider to address service access or fire lane standards)	S
Driveways and off-street loading and unloading shall not be located on PGBT Service Road.		T
Porte cocheres may be permitted on Type 'A' Streets to provide drop-off and valet service.		
Shared driveways and cross access easements are encouraged between lots to minimize curb cuts.		
If driveway and/or off-street service loading and unloading access is provided from PGBT Service Road, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.		

7.4.7 Encroachments

Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.4.8 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

Notes

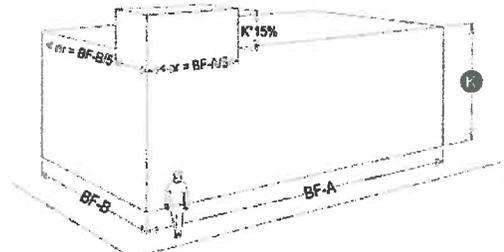
- #4 – Blocks exceeding the maximum block face and perimeter dimensions may be permitted as follows:
 - i. No more than two adjacent blocks may be combined based on the Regulating Plan.
 - ii. An increase in block dimensions shall not eliminate or significantly move a required street or required civic/open space. Required streets and required civic/open spaces may move a maximum of 100 feet.
 - iii. The block shall maintain a continuous Type 'A' Street frontage with adjoining blocks.
 - iv. Ground floor internal pedestrian connectivity shall be provided through the block. Connectivity may be provided inside the building, through a parking garage or outside by way of a pedestrian breezeway.

#5 – First floor heights shall not apply to parking structures.

#6 – All buildings in the Highway Mixed Use shall meet the Building Design Standards in Section 8.

#7 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen that is at least as high as the service equipment being screened. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Bush Central Station Planting List in Appendix B of this ordinance. The required Street Screen shall be located within the BTZ along the corresponding frontage.

#8 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



#9 – Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.

#10 – Setbacks and build-to lines on recessed entries and arcade buildings shall be measured from the building façade line.

#11 – Required parking may be provided anywhere within the Bush Central Station.

#12 – Article III, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas.

#13 – Stepback requirement shall not apply to any perimeter streets of the BCS-PD including S.H. 190 access road, Plano Road, Renner Road, Routh Creek Parkway, and DART R.O.W frontage.

Section 8. Building Design Standards

The Building Design Standards and Guidelines for the Bush Central Station shall establish a coherent urban character and encourage enduring and attractive development. Development plans shall be reviewed by the City Manager or designee for compliance with the standards below.

The key design principles establish essential goals for development in the Bush Central Station to ensure the preservation, sustainability, and visual quality of this unique environment. Buildings shall be located and designed so that they provide visual interest and create enjoyable, human-scaled spaces. The key design principles are:

- a. New buildings shall utilize building elements and details to achieve a pedestrian-oriented public realm.
- b. Compatibility is not meant to be achieved through uniformity, but through the use of variations in building elements to achieve individual building identity.
- c. Building facades shall include appropriate architectural details and ornament to create variety and interest.
- d. Open space(s) shall be incorporated to provide usable public areas integral to the urban environment.

8.1 General to all Character Zones

8.1.1 Building Orientation

- i. Buildings shall be oriented towards Type 'A' Streets, where the lot has frontage along Type 'A' Streets. All other buildings shall be oriented towards Type 'B' Streets or Civic Spaces.
- ii. Primary entrance to buildings shall be located on the street along which the building is oriented. At intersections, corner buildings may have their primary entrances oriented at an angle to the intersection.
- iii. All primary entrances shall be oriented to the public sidewalk for ease of pedestrian access. Secondary and service entrances may be located from internal parking areas or alleys.

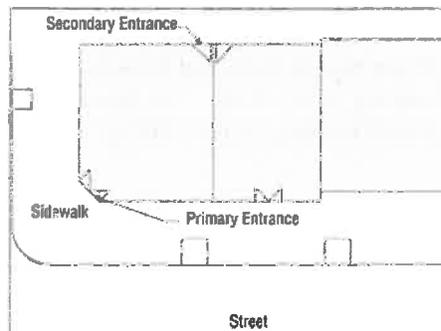


Figure showing required building orientation and location of primary entrances

8.1.2 Design of Parking Structures

- i. All frontages of parking structures located on Type 'A' Streets shall not have parking uses on the ground floor to a minimum depth of 30 feet along the Type 'A' Street frontage. If the frontage is along a designated Main Street Frontage, then the Main Street Frontage requirement shall supersede.
- ii. The amount of Type 'A' Street frontage devoted to a parking structure shall be minimized by placing the shortest dimension(s) along the Type 'A' Street edge(s).

- iii. Parking structure facades on all Type 'A' Streets shall be designed with both vertical (façade rhythm of 20 feet to 30 feet) and horizontal (aligning with horizontal elements along the block) articulation.
- iv. Where above ground structured parking is located at the perimeter of a building with frontage along a Type 'A' Street; it shall be screened in such a way that cars on all parking levels are completely hidden from view from all adjacent public streets. Parking garage ramps shall not be visible from any public street. Ideally, ramps should not be located along the perimeter of the parking structure. Architectural screens shall be used to articulate the façade, hide parked vehicles, and shield lighting.
- v. When parking structures are located at corners, corner architectural elements shall be incorporated such as corner entrance, signage and glazing.
- vi. Parking structures and adjacent sidewalks shall be designed so pedestrians are clearly visible to entering and exiting automobiles.



Images showing appropriate design of Parking Structures

8.1.3 Design of Automobile Related Building Site Elements

- i. Drive-through lanes for commercial uses shall not be located along any Type 'A' Street. Drive-through lanes shall be hidden behind a Street Screen along the Type 'B' Street frontage.
- ii. All off-street loading, unloading, and trash pick-up areas shall be located along alleys or Type 'B' Streets only unless permitted in the specific building form and development standards in Section 7. Any off-street loading, unloading, or trash pick-up areas shall be screened using a Street Screen that is at least as tall as the trash containers and/or service equipment it is screening at the BTZ. The Street Screen shall be made up of (i) the same material as the principal building or (ii) a living screen or (iii) a combination of the two.

8.1.4 Roof Form

- i. Buildings shall have simple, flat fronts with minimal articulations with flat or low pitched roofs with parapets. Corner hip roof elements and gable accents at the parapet may be permitted. Projecting mansard roofs shall not be permitted.



8.1.5 Façade Composition

- i. Buildings shall maintain a façade rhythm of 20 feet to 30 feet along all Type 'A' Streets.

- ii. This rhythm may be expressed by changing materials, or color, or by using design elements such as fenestration, columns and pilasters, or by varying the setback of portions of the façade.
- iii. Buildings shall be designed and built in tri-partite architecture so that they have a distinct Base, Middle and Cap.



Image of Tri-Partite Architecture

- iv. An expression line or equivalent architectural element shall delineate the Base and Cap of all buildings. A Cornice shall delineate the caps of facades that do not utilize a pitched roof.
- v. For retail storefront buildings, a transom, display window area, and bulkhead at the base shall be utilized.

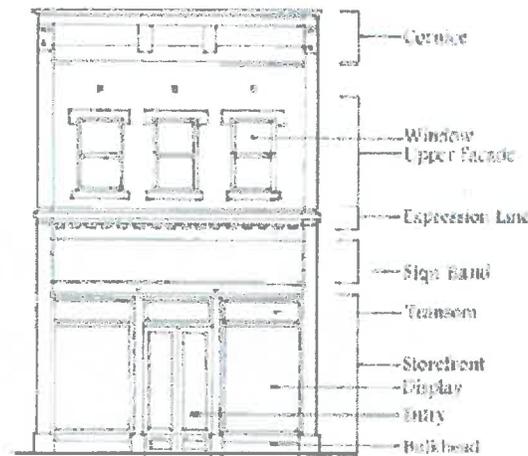


Image of a typical storefront

- vi. Storefronts on facades that span multiple tenants shall use architecturally compatible materials, colors, details, awnings, signage, and lighting fixtures.
- vii. Building entrances may be defined and articulated by architectural elements such as lintels, pediments, pilasters, columns, porticos, porches, overhangs, railings, balustrades, and others as appropriate. All building elements should be compatible with the architectural style, materials, colors, and details of the building as a whole. Entrances to upper level uses may be defined and integrated into the design of the overall building facade.
- viii. Buildings shall generally maintain the alignment of horizontal elements along the block.

- ix. Corner emphasizing architectural features, pedimented gabled parapets, cornices, awnings, blade signs, arcades, colonnades and balconies may be used along commercial storefronts to add pedestrian interest.



Buildings with architectural features and storefront elements that add interest along the street.

- x. Buildings which are located on axis with a terminating street or at the intersection of streets shall be considered as feature buildings. Such buildings shall be designed with features which take advantage of that location, such as an accentuated entry and a unique building articulation which is off-set from the front wall planes and goes above the main building eave or parapet line.

8.1.6 Windows and Doors

- i. Windows and doors on street (except alleys) fronting facades shall be designed to be proportional and appropriate to the specific architectural style of the building. First floor windows shall NOT be opaque, tinted or mirrored glass.
- ii. All ground floor front facades of buildings along Type 'A' Streets or Civic/Open Space shall have transparent storefront windows covering no less than 50% of the façade area. Each upper floor of the same building facades facing a Type 'A' Street or Civic/Open Space shall contain transparent windows covering at least 30% of the façade area. All other street facing facades (except alleys) shall have transparent windows covering at least 15% of the façade area for all floors.



Images showing appropriate storefront display and transparency

8.1.7 Commercial and Mixed Use Building Materials

- i. At least 85% of each building's Base façade (excluding doors and windows) along any Type 'A' Street, Plano or Renner Roads shall be finished in one of the following materials:
- Masonry (brick, stone, stucco utilizing a three-step process, cast stone, rock, marble, granite, curtain glass, or glass block)
- ii. No more than 15% of each Base façade along any Type 'A' Street, Plano, or Renner Road shall use accent materials such as wood, architect metal panel, split-face concrete block,

tile, or pre-cast concrete panels. EIFS shall not be permitted along any Type 'A' Street or Plano or Renner Road facades.



Images showing appropriate building materials within TOD Core, TOD Mixed Use, Arterial and Highway Mixed Use.

- iii. A building's Middle and Cap facades along Type 'A' Streets, Plano, or Renner Roads and all facades along Type 'B' Streets or alleys shall be of a similar finished quality and color that blend with the front of the building. Building materials for these facades may be any of the primary and accent façade materials listed above. EIFS shall not be permitted along any Type 'A' Street façades, ground floor facades along Type 'B' Streets and ground floor facades of alleys. EIFS on upper floors of Type 'B' Street and alley facades are limited to no more than 10% of the respective façade areas. Cementitious-fiber clapboard (not sheet) with at least a 50-year warranty may only be used on the upper floors only of any commercial frontage on any street or alley façade and are limited to no more than 15%.
- iv. Roofing materials visible from any public right-of-way shall be copper, factory finished standing seam metal, slate, synthetic slate, or similar materials.

8.2 Standards Specific to Development with Neighborhood Frontage Designation:

8.2.1 Building Orientation

- i. Garages and carport for Residential Buildings shall be located on alleys at the rear of residential buildings.

8.2.2 Building Massing and Scale

- i. Residential buildings shall have few, if any, articulations and simple roofs (gable, hip, combination) with most building wing articulations set at the rear of the structure. Window projections, bay windows, stoops, porches, balconies, and similar extensions shall be exempt from this standard.
- ii. Gable roofs, if provided for residential buildings, shall have a minimum pitch of 5/12. When hipped roofs are used, the minimum pitch shall be 5/12. Other roof types shall be appropriate to the architectural style of the building. Porch roofs may be a minimum pitch of 3/12.
- iii. Projecting mansard roofs shall be prohibited.



Images showing appropriate massing and scale for Residential Buildings

8.2.3 Façade Composition

- i. Buildings shall maintain a façade rhythm of 20 feet to 30 feet along Type ‘A’ Streets. This rhythm may be expressed by changing materials, or color, or by using design elements such as columns and pilasters, or by varying the setback of portions of the building façade.
- ii. At least one of the following -- porches, stoops, eaves or balconies --shall be added along the front residential facades to add pedestrian interest along the street.
- iii. For residential buildings the grade of the slab or first floor elevation shall be elevated at least 18 inches above the grade of the sidewalk.
- iv. Alley and/or Type ‘B’ Street facing facades shall be of finished quality and of the same color that blend with the front of the building.



Residential buildings with porches, balconies, and stoops to add interest along the street.

8.2.4 Windows and Doors

- i. Windows and doors shall be designed to be proportional and appropriate to the architectural style of the building.
- ii. Windows may have jack arch, keystone arch, flat arch, or ornamental arches.
- iii. All building facades of residential buildings fronting on all streets or civic / open spaces, except alleys, shall have transparent windows covering at least 15% of each façade.



Images showing appropriate window designs and proportions.

8.2.5 Residential Building Materials

- i. At least 85% of the Type ‘A’ Street facing facades of all buildings (excluding doors and windows) shall be finished in one or more of the following materials. No more than three different materials shall be used on any single residential façade:
 - Cementitious-fiber clapboard (not sheet) with at least a 50-year warranty.
 - Masonry (brick; stone; man-made stone, or stucco utilizing a three-step process).
- ii. The following may only be allowed up to 15% as an accent material:
 - Architectural metal panels or similar material over a cementitious base, rock, glass block and tile.
- iii. EIFS shall not be permitted along any Type ‘A’ Street facades.

- iv. Side and rear facades shall be of finished quality and of the same color that blend with the front of the building. Ground floor side and rear facades shall not be EIFS. On upper floors of side and rear facades, EIFS is limited to no more than 10% of the façade area.
- v. Roofing materials (visible from any public right-of-way): copper, factory finished painted metal, slate, synthetic slate, terra cotta, and asphalt shingles (laminated and classified as being at least 300 pounds/100 sf).
- vi. An enclosed garage or carport shall be designed and constructed of the same material as the primary building.

Section 9. Street & Streetscape Design Standards

9.1 Generally: Streets in the Bush Central Station need to support the overall goal of a mixed use, compact, pedestrian oriented district. They should balance all forms of mobility while maximizing convenience for residents and visitors.

The Regulating Plan designates the required and recommended street network within the Bush Central Station. This section specifies the typical configuration of streets within the Bush Central Station. The specifications address vehicular lane width, parkway widths, R.O.W widths, number of travel lanes, on-street parking, and pedestrian accommodation. The character of streets in the Bush Central Station will vary based on the location. The service road of President George Bush Turnpike (State Highway 190) is under the purview of TxDOT while the remaining streets are city streets.

9.2 New Streets: This section specifies standards for all new streets in the Bush Central Station. New streets shall be based on the Mandatory or Non-Mandatory designation on the Regulating Plan.

9.3 Street Classification Established: Table 9.1 and associated cross sections shall establish the cross sections for each street type. The cross sections may be adjusted to fit existing contexts with the approval of the City Engineer. In addition, the proposed cross sections may be adjusted to meet the needs of the Uniform Fire Code as adopted by the City.

Table 9.1

Elements Street Classification	Street Width (Recommended minimum)		Number of Vehicular Lanes	Number of Bike Lanes	Vehicular Lane Widths	Bike Lane Width	On-Street Parking	Pedestrian Sidewalk Width (min.)	Parkway/ Tree Well
	R.O.W	Pedestrian Easement (on both sides)							
TOD Main Street	90 feet	16 feet	4	2	10.5 feet	5 feet	Yes, both sides, reverse angled	16 feet	Tree Well, 5 X 5 feet min.
Parkway	40 feet	16-18 feet	2	N/A	12 feet	N/A	Yes, both sides, parallel	16-18 feet	Tree Well, 5X5 feet
TOD Avenue	64 feet	12 feet	2	N/A	14 feet	N/A	Yes, both sides, parallel	12 feet	Tree Well, 5 X 5 feet min.
Park Avenue	74 feet	14 feet	2	N/A	14 feet	N/A	Yes, both sides, parallel	8 feet	Tree Well, 5 x 5 feet min. or Parkway, 6 feet min.
TOD Street Type 'A'	38 feet	16 feet	2	N/A	11 feet	N/A	Yes, both sides, parallel	16 feet	Tree Well, 5 X 5 feet min.
TOD Street Type 'B'	38 feet	12 feet	2	N/A	11 feet	N/A	Yes, both sides, parallel	12 feet	Tree Well, 5 X 5 feet min.
Court Street	20 feet	16 feet	1	N/A	12 feet	N/A	Yes, one side, parallel	16 feet	Tree Well, 5 X 5 feet min.
Plano Road Slip Road Frontage	60 feet	15 feet	1	N/A	16 feet One Way	N/A	Yes, one side, angled; other side parallel	15 feet	Tree Well, 5 X 5 feet min.

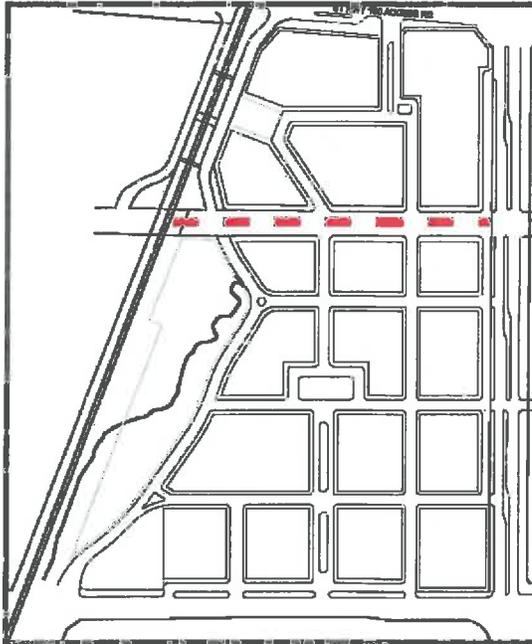
**BUSH CENTRAL STATION
PLANNED DEVELOPMENT CODE**

Elements Street Classification	Street Width (Recommended minimum)		Number of Vehicular Lanes	Number of Bike Lanes	Vehicular Lane Widths	Bike Lane Width	On-Street Parking	Pedestrian Sidewalk Width (min.)	Parkway/ Tree Well
	R.O.W	Pedestrian Easement (on both sides)							
Renner Road Slip Road Frontage	60 feet	15 feet	1	N/A	16 feet One Way	N/A	Yes, one side, angled; other side parallel	15 feet	Tree Well, 5 X 5 feet min.
Commercial Alley	26 feet (16 feet of paving)	None	N/A	None	N/A	N/A	None	None	None

9.4 Street Cross Sections

9.4.1 TOD Main Street

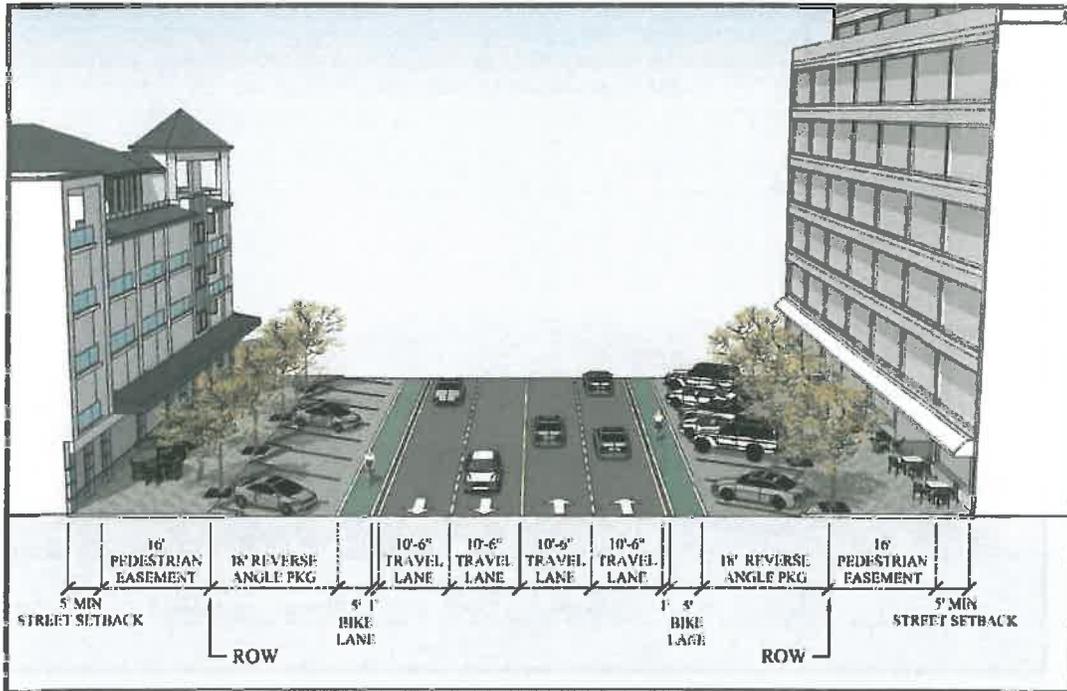
Locator Map



TOD Main Street

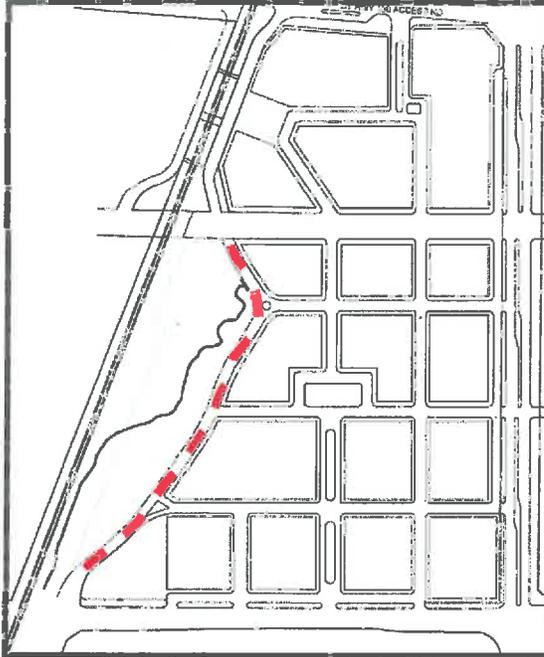
Street Type: A
ROW: 90'
Design Speed: 25 MPH
Parking: Reverse Angle on both sides
Number of Vehicular Lanes: 4
Number of Bike Lanes: 2
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.2 Parkway

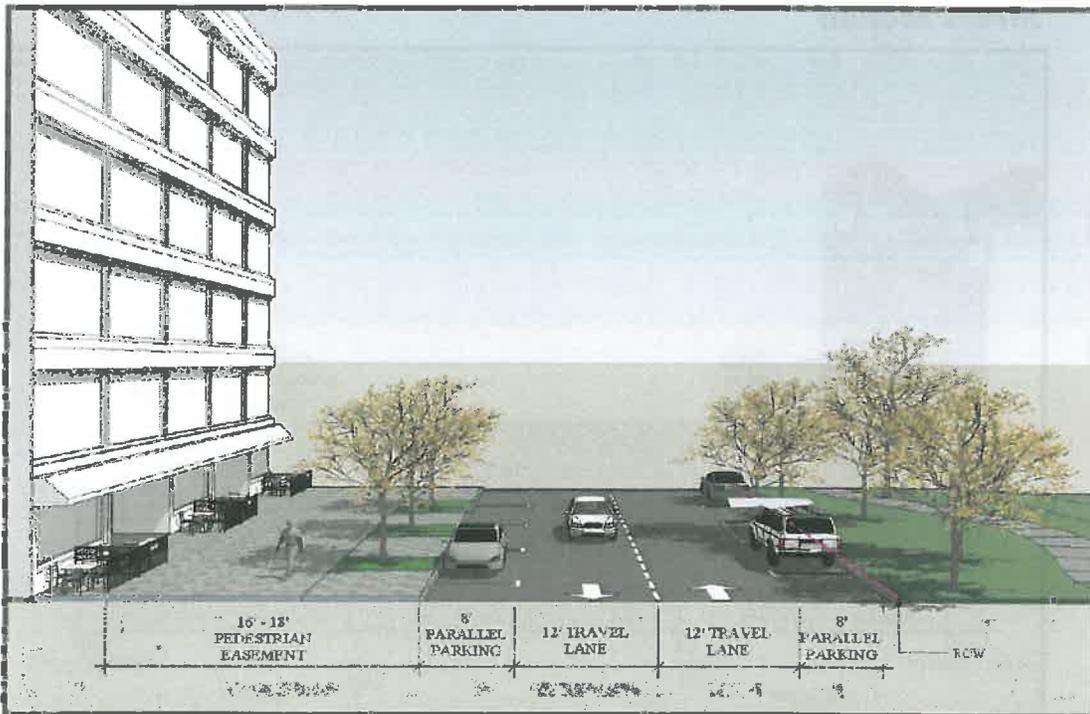
Locator Map



Parkway

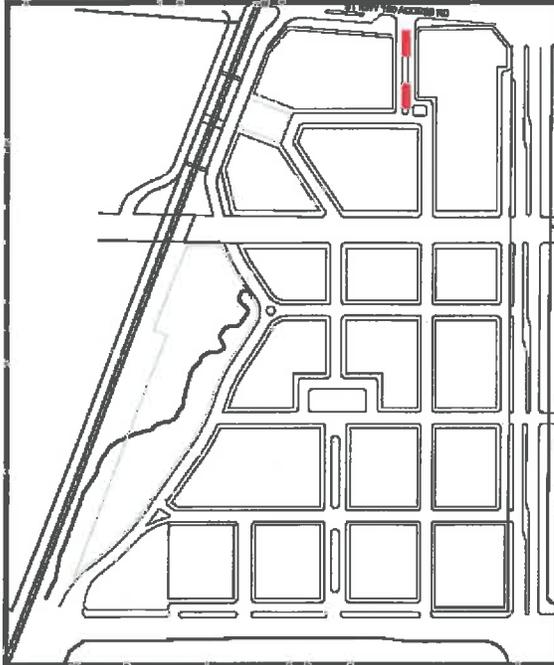
Street Type: A
ROW: 40'
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.3 TOD Avenue

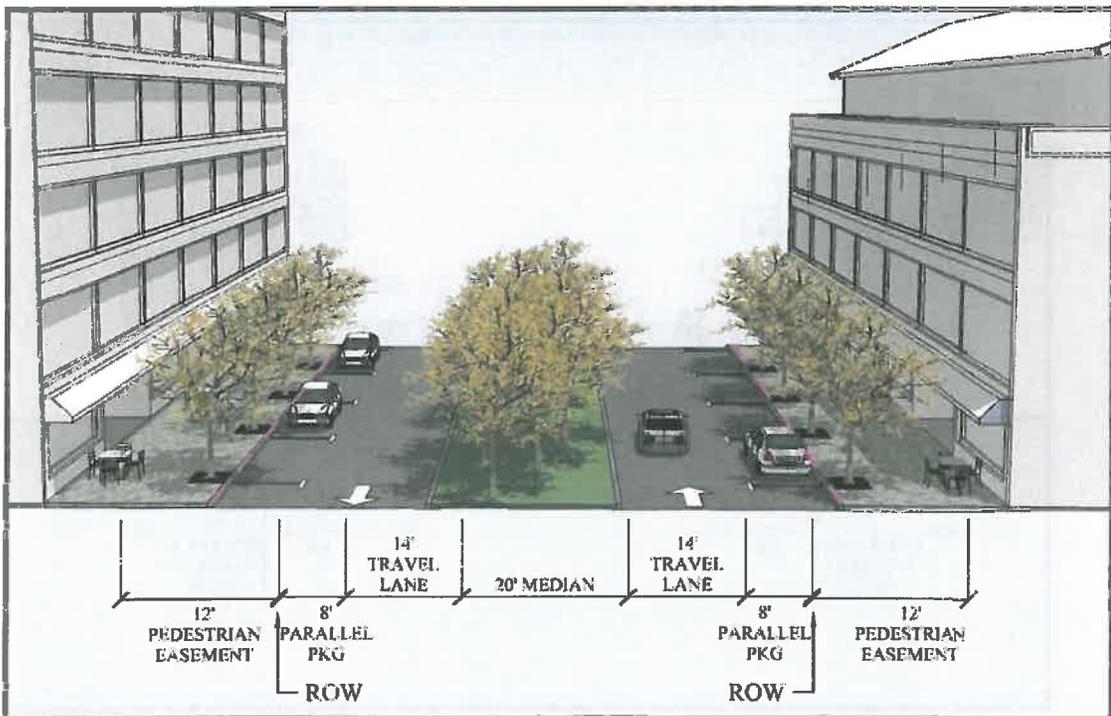
Locator Map



TOD Avenue

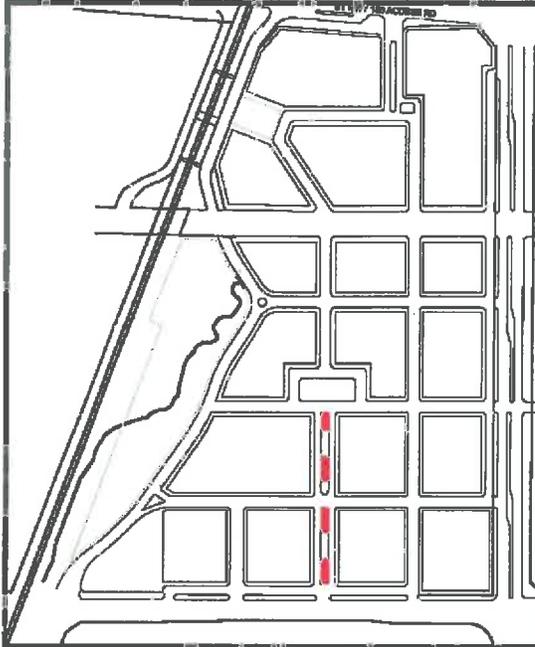
Street Type: B
ROW: 64'
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.4 Park Avenue

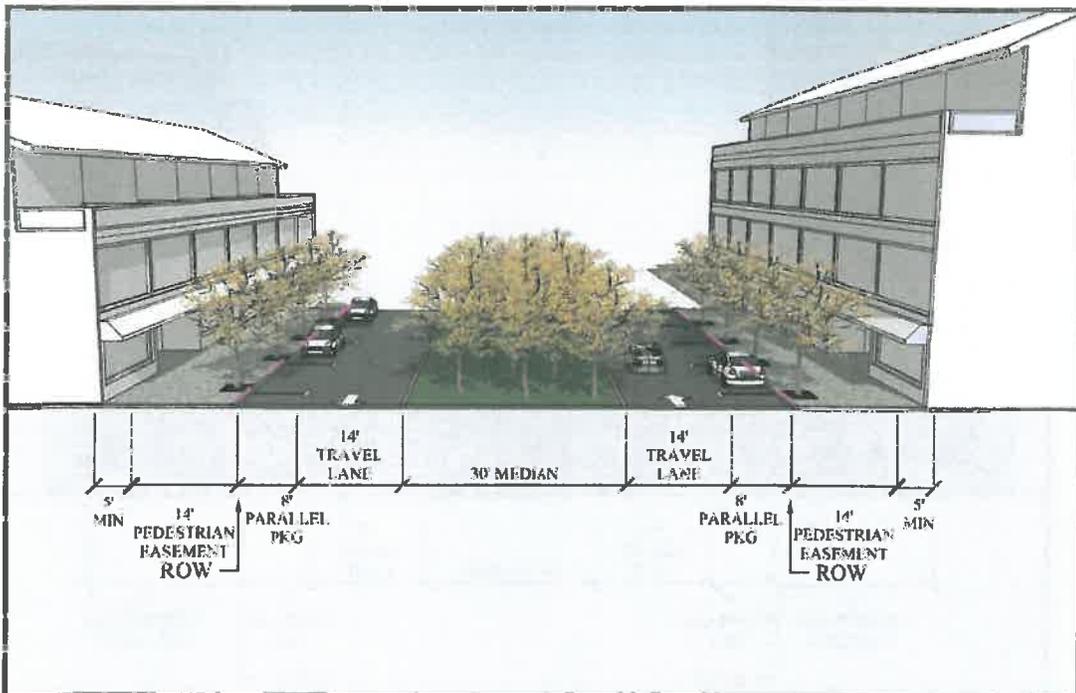
Locator Map



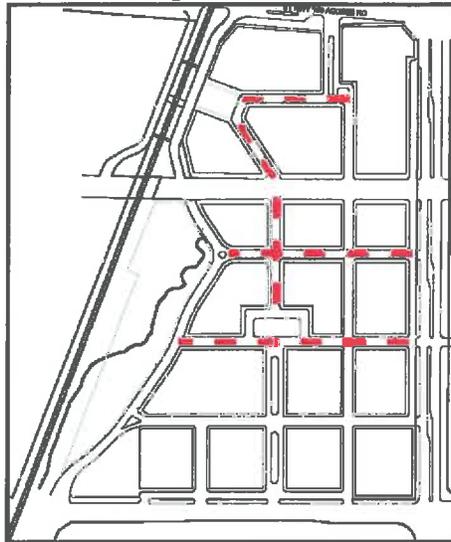
Park Avenue

Street Type: A
ROW: 74'
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



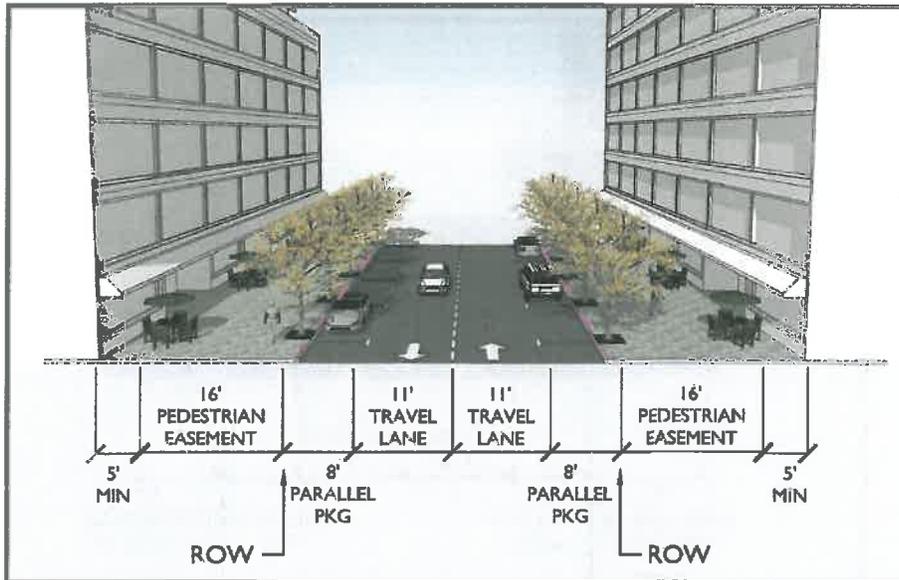
9.4.5 TOD Street Type A
Locator Map



TOD Street Type A

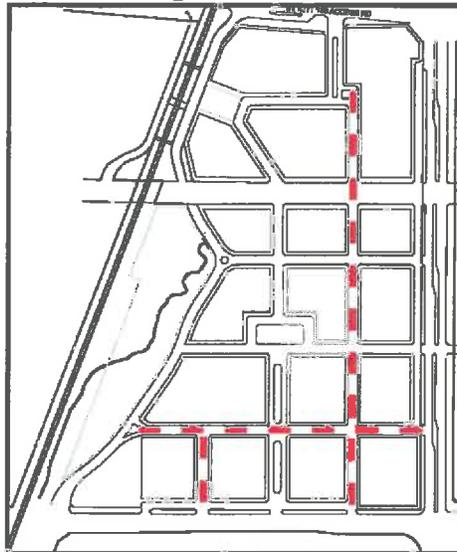
Street Type: A
ROW: 38'
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.6 TOD Street Type B

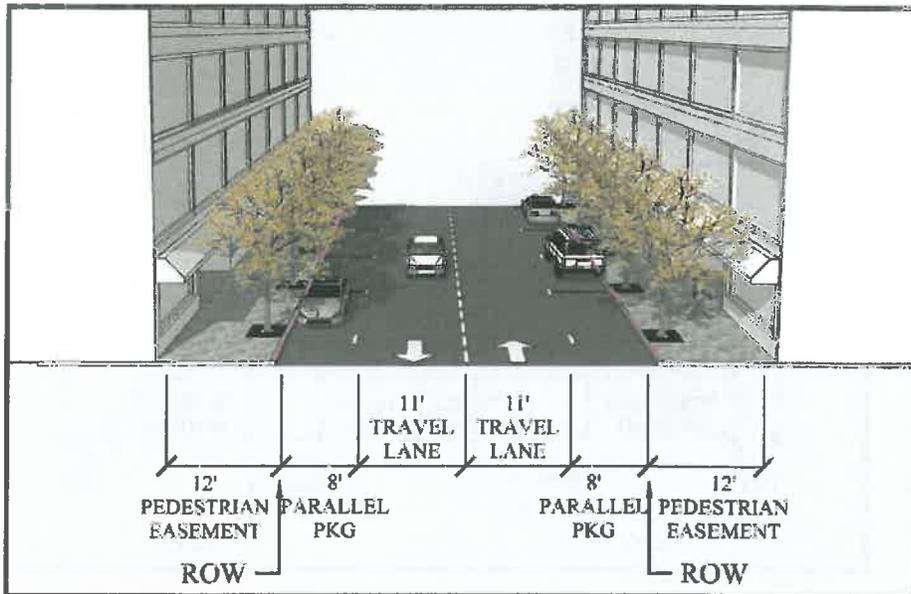
Locator Map



TOD Street Type B

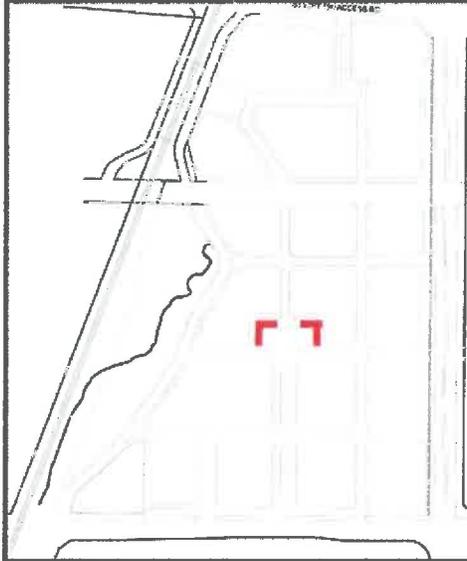
Street Type: B
ROW: 38'
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscapes Type: Street Trees (@ 40' OC avg)

Street Section



9.4.7 Court Street

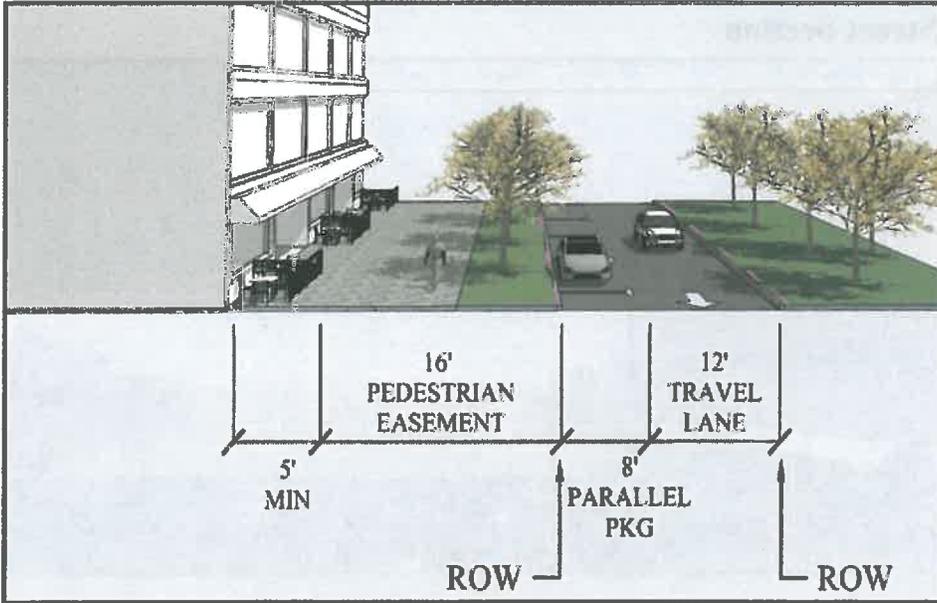
Locator Map



Court Street

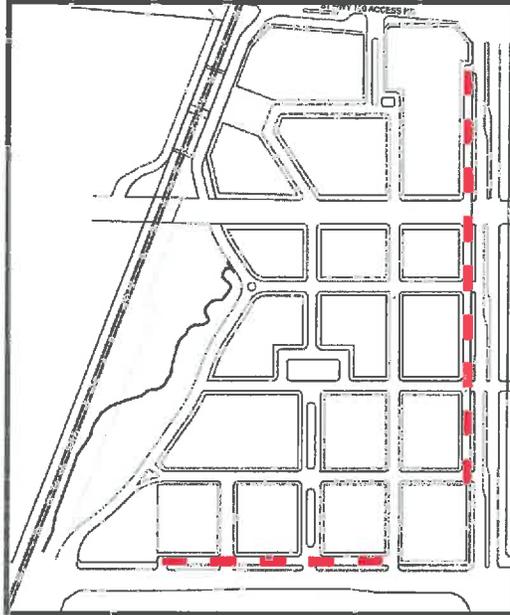
Street Type: A
ROW: 20'
Design Speed: 25 MPH
Parking: Parallel on one side
Number of Vehicular Lanes: 1
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.8 Plano/Renner Slip Road Frontage

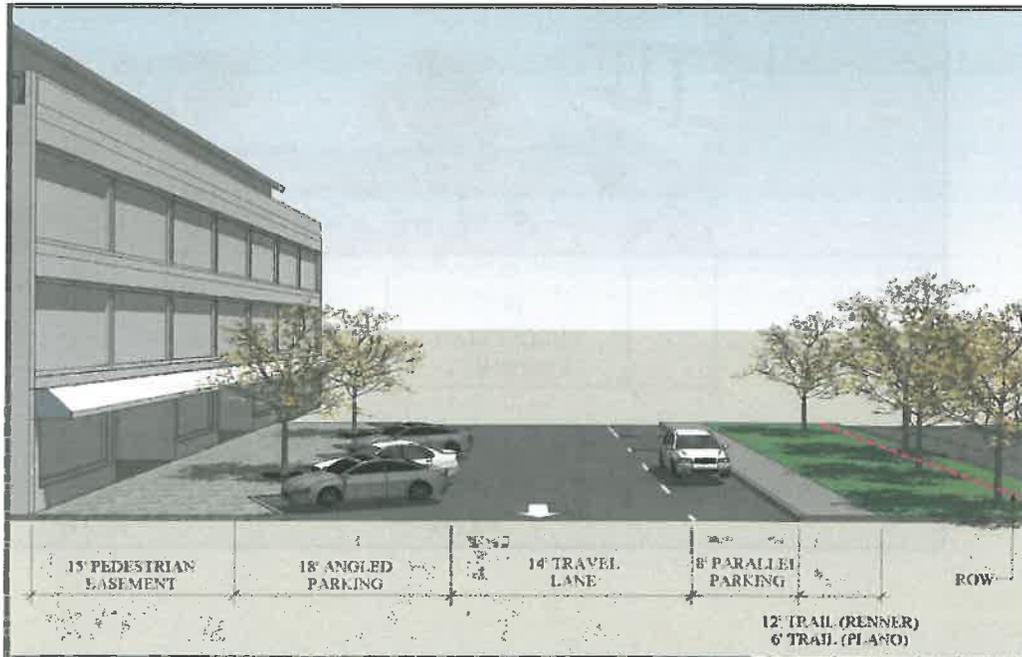
Locator Map



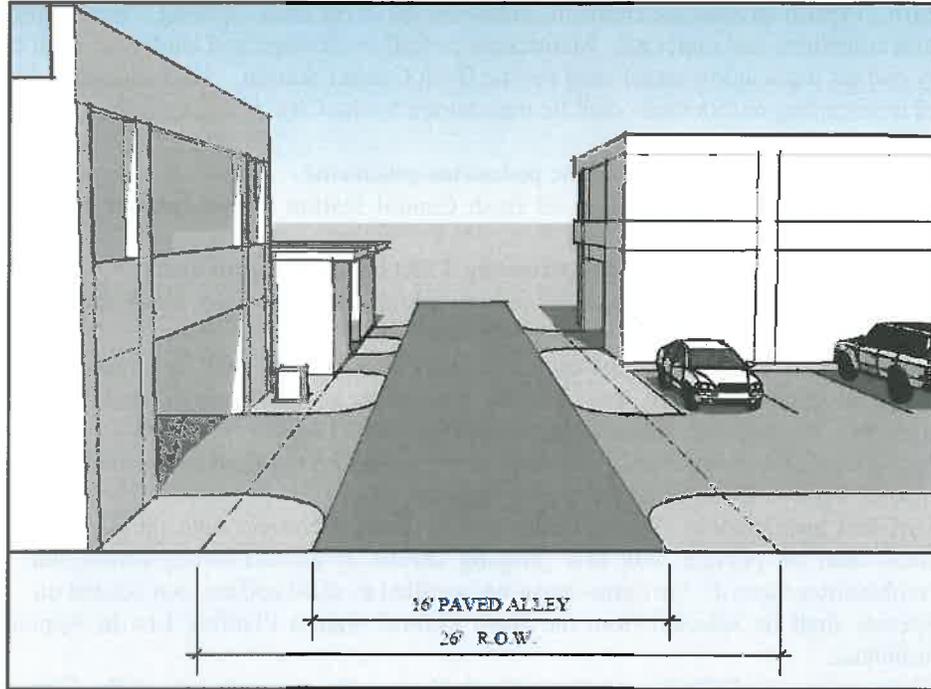
Slip Road (Plano / Renner)

Street Type: B
Slip Road Width: 40'
Design Speed: < 25 MPH
Parking: Head-in angled and parallel
Number of Vehicular Lanes: 1
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement / Trail
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.9 Commercial Alley



- 9.5 Streetscape & Landscape Standards: Streetscape standards shall apply to all streets within the Bush Central Station. Streetscape standards shall address all elements between the building face and edge of the curb. Typical streetscape elements addressed are street trees, lighting, street furniture and pedestrian amenities, and materials. Maintenance of all streetscape and landscape shall be by the property owners association established for the Bush Central Station. All dedicated public rights-of-way and landscaping within them shall be maintained by the City of Richardson.
- 9.6 Street Trees and Landscaping (within the pedestrian easement):
- 9.6.1 Street trees shall be required on all Bush Central Station Streets (except on alleys and PGBT access road.)
 - 9.6.2 Street trees shall be planted approximately 3 feet behind the curb line.
 - 9.6.3 Spacing shall be an average of 40 feet on center (measured per block face) along all streets except alleys.
 - 9.6.4 The minimum caliper size for each tree shall be 3 in. and shall be a minimum of 12 feet in height at planting. Each tree shall be planted in a planting area no less than 36 sq. feet. However, the tree well area may be no smaller than 25 sq.ft.
 - 9.6.5 Along the TOD Avenue and Park Avenue trees shall be required in the median and spacing and species shall be the same as the trees in the parkway.
 - 9.6.6 Turf and groundcover: When clearly visible from the street and alleys, all unpaved ground areas shall be planted with low growing shrubs or ground cover, ornamental grasses, or a combination thereof. Turf grass must be installed as solid sod and not seeded on.
 - 9.6.7 Species shall be selected from the Bush Central Station Planting List in Appendix B of this ordinance.
 - 9.6.8 Maintenance of all landscape materials shall meet the requirements of the City of Richardson Landscape Ordinance Requirements.
 - 9.6.9 Along arterials and highway access roads, street trees shall be planted within the required landscape parkway as per the City of Richardson Landscape Ordinance/Policies.
- 9.7 Street Furniture, Lighting, and Materials:
- 9.7.1 Pedestrian scale lighting shall be required along all Bush Central Station streets (except on alleys and PGBT access road). They shall be no taller than 20 feet.
 - 9.7.2 Street lights shall be placed at 50 feet on center, approximately 3 feet behind the curb line.
 - 9.7.3 The light standard selected shall be compatible with the design of the street and buildings.
 - 9.7.4 Trash receptacles and bike racks shall be required along all Type 'A' Streets. A minimum of one each per block face shall be required.
 - 9.7.5 Street furniture and pedestrian amenities such as benches are recommended along all Type 'A' Streets.
 - 9.7.6 All street furniture shall be located in such a manner as to allow a clear sidewalk passageway of a minimum of 6 feet.
 - 9.7.7 Materials selected for paving and street furniture shall be of durable quality and require minimal maintenance.

Section 10. Signage

Except as specifically listed below, all other signage and sign standards must comply with Chapter 18 of the City of Richardson Code of Ordinances, as amended.

- 10.1 For new signs, the standards in Table 10.1 shall apply and sign permits shall be approved administratively by the City of Richardson Building Official unless specifically noted in this section.

Table 10.1

Character Zone	TOD Core & Special Destination	TOD MU	Arterial MU	Highway MU	Standard
Sign Type					
Wall (Building) Signs	P	P	P	P	<ul style="list-style-type: none"> For all ground floor commercial uses (retail, office, and restaurant): One sign per tenant space; area to be calculated at 1.5 sq. feet per linear foot of public street frontage for the tenant space with a maximum of 100 sq. ft per tenant. Second and upper floor commercial uses may also be permitted one second floor wall sign per tenant space per public street frontage; area to be calculated at 1.5 sq. feet per linear foot of second or upper floor frontage along that public street with a maximum of 125 sq. feet. Institutional uses (non-profits and churches): One sign per tenant space; area to be calculated at 1.5 sq. feet per linear foot of public street frontage with a maximum of 100 sq. feet. Live-Work and Home occupations: One sign limited to an area of 20 sq. feet max. Building sign may encroach a maximum of 12" on to a sidewalk while maintaining a vertical clearance of 8 feet from the finished sidewalk. Building signs may be internally or externally lit. Marquee signs as only permitted as specified below.
Monument Signs	NP	NP	P	P	<ul style="list-style-type: none"> One monument sign per lot per lot street frontage (no more than 2 per lot separated by at least 100 feet) limited to a maximum of 50 sq. feet per sign face and 6 feet in height. Permitted only along PGBT access road, Type 'B' Streets and Slip Road Frontages.
Window Signs	P	P	P	P	<ul style="list-style-type: none"> Limited to 10% of the window area. <p>The following shall be exempt from this limitation:</p> <ul style="list-style-type: none"> Addresses, closed/open signs, hours of operation, credit card logos, real estate signs, and now hiring signs; Mannequins and storefront displays of merchandise sold; and Interior directory signage identifying shopping aisles and merchandise display areas.
Building Blade Signs	P	P	P	P	<ul style="list-style-type: none"> One per building (commercial and mixed use buildings only). Area = 30 sq. feet maximum per sign face. May encroach a maximum of 6 feet over a sidewalk, but shall not encroach over any parking or travel lane. Building blade signs may be attached to the building at the corners of building or along any street facing façade above the first floor facade.
Tenant Blade Signs	P	P	P	P	<ul style="list-style-type: none"> One per commercial tenant space (retail, office, or restaurant use). Area = 16 sq. feet maximum per sign face. May encroach a maximum of 4 feet over a public sidewalk, but shall not encroach over any parking or travel lane. Tenant blade signs shall be oriented perpendicular to the building façade and hung under the soffit of an arcade or under a canopy/awning or attached to the building façade immediately over the ground floor tenant space while maintaining a vertical clearance of 8 feet from the finished sidewalk.
Marquee Signs	P	P	P	P	<ul style="list-style-type: none"> Permitted for theatres, auditoriums, and other public gathering venues of 100 persons or more. Marquee signs shall be attached to the building or located above or below a canopy only. Area = 100 sq. feet maximum. Message board may be changeable copy (electronic and non-electronic). Electronic message boards shall be non-flashing.
For sale/for lease signs	P	P	P	P	<ul style="list-style-type: none"> Size is limited to 32 sq. feet per sign face. All other standards are the same as City or Richardson Sign Regulations.
Address signs	P	P	P	P	Same as City of Richardson Sign Regulations.
Temporary construction signs	P	P	P	P	1 free standing sign per lot during construction only; limited to 32 sq. feet.
Banners	P	P	P	P	Same as City of Richardson Sign Regulations.

Character Zone	TOD Core & Special Destination	TOD MU	Arterial MU	Highway MU	Standard
Sign Type					
Sandwich board signs	P	P	P	NP	<ul style="list-style-type: none"> Permitted only for retail, service, or restaurant uses. Limited to 12 sq. feet per sign face per storefront. Sign may not exceed 4 feet in height. A minimum of 6 feet of sidewalk shall remain clear. Chalkboards may be used for daily changing of messages. Readerboards (electronic and non-electronic) shall be prohibited. Sign shall be removed every day after the business is closed.
Light Pole Banners	P	P	P	P	<ul style="list-style-type: none"> Permitted only with approval of the City. Max. 10 sq. feet per sign face. Limited to one per light pole. All light pole banners shall be approved by the appropriate utility company prior to consideration by the City. Light pole banners shall be limited to publicize community-wide events, holiday celebrations, public art, and other city sponsored events.
Directory signs	P	P	P	P	<ul style="list-style-type: none"> Shall be allowed for all multi-tenant commercial and mixed use buildings only. One directory sign per multi-tenant building limited to 12 sq. feet in area. Design of the sign shall be integral to the façade on which the sign is to be affixed.
Pole signs	NP	NP	NP	NP	<ul style="list-style-type: none"> May only be permitted as part of a Master Sign Plan.
LED Signs	NP	NP	NP	NP	<ul style="list-style-type: none"> Shall be covered by a lens or diffuser. May only be permitted where electronic message boards are permitted within Bush Central Station. Or, may only be permitted as part of a Master Sign Plan.
Off-premise signs	NP	NP	NP	NP	

10.2 An applicant has the option to establish unique sign standards including size, color, type, design, and location. Such applications shall be reviewed as “Master Sign Plans” by the City of Richardson Building Official and are subject to approval of the Sign Control Board. In evaluating a Master Sign Plan, the Sign Control Board shall consider the extent to which the application meets the proposed Sign Plan:

- 10.2.1 Promotes consistency among signs within a development thus creating visual harmony between signs, buildings, and other components of the property;
- 10.2.2 Enhances the compatibility of signs with the architectural and site design features within a development;
- 10.2.3 Encourages signage that is in character with planned and existing uses thus creating a unique sense of place; and
- 10.2.4 Encourages multi-tenant commercial uses to develop a unique set of sign regulations in conjunction with development standards.

Section 11. Civic/Open Space Standards

11.1 The design of Civic/Open Space shall be regulated by the Civic/Open Space standards herein which shall create a network of open spaces that recognizes the natural qualities of the area while providing a range of both passive and active recreational opportunities. These opportunities may be accommodated in a variety of spaces ranging from large regional parks to neighborhood-scaled greens to urban squares and plazas. The open space network will be serviced by an interconnected network of trails and paths for pedestrians and bicyclists alike. Standards in Article 13-A, Section 5 of the City of Richardson Zoning Ordinance shall not apply to any development in the Bush Central Station.

11.2 Park Standards



The required park, as designated on the Regulating Plan, will create an important public space that connects the community within the Bush Central Station and allows for passive recreation. Parks shall primarily be naturally landscaped with many places to sit on benches or low walls. Passive recreation activities in parks may include grassy lawns for unstructured and informal active recreational activities. Appropriate civic elements, fountains or open shelters may be included. TOD Core and TOD Mixed Use buildings shall front onto and activate this space.

Typical Characteristics

General Character

- Large, open space
- Spatially defined by landscaping and building frontages
- Paths, trails, open shelters, lawns, trees and shrubs naturally disposed
- May be lineal, following the trajectories of natural corridors

Location and Size

Location and size shall be as shown on the Regulating Plan (Appendix A).

Typical Uses

- Unstructured and passive recreation
- Casual seating/picnicking

11.3 Green Standards



The required green, as designated on the Regulating Plan, will serve as an important public space for the Bush Central Station. The green will be available for civic purposes, commercial activity, unstructured recreation and other passive uses. Greens shall primarily be naturally landscaped with many shaded places to sit. Appropriate paths, civic elements, fountains or open shelters may be included and shall be formally placed within the green. TOD Mixed Use buildings, Arterial Mixed Use buildings or Neighborhood Frontage buildings shall front onto and activate this space.

Typical Characteristics

General Character

Open space

Spatially defined by landscaping and building frontages

Lawns, trees and shrubs naturally disposed

Open shelters and paths formally disposed

Location and Size

Location and size shall be as shown on the Regulating Plan (Appendix A) and shall range between 0.38 – 0.75 acres.

Typical Uses

Unstructured and passive recreation

Casual seating

Commercial and civic uses

Residential address

11.4 Square Standards



The required square provides an important terminated vista and sight line from the station platform. The square will serve as an open space available for civic purposes, commercial activity, unstructured recreation and other passive uses. The square should have a more urban, formal character and be defined by the surrounding building frontages and adjacent tree-lined streets. All buildings adjacent to the square shall front onto the square. Adjacent streets shall be lined with appropriately scaled trees that help to define the square. The landscape shall consist of lawns, trees, and shrubs planted in formal patterns and furnished with paths and benches. Shaded areas for seating should be provided. A civic element or small structure such as an open shelter, pergola, or fountain may be provided within the square.

Typical Characteristics

General Character

Formal open space

Spatially defined by buildings and tree-lined streets.

Open shelters, paths, lawns, and trees formally arranged

Walkways and plantings at all edges

Located at important intersection

Location and Size

Location and size shall be as shown on the Regulating Plan (Appendix A).

Typical Uses

Unstructured and passive recreation

Casual seating

Commercial and civic uses

11.5 Plaza Standards



Plazas add to the vibrancy of streets within the more urban zones and create formal open spaces available for civic purposes and commercial activity. Building frontages shall define these spaces. The landscape should consist primarily of hardscape. If trees are included, they should be formally arranged and of appropriate scale. Casual seating, along with tables and chairs, should be provided. Plazas typically should be located at the intersection of important streets.

Typical Characteristics

General Character

Formal open space

Primarily hardscape surfaces

Trees and shrubs optional

Spatially defined by building frontages

Location and Size

Location and size shall be as shown on the Regulating Plan (Appendix A).

Shall front on at least one (1) street.

Typical Uses

Commercial and civic uses

Casual seating

Tables and chairs for outdoor dining

Retail and food kiosks

11.6 Pedestrian Passage Standards



Pedestrian passages create intimate passageways through buildings at designated locations. These paths provide direct pedestrian access to residential addresses and create unique spaces for frontages to engage and enter off of. Pedestrian passages allow for social and commercial activity to spill into the public realm. Pedestrian passages should consist of a hardscape pathway activated by frequent entries and exterior stairways. The edges may simply be landscaped with minimal planting and potted plants.

Typical Characteristics

General Character

- Hardscape pathway
- Frequent entries and frontages
- Exterior stairways
- Defined by building frontages
- Minimal planting and potted plants
- Maintain the character of surrounding buildings

Standards

Min. Width 12 feet

Location shown on the Regulating Plan (Appendix A).

Typical Uses

- Pedestrian connection and access
- Casual seating

11.7 Multi-Use Trail Standards



The multi-use trail provides an important place for active recreation and creates a connection to regional paths and biking trails. The multi-use trail will run continuously from the station platform, through the required park, and along Renner Road. The multi-use trail will help activate connections between the open spaces and the uses throughout the Bush Central Station. The multi-use trail may have a different character as it passes through the park and as it is adjacent to Renner Road. Within the required park, the trail shall be naturally disposed with low impact paving materials so there is minimal impact to the existing creek bed and landscape. Along Renner Road, the trail shall have a more formal disposition with a paved trail and regular landscaping.

Typical Characteristics

General Character

Park Multi-Use Trail:

Naturally disposed landscape

Low impact paving

Trees lining trail for shade

Appropriately lit for safety

Renner Road Multi-Use Trail:

Hardscape Path

Formally disposed pedestrian furniture, landscaping and lighting

Trees lining trail for shade

Standards

Min. Width 12 feet

Location shown on the Regulating Plan (Appendix A).

Typical Uses

Active and passive recreation

Casual seating

11.8 Playground Standards



Playgrounds shall be permitted in parks and greens to provide open space designed and equipped for the recreation of children. These playgrounds should serve as quiet, safe places -- protected from the street and typically located where children do not have to cross major to access. Playgrounds may be fenced. An open shelter, play structures or interactive art and fountains may be included with landscaping between. Shaded areas and seating shall be provided. Playground equipment and design must be reviewed and approved by the City prior to installation.

A larger playground may be incorporated into the park, whereas a more intimate playground may be incorporated into the green.

Typical Characteristics

General Character

Focused toward children of all ages

Fenced with minimal exits (non mandatory)

Open shelter

Shade and seating provided

Play structure, interactive art or fountains

Standards

Min. Size N/A

Max. Size N/A

As described by civic space type in which playground is located

Protected from traffic

No service or mechanical equipment

Typical Uses

Active and passive recreation

Unstructured recreation

Casual seating

11.9 Ancillary Structure Standards



Ancillary structures should be formal in character and generally related to but clearly subordinate to surrounding buildings. Each individual structure should keep in character with the style of nearby buildings. Typically, these structures are located at prominent locations within an appropriate civic space. Ancillary structures located in more urban zones may have minor commercial uses, such as small food or news vendors, but may also serve as civic elements for general public use with more passive activities.

Typical Characteristics

General Character

Formal character

Relating to style of surrounding buildings

One or more open sides

Covered or providing shade

Small, stand alone structure

Located within Park, Green, Square or Plaza

Standards

Min. Size N/A

Max. Size N/A

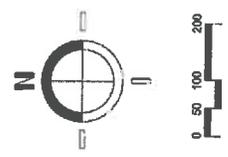
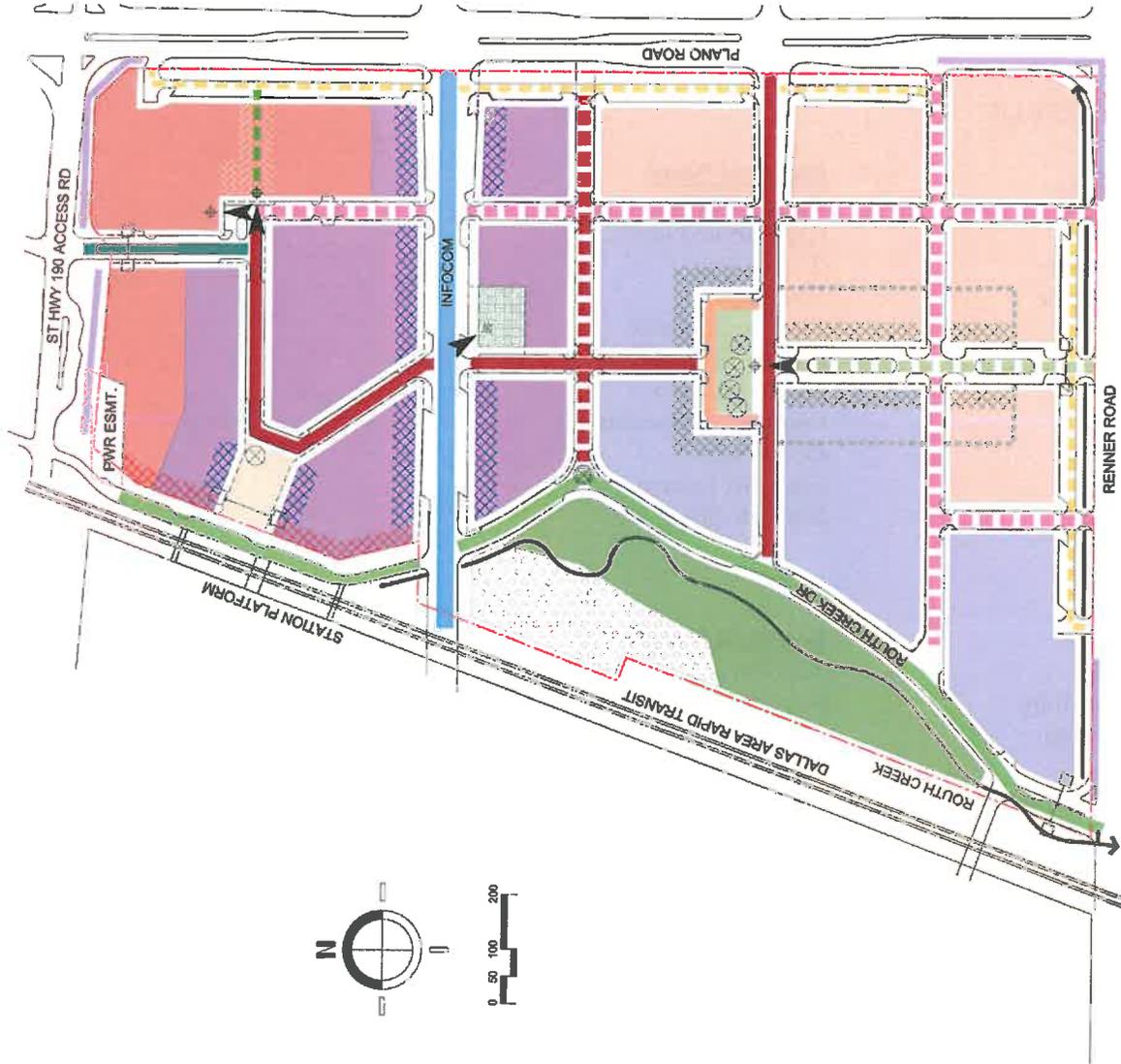
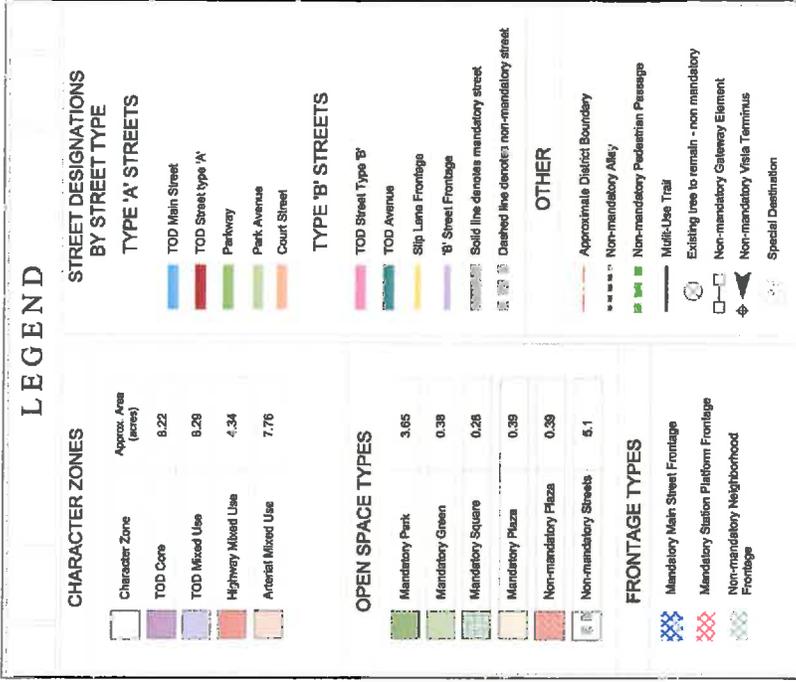
Typical Uses

Civic purposes

Minor commercial uses

Casual seating/picnicking

APPENDIX A



REGULATING PLAN

Bush Central Station Planned Development Code

1/1/2010



**Appendix B
Planting List**

The following lists contain all species approved for use in the Bush Central Station. It contains native and acceptable adapted species. Other species that are drought tolerant and adaptive may be used for planting within the Bush Central Station. The use of alternative species may be permitted with the approval of the City.

CANOPY/STREET TREE LIST

<u>Common Name</u>	<u>Botanical Name</u>
Live Oak	<i>Quercus virginiana</i>
Red Oak	<i>Quercus shumardi</i>
Bald Cypress	<i>Taxodium distichum</i>
Sweetgum	<i>Liquidambar styraciflua</i>
Cedar Elm	<i>Ulmus crassifolia</i>
Lacebark Elm	<i>Ulmus parvifolia</i>
Bigtooth Maple	<i>Acer grandidentatum</i>
Caddo Maple	<i>Acer saccharum 'Caddo'</i>
Texas Ash	<i>Fraxinus velutina 'Rio Grande'</i>
Bur Oak	<i>Quercus macrocarpa</i>
Chinquapin Oak	<i>Quercus muhlenbergii</i>
Escarpment Live Oak	<i>Quercus fusiformis</i>
Ginkgo	<i>Ginkgo biloba</i>

ORNAMENTAL TREE LIST

<u>Common Name</u>	<u>Botanical Name</u>
Yaupon Holly	<i>Ilex vomatoria</i>
Crape Myrtle	<i>Lagerstromia indica</i>
Deciduous Yaupon	<i>Ilex decidua</i>
Southern Crabapple	<i>Malus app.</i>
Chinese Pistache	<i>Pistacia chinensis</i>
Mexican Plum	<i>Prunus Mexicana</i>
Wax Myrtle	<i>Myrica carifera</i>
Chitalpa	<i>Chitalpa tashkentensis</i>
Deciduous Holly	<i>Ilex decidua</i>
Desert Willow	<i>Chilopsis linearis</i>
Eve's Necklace	<i>Sophora affinis</i>

SHRUBS LIST

<u>Common Name</u>	<u>Botanical Name</u>
Dwarf Nandina	<i>Nandina domestica 'nana'</i>
Dwarf Burford Holly	<i>Ilex cornuta 'burfordi nana'</i>
Abelia Grandiflora	<i>Abelia grandiflora</i>
Barberry	<i>Barberry spp.</i>
Yucca (Red, Yellow or Soft Tip)	<i>Hesperaloe parviflora</i>
Texas Sage	<i>Leucophyllum frutescans</i>
Indian Hawthorn	<i>Raphiolepis indica</i>
Dwarf Crape Myrtle	<i>Lagerstromia indica 'nana'</i>
Dwarf Yaupon Holly	<i>Ilex vomitorria 'nana'</i>

Pampas Grass	<i>Cortaderia selloana</i>
Black-Eyed Susan	<i>Rudbeckia hirta</i>
Dwarf Wax Myrtle	<i>Myrica pusilla</i>
Needlepoint Holly	<i>Ilex cornuta 'Needle Point'</i>
Knockout Rose	<i>Rosa 'Knock Out'</i>
Rosemary	<i>Rosmarinus officinalis</i>

GROUND COVER/VINES LIST

<u>Common Name</u>	<u>Botanical Name</u>
Asian Jasmine	<i>Trachelosperum Asiaticum</i>
Big Blue Liriope	<i>Liriope muscari</i>
Mondograss	<i>Ophiopogon japonicus</i>
Purple Winter Creeper	<i>Euonymum coloratus</i>
Santolina	<i>Santolina virens</i>
Trumpet Vine	<i>Campsis radicans</i>
Virginia Creeper	<i>Parthenocissus quinifolia</i>
Lady Banks Rose	<i>Rosa banksiaw lutea</i>
Confederate Jasmine	<i>Trachelospermum jasminoides</i>
Crossvine	<i>Bignonia capreolata</i>
Evergreen Wisteria	<i>Millettia reticulata</i>
Lantana 'New Gold'	<i>Lantana camara 'New Gold'</i>
Liriope 'Silver Dragon'	<i>Liriope muscari 'Silver Dragon'</i>
Prostrate Rosemary	<i>Rosmarinus officinalis prostrata</i>
Sweet Autumn Clematis	<i>Clematis terniflora</i>

ORNAMENTAL GRASSES LIST

<u>Common Name</u>	<u>Botanical Name</u>
Dwarf Fountain Grass 'Little Bunny'	<i>Pennisetum alopecuroides 'Little Bunny'</i>
Dwarf Maiden Grass	<i>Miscanthus sinensis 'Adagio'</i>
Fountain Grass	<i>Pennisetum alopecuroides</i>
Inland Sea oats	<i>Chasmanthium latifolium</i>
Maiden Grass	<i>Miscanthus sinensis 'Gracillimus'</i>
Mexican Feather Grass	<i>Stipa tenuissima</i>
Muhly Grass	<i>Muhlenbergia capillaris</i>
Weeping Lovegrass	<i>Eragrostis curvula</i>

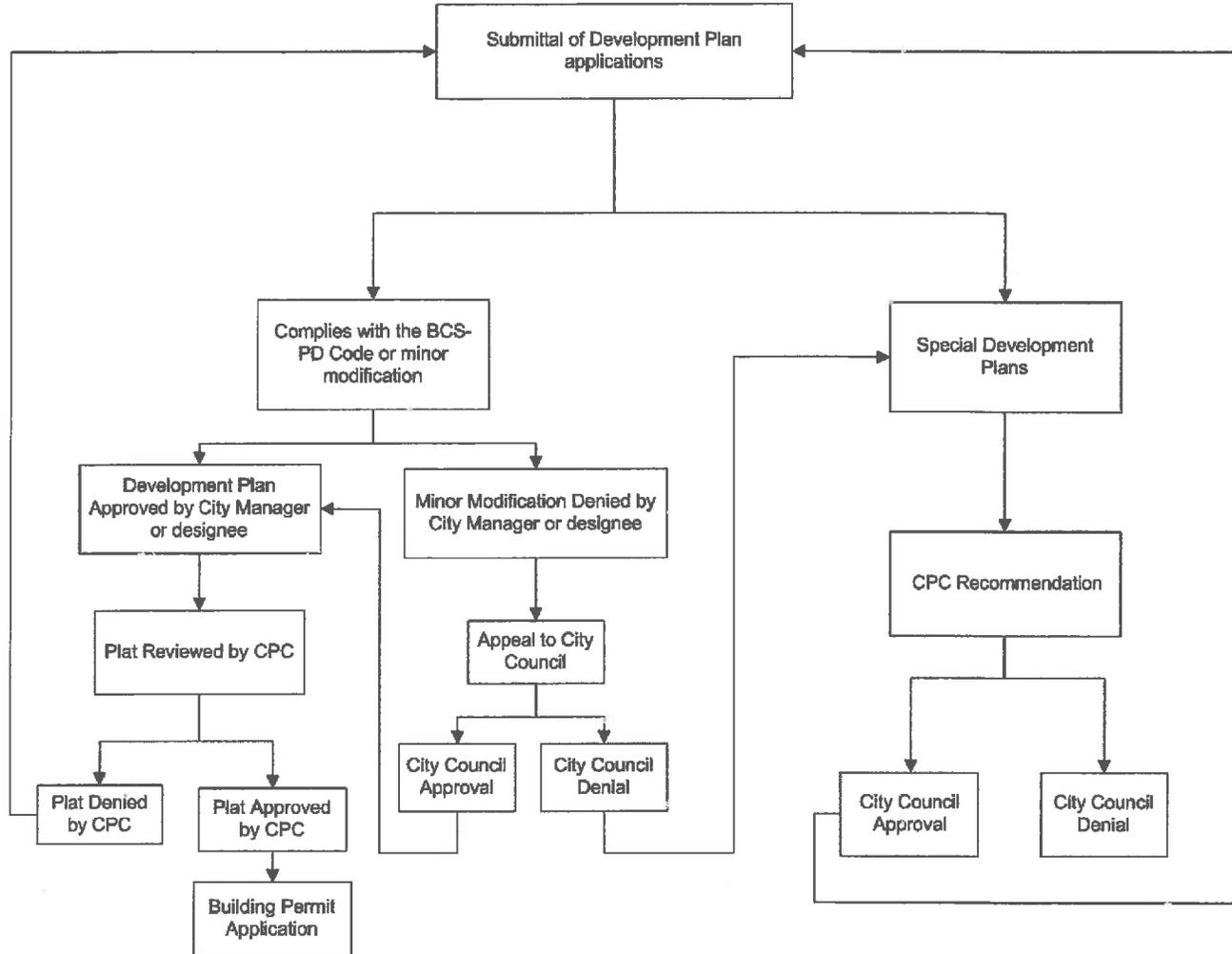
TURF

<u>Common Name</u>	<u>Botanical Name</u>
Bermuda	<i>Cynodon dactylon</i>
St. Augustine	<i>Stenotaphrum secundatum</i>
Zoysia	<i>Zoysia tenuifolia</i>

These plantings may be placed in Civic/Open Spaces or used to meet the private landscaping requirements of the Code. The applicant shall select drought tolerant, low maintenance, and adaptable shrubs and ground cover based on the placement on the site subject to approval by the City.

Appendix C

**Bush Central Station
Development Review Process**



BCS-PD: Bush Central Station Planned Development
 CPD: City Plan Commission
 CC: City Council
 SDP: Special Development Plan

Note: Development Plans include site plans, building elevations, landscape plans, civil engineering plans, and all plats

ZF 10-21

**Letters in
Support**

the Parliament planned development

Michael Tannery

to:

Chris.Shacklett

12/06/2010 03:45 PM

Cc:

amir.omar, gary.slagel, bob.macy, steve.mitchell, bob.macy, john.murphy, bob.townsend,

Mark.Solomon

Show Details

Chris,

Unfortunately I will not be able to attend the hearing on Tuesday.

I am in favor of the continued development of Richardson in the quality and manner of the current multi use developments similar to Eastside and the Eisemann area.

Maitri Smithhisler [chair@protectrichardson.org] does not represent the ideas and thoughts of the many, only the few.

You have my support on this project

Michael Tannery

1117 Stratford

Richardson

214-577-7900



Re: Zoning: Land Held By Bush/75 Partners LP and Others

Robert Quance

to:

Chris.Shacklett

01/07/2011 04:21 PM

Show Details

My wife and I are in favor of the rezoning request for the referenced property as the land would fall under new regulations to be approved by our City Council, that would be specify requirements to promote quality design and construction.

Robert and Ruth Quance

2107 White Cliff LN

Richardson, TX 75080

Ph 972-231-4065

On Fri, Jan 7, 2011 at 4:01 PM, <Chris.Shacklett@cor.gov> wrote:

Here is my email.

Chris Shacklett

Planner

Department of Development Services

City of Richardson

972.744.4249

chris.shacklett@cor.gov

ZF 10-21

**Letters in
Opposition**

Proposed Parliament project
Alan Melson
to:
Chris.Shacklett
12/06/2010 10:06 PM
Show Details

Mr. Shacklett:

My name is Alan Melson. I grew up on the east side of Richardson, and returned to the city in 2004 after purchasing a house in Heights Park. The main reason I moved back after college was because I feel strongly about the quality of life in this city - the solid city government, schools, corporate base and infrastructure, among other things.

Thus, I was extremely concerned to learn of the plans by Parliament Group to build high-density apartment housing on the old Hunt family land along Renner Road on the city's northern edge. The information I was given suggests buildings up to ten stories high, with up to 3,760 units. This is an unacceptably high number of units for an area that already sees heavy traffic during morning and evening rush hour, particularly with the addition of Routh Creek Parkway and traffic related to the new Blue Cross Blue Shield headquarters.

A development of that size does not seem like the best long-term use for that land, given potential effects on adjacent areas. It could hurt property values in the Foxboro neighborhood to the southeast, and damage the serenity of the Spring Creek Nature Preserve across Renner. It may also have a negative effect on existing higher-density developments in the city (Galatyn, Brick Row, etc.) that continue to struggle with occupancy rates.

I am an advocate of mixed-use development and higher levels of density, especially given the property's proximity to DART light rail. I also understand that this particular land will be developed sooner or later. However, I feel strongly that these plans are too much for this tract, and urge you and other planners to favor a smaller project that is a more appropriate use.

Thank you for taking the time to read and consider this message.

Sincerely,

Alan Melson



Apartments at Renner and Plano Roads
Anne Patillo to: Chris.Shacklett

12/06/2010 08:33 PM

As a citizen and a property-tax-paying resident of Richardson, I strongly oppose more apartments in Richardson, Texas. We need no more as there is an overly abundant supply of new and old apartments in the city. Look at Spring Valley Road, the Brick Row project, Campbell Road on the east side of Central Expressway, and others.

Please stop this infiltration of rent property before our city becomes just another slum of Dallas County.

Anne S. Patillo
418 Lynn Street
Richardson, TX 75080
972-231-8614

Richardson Apt. development
Caroline Mecom
to:
Chris.Shacklett
12/06/2010 10:09 PM
Show Details

PLEASE let's NOT have 10-story high apt. buildings in Richardson! Smaller, attractive apartment buildings set back from the street and set at angles to each other allowing for green space and trees and some walking room is what will benefit the residents, the neighbors and the city in the long run. PLEASE let's place more restrictive parameters on the developers of both the Caruth and Parliament tracts!

**Thank you! Caroline Mecom
1232 Comanche Dr.
Richardson**

Concern regarding proposed development at Plano and Renner
Kinnaird, Clark

to:

Chris.Shacklett@cor.gov

12/06/2010 09:26 PM

Cc:

"rsvp@richardsoncitizensalliance.com"

Show Details

Chris Shacklett,

I will be unable to attend the CPC meeting tomorrow night due to previous scheduled work meetings.

Regarding the TOD Request North of Renner Road which is on the agenda for tomorrow nights CPC meeting; there seem to be several issues that have not been fully analyzed with regards to local occupancy rates, impact on traffic, and development density. I would request that rather than move forward with any decision about the zoning, city staff should address the concerns of local homeowners regarding these issues. As now proposed, I would be strongly against the TOD as described in the agenda packet.

-Clark Kinnaird
235 Shady Hill Drive
Richardson Texas

"The future depends on what we do in the present."

- Mahatma Gandhi



TOD Request North of Renner Road
Cyndi Dupuis to: Chris.Shacklett

12/06/2010 09:24 PM

As an 8 year resident, and business owner in Richardson, I would just like to let you know that I am highly opposed to the proposed construction of the apartment complex at Plano & Renner Road.

I certainly hope that you will share in, and value the opinions of the residents of Richardson. I do not believe that this is a direction that would be good for the city whatsoever.

Thanks you,
Cyndi Dupuis
1316 Chesterton Dr.
Richardson, TX 75080

RE: Rezoning request for Renner Road
Joel Crisalli
to:
Chris.Shacklett
12/06/2010 09:06 PM
Cc:
jrcrisalli
Show Details

I hope this one comes to you.

From: Joel Crisalli [<mailto:Joel.crisalli@bx.rr.com>]
Sent: Monday, December 06, 2010 9:04 PM
To: 'Chris.Shacklett@cor.gov.'
Cc: 'g.human@sbcglobal.net'; 'jrcrisalli@gmail.com'
Subject: Rezoning request for Renner Road

I have been reading some of the material that has been circulated about the proposed mixed use complex on Renner Road. Unfortunately I am unable to attend tomorrow's meeting but would like to express my deep concern about the dramatic effect a project of the size mentioned would have on this area.

We have lived in the Sherrill Park North development for nearly 25 years and have been pleased, until now, with the way that this section of our town has grown, been maintained and prospered. Our City and its government has handled an aging city with a delicate hand and kept it from becoming an urban afterthought. Roads and city resources have been well managed and our planners have earned our confidence.

I am confident that you and the rest of our city government wish to maintain this balance. Unlike the previously undeveloped section of the telecom district which has now become our new urban heart near the Eisemann or the Central/Campbell complex which carefully places a mixed use hub on the Dart Line which is second to none, the current project under consideration adds an element of stress to an already taxed road system and places a residential area, a nature preserve and even our municipal golf course in the shadow of what seems to be a disproportionately large and potentially threatening environment.

In fairness, the Parliament Group should be permitted to submit their proposal for open-minded review and joint discussion. However, I'd like to register that any residential complex which exceeds two stories or, in combination, more than 500 units would not be acceptable to me or my family.

If there is a written proposal published, I should appreciate knowing how to access it.

I repeat my thanks to you and our other City of Richardson staff for your work and regret that I will not be able to be present at this meeting.

Thank you,

Joel R. Crisalli
1511 Ambleside Lane
Richardson, TX 75082

Re: Request to Richardson to allow the construction of a very large apt complex

Gary Henderson

to:

<rsvp@richardsoncitizensalliance.com>, chris.shacklett@cor.gov

12/06/2010 10:48 PM

Show Details

Chris,

Cannot make the meeting but stand opposed to this complex. If anything we need more single family housing.

Have not heard all the data. But, on the service it doesn't sound like the best type of development for COR.

Gary Henderson

214.282.2222

Sent from my iPhone

On Dec 6, 2010, at 8:26 PM, <rsvp@richardsoncitizensalliance.com> wrote:

This correspondence is intended to make you aware of a request to the City of Richardson to allow the construction of an extremely large apartment complex at the northwest corner of Plano and Renner Roads. The applicant is requesting not to be limited to the number of apartments, but to only be limited to 100 feet in height with the building wall being as close as 6 feet from the road. This approach allows the applicant to build as many apartments on this property as they deem appropriate as long as they don't exceed the height limit which is similar to a ten story building. For planning purposes, City staff has estimated there would be around 3,760 apartments. This would be the largest apartment complex in Richardson and it is only Phase I as the applicant also owns the land across Renner Rd. which is currently approved to have 2,000 additional apartments built on it. There are additional apartments also approved to be built on the southwest corner of this intersection.

The concerns are; a significant increase in traffic and crime, and the over 6,000 apartments at this intersection will cause significant downward pressure on apartment rental rates. This last item will bear an even greater burden to other Richardson apartment complexes like the The Block, Eastside, Brick Row, Galatyn, etc. which have not been able to achieve even reasonable occupancy rates. Reduced rental rates can easily open the door to having West Spring Valley Road crime problems in multiple areas of Richardson. With only 3% left of undeveloped land in Richardson, this is not the direction we need to be taking!

You can help do something to stop this nonsense. Please read the letter below and let your Richardson elected officials know you do not want this happening in Richardson.

The Richardson Citizens Alliance

**7PM, December 7th Plan Commission Hearing
On the TOD Request North of Renner Road**

I just completed studying the 238 page, December 3rd City Staff report on the proposed development north of Renner Road including the Caruth 86 acre tract west of the DART rail and the 57 acre Parliament tract east of the DART rail. Our neighborhood is most concerned about the 100 foot tall buildings planned to house an unlimited number of apartments that would be located east of the rail line and west of Plano Road with almost no setbacks from Renner Road.

The staff's report and traffic impact analysis, based on 3,756 apartments in 100 foot tall buildings and 300 foot tall office buildings, recommends very minimal improvements for the expected traffic and is silent about the resulting level of service on the already congested Renner and Plano Roads and the US75/Renner intersection, which is already heavily congested during the AM and PM travel peak hours.

The staff report contains no concerns or reservations about this development which places no restriction on the actual number of apartments, almost no street setbacks and even allows 15 to 25% increases on building heights if desired by the applicants.

No development in Richardson, even including those along the DART line, currently has more than 528 units. So, these owners are asking for at least seven times the number of apartments in the largest complex in Richardson. The Caruth tract now has no zoning for apartments and the Parliament tract zoning allows 429 units. We already have existing about 2,000 apartments mostly fronting on Renner Road west and south of the Bush Toll Road.

The Neighborhood Protections Alliance of Richardson (NPAR) sharing our concerns created a committee to review the plans and form an official position on the quality of life impact of this development. But time is of the essence and your presence is need Tuesday night in order for the CPC to realize this issue is of concern to the citizens. If you cannot attend please send your comments to Chris.Shacklett@cor.gov.

Of even greater concern to our neighborhood and that of Fairways of Sherrill Park to the east of our Sherrill Park neighborhood is that Parliament owns the property to the east of Plano Road as well and it is already zoned for 2,000 more apartments and the forested area southwest of the Plano/Renner Roads intersection is zoned for somewhere around 300 apartments.

Our only hope to get all apartment zoning along Renner Road down to a reasonable level, is to start with restricting the currently proposed development to a reasonable increase in the allowed number sufficient to support the Transportation Oriented Development desired by our current City Council.

Please plan to attend Tuesday night's City Plan Commission Hearing, and if you have time study the Plan Commission Packet found at <http://www.cor.net> under Plan Commission Agenda. At the Plan Commission Meeting, only a total of 15 minutes is allowed for speakers in opposition to the proposed rezoning. So we need a full house (150+people) at the hearing to see what the Plan Commission plans to do with this application. I fear that if only a few people attend, that the Commission will assume that there is no opposition and will recommend approval to the City Council without any restrictions.

George Human, President, Sherrill Park Neighborhood Association
12/5/2010



Apartment zoning
Nancy Burger to: Chris.Shacklett@cor.gov

12/06/2010 08:42 PM

I am very much opposed to the large number of apartments the developer wants to build at Plano and Renner. There is absolutely no need for these apartments, as so many of those already built are still vacant. We are fortunate to live in a city where the planners consider all aspects of a project before voting, and I sincerely hope that they will do the same in this instance.

Thank you for considering my opinion.
Nancy Burger

Sent from my iPhone
Nancy Burger

Parliament Development Renner & Plano Rd.

mansonpr

to:

chris.shacklett

12/06/2010 08:46 PM

Show Details

As a resident of Sherrill Park North, I am both excited and concerned about the proposed development at Plano Rd. and Renner Rd. While I think that the addition of nearby shopping and restaurants can be an asset to the area, I am highly concerned about the proposed number of apartments, and the overall height of the buildings, especially near the intersection itself (and not closer to the George Bush DART station).

Traffic congestion in the area at morning and evening rush hours already makes it quite difficult to get in and out of our subdivision at both Owens Blvd. / Renner Rd. and Plano Rd. / Braeburn. The addition of more vehicles in and out of the area at these times would only exacerbate the problem. Regardless of the intent of this being a walking urbanism type development, the reality is that there will still be people who are working in the development who do not live there, and people who live there that do not work there. And with the higher number of apartments proposed, the likelihood of the number of people who fall into the category of living there and not working there would be greater. While hopefully many would be living there to take advantage of access to the DART rail, there will still be an increase in traffic, and I have heard of no proposed improvements to the roadways to try and deal with this added congestion.

I would be in support of a development that is less "urban" feeling re: the height of the buildings, and less dense as far as the number of apartments. Ideally I'd love to see the residential aspect of the development geared more toward townhouses, but realize that the value of the land might make that impractical to the developers.

Peggy R. Manson
1503 Banbury Ct.
Richardson, TX 75082



Objections to overbuilding the Plano Parkway transit station development

richard semock to: chris.shacklett

12/06/2010 09:28 PM

Cc: spnfana

I moved to the Sherrill Park neighborhood because of the rural atmosphere and uncongested streets near the heart of Richardson City. The development at Plano&Renner roads as currently planned will destroy this atmosphere with a glut of apartments and soaring office towers which will overshadow and crowd out the residential setting.

I urge the city council therefore to rule against overbuilding this complex and return to a more pedestrian concept as originally envisaged. Done right, this development can add value to the surrounding neighborhoods by adding to the string of pearls along the Dart corridor and providing ready access to DFW via the Cottonwood terminal.

--

Richard Semock
3206 Owens Blvd



svargas to: chris.shacklett

12/06/2010 09:13 PM

Mr. Shacklett,

I am a resident who lives at 2702 Foxboro in Richardson. I am writing to voice my displeasure over the planned development at the NW corner of Renner and Plano Roads. While I am very much in favor of transit oriented development, I believe that this development must be done taking into account of the existing neighborhoods and businesses surrounding that development. I would urge you to greatly consider reducing the number of apartments proposed, heights of buildings and look at traffic impact/improvements.

As a 15 year resident of Richardson, I urge you to consider the input of the residents who live in this area.

Thank you,

Stella Vargas



**The Parliament planned development at the NW corner of Renner and Plano
Roads**

BALDWIN, AARON R (ATTSI) to: Chris.Shacklett
Cc: "Angela Baldwin"

12/07/2010 09:28 AM

Mr. Shacklett,

As a 9 year resident of Richardson's Sherrill Park, I want to document my concerns over the planned development at the NW corner of Renner and Plano Roads.

My wife and I will unfortunately not be able to attend tonight's Plan Commission Meeting but in general, we support the position taken by NPAR and George Human, President of Sherrill Park HOA.

Our primary concern is with the overdevelopment of apartments and any buildings over 100 feet.

Thank you for your attention to our concerns.

Aaron and Angela Baldwin
1604 Pickwick Ln.
Richardson, TX 75082

Request to Richardson to allow the construction of a very large apt complex

Ann Nash

to:

Chris.Shacklett

12/07/2010 08:51 AM

Cc:

Betty Morton, Angie Wright, Chrissy Cortez-Mathis, Ed Hassler, Hermayne Merritt, Howard Davis, Ida Hassler, Janice Leventhal, Jim Bates, M ELISE DOHERTY, "Robert 'Trey' Kazee III", Sean Cortez-Mathis, Virginia Costlow, gary.slagel, john.murphy, Amir Omar, bob.townsend, mark.solomon, bob.macy, steve.mitchell

Show Details

I am unable to attend the meeting tonight.

I have received communication (via email from a group called Richardson Citizens Alliance) concerning this proposal for a large apartment complex in east Richardson.

I am totally opposed to adding this number of apartments to Richardson. Short term, I think this creates problems with overcrowded schools and more policing necessary. Long term I think it creates a future situation like we currently have with Spring Valley and the underkept older apartments in east Richardson. Apartments generally do not maintain value over the long term.

We should not squander our last pieces of undeveloped land just to have something on it. This needs to be part of an overall development plan for the city.

Ann Nash

317 Dogwood

Richardson, TX 75080



apartments at Plano and Renner
anne.mattson to: Chris.Shacklett

12/07/2010 04:07 PM

Dear Sir-

I have just now been made aware of the massive amount of apartment that are considering being built at Renner and Plano roads. The information I received show that there is no restrictions on numbers of apartment or setbacks. That nothing is in the planning for improvement of the roads in an already congested area at traffic times. There are already a large number of good apartments in Richardson that are vacant. Normally when this large a number of apartments goes in there is an increase in traffic and crime. Is this what we really want for this neighborhood. Please consider restrictions for this land at your meeting tonight. Thank you Anne Mattson

TOD Development North of Renner Road

ardismgood

to:

Chris.Shacklett

12/07/2010 04:21 PM

Show Details

I will be unable to attend the Council meeting on Dec. 7th but do want to voice my opposition to the Zoning Change Request for the TOD Development.

I feel the zoning change requested is unreasonable and will not enhance the area but, to the contrary, will degrade the lives of those living in the adjoining six neighborhoods.

The traffic impact is of great concern for the neighborhoods already suffering from congested Renner and Plano Roads and the US75/Renner intersection. The area is already heavily congested during the AM and PM travel peak hours - one has to put your life at risk leaving or entering the neighborhood as it is now. The Blue Cross Campus has already added to traffic problems in the area. Additional traffic from such a high density project will make it impossible to leave the neighborhood.

The proposed number of units is unreasonable. No development in Richardson, even including those along the DART line, currently has more than 528 units. So, these owners are asking for at least seven times the number of apartments which would make it the largest complex in Richardson. The Caruth tract now has no zoning for apartments and the Parliament tract is zoning allows 429 units. **The current zoning needs to be left in place**

The City of Richardson has always been known as a good, safe family oriented city and the huge increase in the number of apartments certainly does not preserve the image of family friendly. I would certainly hate to see our area degraded in future years as is what happened with the high density apartment communities along Spring Valley Road in past decades.

Sincerely,

Ardis Good

2909 Ambleside Lane

Richardson

Commission Meeting
Nanci Huskey
to:
Chris.Shacklett
12/07/2010 12:39 PM
Show Details

Chris, I sent this earlier but it didn't go through. I am trying again.

Chris:

Because of family illness, I may not be able to make tonight's meeting. Here are my thoughts on the subject:

1. Councilman Solomon stated in his presentation to the Homeowners Association that one of the features of residing in Richardson was low property taxes for residents because of the high tax on commercial and industrial property.
2. This development would change the nature of the properties from mainly commercial/industrial to largely residential.
3. Has a cost/benefits analysis been undertaken? It seems reasonable to assume that a residential population uses more city services such as, fire department, police department and trash disposal than commercial and industrial.

What will be the affect of this zoning change on our residential tax bills?

If this question cannot be answered directly in the meeting, no action should be taken until it is answered.

Respectfully Submitted

Art Huskey
Nanci Huskey

Parliament Group plans for Renner Road - tonights meeting

bob quillen

to:

Chris.Shacklett

12/07/2010 08:10 AM

Show Details

Hello. I just received notice of this meeting from our neighborhood association and the planned apartments in the area of Plano Road and Renner.

I have to tell you that I am appalled at the idea. Renner Road is already heavily traveled and those of us whose homes back up to Renner suffer greatly everyday - all day, not just during rush hours - from the noise and amount of traffic speeding up and down the road and all the heavy truck traffic as well. During rush hour it takes re-routing our drive just to be able to get out of our neighborhood. From my back yard the noisy trucks, buses, etc. tower over my privacy fence and the wind they generate blows dirt, dust and pollution into my yard.

I have considered getting some of my neighbors to band together and consult with the city to see if trucks could be banned from using Renner Road (like on Campbell Road).

I just can't see increasing the congestion in the area any more than at present. Please take this into account at the meeting.

Thank you,

Betty & Bob Quillen
3305 Bluebell Place
Richardson, TX 75082

FW: CPNA
William Kinder
to:
Chris.Shacklett
12/07/2010 04:55 PM
Show Details

Chris,

I am attaching a copy of an email with an attachment I received from one of our active, positive residents, Mr. James Stinson, who has been a party to the creation of the Crowley Park Neighborhood Association representing 1200 homes or about 4,000 Richardson residents.

Mr. Stinson's email attachment clearly and appropriately represents the very strong concerns we have about turning our neighborhood into an apartment cluster with the same potential you are now fighting on Spring Valley, This will create an apartment city for 15,000, a glut which will invariably drive our property values down. Not to mention the traffic snarl it will create for all the neighborhoods that stretch from 75 all the way out Renner Road through the Richardson Panhandle.

We consider the zoning change to permit the construction of this gross number of apartments to be unacceptable.

Your consideration will be appreciated.

*Bill Kinder
President,
Crowley Park Neighborhood Association*

Bill,
I have attached what I would say if I were allowed to speak tonight.
James

Thank you for allowing me to address this body.

I view these developments with great concern. I am guided by thoughts expressed many years ago in one of Patrick Henry's lesser known quotes. He said, "I have no way of judging the future except by looking at the past." That is what I would like to do. Look at the past and you will see that these apartments are a potential and almost certain disaster waiting to happen.

A number of years ago I retired as a teacher in the Richardson ISD and while there I saw apartments ruin two school areas and devastate the communities around them. Along the Spring Valley and Coit Road areas a number of apartments were built back in the late 1970 and early 80's. When folks in this upscale area (at that time) objected they were told not to worry, the apartments would be first class and well maintained. They were for a couple of years. Soon the original owners sold out to new groups and the troubles began. When the apartments began to age and had empty units the owners turned to "public housing" and "government subsistence" to fill them and bring in money. Soon all kind of crime and vice racked the area. The police soon referred to the area as "Crack House North." People moved from the area and the property values dropped. The schools became drug infested and learning became secondary to survival. Northwood Jr. High was closed and turned into the RISD Academy.

The areas near Forest Meadow and Liberty Jr. Highs went through similar experiences. Ask any teacher who ever taught in any of these schools what they think caused the decline and you will hear one answer.....apartments. The Richardson High and Lake Highlands High areas have suffered for many years because of this blight.

Now you want to do the same thing to our area. Rest assured that in the short run everything will look good, but wait a few years and watch it collapse. It is not a case of "if it will happen" it is a case of "when will it happen." Our property values will tumble just like the other areas I mentioned. These good folks requesting the change will tell us it will not hurt the area, but I challenge them to show us where in the Richardson/Plano area that large apartment complexes have not had a negative impact. I don't know any.

The City of Richardson most likely will ignore our objections for the same reason all cities use. They need the money! They are only concerned with "now." What happens to our schools and property values in the future is not one of their concerns. It is a shame that property tax income trumps quality of life. Negative history will repeat itself once again.

I had to at least voice my concerns, even if they fall on deaf ears. After all...Richardson needs the money and that is the bottom line.

Thank You.

James Stinson

Apartments in NW Corner of Renner and Plano
William Wilkinson

to:

Chris.Shacklett

12/07/2010 07:51 AM

Show Details

Gentlemen:

A long time ago the City allowed an apartment ghetto to develop along Spring Vally. The situation is improving, and we do not need to repeat past mistakes. I plan to attend the Planning Commission meeting tonight to learn more about the proposal. However, I can tell you right now that I will be unalterably opposed to any plan that fails to limit the number of units that can be constructed on the property. Hopefully, a plan that makes sense can be developed.

Bill Wilkinson
300 Shady Hill Drive

Massive Renner Road Development
Martha Valdivia
to:
Chris.Shacklett
12/07/2010 10:28 PM
Show Details

Chris:

We Carlos and Martha Valdivia are very concern about the massive development in Renner Rd. We are not agree with this development. We are worry about the insecurity and a massive traffic in the area. This neighborhood is special for the quiet and family residency where families can live in a peace full area. There is not enough reason to build more construction where there are so many areas in Richardson were there are so many abandon buildings and plenty empty areas in a industrial zones for more commercial buildings. In addition it is not secure to combine small apartments with this type of homes in this area which the price will be down for the contraction apartments that we know in few years the quality of these apartments will not be the same as news. Unfortunately, apartments usually lose their appreciation in a quit time and the neighborhood is always affected with that deterioration.

We really apreacite your attention.

Sincerely,

Carlos and Martha Valdivia

On the TOD Request North of Renner Road

Cary Welch

to:

Chris.Shacklett

12/07/2010 10:36 AM

Show Details

Chris,

My comments, I understand they paid a lot of money for the land and would like them have a fair and reasonable profit if not an excellent return on their investment, but at the same time when I moved out of my parents house as a young man and lived in a number of apartments in my younger years, when I drive by them now all most all of them are an eye sore and a crime ridden area, did not take that many years for it to become that way. Why that happens I do not know, but it does and how to keep that from happening I do not know, but hopefully they do and can show us how this will not happen and back it up with penalty and forfeit ownership of the property to the city if it happens, can they come up with this plan before being approved for any mega project, if they are willing to do this ask them to pretend they live down the street and what would they want in place before it is approved.

Also I am not for Mega projects with out seeing what they want to do build if it is not already zoned for it

Let's have them back it up in writing and have a reasonable way of measuring it. If it has a negative impact on surrounding homeowners they share in that, I would even be willing to say if our home values go up because of their project the city give them a break on some of there taxes, but do not thing a mega apartment complex is going to make our home values go up but would be curious of their thought on this.

Cary

Cary Welch

Accucom

Director

660 N. Glenville Dr

Richardson, Texas 75081

972-265-9758

~ Face your past without regrets, handle your present with confidence and prepare for the future without fear ~

Planning Meeting for the Renner Road Development

Cathy Jackson

to:

Chris.Shacklett

12/07/2010 12:37 PM

Show Details

Chris Shacklett,

I may not be able to attend the planning meeting reviewing the Renner Road, I-75, George Bush Development , or if I do, it may not be for the entire meeting. So, I am forward my thoughts on this project. I feel the number of apartments are too high; they should be lower, and close to DART and the Cottonbelt (west of Plano Rd.) to reduce traffic. I can't see someone walking from Owens Blvd. past Plano Rd. to the DART/Cottonbelt station to use the rail system everyday to go to work. People like convenience, so I don't think apartments east of Plano Rd. would help reduce the traffic. I feel apartments east of Plano Rd would increase traffic as there will be traffic from the businesses located there as well, and therefore, the land east of Plano Rd. should not be zoned for apartments. The only other area of concern is the massive size of the buildings. It seems more like New York City. I like Richardson as it has the city amenities with a hometown feel, so this area is a concern. Thanks for your time.

Regards,

Cathy



**COR's Hearing Re The Proposed Parliament Planned Development @
Renner and Plano Roads**

Charles Johnson to: Chris.Shacklett
Cc: William Kinder

12/07/2010 06:29 PM

Chris -

Just learned that COR was considering the proposed Parliament Planned Development at the NW corner of Renner and Plano Roads.

Have spoken to several neighbors and all of us are adverse to the thought that the COR would even consider allowing a large number of apartments (2000-6000) to be built on this property.

If anything, COR should learn from, and take to heart, the results that past practices involving apartment construction in our economy have demonstrated. From the historical effects that adding apartments, in any large number, has produced on the surrounding residential area(s), the last thing COR should be considering is to add more of the same.

We would hate to see history repeat itself and COR be seen as implementing what they thought was a good thing, against residents wishes, only to see the project go bad over the next few years.

Unless COR is prepared to 1) underwrite all future costs to insure everything is done to maintain the highest standards for the these proposed apartments and 2) insure the residents of The Fairways of Sherrill Park, Sherrill Park, and Crowley Park neighborhoods that their properties would NOT decrease in value due to this proposal....we can only say NO to any action that involves the building of any apartments in this location.

Respectfully,

Charles Johnson
Col, USA, Retired

Joan Johnson
Secretary, Crowley Park Neighborhood Association

2304 Primrose Dr, Richardson, TX 75082
(Crowley Park)

Renner Road Apartments
Christopher Ulrich
to:
Chris.Shacklett
12/07/2010 08:54 AM
Show Details

We are opposed to the proposed apartment development north of Renner Road.

Thank you.

Chris and Vickie Ulrich

apartments at Renner @ Bush
Patterson, Chris (GE Capital)

to:

Chris.Shacklett

12/07/2010 08:56 AM

Show Details

I personally don't think we need anymore apartments in the city of Richardson. Especially giant complexes like these are proposed to be.

I agree with the concerns over traffic and crime. I also agree that if we already have apartments that are not at least 90% occupied, then we certainly don't need to add more!

Thank you,
Chris Patterson

TOD Request North of Renner Road

Dan Bryan

to:

Chris.Shacklett, gary.slagel

12/07/2010 07:38 PM

Show Details

Gary and Chris - I read the information on the above request in disbelief earlier today. Unfortunately I am out of town and unable to attend tonight's meeting. As a citizen of Richardson for 25 years, I am baffled on how any of our leaders would even consider such a request. The infrastructure is not there, crime will rise, congestion will increase, property values will decrease, school will degrade and quality of life will diminish. I see no value of this complex to the citizens of our city. Please help me understand who is benefiting from this complex and what those benefits are?

Dan Bryan

Public Hearing on the Parliament Development

David and Christina Kieffer

to:

Chris.Shacklett

12/07/2010 04:20 PM

Cc:

spnfana

[Show Details](#)

Mr. Shacklett,

My family and I are new residents to Sherrill Park, having bought a home there this past April. We will be unable to attend the public hearing tonight regarding the Parliament planned development, but we wanted to nevertheless express our concerns. The first has to do with the immediate changes to our quality/convenience of life. The second has to do with property values in the long run.

First, such a massive apartment/commercial development is going to drastically increase the traffic patterns in and around the Renner Road/Plano Road corridor. Getting in and out of the Sherrill Park area can be difficult during morning and evening rush hours; the proposed development will possibly magnify this difficulty from a mere inconvenience to hazardous. It is obvious that Renner Road and Plano Road are not capable of handling the potential increase in traffic. These roads will likely have to be reconfigured (stop lights, wider lanes, etc.), which brings in new concerns. Changes to Renner and Plano will somewhat mitigate any potential hazards, but not eliminate them completely.

Besides increased traffic hazards immediately outside Sherrill Park, I can easily foresee drivers cutting through the neighborhood streets to avoid traffic congestion at the intersections, increasing the dangers directly within our area. There are numerous families with young children in the area (ourselves included) and there is a lot of foot traffic and bike riding up and down the streets, especially towards the park. I do not want to see this activity curtailed due to safety concerns.

Additionally, the new development is going to substantially increase the noise, light and air pollution around our neighborhood. Sherrill Park is one of the few places in Richardson you can sit outside your home at night and almost forget you are living in the middle of a city. Its seclusion and quietness is a major draw for families in the area, and was one of the factors that kept us in Richardson instead of going to one of the outer cities.

Second, such a massive development can have a drastic impact on our property values. Simply put, apartment complexes do not age well. They are designed and built with the prevailing architectural and urban trends in mind to immediately attract as many residents as possible. However, once those trends pass, the apartments remain.

In our part of the country people do not rent apartments for fifteen or twenty years. They are transitory housing, serving a purpose until a home can be purchased or the resident relocates to a new area (I speak from experience; my wife and I lived in apartments and followed this pattern for the first seven years of our marriage.) Residential turnover is high. Apartment complexes need continual updating to attract new residents. Without such care, a massive complex can become a massive blight on the neighborhood.

In general, one would be hard pressed to find an apartment complex in the Dallas area that is vibrant and attractive after twenty years (I can think of two along my route to work that went into rapid decline and eventually had to be demolished in the sixteen years we have lived here). However, my wife and I plan to be at the same house in twenty years. Sherrill Park is nearly thirty years old and as valuable as ever. Will the same be said for the complex across Renner?

I understand that such a development could be beneficial for the future of Richardson. My wife and I love this city and want to see it continue to flourish. Surely there is some way to balance the concerns of today's residents with the future potential of the city. To summarize, I hope that there could be a way with this development to:

- Mitigate the traffic flow and resultant potential hazards to the residents
- Mitigate the noise, light, and air pollution
- Ensure that Sherrill Park property values remain stable over the long run

Many thanks,

David & Christina Kieffer
1514 Ambleside Lane

Proposed Development on Renner Road

Reynolds, Dianna

to:

Chris.Shacklett

12/07/2010 10:56 AM

Show Details

Dear Chris,

As a 20 yr homeowner in the Crowley Park neighborhood, I am distressed to hear about the multifamily development being planned on Renner. As a parent, I am EXTREMELY concerned for this growth. As a homeowner, I am very upset. Our neighborhood and Sherrill Park to our west have been totally ignored by PISD as "red headed stepchildren" and the school originally planned in one end of Crowley Park was abandoned decades ago, never to be readdressed, though at least four brand new elementary schools have been constructed in the last 10 years further to the east of us toward Murphy.

I very concerned that additional multifamily communities will negatively impact the situation, which is already grim. Our kids are already schooled with low income families of old east Plano, which while giving them exposure to some critical social issues, also exposes our children to a high percentage of transient families and significant safety issues.. Instead of the elementary school that was planned in our neighborhood 30 years ago and abandoned, our children have bussed for the last 30 years to Mendenhall Elementary which again, is primarily low income and apartment community families when Aldridge, Boggass, Schell and Stinson are closer. Our children continue to be bussed in middle school to Bowman, again, in the lower income area of PISD, then to Williams HS, another old school in another old area, and finally, to PESH. The fact that our neighborhood has no school that is a reasonable distance for our children and a student body made up of committed homeowners is adversely impacting our home property values and the welfare of our children. Truly, had I anticipated that we would have been raising a child who would feed into the Elementary School and Middle School these neighborhoods are districted in, I would have purchased a home elsewhere in Richardson.

Thank you for your time and attention and your service to our community.

Best Regards,

Dianna H. Reynolds

Senior Claims Specialist

SAFECO INSURANCE

(972) 808-4225; 1-800-332-3226, ext 32 4225;

fax: 1-888-268-8840

Dianna.Reynolds@safeco.com

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one more voice on propostion

E Berglund

to:

Chris.Shacklett

12/07/2010 08:11 PM

Show Details

Hello Chris,

Sorry I could not attend tonight but I am just a 40 yr resident of Richardson (2nd generation in same house) who does not want the proposed zillions of apartments near Plano Rd and Renner! Yes I know there are so many still vacant but how about limiting the developer to 500 units with lots of parks! (Maybe some more shops for jobs too....)

Thank you,

Ellie Berglund

NW Plano and Renner
Fred Oliver Jr.
to:
Chris.Shacklett
12/07/2010 06:31 PM
Show Details

I wanted to let you know that I have concerns about building apartment buildings at the NW corner of the intersection of Plano and Renner roads. I use Renner road in that area daily and do not want a lot of traffic and congestion there. I can see the use as more of a business park that would be friendly to pedestrians.

Sincerely, Fred Oliver

Re: TOD Request North of Renner Road
 Dan Bryan
 to:
 Gary Slagel, Chris.Shacklett
 12/13/2010 07:42 PM
 Show Details

RECEIVED
 DEC 13 2010
 DEVELOPMENT SERVICES

Gary, Let me start off by saying how appreciative I am of your response and explanation. I know you are busy and I do thank you for spending the time to reply. As the old proverb goes "there are always two sides to every story" and your view is certainly different then the 3-4,000 potential apartments story I heard.

With that said, I believe the city should heavily scrutinize any multi-family project. No need to remind you and the council the Spring Valley complexes were once considered luxury units meant for young or mobile professional.

Renner and Central is already a very congested area during peak traffic times and the infrastructure currently in place would be extremely expensive if not impossible to modify. The mixed use developments you described causes localized traffic nightmares. Long and careful consideration should be given to the changes in traffic patterns and any modifications required.

Gary, I have always supported your positions as I feel they have always been for the betterment of the community and its citizens. I believe, as you stated on many occasions, corporate citizen make very good neighbors. I trust the council will maintain the direction that made this community so desirable.

All the Best,

Dan

On Fri, Dec 10, 2010 at 2:21 PM, Gary Slagel <gary.slagel@capitalsoft.com> wrote:

Dan,

I don't know what you saw on this request, but this is not a request for an apartment complex. I do, however, understand your concerns based on what must have been sent to you. I would like to give you an update on the process. Every land owner has the right to make an application and that application must go through a public process with the City Plan Commission and then the City Council. That process is what had just started with the meeting you missed. The land in question is an important piece of property because of the DART rail station, which will likely be our station with a direct connection on the Cotton Belt rail line to DFW Airport. The input I get from residents of all ages is that they would like a high end retail development like Waters Creek in Allen, West Village in Dallas or Legacy Town Center in Plano. The applicant/property owner is proposing such a development, similar to those mentioned, at the location bounded by US 75, Bush, Renner and Plano Rd. with the DART station located in the center. A successful retail developments like those mentioned always have office and housing as part of that mix. The question is, how do we ensure quality over the long term, so that the development enhances the value of property around the development. Second, how do we leverage the major transportation assets of US 75, DART and Bush Turnpike so as to minimize impact on local streets. Those questions and many others will be addressed with a series of public meetings with the property owner. The next City Plan Commission meeting on this application is scheduled for December 21st. I hope you will be able to attend that meeting. I do know the City Plan Commission, City Planning Department and City Council will be very focused on these issues throughout the process. Again, thank you for your interest in our city.

Best Regards,

Gary

From: Dan Bryan [mailto:dan.bryan2@gmail.com]
Sent: Tuesday, December 07, 2010 7:39 PM
To: Chris.Shacklett@cor.gov; gary.slagel@cor.gov
Subject: TOD Request North of Renner Road

Gary and Chris - I read the information on the above request in disbelief earlier today. Unfortunately I am out of town and unable to attend tonight's meeting. As a citizen of Richardson for 25 years, I am baffled on how any of our leaders would even consider such a request. The infrastructure is not there, crime will rise, congestion will increase, property values will decrease, school will degrade and quality of life will diminish. I see no value of this complex to the citizens of our city. Please help me understand who is benefiting from this complex and what those benefits are?

Dan Bryan

City Plan Commission Members

12/07/2010

My name is George Human. I have lived at 1510 Amesbury Drive in the Sherrill Park Neighborhood for 22 years. I have been President of our Neighborhood Association for four years.

Our neighborhood and others are members of the Neighborhood Protection Alliance of Richardson which currently includes six neighborhoods and other organizations.

I am currently working with the four neighborhoods that are adjacent to Renner Road to develop a consensus for response to the proposed TOD.

I'm a civil engineer by education and have worked with and for Cities for over 50 years, having served as City Planning Director for Fort Worth, City Engineer for Plano, Transportation Director for Richardson and many years in private industry as a consultant to Cities in between those jobs and since.

I am very supportive of the TOD concept and hope a mutually beneficial solution can be worked out that will increase the pride we have for our city, but not be a detriment to our neighborhoods.

Most of the residents living in the neighborhoods along Renner Road have lived there for many years and looked forward to more Cisco and TI type development along Renner.

But they have become outraged at the prospect of 6,000 apartments just in the short distance between US75 and Wyndham Lane just west side of the Shire.

If this apartment zoning were to be approved, it would be 7 times the largest complex ever approved in Richardson. Those of us who have lived in Richardson for many years have seen many apartment complexes become very undesirable to live in and near. We fear adding over 3300 more to the currently zoned almost 3000 approved units would be most detrimental to our neighborhoods and to the City as a whole, since all rents would go down and the complexes would suffer or fail.

The attorney representing Parliament offered me just last night an option which I have not been able to fully appraise the other neighborhoods of or reach a consensus on what we feel we all could live with.

The neighborhood's leadership's responses have ranged from NO MORE APARTMENTS – PERIOD to we need more time to analyze and reach a consensus. And the Alliance needs more time to poll its member neighborhoods leadership to form a consensus after our four neighborhoods on Renner have come to agreement.

We only received the final staff report this morning and the Traffic impact Analysis late yesterday afternoon and have not had time to digest and discuss all of this information much less fully digest the full 238 page staff report put out late Friday night.

I suspect you Plan Commission members haven't had sufficient time either to digest all of this last minute information.

On behalf all of us, I beg you to not make a final decision on the massive development request tonight.

Apartments at Renner and Plano Road

Gini Moore

to:

Chris.Shacklett

12/07/2010 02:27 PM

Show Details

Chris:

We live in the Reservation and I am very opposed to 6,000 apartments going up at Renner and Plano Road. We will have another problem like we had on Spring Valley and to some extent still do have the problem.

I believe that Richardson has ENOUGH apartments and that we need single family neighborhoods and more good shopping areas and restaurants.

The city needs to stop looking at the bottom line and realize that this is a TERRIBLE idea.

Let me know if there is anything else I can do.

Gini Moore

1236 Navaho Trail

Richardson, TX 75080

975-238-0674 (home)

972-643-6339 (work)

Greg Smith

to:

Chris.Shacklett

12/07/2010 11:34 AM

Show Details

Chris,

One other point. The City staff should be very concerned that if it grants this apartment zoning, the land owner will likely sell as many of these tracts as possible to local apartment developers. There are not many good suburban development tracts available right now so I would expect these tracts will be developed rather quickly. Most apartment developer's want to built, lease and sell the property as fast as possible, leaving the long term operations to investment groups. This is where problems can start if the number of new projects is not restricted. As I stated in the earlier email, the City staff should see how the current land owner develops its existing 2,000 apartment units before granting this zoning request.

Best Regards,

Gregory S. Smith

President

Rockwood Capital, Inc.

Greg Smith
to:
Chris.Shacklett
12/07/2010 11:16 AM
Show Details

Chris,

I own a 442 unit apartment complex in the City of Richardson named the Clearwater Creek Apartments. I agree that the addition of this project would be very detrimental to the overall apartment market in Richardson causing downward pressure on rental rates and making existing projects struggle economically. In addition, our projects were built under stringent lower density requirements so that they would fit in with the overall planning within the City of Richardson. I cannot believe that the Planning Department and the City Council would seriously consider this application. The purchaser of these tracts was fully aware of the existing zoning limitations when they purchased the land tracts. To add this apartment zoning would be a mistake and certainly not equitable to existing land owners that have attempted to secure apartment zoning in the past.

Since they already have zoning for 2,000 apartment units, which is probably the amount the market could absorb over the next 5 years, I am strongly opposed to this request. Once these 2,000 units are constructed and successfully operated, the Planning Department and City Council could consider an additional zoning request at that time. I am sure that the owner of the tracts has planned to zone as much of the land as apartments as possible. Then they will commence to sell the tracts to every apartment developer in town and there will be a gross over building of the market. Land for apartment units will sell around \$10,000-\$12,000 per unit in today's market. Therefore, they have requested zoning that will be worth approximately \$45,000,000 which is probably the same amount they recently paid for the entire tract.

Let me know if I can be of assistance in further discussions. I am not available to attend the hearing on December 7th but you can share my sentiments at the hearing if you desire.

Best Regards,

Gregory S. Smith
President
Rockwood Capital, Inc.

Renner Road Development.

Jeff Douglas

to:

Chris.Shacklett

12/07/2010 12:14 PM

Cc:

Maitri Smithhisler, SPNFANA

Show Details

Hi Chris,

I'm a long-time Richardson resident and a member of the Sherrill Park/Foxboro neighborhood assoc. My family and I have big concerns over this massive development at Renner & Plano Rd.

Currently the traffic at that intersection and at US -75 and Renner is terrible. If this large development moves in the infrastructure in the area will not be able to support the traffic.

I'm all for developing and progression, but smart progression and this is too large of a development in this area. It should start at about a 1/3 of the number of apartment units and land area and possible grow from there depending on demand and growth potential.

After researching the the Campbell - 75 area I've noticed many of the business cannot stay open and many of the apartment units are empty so I'm not sure why this developer thinks this will be different.

Bottom line the development is too large, the roads and infrastructure cannot handle the massive traffic this will bring. So I will fight along with the association to stop this as long as it is in it's current form.

Please bring these issues to the meeting and let them know many longtime residences in the Sherrill Park area are not happy and this development plan needs to be toned down.

Thank you,

Jeff Douglas

Zoning Change on Plano & Renner Roads to Plano & George Bush Tollway

Jerry Whitten

to:

Chris.Shacklett

12/07/2010 03:10 PM

Show Details

The plan as best I can figure out looks like the planning for a mini-city in the area noted. Maybe I missed it, but where are the children who will be living in this area going to school. Do you have plans in the school system planning to adequately school these children? How about the additional support structures for the people living here, i.e. rec centers, parks, etc. Of course these would be in addition to those planned by the developer, which seldom seem to be adequate. Since we are on the subject, we don't seem to need the additional tax revenues, if there would be any coming after the dust has settled on the various incentives given for a project of this magnitude, in that we seem to have money to enlarge turn lanes on Campbell Road (for no apparent advantage), as well as the building of huge amounts of brick and stone fencing around many of the neighborhoods. Yes, I realize that at least some of these costs were included in various bond issues, but in the current recession, you would think that we should be saving some of these funds for that "rainy day" which always seem to come around sometime.

I would encourage you to deny the approval of this mass of apartments in favor of a much reduced maximum number. Say somewhere in the neighborhood of 300-500 units total.

Sorry I will not be able to make tonights meeting in that I have other plans.

Thanks

Jerry Whitten

Richardson resident of 37 years.

Parliment Development - Renner and Plano Roads

shanksjoyce

to:

Chris.Shacklett

12/07/2010 10:52 AM

Show Details

Dear Chris:

This note is regarding the development that is being planned by Parliment Development at the corner of Renner and Plano Roads.

We have lived in Sherrill Park for about 25 years and thoroughly enjoy the lifestyle of the neighborhood. The neighborhood is well located to an abundance of shopping, restaurants, city facilities and hiking trails. Needless to say, we find the proposed Parliment Development alarming in it's scope of density and traffic. A compromise between the neighborhood and the city is needed. The number of apartments currently being proposed would threaten the tranquility of our neighborhood. Please take into consideration a reduction of the apartments to a level more conducive to the lifestyle enjoyed by Richardson residents. We are asking for a 50% reduction.

It would appear, and I could be wrong, that the City Planning Commission is only interested the "\$", aka taxes. The long term residences near this development would be the ones that would suffer the most. Simply put, more people in such a small area would degrade the quality of life for the adjacent neighborhoods.

Joyce and Don shanks
2801 Foxcreek
Sherrill Park

Renner Road Apartments
Kerry Brown
to:
Chris.Shacklett
12/07/2010 11:39 AM
Show Details

I vote AGAINST this. It would greatly impact my quality of life and serenity.

Kerry Brown
1508 Margate Lane
Richardson Texas 75082

Thank you for your consideration.

huge apt. complex issue

Laverne Domel

to:

Chris.Shacklett

12/07/2010 11:32 AM

Show Details

I have been a resident of Richardson since 1958 (or 52 years). My family of 3 children (before growing up and now living elsewhere) and I stayed here after the sudden death of their father, they were 7, 12 and 14 years old; they were born and raised here and the only home they knew. It was a good choice because they kept all their friends and I decided if the children were happy, I'd be happy.

I do not want to live in a city that the most important thing is to grow larger and larger! Our population of near 100,000 with plenty of industry, parks, churches and great schools within a 10 - 15 mile radius, and dart rail to surrounding areas is marvelous! We don't want to get huge apartment complexes in our area because then we'd be like the area around Valley View....just wall to wall concrete and tall buildings. We are a comfortable size for a wonderful suburban town with lots of recreation, theatre and great schools including UTD! Lets keep it that way and let the surrounding areas around us deal with with the huge populations! Thank you.

Sincerely LaVerne Domel

1328 Cherokee Dr

Richardson TX 75080

(my address for 47 years and before that at 200 Dublin for 5 years...(Rich. was about 10,000 citizens in Spring of 1958)!

proposed apartment complex
Linda Slocum
to:
Chris.Shacklett
12/07/2010 09:46 AM
Show Details

My husband, Robert, and I cannot attend the meeting this evening concerning the huge apartment complex being considered at the NW corner of Renner and Plano Rd. We have been residents of Richardson for 40 years, and we want to express our OPPOSITION to this project.

Linda Slocum
307 Arborcrest Dr.
Richardson, TX 75080

renner road apts
Marilyn Schwartz
to:
Chris.Shacklett
12/07/2010 11:19 AM
Show Details

as a resident of crowley park, i wish to state my great concern and objection to massive apartments proposed on renner road. huge apartment complexes are never a good value to neighborhoods over the test of time.

marilyn schwartz
2556 honeysuckle
richardson tx 75082

Development along Renner Rd.
martin smith
to:
Chris.Shacklett
12/07/2010 07:50 AM
Show Details

Dear Mr. Shacklett,

As a long time resident of Sherril Park I am very dissappointed that our local government would consider over developing the area betwven Renner road and the GB tollway. One of the appeals of living here is the relatively tidyness of the area and the low crime rate, with such a huge developent I am sure that these statistics will be skewed in the negative direction.

I would also like to int out that with the current developemtns that have gone on over the past 10 years towards the east side of Renner the traffic volume has increased drematically. The installation of traffic lights at Wyndham has not been a good solution for us. Traffic continuously flows on Renner and the time wait is excessive for us residents. Also, with it being a 4 way junction when the lights are green all this does is allow traffic from the 190 bypass to get to their destination while we sit and wait.

Also, with the increase in properties how does the city plan to handle waste. The large trucks flowing down Plano road are dangerous, they leave large amounts of litter and cost us in road repairs.

I am very apposed to the proposed plan to change the zoning in this area for these reason.

--

Regards,

Martin smith
Ph 972 664 0042
Cell 214 995 5321



Renner Apartments
Mike Heinz to: Chris.Shacklett

12/07/2010 01:42 PM

The proposed apartment development at Renner Road and Plano Road is concerning. The crime rate likely will spike and will leave existing apartments in dire straits. They will have to lower their rates which will likely bring in trouble-making residents. This is undesirable for traffic and composition of businesses that make the area appealing in general.

Do not let this apartment complex get built without unit limits under 500 and requirements to bring in classy retail establishments!,

Mike
Fellowship Bible Church Dallas
Junior High Pastor
Ph: (214)739-3881 x-167
Fx: (214)739-1147
Eml: mikeh@fellowshipdallas.org



Renner apartments
coconino to: Chris.Shacklett

12/07/2010 11:55 AM

As a 38 year resident of Richardson, it is both discouraging and a little frightening to think that one builder can ruin not only the city atmosphere of Richardson, but the economy as well. It seems apparent to me that by requesting only the one restriction apply and disregarding all other limitations possible on the property, that as many apartments as humanly possible is the goal. The traffic, the crime, the overall congestion to businesses in the area, along with the fact that not all apartments in the same neighborhood are rented is something I do not want in my environment. Greed alone seems to be the objective. Please do not allow this request, and please preserve the integrity of our community. I am completely against these apartments being constructed - on any corner of that intersection. I regret I will not be able to attend the meeting this evening, but wanted to make my concerns known.

Norma-Ree Lueders
406 Crestover Circle

Renner/Plano Rd. development

Sara Gates

to:

Chris.Shacklett

12/07/2010 10:28 AM

Show Details

Mr. Shacklett-

I have lived in the residential area at Renner and Plano Rd. for 25+ years now. Please consider the preferable usage of the land north of Renner for condominiums (which would be owned) as opposed to apartments. This brings in more stable neighbors who are greatly more interested in the looks and care of the area. This part of Richardson has long maintained an apple in the eye of the city of Richardson. For my part I believe the construction of the proposed number of apartments would blacken that eye. Of course, I would prefer the construction of single family dwellings, but I could live with at most the zoning for condos. Thank you for your consideration.

--

Sara Gates

Sara Gates Gallery/3 Day Framing & Gallery, Inc.

214-821-9184

www.saragatesgallery.com

Renner Road Development.

S Howe

to:

Chris.Shacklett

12/07/2010 02:51 PM

Show Details

Chris Shacklett,

My wife and I had intended to be present at the meeting tonight, but due to health, we will not be able to make it

We feel that the current zoning requirements are very liberal and should not be changed. We appreciate the opportunity to have a voice on this issue.

Our thanks go out to those who have led the effort to maintain our neighborhood.

Stewart and Dorothy Howe

1508 Amesbury Dr

Richardson, Tx



Fw: TOD Request North of Renner Road
Pamela Schmidt cc: Sam Chavez, Chris Shacklett
Cc: bill.keffler, Cliff Miller, Dan Johnson

12/07/2010 02:43 PM

Pam

Pamela Schmidt
City Secretary
City of Richardson
972-744-4290
972-744-5803 (F)
pamela.schmidt@cor.gov

----- Forwarded by Pamela Schmidt/CH/Cor on 12/07/2010 02:42 PM -----

From: "Suzanne Juliussen" <italia@sbcglobal.net>
To: <Pamela.schmidt@cor.gov>
Cc: <barryhand@netzero.net>, <chrisbdavis@sbcglobal.net>
Date: 12/07/2010 11:15 AM
Subject: Re: TOD Request North of Renner Road

City Secretary: Please forward this to all members of the Plan Commission & the City Council.
Thank you, Suzanne Juliussen

To: Richardson City Plan Commission
Richardson City Council

From: Suzanne Juliussen
908 Dumont Drive
Richardson, TX 75080
972-234-4545
italia@sbcglobal.net
(Cottonwood Heights NA)

Subject: TOD Request North of Renner Road

Date: December 7, 2010

I am unable to come to the Plan Commission meeting this evening. However, I would like to make my wishes be known to those making important decisions for the City of Richardson.

I am 100% against the idea of building humongous apartments at the NW corner of Plano and Renner Roads. My concerns:

- Increase in traffic
- Potential crime
- Affect on other Richardson apartment complexes i.e. Brick Row, Galatyn, etc. which are already having such a difficult time renting
- With 3% left of undeveloped land in Richardson, can't we do a lot better than this?

We need to get apartment zoning along Renner Road down to a reasonable level. I implore those in charge to restrict the currently proposed development to a reasonable increase in the allowed number sufficient to support the TOD desired by our current City council.

apt. complex
Susie Hatley
to:
Chris.Shacklett
12/07/2010 08:40 AM
Show Details

Hello,

I am a resident of Richardson and have been for over 20 years. My family grew up in the Prairie Creek area and that is where my family now resides. My husband and I feel that the building of such a large complex is the wrong choice for Richardson because of the crime it would bring to this area. We also see the nice apartment complexes we already have in Richardson and don't believe they are filled so why build more.

We love living in Richardson because of the care and pride the residents and City Council take in keeping this community a safe and wonderful place to live. Let's keep it that way.

Thank you, Susie Hatley

December 3rd City Staff report

walt barnett

to:

chris.shacklett

12/07/2010 01:21 PM

Cc:

gary.slagel, bill.keffler

Show Details

Complete Subject: December 3rd City Staff report on the proposed development north of Renner Road including the Caruth 86 acre tract west of the DART rail and the 57 acre Parliament tract east of the DART rail.

Attn: Chris Shacklett COR Planning Commission

I oppose the approval of the subject report. After reviewing the situation I can see no reason to put the tracts of land involved to this use. The mixed use concept is acceptable, but the number of potential housing units is not.

This area has numerous apartments that have not achieved their occupancy potential. I see no economic viability for more units. With more units the traffic impact certainly cannot be readily dismissed.

More units will negatively impact the rental rate structure. The West Spring Valley area is an example that should not be repeated. Having worked as a volunteer for Meals on Wheels in the West Spring Valley, I know firsthand the security issues involved and the area. People were wary of strangers with good reason.

Thank you for time time and efforts

Walther Barnett

1114 N Cottonwood Drive

Richardson, TX 75080

Phone 972 231 6038



Renner Apartments

Warren D. Caldwell to: Chris.Shacklett

Cc: "Jason Davidson", "Dad"

12/07/2010 08:08 AM

History: This message has been replied to.

Hello,

I was unable to attend the meeting last night but wish to briefly air my concerns about this project.

As a lifelong Richardson resident and local real estate broker, I am extremely concerned about the negative impact this development will have on the surrounding neighborhoods. Do we really need another Lake Highlands situation where a wonderful neighborhood is ruined by crime?

It will be peaches and cream for the first 10 years while they are still a shiny penny. It will only be later, long after the developers cash out, when it is sold to a faceless holding company with no ties to our city, who cut expenses to the bone and the riff raff moves in and criminalizes another part of Richardson. This part of town already is bearing an increased crime burden caused by the train station.

Highest and Best Use? I think not.

Respectfully,

Warren D. Caldwell
972-814-0400
onecall@tx.rr.com

"I'd rather wear the Medal of Honor around my neck than be President."
- Harry S. Truman -

Apartment Project

Warren

to:

Chris.Shacklett

12/07/2010 09:59 AM

Show Details

This is the worst idea we could ever have for our city. This many units would indeed be the equivalent of a small city itself. The additional police and fire protection needed would be dramatic and costly beyond what taxes these units would generate. Where would all the kids go to school.?

Richardson was built along the premise of being a residential community--that does not mean that we have to turn down every request for additional buildings--commercial or retail, but it should not include apartment projects of this size. Growth can be good but only if it is the right kind.

How about some department store options or more restaurant options--besides fast food places.?

The general trend is that as apartments age they attract more crime--regardless of the location. The comments that this new project would hurt other near projects that are either recently opened or still in progress --Eastside and Brick Row --are true. Smaller apartment developments are easier to control the original quality/prices/upkeep, etc etc.

I feel approval of this over-sized request for this many units (and office building height) is ridiculous. Land use in this spot should be used for offices, retail, etc ---worst case is down-size to several hundred units --maximum height of 3 stories.

Sincerely

Warren F. Caldwell



FW: Concerns Regarding Renner Road Development
HiTech Marketing to: chris.shacklett
Cc: "HiTech Marketing"

12/07/2010 11:32 AM

From: "HiTech Marketing" <htm1983@sbcglobal.net>
To: <chris.shacklett@cor.gov>
Cc: "HiTech Marketing" <htm1983@sbcglobal.net>

I am Wes Pettinger at 1501 Banbury Ct. My concerns are:

Traffic/accident increase: presently the intersection of Renner/Plano Rd sees many traffic accidents per week. The intersection handles an increasingly higher volume due to 190 access from 75. I do not buy the argument that most apartment dwellers will not use cars. There is not enough local restaurants, shopping, food stores, etc. to maintain the proposed increase in population to promote walking and not using cars.

Noise increase due to tall, high density apartments reflecting car noise from 75 and 190

DART rail loading saturation

Access to Plano and Renner Roads congestion problems from local neighborhood residential roads that do not have traffic signals (Braeburn and Owens Blvd).

Set backs from Plano and Renner Roads

Adverse affect on property values over time due to transient nature of apartment dwellers and the usual poor building maintenance down the road.

Future city costs and taxes to fix traffic congestion (fly-over's, traffic lanes, lights), police loading, utilities loading.

Why does the city jump into projects like this without doing the necessary studies and present the truth to our citizens? Does the staff's study go into any detail of traffic loading, environmental impact of cutting down the forest south of Renner and west of Plano?

Propoed Apartments at Plano/Renner Rd. Intersection

macraig23

to:

Chris.Shacklett

12/08/2010 08:27 AM

Show Details

The letter from the president of the Sherrill Park Association seems to say it all. I am opposed to this development.

Mary Anne Craig

514 Lawnmeado Dr.

Richardson, TX 75080

Plano/Renner Apartments-I Vote NO

Nobie Hendricks

to:

Chris.Shacklett

12/08/2010 02:50 PM

Show Details

I am not for this use of the land....it will bring a more depressed market to our existing rentals which will drive down prices even further causing them to be leased to those who might be more inclined to crime. We need City of Richardson's infrastructure to remain resolved to continue to make Richardson a better and better place to call home than for it to take a step that can cause the City to back slide.

Nobie Bremond Hendricks, CPM, RPA

Bremond Group, Inc.

777 S. Central Expwy, #3-E

Richardson, Texas 75080

972/231-6551 o, 972/231-6577 f

972/523-0562 c

Nobie7@BremondGroup.com

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thomas.l.fleming
to:
Chris.Shacklett
12/08/2010 08:03 AM
Show Details

Sir

I wanted to express my displeasure with the proposed zoning changes to the tract of undeveloped land at the corner of Renner and Plano Roads.

Re-zoning is only going to cause a significant increase in problems related to infrastructure, traffic, crime!!

Under this current plan the amount of people, if capacity is reached, would cause a bottleneck to say the least.

As stated by others, apartment rentals in other large projects in Richardson have yet to see capacity achieve projected levels.

Please consider my opinion to the fullest...

Thanks

Tom Fleming

Paliament planned development

Christine

to:

Chris.Shacklett, g.human

12/09/2010 11:21 AM

Show Details

Do y'all have a plan for the coyotes and all the other wild animals that call the woods there their home?

Christine Coutu

1101 Pacific Drive

Richardson, TX 75081

thomas.l.fleming
to:
Chris.Shacklett
12/08/2010 08:03 AM
Show Details

Sir

I wanted to express my displeasure with the proposed zoning changes to the tract of undeveloped land at the corner of Renner and Plano Roads.

Re-zoning is only going to cause a significant increase in problems related to infrastructure, traffic, crime!!

Under this current plan the amount of people, if capacity is reached, would cause a bottleneck to say the least.

As stated by others, apartment rentals in other large projects in Richardson have yet to see capacity achieve projected levels.

Please consider my opinion to the fullest...

Thanks

Tom Fleming

Received this 12-8
David Gee
to:
Chris.Shacklett
12/08/2010 01:44 PM
Show Details

Hello Chris, just received this today. This is not good for Richardson, Richardson ISD, Central Expressway and so on. Please bring me up to date on the results of this meeting. Thank you, David Gee

7PM, December 7th Plan Commission Hearing
On the TOD Request North of Renner Road

I just completed studying the 238 page, December 3rd City Staff report on the proposed development north of Renner Road including the Caruth 86 acre tract west of the DART rail and the 57 acre Parliament tract east of the DART rail. Our neighborhood is most concerned about the 100 foot tall buildings planned to house an unlimited number of apartments that would be located east of the rail line and west of Plano Road with almost no setbacks from Renner Road.

The staff's report and traffic impact analysis, based on 3,756 apartments in 100 foot tall buildings and 300 foot tall office buildings, recommends very minimal improvements for the expected traffic and is silent about the resulting level of service on the already congested Renner and Plano Roads and the US75/Renner intersection, which is already heavily congested during the AM and PM travel peak hours.

The staff report contains no concerns or reservations about this development which places no restriction on the actual number of apartments, almost no street setbacks and even allows 15 to 25% increases on building heights if desired by the applicants.

No development in Richardson, even including those along the DART line, currently has more than 528 units. So, these owners are asking for at least seven times the number of apartments in the largest complex in Richardson. The Caruth tract now has no zoning for apartments and the Parliament tract zoning allows 429 units. We already have existing about 2,000 apartments mostly fronting on Renner Road west and south of the Bush Toll Road.

The Neighborhood Protections Alliance of Richardson (NPAR) sharing our concerns created a committee to review the plans and form an official position on the quality of life impact of this development. But time is of the essence and your presence is need Tuesday night in order for the CPC to realize this issue is of concern to the citizens. If you cannot attend please send your comments to Chris.Shacklett@cor.gov.

Of even greater concern to our neighborhood and that of Fairways of Sherrill Park to the east of our Sherrill Park neighborhood is that Parliament owns the property to the east of Plano Road as well and it is already zoned for 2,000 more apartments and the forested area southwest of the Plano/Renner Roads intersection is zoned for somewhere around 300 apartments.

Our only hope to get all apartment zoning along Renner Road down to a reasonable level, is to start with restricting the currently proposed development to a reasonable increase in the allowed number sufficient to support the Transportation Oriented Development desired by our current City Council.

Please plan to attend Tuesday night's City Plan Commission Hearing, and if you have time study the Plan Commission Packet found at <http://www.cor.net> under Plan Commission Agenda. At the Plan Commission Meeting, only a total of 15 minutes is allowed for speakers in opposition to the proposed rezoning. So we need a full house (150+people) at the hearing to see what the Plan Commission plans to do with this application. I fear that if only a few people attend, that the Commission will assume that there is no opposition and will recommend approval to the City Council without any restrictions.

George Human, President, Sherrill Park Neighborhood Association

12/5/2010

J

Re: Renner Apartments
Warren D. Caldwell
to:
Chris.Shacklett
12/08/2010 12:13 PM
Show Details

Thank you for submitting my thoughts. I was unable to attend and am curious about the outcome.

Best,

Warren

----- Original Message -----

From: Chris.Shacklett@cor.gov
To: Warren D. Caldwell
Sent: Tuesday, December 07, 2010 8:48 AM
Subject: Re: Renner Apartments

Mr. Caldwell,

The meeting for the zoning cases on Renner is actually tonight at 7pm. I will print this email and make sure the Commissioners receive it however.

Chris Shacklett
Planner
Department of Development Services
City of Richardson
972.744.4249
chris.shacklett@cor.gov

"Warren D. Caldwell" ---12/07/2010 08:08:43 AM--Hello, I was unable to attend the meeting last night but wish to briefly air my concerns about this

From: "Warren D. Caldwell" <onecall@tx.rr.com>
To: <Chris.Shacklett@cor.gov>
Cc: "Jason Davidson" <burntorange98@hotmail.com>, "Dad" <warrencaldwell@tx.rr.com>
Date: 12/07/2010 08:08 AM
Subject: Renner Apartments

Hello,

I was unable to attend the meeting last night but wish to briefly air my concerns about this project.

As a lifelong Richardson resident and local real estate broker, I am extremely concerned about the negative impact this development will have on the surrounding neighborhoods. Do we really need another Lake Highlands situation where a wonderful neighborhood is ruined by crime?

It will be peaches and cream for the first 10 years while they are still a shiny penny. It will only be later, long after the developers cash out, when it is sold to a faceless holding company with no ties to our city, who cut expenses to the bone and the riff raff moves in and

criminalizes another part of Richardson. This part of town already is bearing an increased crime burden caused by the train station.

Highest and Best Use? I think not.

Respectfully,

Warren D. Caldwell
972-814-0400
onecall@tx.rr.com

"I'd rather wear the Medal of Honor around my neck than be President."
- Harry S. Truman -

Massive Renner Road Development

Martha Valdivia

to:

Chris.Shacklett

12/07/2010 10:28 PM

Show Details

Chris:

We Carlos and Martha Valdivia are very concern about the massive development in Renner Rd. We are not agree with this development. We are worry about the insecurity and a massive traffic in the area. This neighborhood is special for the quiet and family residency where families can live in a peace full area. There is not enough reason to build more construction where there are so many areas in Richardson were there are so many abandon buildings and plenty empty areas in a industrial zones for more commercial buildings. In addition it is not secure to combine small apartments with this type of homes in this area which the price will be down for the contraction apartments that we know in few years the quality of these apartments will not be the same as news. Unfortunately, apartments usually lose their appreciation in a quit time and the neighborhood is always affected with that deterioration.

We really apreacite your attention.

Sincerely,

Carlos and Martha Valdivia

one more voice on propostion

E Berglund

to:

Chris.Shacklett

12/07/2010 08:11 PM

Show Details

Hello Chris,

Sorry I could not attend tonight but I am just a 40 yr resident of Richardson (2nd generation in same house) who does not want the proposed zillions of apartments near Plano Rd and Renner! Yes I know there are so many still vacant but how about limiting the developer to 500 units with lots of parks! (Maybe some more shops for jobs....)

Thank you,

Ellie Berglund

TOD Request North of Renner Road

Dan Bryan

to:

Chris.Shacklett, gary.slagel

12/07/2010 07:38 PM

Show Details

Gary and Chris - I read the information on the above request in disbelief earlier today. Unfortunately I am out of town and unable to attend tonight's meeting. As a citizen of Richardson for 25 years, I am baffled on how any of our leaders would even consider such a request. The infrastructure is not there, crime will rise, congestion will increase, property values will decrease, school will degrade and quality of life will diminish. I see no value of this complex to the citizens of our city. Please help me understand who is benefiting from this complex and what those benefits are?

Dan Bryan

NW Plano and Renner
Fred Oliver Jr.
to:
Chris.Shacklett
12/07/2010 06:31 PM
Show Details

I wanted to let you know that I have concerns about building apartment buildings at the NW corner of the intersection of Plano and Renner roads. I use Renner road in that area daily and do not want a lot of traffic and congestion there. I can see the use as more of a business park that would be friendly to pedestrians.

Sincerely, Fred Oliver

Re: Request to Richardson to allow the construction of a very large apt complex

Ann Nash

to:

Gary Slagel

12/07/2010 08:01 PM

Cc:

Chris.Shacklett, gary.slagel, john.murphy, Amir Omar, bob.townsend, mark.solomon, bob.macy, steve.mitchell

Show Details

Gary: thanks for your response. I don't know much about this citizens alliance either and I don't know how they got my email address. I knew I wanted to get a quick response off if all they indicated was a fact. I was in a hurry this morning because I had the crime patrol training class today so I didn't have time to do much research.

Hopefully I answered all my test questions correctly and can now be "Officer Purple Poodle Mom"! (Just kidding, I hope I can do a good job of being observant in my neighborhood.)

Isn't it great to live in Richardson!

Ann Nash

On Tue, Dec 7, 2010 at 5:51 PM, Gary Slagel <gary.slagel@capitalsoft.com> wrote:

Ann,

Thank you for your input on this case. The application is still at the planning level and the meeting tonight is with the City Plan Commission. The City Council should see something early next year. I'm not familiar with the Richardson Citizens Alliance or the email you mentioned, so I don't know what you were told. I do know the Plan Commission and City Planning Department will be diligent.

Best Regards,

Gary

Gary Slagel

Mayor, City of Richardson

411 W. Arapaho Rd

Richardson, TX 75080

NO 214-707-8418

O) 972-220-1568

gary.slagel@cor.gov

From: Ann Nash [mailto:purplepoodlemom@gmail.com]
Sent: Tuesday, December 07, 2010 8:51 AM

To: Chris.Shacklett@cor.gov
Cc: Betty Morton; Angie Wright; Chrissy Cortez-Mathis; Ed Hassler; Hermayne Merritt; Howard Davis; Ida Hassler; Janice Leventhal; Jim Bates; M ELISE DOHERTY; Robert 'Trey' Kazee III; Sean Cortez-Mathis; Virginia Costlow; gary.slagel@cor.gov; john.murphy@cor.gov; Amir Omar; bob.townsend@cor.gov; mark.solomon@cor.gov; bob.macy@cor.gov; steve.mitchell@cor.gov

Subject: Request to Richardson to allow the construction of a very large apt complex

I am unable to attend the meeting tonight.

I have received communication (via email from a group called Richardson Citizens Alliance) concerning this proposal for a large apartment complex in east Richardson.

I am totally opposed to adding this number of apartments to Richardson. Short term, I think this creates problems with overcrowded schools and more policing necessary. Long term I think it creates a future situation like we currently have with Spring Valley and the underkept older apartments in east Richardson. Apartments generally do not maintain value over the long term.

We should not squander our last pieces of undeveloped land just to have something on it. This needs to be part of an overall development plan for the city.

Ann Nash
317 Dogwood
Richardson, TX 75080

Bush transit station multi use project

Richard Semock

to:

'Chris.Shacklett@cor.gov'

12/20/2010 03:16 PM

Show Details

History: This message has been forwarded.

Planning Council:

I am impressed that the whole dev team turned up at the NPAR meeting last week giving up their evening to answer questions when they have covered these at public hearings with the planning council. This might be an indication of their dedication and consideration of developing better neighborhoods thru sound building principles. I suspect this might be the case after hearing them speak at the meeting and at city hall. From what I have observed, they are experienced professionals who are competent in what they do and are capable of making responsible decisions in the proper development of the transit station project.

Nevertheless, the council should apply a Reagan principle to its dealings with the developers and Trust but Verify. You can do this by establishing a '16 points' approach that was proposed by the surrounding neighborhoods for the transfer station agreements recently. Some of these points could be:

- 1) Hold them to the high quality construction standards they describe in their presentations
- 2) Put controls in place that prevent the misuse of form based zoning by cramming apartments instead of more expensive townhomes, business, and retail.
- 3) Cap the height of buildings visible from Renner&Plano.
- 4) Cap the number of family dwellings on the site.
- 5) Install stoplights at Owens&Renner and Braeburn&Plano for safe access of the neighborhood due to increased traffic.
- 6) Existing apartments in the area such as the large Prairie Creek complex to the west should be factored into calculations when proposing more. (The complex also has a vested interest in this planning and could be invited to join in the planning)
- 7) ...

Richard Semock - 3206 Owens

From: SPNFANA [<mailto:spnfana@yahoo.com>]

Sent: Monday, December 13, 2010 09:50

To: undisclosed recipients:

Subject: Meeting TONIGHT, Monday, December 13th at 7pm with developers

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CPC Meeting

Prissy Mount

to:

Chris.Shacklett, SPNFANA

12/20/2010 06:03 PM

Please respond to "Prissy Mount"

Show Details

I have lived on Scottsboro Lane for 30 years - Plano Road was a 2 lane black top road, no one had a home computer, very few people had car phones (the size of bricks) and the term "Telecom" had not been imagined. There was a large wooded area across Plano Road, but I - foolishly - believed that Richardson was a wonderful city that put it's residents first and that any development would take the neighborhood into consideration. My husband nor I saw any zoning change signs put up in that area when it was rezoned in the 80s. Now you are planning to give carte blanche to developers to build for any use as long as the streets look like they promise and the buildings are built in a certain way and at a certain height. They show pictures of happy people and nice buildings - but the pictures are not of 10-30 story buildings! They don't talk about dead trees because they can't get any sun due to the towering buildings. They don't talk about Section 8 people filling up apartments that can't be rented. They don't talk about the increased crime in neighborhoods near large apartment communities. Did you see the front page story in the Dallas Morning News about packages stolen from front porches? The residents had a camera put in because they lived near apartments! They don't talk about the slums these buildings can become in 20-40 years when the next "hot" area is built and everyone moves on. How about the neighbors who can hardly leave the neighborhood during rush hour now because of the heavy traffic? The representative from Caruth showed possible development plans for the Caruth section of the property. He showed planned office buildings - but admitted that they could be apartments instead! Would you allow this open ended development to be put in your neighborhood? Can you look at yourself in the mirror if you do it to us?

Priscilla Mount

1404 Scottsboro Lane



Tues meeting

Janice Kummer to: Chris.Shacklett

12/21/2010 10:54 AM

Chris,

Though we may not be stirred up like the masses, this is NOT a reasonable time to have a critical session such as this when many, including us, are already out of town for the holidays. The Bush Turnpike developement can wait a couple of more weeks.

Janice and Stan Kummer

3002 Wren

Sent from my Verizon Wireless Phone

CPC Meeting of 12/21

BHASKAR GHATE

to:

Chris.Shacklett

12/21/2010 12:10 PM

Cc:

Sandy12, rrtmrt, rajiv_r_shah, Kirit Mehta, PBGhate, sushanta106

Show Details

STOP ACHTUNG ACHTUNG

Hello:

Until a few days ago, I thought we were living in a truly Citizen Oriented City. In the last 20 years of my stay in Richardson, I have not experienced such a rapid erosion of my trust in the City's handling of projects of this magnitude and without regard to citizens' interests, as I am experiencing now.

Please postpone this meeting. It is not fair and it is not good governance. Whoever is being lobbied to in the City Administration, they are just rolling over to these developers. We haven't had a decent look at what we are facing and what's in store in the future because of this huge mind boggling development in our neighborhood. We are directly affected by this.

Please stop this project. Let the citizens get a grip of this situation before you begin further work.

A very annoyed citizen.

B. B. Ghate
3105 Wyndham Lane
Richardson, TX

SPHOA

Transit Oriented Planned Development @ Renner & Plan Rd
Bronwyn
to:
Chris.Shacklett
12/21/2010 01:18 PM
Show Details

This matter should be put off in its entirety until after the 1st of the year, when all underlying issues, planning code changes, and overall plan changes to the original proposal can be heard, disclosed and reviewed prior to any commitment/vote. Thank you,

*Bronwyn MacIsaac
1814 Waterford Lane
Richardson, TX 75082*

YOU must be the change you wish to see in the world. -- Gandhi



**City Plan Commission Meets Tonight Concerning Renner /Plano Rd. Planned
Development**

Barbara Baughman to: Chris.Shacklett

12/20/2010 05:06 PM

This huge development deserves more scrutiny by the community. Scheduling community input at such an inconvenient time is equivalent to saying you purposely do not want community input. It won't hurt to wait a few weeks.

--

Barbara Baughman
Systems Analyst
X2157

CPC Meeting Tonight Concerning Renner Road Planned Development

Rowena Graham

to:

Chris.Shacklett

12/21/2010 08:28 PM

Show Details

Chris,

I would just like to let you know that I will not be able to make the meeting tonight and would like to be kept informed as to the out come of the meeting tonight.

I am a resident of Fairways of Sherrill Park and I do like most of the residents have a lot of concerns about the development that is trying to take place on Renner Road.

thanks,

Rowena Graham

rowenag@tx.rr.com

972-898-0596



Scam
Gregory Cavanagh to: Chris.Shacklett

12/21/2010 04:09 PM

Mr. Shacklett,

I am shocked that this meeting was scheduled during the holidays when so many of us are traveling. It seems to have been planned on purpose with the hopes that the attendance would be minimal and therefore benefiting the developers. Which of course, causes greater concern as they seem to be hiding something.

Greg Cavanagh
3207 Westgate Lane

Fw: City Planning Commission meeting Tuesday night at 7pm
George Human
to:
chris.shacklett
12/21/2010 06:01 AM
Cc:
rnune
Show Details

For distribution to Plan Commission for TOD consideration

From: rama nune
Sent: Monday, December 20, 2010 6:18 PM
To: SPNFANA ; g.human@sbcglobal.net
Subject: Re: City Planning Commission meeting Tuesday night at 7pm

I believe, we should seek remedies along the following lines

1. Limit multi-family (apartments) in the 190 corridor (alma to jupiter/ renner)

this could be done by

a) city considering current zoning and all projections and limiting to current zoning or some reasonable amount (given all existing), say 4000

b) city planning commission can then (based on a) above) restrict MF/apt in carruth and parliament requests

to 2500 given

- this can be done in reg. plan by limiting bldg heights to more reasonable 60-70 ft along renner/plano intersection (as galatyn, 15th and other comparable TOD apts are only 5 story or less)

2. Traffic impact analysis

Given that Renner/75 and renner/ plano is bearing the brunt of traffic increase

a) clearly show worst case peak delays at these intersections, with new signals (2?) between renner and routh parkway and between renner and 190 access; is <5 min target between wyndham and 75 ?

b) why not consider alternate access in/out of development from 75 and 190, besides renner/75

3. Request city PC to show all expenses city will incur including projection due to this proposed development

a) any tax incentives

b) include all city street improvements

b) all traffic mitigation costs

Regards,
rama
cell 9728140923

On Dec 20, 2010, at 1:05 PM, SPNFANA wrote:

Morning



**COR's Hearing Re The Proposed Parliament Planned Development @
Renner and Plano Roads**

**Charles Johnson to: Chris.Shacklett
Cc: William Kinder**

12/07/2010 06:29 PM

Chris -

Just learned that COR was considering the proposed Parliament Planned Development at the NW corner of Renner and Plano Roads .

Have spoken to several neighbors and all of us are adverse to the thought that the COR would even consider allowing a large number of apartments (2000-6000) to be built on this property .

If anything, COR should learn from, and take to heart, the results that past practices involving apartment construction in our economy have demonstrated . From the historical effects that adding apartments , in any large number , has produced on the surrounding residential area (s) , the last thing COR should be considering is to add more of the same .

We would hate to see history repeat itself and COR be seen as implementing what they thought was a good thing , against residents wishes , only to see the project go bad over the next few years .

Unless COR is prepared to 1) underwrite all future costs to insure everything is done to maintain the highest standards for the these proposed apartments and 2) insure the residents of The Fairways of Sherrill Park , Sherrill Park , and Crowley Park neighborhoods that their properties would NOT decrease in value due to this proposalwe can only say NO to any action that involves the building of any apartments in this location .

Respectfully,

Charles Johnson
Col, USA, Retired

Joan Johnson
Secretary, Crowley Park Neighborhood Association

2304 Primrose Dr, Richardson, TX 75082
(Crowley Park)



Notice of Public Hearing

City Plan Commission • Richardson, Texas

An application has been received by the City of Richardson for a:

TRANSIT ORIENTED PLANNED DEVELOPMENT

File No./Name: ZF 10-21 / Bush Central Station
Property Owner: Joe Altemore / Bush/75 Partners LP
Applicant: Scott Polikov / Gateway Planning Group, Inc.
Location: North side of Renner Road between the DART Light Rail and Plano Road (See map on reverse side)
Current Zoning: TO-M Technical Office District, I-M(1) Industrial District, and PD Planned Development District
Request: A request to rezone approximately 57.1 acres of land to a Planned Development District with modified development standards under a Form Based Code.

The City Plan Commission will consider this request at a public hearing on:

TUESDAY, DECEMBER 7, 2010
7:00 p.m.
City Council Chambers
Richardson City Hall, 411 W. Arapaho Road
Richardson, Texas

This notice has been sent to all owners of real property within 200 feet of the request; as such ownership appears on the last approved city tax roll.

Process for Public Input: A maximum of 15 minutes will be allocated to the applicant and to those in favor of the request for purposes of addressing the City Plan Commission. A maximum of 15 minutes will also be allocated to those in opposition to the request. Time required to respond to questions by the City Plan Commission is excluded from each 15 minute period.

Persons who are unable to attend, but would like their views to be made a part of the public record, may send signed, written comments, referencing the file number above, prior to the date of the hearing to: Dept. of Development Services, PO Box 830309, Richardson, TX 75083.

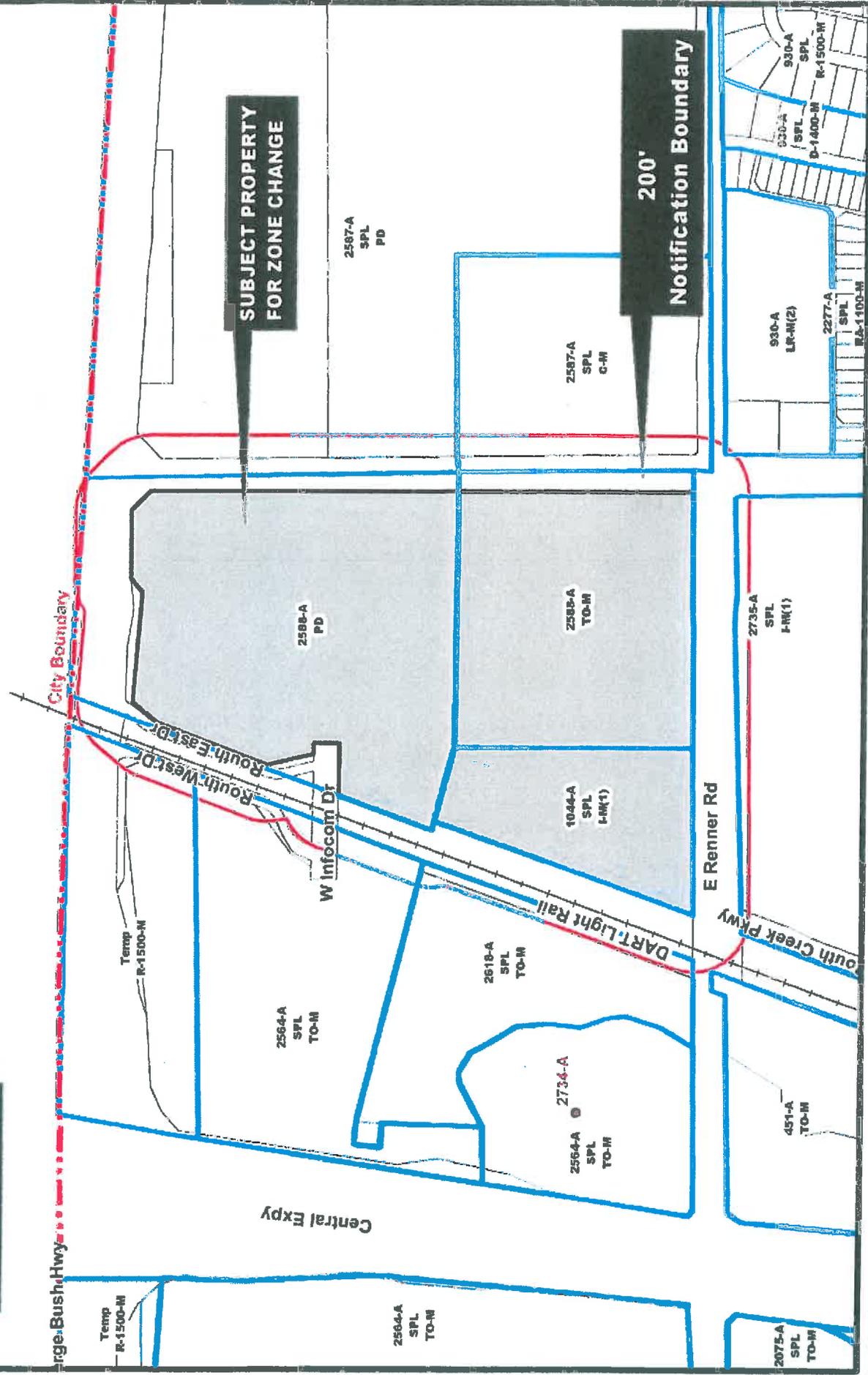
The City Plan Commission may recommend approval of the request as presented, recommend approval with additional conditions or recommend denial. Final approval of this application requires action by the City Council.

Agenda: The City Plan Commission agenda for this meeting will be posted on the City of Richardson website the Saturday before the public hearing. For a copy of the agenda, please go to: <http://www.cor.net/DevelopmentServices.aspx?id=11512>.

For additional information, please contact the Dept. of Development Services at 972-744-4240 and reference Zoning File number ZF 10-21.

Date Posted and Mailed: 11/24/10

ZF 10-21



ZF 10-21 Notification Map



Updated by: shucklato, Update Date: November 17, 2010
File: L:\S\Planning\Cases\ZF10-21\ZF10-21 notification.mxd

**TETCO STORES LP
C/O GUY FRENCH
PO BOX 171720
SAN ANTONIO, TX 78217-1020**

**GALATYN PROPERTIES LTD
1601 ELM ST STE 4700
DALLAS, TX 75201-4782**

**CARUTH W W FOUNDATION
NATIONSBANK OF TX-TRUST
PO BOX 831500
DALLAS, TX 75283-1500**

**BUSH/75 PARTNERS LP
4801 W LOVERS LN
DALLAS, TX 75209-3137**

**DALLAS AREA RAPID TRANSIT
1401 PACIFIC AVE
DALLAS, TX 75202-2732**

**DALLAS AREA RAPID TRANSIT
PO BOX 660163
DALLAS, TX 75266-0163**

**RICHARDSON I S D
400 S GREENVILLE AVE
RICHARDSON, TX 75081-4181**

**CITY OF PLANO
PHYLLIS JARRELL, PLANNING
1520 AVE K
PLANO, TX 75074**

**JOE ALTEMORE
BUSH/75 PARTNERS LP
4801 W LOVERS LANE
DALLAS, TX 75209**

**SCOTT POLIKOV
GATEWAY PLANNING GROUP, INC.
101 SUMMIT AVENUE STE 606
FORT WORTH, TX 76102**

ZF 10-21



**City of Richardson
City Council Meeting
Agenda Item Summary**



City Council Meeting Date: Monday, January 17, 2011

Agenda Item: ZF 10-20 – Caruth Transit Oriented Planned Development

Staff Resource: Sam Chavez, Assistant Director of Development Services *SC*

Summary: The applicant's request is to rezone approximately 85.9 acres of land (which includes an approximate 0.2-acre tract - City of Plano) to a Planned Development District with modified development standards under a Form Based Code. The 0.2 acre tract of land located on the east side of US 75, just north of Spring Creek is owned by the City of Plano. The City of Richardson is the applicant for the tract for the purpose of establishing zoning authority (see letter from City of Plano) because the applicant does not have written authority to rezone the tract. The site encompasses two (2) tracts of land. The East Caruth Tract, located on the east side of US 75 is a 54.5-acre tract of land and the West Caruth Tract, located on the west side of US 75 is a 31.4-acre tract of land. When combined the total acreage is approximately 85.9 acres. Of that acreage, approximately 52.5 acres of land is designated for development due to the location of Spring Creek which bisects both tracts.

Numerous citizens spoke in opposition to the proposed request.

Board/Commission Action: The request was considered by the City Plan Commission on December 7, 2010. The Commission voted 6-1 to recommend approval of the request with amendments.

Action Proposed: The City Council may approve the request as presented, approve with conditions, or deny the request.

Exhibit A

BACKGROUND:

Historical Context

Since 1997, much of the vacant property in that area has been the subject of various land use studies designed to see what type of development would be appropriate for the site. The studies followed Dallas Area Rapid Transit's accelerated plan to bring light rail into Richardson. The 1997 update of the Comprehensive Planning Guide noted the location, for what is now the Bush Turnpike Station.

As the arrival of multi-modal transportation options approached, the City's 2000 Comprehensive Planning Guide, following community input and ratification by the City Plan Commission and City Council, designated the area for mixed-use Transit Oriented Development (TOD).

In 2000, the City started the process of determining future development along the light rail corridor by commissioning an Urban Land Institute (ULI) panel study. The study, published in early 2001, laid out growth priorities for the property and reinforced using the property for transit-oriented development. The study indicated that the limited number of landowners would increase the likelihood that the development would use high-quality, master-planned development strategies, rather than short-term profit maximization strategies, which are often associated with the development of small parcels. The site should allow for a mix of land uses, so that the development community can respond to potential future demand for residential and/or retail development.

The 2009 update of the City's Comprehensive Plan designated the area around all current and potential rail transit stations as the Transit Village future land use classification. Transit Villages are nodes of mixed or multiple land uses within a development and/or a single building, often in a vertical or "stacked" format, built around small-scale pedestrian-friendly blocks. Uses were to include medium- to high-density residential (townhomes and multi-family), retail, entertainment, hospitality and offices. The intensity of development within Transit Villages can range from medium to high based on the proximity of the rail transit facility, the adjacent roadway infrastructure, and surrounding land uses.

Form Based Codes

A Form Based Code (FBC) is a zoning technique that emphasizes form, as opposed to conventional zoning whose emphases is on the separation of uses. Form-based codes foster predictable built results and a high-quality public realm by using physical form (rather than separation of uses) as the organizing principle for the code. They are regulations, not guidelines.

Form-based codes address the relationship between building facades and the public realm, the form and mass of buildings in relation to one another, and the scale and types of streets and blocks. The regulations and standards in form-based codes, presented in both text and graphics, are keyed to a Regulating Plan that designates the appropriate form and scale (and therefore, character) of development rather than only distinctions in land-use types. Form-based codes are drafted to achieve a community vision based on time-tested forms of urbanism.

A Form-Based Code commonly includes the following elements:

- *Regulating Plan.* A plan or map of the regulated area designating the locations where different building form standards apply based on clear community intentions regarding the physical character of the area being code.
- *Public Space Standards.* Specifications for the elements within the public realm (e.g., sidewalks, travel lanes, on-street parking, street trees, street furniture, etc.).
- *Building Form Standards.* Regulations controlling the configuration, features, and functions of buildings that define and shape the public realm.
- *Administration.* A clearly defined application and project review process.
- *Definitions.* A glossary to ensure the precise use of technical terms.

Form-based codes also sometimes include the following elements:

- *Architectural Standards.* Regulations controlling external architectural materials and quality.
- *Landscaping Standards.* Regulations controlling landscape design and plant materials on private property as they impact public spaces (e.g. regulations about parking lot screening and shading, maintaining sight lines, insuring unobstructed pedestrian movements, etc.).
- *Signage Standards.* Regulations controlling allowable signage sizes, materials, illumination, and placement.

A well-crafted form-based code can be an effective form of development regulation for shaping pedestrian-scaled, mixed use and fine-grained urbanism. To determine if a proposed development regulation is a well-crafted form-based code, the following should apply:

- The code's focus is primarily on regulating urban form and less on land use.
- The code is regulatory rather than advisory.
- The code emphasizes standards and parameters for form with predictable physical outcomes (build-to lines, frontage type requirements, etc.) rather than relying on numerical parameters (FAR, density, etc.) whose outcomes are impossible to predict.
- The code requires private buildings to shape public space through the use of building form standards with specific requirements for building placement.
- The code promotes an interconnected street network and pedestrian-scaled blocks.
- Regulations and standards are keyed to specific locations on a regulating plan.
- The diagrams in the code are unambiguous, clearly labeled, and accurate in their presentation of spatial configurations.

A form-based code, if effective, should receive affirmative answers to the following questions:

Is the code enforceable?

- Does the code implement a plan that reflects specific community intentions?
- Are the procedures for code administration clearly described?
- Is the form-based code effectively coordinated with other applicable policies and regulations that control development the property?

Is the code easy to use?

- Is the overall format and structure of the code readily discernable so that users can easily find what is pertinent to their interest?
- Can users readily understand and execute the physical form intended by the code?
- Are the intentions of each regulation clearly described and apparent even to planning staff and citizens who did not participate in its preparation?
- Are technical terms used in the code defined in a clear and understandable manner?
- Does the code format lend itself to convenient public distribution and use?

Will the code produce functional and vital urbanism?

- Will the code shape the public realm to invite pedestrian use and social interaction?
- Will the code produce walkable, identifiable neighborhoods that provide for daily needs?
- Is the code based on a sufficiently detailed physical plan and/or other clear community vision that directs development and aids implementation?
- Are parking requirements compatible with pedestrian-scaled urbanism?

Exhibit B

TRAFFIC IMPACT:

As part of the application, a Traffic Impact Analysis (TIA) was submitted by the applicants. The TIA was conducted by Kimley-Horn & Associates, Inc., a traffic engineering and planning consulting firm hired by the applicants to evaluate the combined impact of the developments on the roadway system. As part of the review process, the City hired an independent traffic engineering consultant, Lee Engineering, to perform a comprehensive evaluation of the TIA, its assumptions, and results.

The TIA examined both the Bush Central Station and Caruth TOPD requests in a single analysis. The applicant's consultant utilized the City's standard TIA guidelines and prior Kimley-Horn studies conducted in Richardson. As part of the study, new traffic counts were conducted at signalized intersections and on all surrounding arterial roadway and frontage roads.

The following scenarios were initially analyzed per the City TIA guidelines:

- 2020 Background Traffic (existing 2010 plus typical growth)
- 2020 Background Traffic plus Full Site build-out Traffic
- 2035 Background Traffic (existing 2010 plus typical growth)
- 2035 Background Traffic plus Full Site build-out Traffic

At the December 9, 2010 City Planning Commission meeting, members of the commission requested additional information pertaining to the existing traffic conditions of the roadway network so the public could better understand how the proposed development would potentially impact the roadway network. Following the CPC meeting and subsequent meetings with the public, City staff requested that Kimely-Horn conduct analysis of additional scenarios including the following:

- 2010 Existing Traffic
- 2010 Existing Traffic plus Full Site build-out Traffic
- 2035 Traffic plus Full Site build-out without any of the applicant's proposed mitigation
- 2035 Traffic with other potential intersection improvements at the Renner and US75 intersections which could be entertained by the City and TxDOT to combat the congestion generated by background traffic and future background traffic growth not associated with the development.
- 2035 Traffic plus Full Site build-out with additional background improvements and the developer's mitigation at the Renner and US75 intersections

The results of all these additional studies will be presented at the City Council meeting.

The existing 2010 traffic data collected by Kimley-Horn and the historical daily traffic volume counts conducted by the City are attached to this exhibit for review. The bar graph shows historical and current traffic volume counts for Renner Road and Plano

Road in the area of the development, and includes traffic volume counts for other arterial roadways in Richardson for comparison purposes.

To assess traffic impacts associated with the proposed development on the adjacent roadway network, basic land use assumptions were established by the applicants. The table below depicts land use intensity assumptions utilized in the TIA for the proposed zoning, as well as currently allowed land use intensities allowed by the existing zoning.

TIA Land Use Assumptions and Existing vs Proposed Zoning Comparison

All Tracts Combined				
	Units	Existing	Proposed	Difference
General Office	SF	5,172,584	2,207,000	(2,965,584)
Shopping Center/Retail	SF	293,403	386,000	92,597
Hotel	Rooms	0	400	400
Apartment/Multifamily	DU	426	3,765	3,339
Townhome	DU	0	175	175
Bush Central Station Tract				
	Units	Existing	Proposed	Difference
General Office	SF	2,640,632	600,000	(2,040,632)
Shopping Center/Retail	SF	293,403	200,000	(93,403)
Hotel	Rooms	0	400	400
Apartment/Multifamily	DU	426	2,400	1,974
Townhome	DU	0	100	100
East Caruth Tract				
	Units	Existing	Proposed	Difference
General Office	SF	1,507,006	767,500	(739,506)
Shopping Center/Retail	SF	0	161,000	161,000
Apartment/Multifamily	DU	0	1,365	1,365
West Caruth Tract				
	Units	Existing	Proposed	Difference
General Office	SF	1,024,946	839,500	(185,446)
Shopping Center/Retail	SF	0	25,000	25,000
Townhome	DU	0	75	75

In summary, the amount of square footage for non-residential uses anticipated in the proposed zoning scenario is reduced by a total of 2,965,584 square feet and the number of multi-family units is increased by 3,339 units over what is currently allowed in the existing zoning entitlements. In addition, 400 hotel room and 175 townhome units are being added.

The table below depicts new automobile trip generations associated with the proposed development and trip generations associated with the currently allowed land uses.

Trip Generation Table (existing and proposed zoning)

New Auto Trips - Proposed Zoning

Proposed Tract	Daily Total	AM Peak Hour			PM Peak Hour		
		IN	OUT	TOTAL	IN	OUT	TOTAL
Bush Central Station	28,255	1,066	1,103	2,169	1,363	1,435	2,798
Caruth - East	19,237	966	634	1,600	866	1,232	2,098
Caruth - West	9,082	898	166	1,064	287	917	1,204
Totals:	56,574	2,930	1,903	4,833	2,516	3,584	6,100

New Auto Trips - Existing Zoning

Proposed Tract	Daily Total	AM Peak Hour			PM Peak Hour		
		IN	OUT	TOTAL	IN	OUT	TOTAL
Bush Central Station	27,004	2,484	592	3,076	1,321	3,275	4,596
Caruth - East	9,696	1,301	177	1,478	270	1,320	1,590
Caruth - West	7,606	1,009	138	1,147	199	967	1,166
Totals:	44,306	4,794	907	5,701	1,790	5,562	7,382

28%	-39%	110%	-15%	41%	-36%	-17%
Increase	Decrease	Increase	Decrease	Increase	Decrease	Decrease

Based on the findings of the TIA, the following roadway improvements were recommended as mitigation measures to be implemented by the developer:

- Renner Road:
 1. Westbound Auxiliary Lane – Plano Road to Routh Creek Parkway
 2. Westbound Deceleration Lanes at All Other Driveways
 3. Westbound to Northbound free-flow right-turn lane and auxiliary lane at US75 leading north at least to the first driveway
 4. Median Opening with Turn Lanes at Drive P2 (proposed street designated as “Park Avenue” on the Bush Central Station Regulating Plan, located between Plano Road and proposed Routh Creek Parkway)
 5. Traffic Signal at Drive P2
 6. Eastbound Left Turn to Routh Creek Parkway
- Plano Road:
 1. Southbound Right Turn bays / Auxiliary Lane – Bush Turnpike to Renner Road
 2. Northbound Left Turn Lanes at Infocom and Drive P5
 3. Traffic Signals at Infocom Drive and Drive P5 (proposed street designated as “TOD Street” on the Bush Central Station Regulating Plan, located on Plano Road, between Infocom and Renner Road)
 4. Improved Right Turn Lane to Renner Road with free-flow into auxiliary lane on Renner Road
- US-75 Frontage Roads
 1. Deceleration Lanes at All Driveways (All driveways and right turn bays to satisfy TxDOT Access Management Requirements)
- Other Transportation Recommendations:
 1. Cotton Belt Transit Line Connection to Bush Turnpike Station
 2. Plano Road at Renner Road Intersection to remain as an At-Grade Intersection in the City’s Master Transportation Plan.

While the TIA identifies the measures which should be the responsibility of the developer to return traffic operations to the background conditions, the following modifications are also recommended if the City desires to improve the background operating conditions shown in the 2035 background scenarios. With these measures in place, there are no LOS F conditions in 2035. These recommendations would not be the responsibility of the developers

Renner Road at US 75 Frontage Roads

1. Modify the existing five-lane SBFR approach to Renner Road to provide two left-turn lanes (one shared with the U-turn lane), two through lanes, and one right-turn lane.
2. Modify the existing five-lane NBFR approach to Renner Road to provide two left-turn lanes (one shared with the U-turn lane), two through lanes, and one right-turn lane.

3. Convert the existing northbound right-turn lane to be a free right turn by the addition of an eastbound auxiliary lane on Renner Road between the NBR and the eastbound right-turn bay at Routh Creek Parkway.

Renner Road at Plano Road

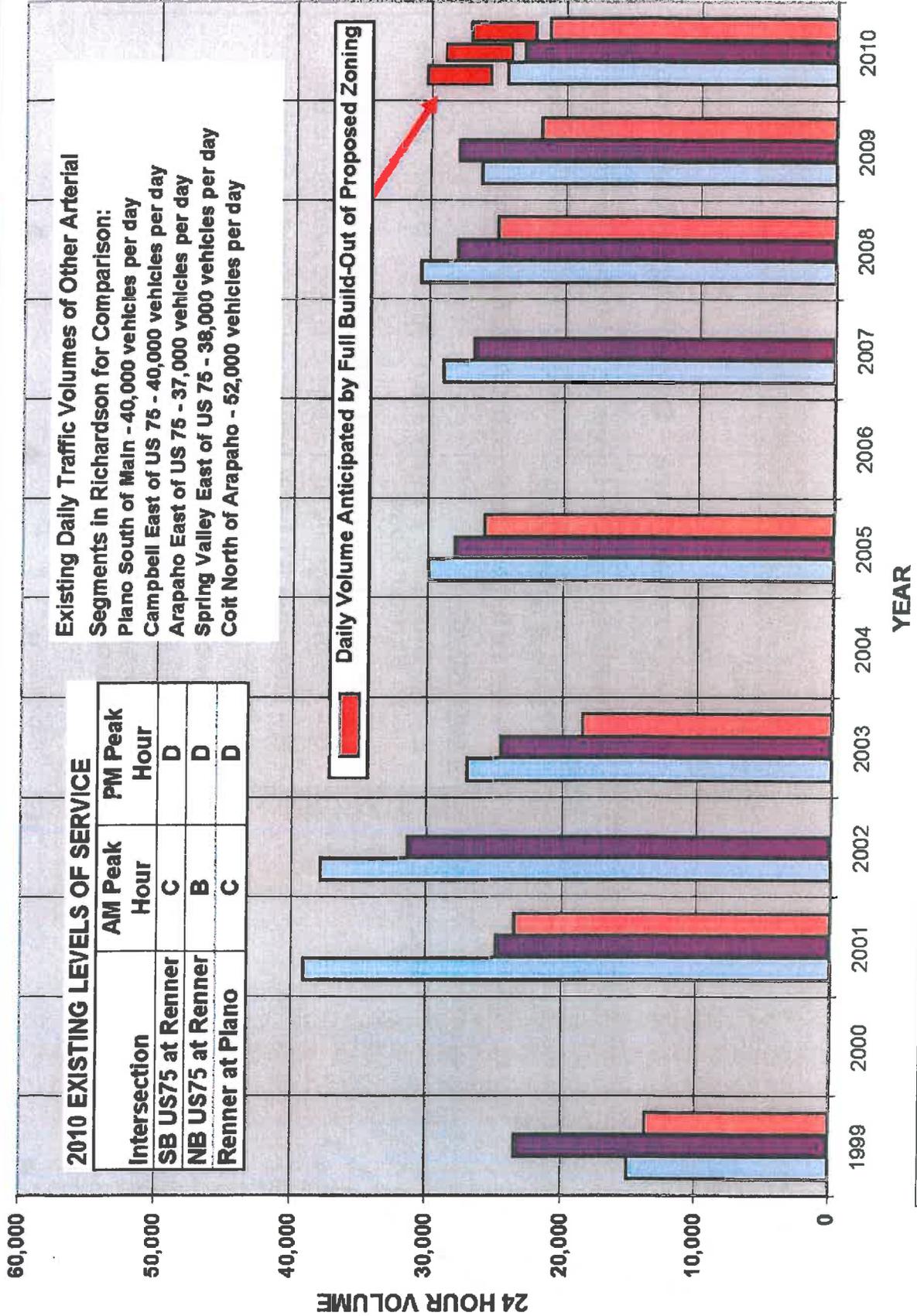
1. Convert the existing northbound right-turn lane to be a free right turn by the addition of an acceleration lane on eastbound Renner Road.

After a thorough review of the TIA and its recommendations, City staff and staff's traffic consultant (Lee Engineering) were able to conclude the following:

- The proposed TOD Mixed Use Zoning results in more daily traffic than the existing zoning entitlements, but it is more spread out over the entire day due to the mixture of proposed uses and the Peak Hour traffic volumes will be lower than the existing zoning allows with a better distribution of inbound and outbound traffic.
- Adequate provision of access and circulation drives will evenly distribute the traffic to the Arterial and Freeway frontage road system minimizing the impact to any specific link on the roadway network.
- The Cotton Belt Transit Line Connection to Bush Turnpike Station will greatly benefit the development and reduce the impact on traffic even more than depicted in the results of the TIA because staff limited the study's assumption of Transit usage to only a 10% reduction of trip capture rate.
- Significant levels of roadway capacity enhancements including additional turn bays and auxiliary lanes on the US 75 Frontage Roads, Plano Road and Renner Road are proposed by the applicant as part of the regulating plan to maximize the efficiency of the roadway network.
- The Plano Road at Renner Road Intersection can remain as an At-Grade Intersection. The Thoroughfare Plan will need to be amended to reflect this change at a future time.

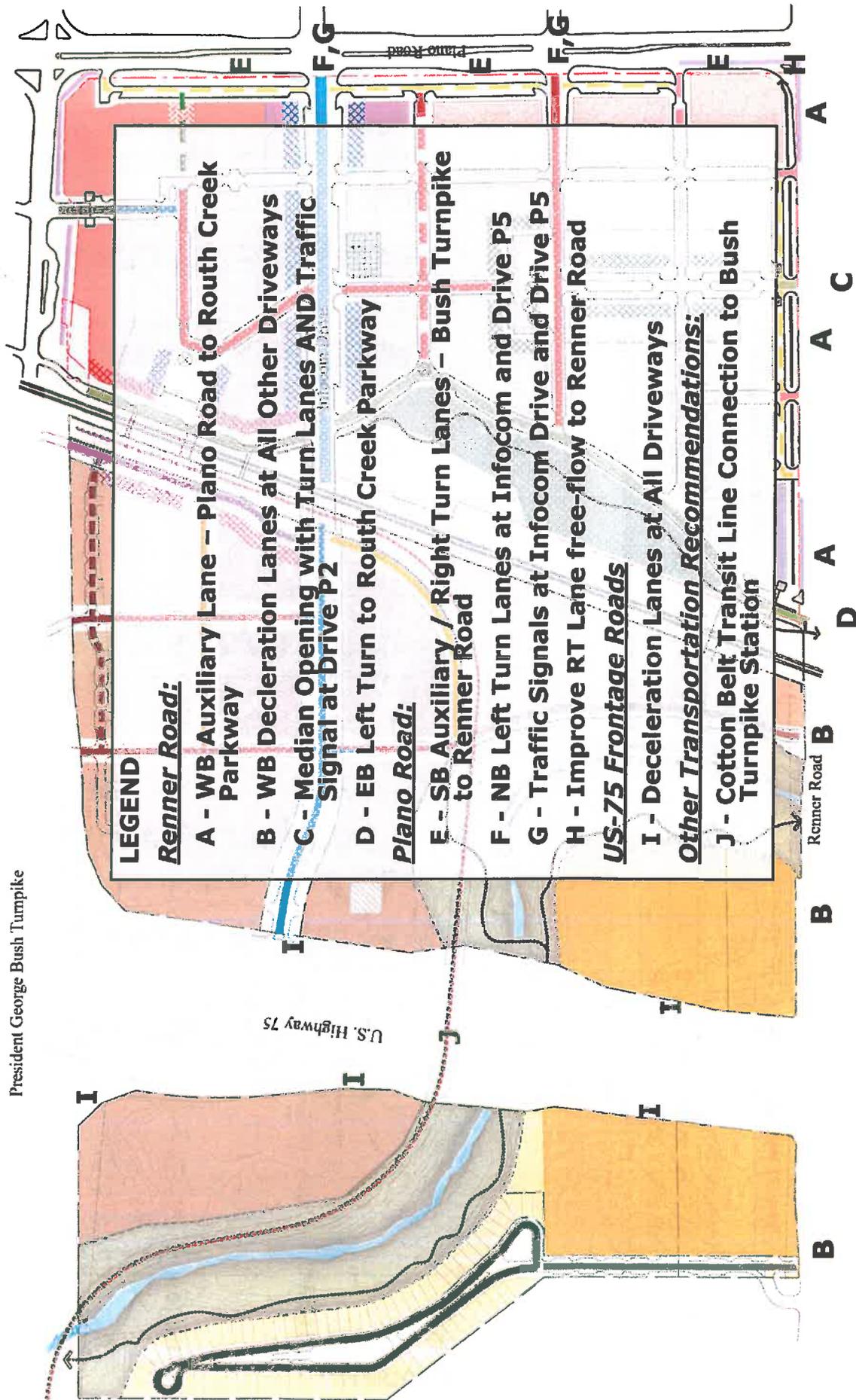
The above conclusions were based on the land use assumptions utilized in the TIA; therefore, if a significant increase in land use intensity or a major modification to the proposed mix of land use are proposed in the future, staff recommends that a revised TIA should be required prior to any future PD amendment similar to the process in which other major modifications must be submitted through the City Council.

2010 Traffic Analysis and Development Traffic with Historical Data



■ Plano South of Renner
 ■ Renner East of 75
 ■ Renner West of 75

Traffic Impact Analysis Recommended Mitigation Measures



Staff Updates

Applicant Code Revisions

As of January 13, 2011, the following revisions have been incorporated in the Code as a result of the Commission's December 21, 2010 meeting.

1. Added an appeals procedure to the City Managers decisions on minor modifications (Page 5, Section 3.8.6) and modified the associated Development Review Process flow chart accordingly (Page 57).
2. Review periodic review of ordinance (Page 5, Section 3.9).
3. Excluded multi-family from Arterial Mixed Use Character Zone (Page 14, Schedule of Use Table.)
4. Added minimum building height to TOD Core Character Zone (Page 17) and TOD Freeway Hi-Rise Character Zone (Page 26)
5. Added Retail Ready depiction to Street Sections (Pages 39, 42 and 43).

December 7, 2010, Commission Requested Information

Applicant and Neighborhood Meeting

The applicants and interested area residents met on Monday, December 13, 2010 at 8 PM at the Renner Road police substation to discuss the applicant's zoning proposals. Approximately 75 people were in attendance that evening. The purpose of the meeting was to educate and answer any questions from those in attendance.

At staff's request, a meeting was held with area representatives on Thursday, December 16, 2010, to review and discuss the findings and recommendations of the Traffic Impact Analysis.

Adopted Area Form Based Codes Qualitative Responses

City of Roanoke, Tx

Oak Street - Form Based Code

Jimmy Stathatos, City Manager

- Adopted in 2008
- Attracting new businesses
- No residential to date
- Increased pedestrian traffic
- Success for property owners, businesses and City
- Flexible
- Extend Form Based Code to other areas of the City
- Recipient of 2010 Public Improvement Project by Texas Downtown Association

City of Duncanville, Tx

Downtown Duncanville District – Form Based Code
Mike Bromley, Assistant Director Public Works

- Adopted in 2008
- No private development projects to date
- Completed infrastructure project
- Issues appear to be related to retrofitting existing developed conditions with Code requirements (existing right-of-way, building setbacks)

City of McKinney, Tx

Regional Employment Center (REC) and Craig Ranch – Hybrid Form Based Code (more New Urbanism than Form Base Code)
John Kessell, Director of Development Services

- Flexible
- Shifts with the market
- No concerns with lack of density or intensity (allows development to reach its full potential)
- Most successful aspects has been found in vertical use developments
- \$51 advalorem tax return on \$1 City investment
- Vertical use developments more successful
- Pattern book (design standards) controlled by Craig Ranch, not the City

City of North Richland Hills, Tx

Transit Oriented Development Code – Form Based Code
John Pitstick, Planning Director

- Adopted in 2009
- Applied to 180 acres of land which contains existing development
- Positive reception from property owners, business owners, homeowners
- Code laid out well, easily administered by staff

General Information

The following codes were awarded the 2010 Driehaus Award and were recognized in May of 2010 at CNU 18 in Atlanta.

The Denver Commons

The Denver Commons is a “legacy code”, adopted in 1997, and now showing extensive built results in a critical redevelopment area of the center city. The code has shown remarkable ability to guide development according to the city's initial vision for a former “brownfield”. Its key characteristics:

- An early, groundbreaking form-based code with excellent built results.
- Visioning and intent statements are so well crafted that they make a convincing case for public and private support of the code. Area by area, the intent is clearly outlined.

- The code's public realm requirements were provided in great detail. A distinction was made between the required standards and additional guidelines for each plan component.
- While this code was adopted as a PUD using “design guidelines” language, its essential elements are replicable today as a form-based zoning district.
- This code could be emulated at any scale.

The Heart of Peoria

The Heart of Peoria Form-Based Code "form-districts" incorporate a form-based code into specific areas. These special FBC districts, which target small areas offering high potential for revitalization, fit within a conventional zoning rewrite for the balance of the city. Key characteristics:

- The code counters the misconception that a form-based code can only be undertaken citywide. In Peoria the entire city code was updated in a conventional fashion, while the form-based portions of the code were targeted to specific revitalization areas.
- The visioning process for the FBC districts gave a clear basis for the regulations applied to those districts.
- The street type organization of the code handles corner buildings particularly well.

Miami 21

Miami 21 took on – and very successfully met – the challenge of a new code for the entire City of Miami, setting up walkable urbanism as the default setting for the entire city and offering a highly stream-lined approval process for projects that conform to the code. Notable aspects:

- The code is ground-breaking in that city officials completely replaced the existing zoning code.
- The code sets up walkable urbanism as the city-wide default pattern for development and redevelopment.
- The entire document is carefully considered and well written. The table of contents and structure are very clear.
- Restricted, limited and open categories affect density and use for each zone; there are essentially 18 carefully calibrated zones for the entire city.
- If an applicant complies with the regulations, the project is approved without layers of time-consuming and unpredictable discretionary reviews.
- Miami 21 has all the provisions that would be found in a typical zoning ordinance but goes further, addressing such important aspects as sustainability, view corridors, and solar access. Preservation of historic districts is included. A public benefits program gives developers additional density for affordable housing and green building standards, while a contribution is required to a trust fund for open space.



Staff Report

TO: City Council
FROM: Sam Chavez, Assistant Director of Development Services SC
DATE: January 17, 2011
RE: Zoning File 10-20: Caruth Transit Oriented Planned Development

REQUEST:

To rezone approximately 85.9 acres of land (including an approximate 0.2-acre tract owned by the City of Plano) to a Planned Development District with modified development standards under a Form Based Code.

The Commission requested additional information to be submitted for their December 21, 2010 public hearing which also provided additional time for the applicant to meet with area interested parties. The requested information can be found under a separate attachment labeled "Staff Update". The "Staff Update" also includes amendments to the Code as a result of the Commission's December 21, 2010 meeting.

APPLICANT:

Brian E. Moore / GFF Planning
City of Richardson (with respect to the property owned by the City of Plano)

PROPERTY OWNER:

Pamela A. Spadaro / U.S. Trust, Bank of America
Vester T. Hughes, Jr. / Estate of W.W. Caruth, Jr.
City of Plano

TRACT SIZE AND LOCATION:

Approximately 85.9 acres located at the NEC and NWC of US Hwy 75 and Renner Road

EXISTING DEVELOPMENT:

The subject tracts are undeveloped.

ADJACENT ROADWAYS:

PGBT: Freeway/Turnpike; 69,000 vehicles per day on all lanes, eastbound and westbound, at the Shiloh Road Toll Plaza; 155,000 vehicles per day on all lanes, eastbound and westbound at the Coit Road toll Plaza (December 2009).

US Hwy 75: Freeway/Turnpike; 267,000 vehicles per day on all lanes, northbound and southbound, north of Campbell Road (December 2009).

Renner Road: Six-lane, divided arterial; 21,800 vehicles on all lanes, eastbound and westbound between Renner Parkway and US Hwy 75 (April 2009). 27,900 vehicles on all lanes, eastbound and westbound between Plano Road and DART Light Rail (March 2009). No traffic counts available between US Hwy 75 and DART Light Rail.

SURROUNDING LAND USE AND ZONING:

Caruth East

North: City of Plano and Undeveloped; R-1500-Temp
South: Office and Parks/Open Space; TO-M Technical Office
East: Undeveloped and existing Bush Turnpike Station; I-M(1) Industrial, TO-M Technical Office and PD Planned Development
West: Undeveloped; R-1500-Temp and TO-M Technical Office

Caruth West

North: City of Plano and Undeveloped; R-1500-Temp
South: Public/Institutional/School; TO-M Technical Office
East: Undeveloped; R-1500-Temp and TO-M Technical Office
West: Multi-Family/Group Quarters; PD Planned Development

FUTURE LAND USE PLAN:

Caruth East

Transit Village and Regional Employment

Mixed or multiple land uses built around small-scale pedestrian blocks located at the City's rail stations. Uses include medium- to high-density residential, retail, entertainment, hospitality and offices.

Higher density development is appropriate with the primary use being high-rise office. Secondary uses include retail centers and entertainment venues.

Future Land Uses of Surrounding Area:

North: City of Plano
South: Parks & Open Space and Regional Employment
East: Transit Village and Regional Employment
West: Regional Employment

Caruth West

Regional Employment

Higher density development is appropriate with the primary use being high-rise office. Secondary uses include retail centers and entertainment venues.

Future Land Uses of Surrounding Area:

- North: City of Plano
- South: Regional Employment
- East: Transit Village and Regional Employment
- West: Multi-Family Residential

EXISTING ZONING:

Caruth East

The subject property is zoned TO-M Technical Office (Ord. 2564-A, February 1987 and Ord. 2618-A, August 1987) and R-1500-Temp.

Caruth West

The subject property is zoned TO-M Technical Office (Ord. 2564-A, February 1987).

APPLICANT'S STATEMENT

(Please refer to the complete Applicant's Statement.)

BACKGROUND INFORMATION

Please see Exhibit A

TRAFFIC IMPACT:

Please see Exhibit B

STAFF COMMENTS:

Applicant's Request:

The applicant's request is to rezone approximately 85.9 acres of land (which includes an approximate 0.2-acre tract - City of Plano) to a Planned Development District with modified development standards under a Form Based Code. The 0.2 acre tract of land located on the east side of US 75, just north of Spring Creek is owned by the City of Plano. The City of Richardson is the applicant for the tract for the purpose of establishing zoning authority (see letter from City of Plano) because the applicant does not have written authority to rezone the tract. The site encompasses two (2) tracts of land. The East Caruth Tract, located on the east side of US 75 is a 54.5-acre tract of land and the West Caruth Tract, located on the west side of US 75 is a 31.4-acre tract of land. When combined the total acreage is approximately 85.9 acres. Of that acreage, approximately 52.5 acres of land is designated for development due to the location of Spring Creek which bisects both tracts.

The table below represents the current zoning entitlements for the subject properties and the land use assumptions used for the purpose of analyzing traffic impacts associated with the request:

East Caruth Tract

Current Zoning Entitlement Rights		TIA Land Use Assumptions		Difference
General Office	1,507,006 SF	General Office	767,500 SF	(739,506 SF)
Shopping Center/Retail	0	Shopping Center/Retail	161,000 SF	161,000 SF
Hotel	0	Hotel	0	0
Multi-Family	0	Multi-Family	1,365 Units	1,365 Units

West Caruth Tract

Current Zoning Entitlement Rights		TIA Land Use Assumptions		Difference
General Office	1,024,946 SF	General Office	839,500 SF	(185,446 SF)
Shopping Center/Retail	0	Shopping Center/Retail	25,000 SF	25,000 SF
Townhome	0	Townhome	75 Units	75 Units

In summary, the amount of square footage for non-residential uses anticipated in the subject request is reduced by 738,952 square feet under what is currently allowed in the current zoning entitlements. In addition, 1,365 multi-family units and 75 townhome units are being added.

The Traffic Impact Analysis (TIA) submitted by the applicant examined the subject request and the Bush Central Station-PD application (Zoning File 10-21) in a single analysis. The table below represents the current zoning entitlements for the subject properties and land use assumptions for both requests.

All Tracts Combined	Unit	Current Entitlement	TIA Land Use Assumptions	Difference
General Office	SF	5,172,584	2,207,000	(2,965,584)
Shopping Center/Retail	SF	293,403	386,000	92,597
Hotel	Rooms	0	400	400
Multi-Family	Units	426	3,765	3,339
Townhome	Units	0	175	175
Bush Central Station				
General Office	SF	2,640,632	600,000	(2,040,632)
Shopping Center/Retail	SF	293,403	200,000	(93,403)
Hotel	Rooms	0	400	400
Multi-Family	Units	426	2,400	1,974
Townhome	Units	0	100	100
East Caruth Tract				
General Office	SF	1,507,006	767,500	(739,506)
Shopping Center/Retail	SF	0	161,000	161,000
Multi-Family	Units	0	1,365	1,365
West Caruth Tract				
General Office	SF	1,024,946	839,500	(185,446)
Shopping Center/Retail	SF	0	25,000	25,000
Townhome	Units	0	75	75

In summary, the amount of square footage for non-residential uses anticipated in the proposed overall zoning scenarios is reduced by a total of 2,872,987 square feet and the number of multi-family units is increased by 3,340 units over what is currently allowed in the existing zoning entitlements. In addition, 400 hotel rooms and 175 townhome units are being added.

The proposed FBC includes the following elements:

- Administration
- Definitions
- Regulating Plan
- Schedule of Permitted Uses
- Building Form Standards
- Architectural Standards
- Street and Streetscape Design Standards
- Signage Standards
- Civic Space Standards
- Appendix A: Regulating Plan
- Appendix B: Planting List
- Appendix C: Development Process Flow Chart

Administration (Section 3) establishes the provisions for reviewing and approving development applications to ensure that all development is consistent with the Code. *In addition to the Code requirements, all development plans are also subject to all other applicable City of Richardson codes and ordinances, such as the Building Code, Fire Code, Engineering Design Standards, Thoroughfare Plan, Subdivision and Development Code and Sign Code.*

The Code outlines the basic steps for review of a development application. The outline serves as a checklist to ensure that the development plan complies with the applicable code requirements. The following steps are to be taken:

- Locate the property on the Regulating Plan
- Identify
 - Character Zone
 - Street Type
 - Special Frontage Standards
- Review the Schedule of Uses by Character Zone
- Examine the Building Form and Development Standards
- Refer to Building Design Standards
- Refer to Street Type and Streetscape Standards

If the development plan does not comply with the Code, the plan is deferred to the City Manager or designee for interpretation, for review as a minor modification as prescribed in the Code, if appealed-forwarded to City Council for consideration or forwarded to the City Plan Commission and City Council for consideration as a Special Development Plan.

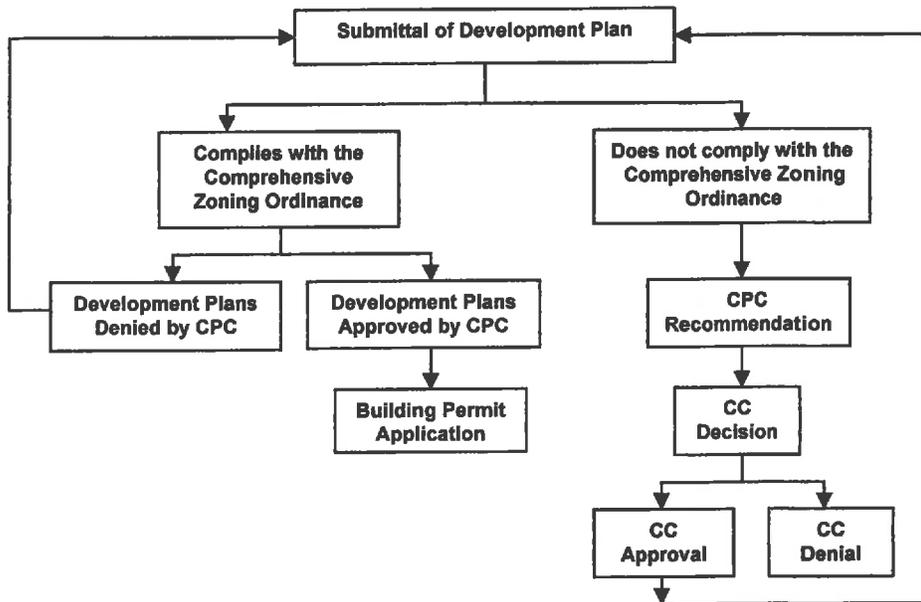
Minor Modifications (Subsection 3.8) designates the approving authority with regard to minor modifications to the Code and establishes thresholds (Table 3.1) for specific allowed modifications, including an appeal process for uses not listed in the Code.

- Minor Modifications (approving authority City Manager or designee)
 - Does not materially change the circulation and building location
 - Does not increase the building area permitted
 - Does not change the relationship between the building and streets
 - Does not allow a prohibited use
 - Does not increase building height or reduce required parking
 - Limited changes to a street cross section and Street and Streetscape Design Standards
 - Uses not listed, but are substantially similar (appeals are forwarded directly to the City Council)

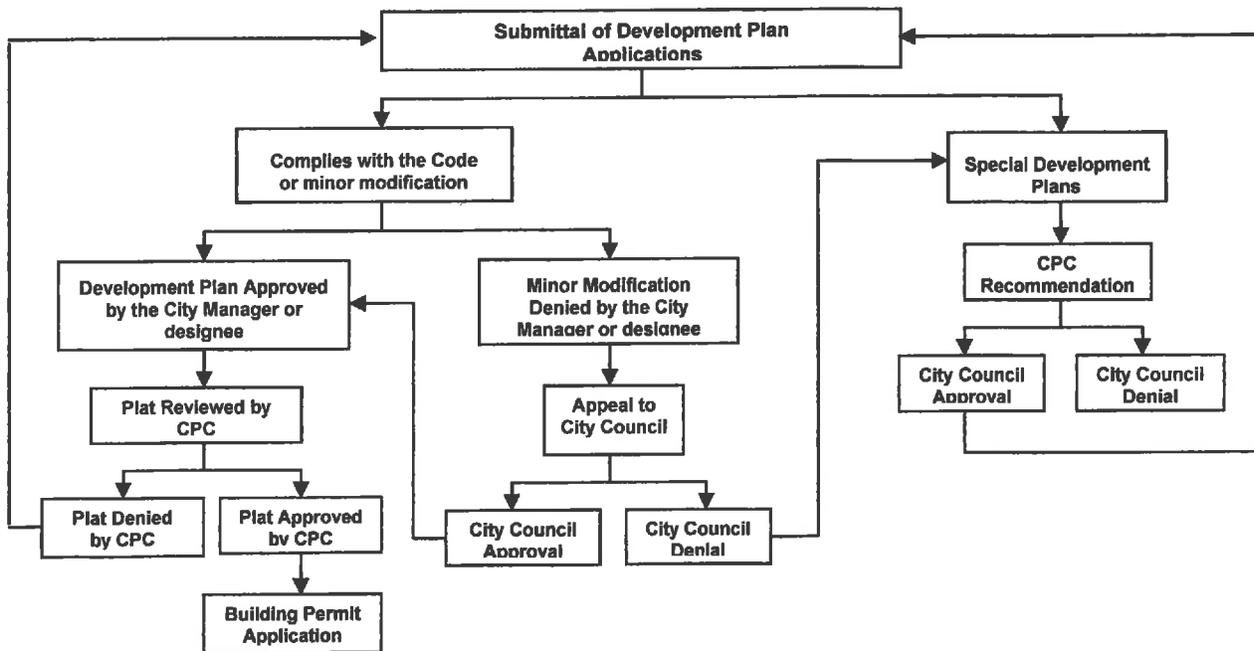
Special Development Plans (Subsection 3.7) establishes the approving authority for any proposed modification to the Code; other than those permitted as minor modification, and are consider major modifications and thus treated as Special Development Plans. Special Development Plans require the request to be reviewed and considered by the City Plan Commission and City Council.

The flow chart below depicts the City’s current development review process as a comparison with what is proposed by the applicant.

City of Richardson Development Review Process



Proposed Development Review Process



The proposed review and approval process defers from the City's current process in that site plans, building elevations, landscape plan, and civil engineering will be approved at a staff level if the development application conforms with the Code. Currently, these require approval by the Commission. Development plats will continue to be reviewed and approved by the Commission.

The Code

The main elements of a Form Based Code are the Regulating Plan and the Development Standards.

Regulating Plan (Appendix A) represents the zoning map for the subject sites and creates distinct character zones which are different from other areas/zones within the subject site. Other elements represented on the Regulating Plan include Street Designations, Special Frontages, Civic/Open Space and Other Designations.

The Regulating Plan also depicts a future alignment for the Cotton Belt. The Cotton Belt Rail Line alignment has not been finalized, but the City's preferred route will take the rail line through the Caruth property west of US75, across US75, and north through the Caruth property east of US75 to the existing Bush Turnpike Station. The Cotton Belt should serve as an "economic engine" for the area with its east – west connection to the region via the existing Bush Turnpike Station.

The following five (5) Character Zones and a general description of each and general allowable uses are indentified below. For a detailed list of proposed allowed uses in each Character Zone please refer to the Schedule of Permitted Uses (Section 6, Table 6.1) in the Code.

- **Character Zones**

- TOD Core (High Pedestrian Activity, Highest Density, Greatest Variety of Uses)
- TOD Mixed Use (High Intensity Commercial and Residential with supporting retail uses)
- Arterial Mixed Use (Transition to Regional Streets for Auto-Oriented Sites, Gateway to Development)
- TOD Freeway Hi-Rise (Transition from PGBT and US 75, High Intensity Development)
- Urban Neighborhood (Primarily residential with commercial at street intersection)

General Allowable Uses (by Character Zone)

TOD Core, TOD Mixed, Arterial Mixed, TOD Freeway Hi-Rise

- Retail Sales or Service, Food Service, Office, Research
- Arts, Entertainment, and Recreation
- Education, Public Administration, Health Care, Institutions
- Home Occupation, Multi-Family, Residential Lofts, Live-Work Units,
- Hotel, Surface Parking (accessory), Structured Parking, Sales from Kiosk, Community Garden, Roof-Mounted Antenna, Special Event, Equipment (utility, wind, solar, rain harvesting)

Additional Uses (by Character Zone)

TOD Mixed Arterial Mixed TOD Freeway Hi-Rise

- Drive-Through
- Private Attached Garage
- Veterinary Clinic
- Townhome

Arterial Mixed

- Drive-Thru, Auto-Related Sales and Service
- Townhome
- Veterinary Clinic

TOD Freeway Hi-Rise

- Drive-Thru
- Auto-Related Sales and Service
- Veterinary Clinic
- Surface Parking

Urban Neighborhood

- Retail Sales or Service, Office, Food Service
 - Art Gallery, Studio (art, antique, furniture or electronic), Open Space
 - Child Day Care and Preschool, Religious Institution
 - Home Occupation, Multi-Family, Residential Lofts, Live-Work Unit, Townhome, Residential (detached), Accessory Residential Unit
 - Hotel, Surface Parking (accessory), Structured Parking, Private Garage, Community Garden, Roof-Mounted Antenna, Special Event, Equipment (utility, wind, solar, rain harvesting)
- **Street Designations** (Mandatory and Non-Mandatory Streets) illustrate the design, configuration and development standards for all streets and includes the following street designations:
 - TOD Main
 - TOD Street
 - Parkway
 - TOD Street Promenade
 - TOD Station
 - TOD Trail Connector
 - Park Avenue G-1
 - Park Avenue G-2
 - **Special Frontage** (Mandatory) are applied to a certain block to address specific requirements and transitions, and includes the following designation:
 - Station Platform
 - **Civic/Open Space** (Mandatory and Non-Mandatory) creates a network of open spaces that provide passive and active recreational opportunities, and includes the following designations:
 - Mandatory Park
 - Non-Mandatory Plazas
 - **Other Designations** (Mandatory) indicates an area for a specific use with applicable standards, and includes the following designation:
 - Multi-Use Trail

The **Development Standards** are established in text and graphic form for each Character Zone, and include Building Form and Development Standards, Building Design Standards, Street & Streetscape Standards, Signage, Civic/Open Space and related standards for all streets, and public and private development. All development plans will be reviewed for compliance with each standard.

Building Form and Development Standards (Section 7) are established in text and graphic form for each Character Zone, and includes following standards:

- **Building Placement**
 - Street-Setback Line
 - Build-To Zone
 - Setback
 - Building Frontage
- **Block Standards**
 - Block Face Dimension
 - Block Perimeter
- **Building Height**
 - TOD Core (maximum 80 feet)
 - TOD Mixed Use (maximum 70 feet)
 - Arterial Mixed Use (maximum 100 feet)
 - TOD Freeway Hi-Rise - West Caruth (80 feet minimum, maximum 120 feet)
East Caruth (80 feet minimum, maximum 100 and 180 feet)
 - Urban Neighborhood – West Caruth only (maximum 36 feet)
- **Commercial Frontage Requirement**
 - TOD Core (Ground floors of all buildings fronting on Type ‘A’ Streets built to Retail Ready standards)
 - Arterial Mixed Use (Ground floors of all buildings fronting on Type ‘A’ Streets built to Retail Ready standards)
 - TOD Freeway Hi-Rise (Ground floors of all buildings fronting on PGBT and US 75 service roads built to Retail Ready standards)
- **Special Frontage Requirements**
 - TOD Core – East Caruth (Station Platform Frontage requirements)
 - TOD Freeway Hi-Rise – East Caruth (Station Platform Frontage requirements)
- **Parking & Service Access**
 - Parking Location
 - Off-Street Parking Standards
 - Driveways and Services
- **Encroachments**

- Allowed for specific elements, require 8' vertical clearance
- Applicability
 - Various additional development standards

Building Design Standards (Section 8) are used to establish a coherent urban character, which will be reviewed for compliance and includes the following standards:

- Building Orientation
- Design of Parking Structure
- Design of Automobile Related Building Site Elements
- Roof Form
- Façade Composition
- Windows and Doors
- Commercial and Mixed Use Building Materials
- Standards Specific to the Urban Neighborhood

The tables below depict the proposed building materials for the development.

TOD Core, TOD Mixed Use, Arterial Mixed Use & TOD Freeway Hi-Rise Building Materials

Proposed Building Material	Permitted by City Code	Caruth Properties
Building Façade		
Masonry (brick, stone, stucco, cast stone, glass, glass block)	Min. 85% of entire façade area (min. 80% per elevation) • Proposed	• Min. 85% along streets • Masonry or accent materials when not along streets
Accent material (wood, architect metal panel, split-face concrete block, tile, pre-cast concrete panels and EIFS)	Max. 15% of entire façade area (max. 20% per elevation) • Proposed • EIFS not permitted below a height of 8'	• Max. 15% along streets • 100% when not along streets • EIFS must be a least 8' above ground floor
Roof material (copper, standing seam metal, slate, synthetic slate or similar materials)	Class 'C' Fire Classification (asphalt shingles, copper, standing seam metal roof, slate, synthetic slate or similar materials)	• When visible from streets and alley

Urban Neighborhood Building Materials (Commercial and Mixed Use)

Proposed Building Material	Permitted by City Code	Caruth Properties
Building Façade		
Masonry (brick, stone, stucco, stone, cast stone, glass block, tilt-up gridded concrete panel, architectural panels)	Min. 85% of entire façade area (min. 80% per elevation)	• Min. 85% along streets • Masonry and/or accent material when not along streets
Accent material (wood, metal, cementitious-fiber clapboard, EIFS)	Max. 15% of entire façade area (max. 20% per elevation) • EIFS not permitted below 8 feet	• Max. 15% along streets • 100% when not along streets • EIFS must be a least 8' above ground floor

Urban Neighborhood Building Materials (Residential)

Building Material	Permitted by City Code	Caruth Properties
Building Façade		
Masonry (brick, stone, cementitious-fiber clapboard, cast stone,)	Min. 85% of entire façade area (min. 80% per elevation) • Proposed (-cementitious-fiber clapboard)	• Min. 85% along street frontage • (+and stucco), 0% when not along streets • EFIS not allowed on ground floors
Accent material (architectural metal panels, rock, glass block, tile, EIFS)	Max. 15% of entire façade area (max. 20% per elevation) • Proposed • EIFS not permitted below 8 feet	• Max. 15% along streets • (+ EIFS must be a least 8' above ground floor and EI
Roof material (asphalt shingle, copper, standing seam metal, slate, synthetic slate or similar materials)	Class 'C' Fire Classification (asphalt shingles, copper, standing seam metal roof, slate, synthetic slate or similar materials)	• When visible from streets and alley (+ terra cotta)

- ❖ The sides and rear façades in the Urban Neighborhood are to be of finished quality and the same color that blend with the public street façades of the building.

Street & Streetscape Design Standards (Section 9) specify typical street configurations for the development in text and graphic form. The standards include specifications for:

- Right-of-way widths
- Vehicular lane width
- Parkway widths
- Number of travel lanes
- On-street parking
- Pedestrian accommodations

Signage Standards (Section 10) are established to ensure compatibility throughout the development. Except as specially listed in the proposed Sign Standards section of the Code, all other signage shall comply with the City's Sign Code. For a detail list, please refer to Table 10.1 of the proposed Code. In general the following types of sign are proposed:

- Wall (Building)
- Monument
- Window
- Building Blade
- Tenant Blade
- Marquee
- For Sale/For lease
- Address
- Temporary
- Banner
- Sandwich Board
- Light Pole Banner
- Directory
- LED Signage (requires lens covers or diffuser)

Civic/Open Space (Section 11) creates a network of open spaces and standards that recognizes the natural qualities of the area while providing a range of both passive and active recreational opportunities.

- Park
- Plaza
- Pedestrian Passage
- Multi-Use Trail
- Playground
- Ancillary Structure

On November 7, 2010, the Commission and City Council met at a Joint Work Session to receive a presentation from staff and the applicants on the proposed project. As a result of the work session, the following items were discussed:

The following items have been resolved and the Code has been amended accordingly.

- Require major modifications to be reviewed by the City Council (CC), after a City Plan Commission (CPC) recommendation. *The Code and Development Process Flow Chart have been amended to reflect the required action..*
- Prohibit EIFS as an allowed building material...*The Code has been amended to prohibit along ground floors and allow a maximum of 15% EIFS 8 feet above ground level.*
- Permit LED lighting for lighting and signage...*The Code has been amended to require LED's to be covered by a lens or diffuser.*
- "Supergraphics" allowance...*The City's Sign Ordinance which does not allow "supergraphics" will supersede.*

The following were discussed with the applicant; however, they remain items for discussion:

- Provide caps on density (FAR), or limits/phasing restrictions on residential development.
- Require minor modification to be approved by CPC
- Incorporate "green" design standards. *(Applicant Response: appropriate for CC&Rs)*
- Incorporate iconic elements/branding for the development. *(Applicant Response: appropriate for CC&Rs)*
- Incorporate additional architectural/design controls. *(Applicant Response: appropriate for CC&Rs)*

The following elements have been identified by staff that may require further refinement:

- Development Rights as they relate to the Traffic Impact Analysis (TIA)
The Regulating Plan and the Code do not designate density and/or intensity values, which is common for a Form Based Code not to include. However, the appropriateness of the roadway network for the development is based on the land use assumption used in the TIA's findings. Establishing maximum number of units and maximum non-residential square footage could be considered as assumed in the applicant's TIA.
- Civic/Open Space Standards - Park Standards
The applicant intends to utilize the Spring Creek flood plain area to meet the need of the residents of the development for active recreational opportunities. A trail is under construction along the flood plain that will provide passive recreational opportunities; however, an active recreational area may not be feasible within flood plain area.

Throughout the development passive recreational opportunities will be provided, which will be in the forms of urban plazas/opens space. The details of either active or passive recreational area will be further defined at the time of development.

Conclusion

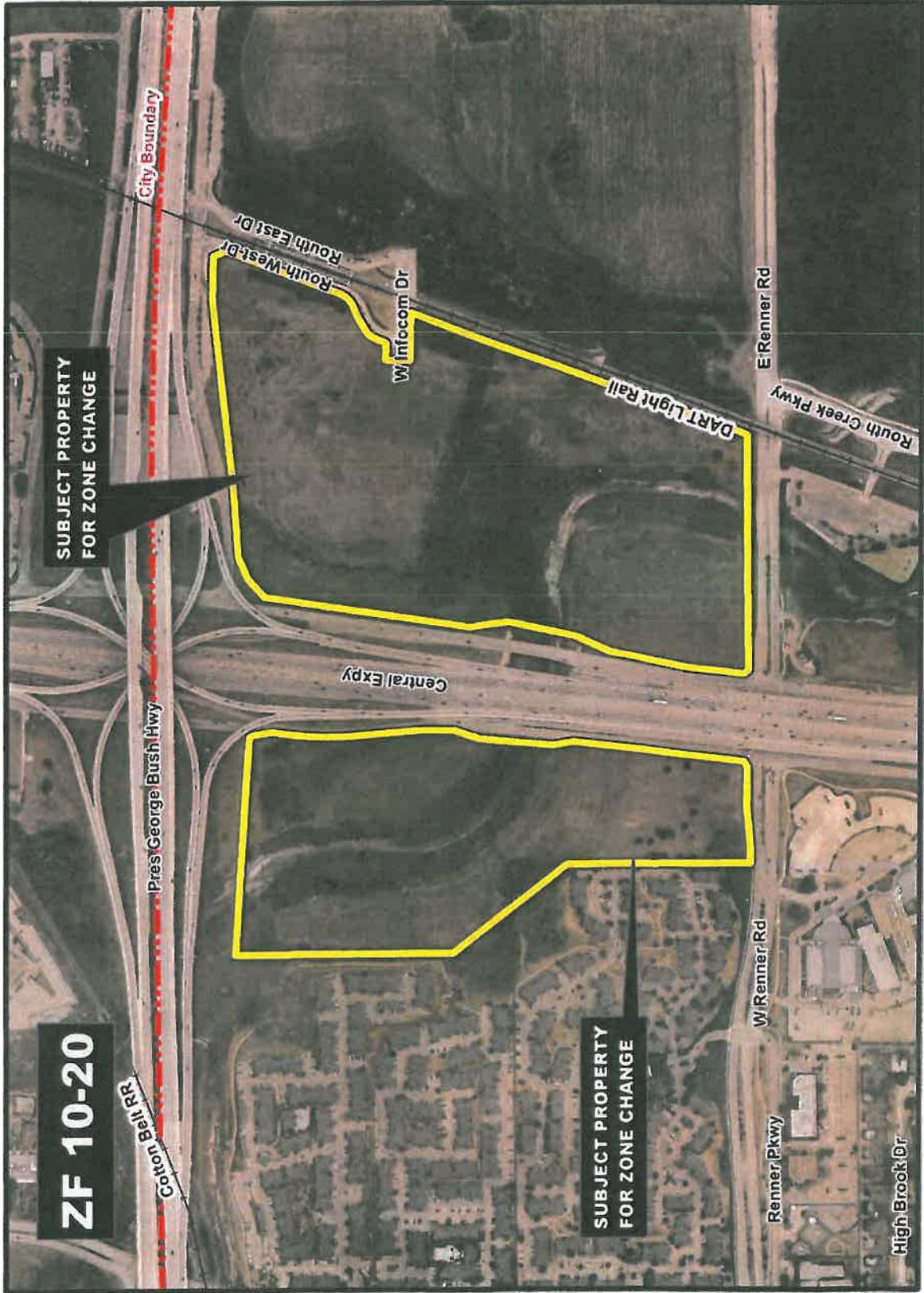
Based on staff's review of the applicant's request, the proposal of a Form Based Code appears to be appropriate. The code achieves a predictable community vision through its regulatory nature. It achieves a predicable physical result by its concentration on the visual aspect of the development through building heights, façade treatment and the relationship of the building to the street through compact, walkable urbanism. If applied appropriately, it could provide the

community with the opportunity to respond to market demands in an expedient and predictable manner.

Correspondence: See attached correspondence.

Motion: On December 21, 2010, the Commission on a vote of 5-2 recommended approval of the request, subject to the following special conditions:

1. The subject site shall be zoned PD Planned Development and shall be developed in accordance with the Caruth Properties Transit Oriented Planned Development Code attached hereto as Exhibit "D".
2. In addition to the maximum building heights in the TOD Freeway Hi-Rise Character Zone, a minimum 80 foot building height shall be included.



ZF 10-20

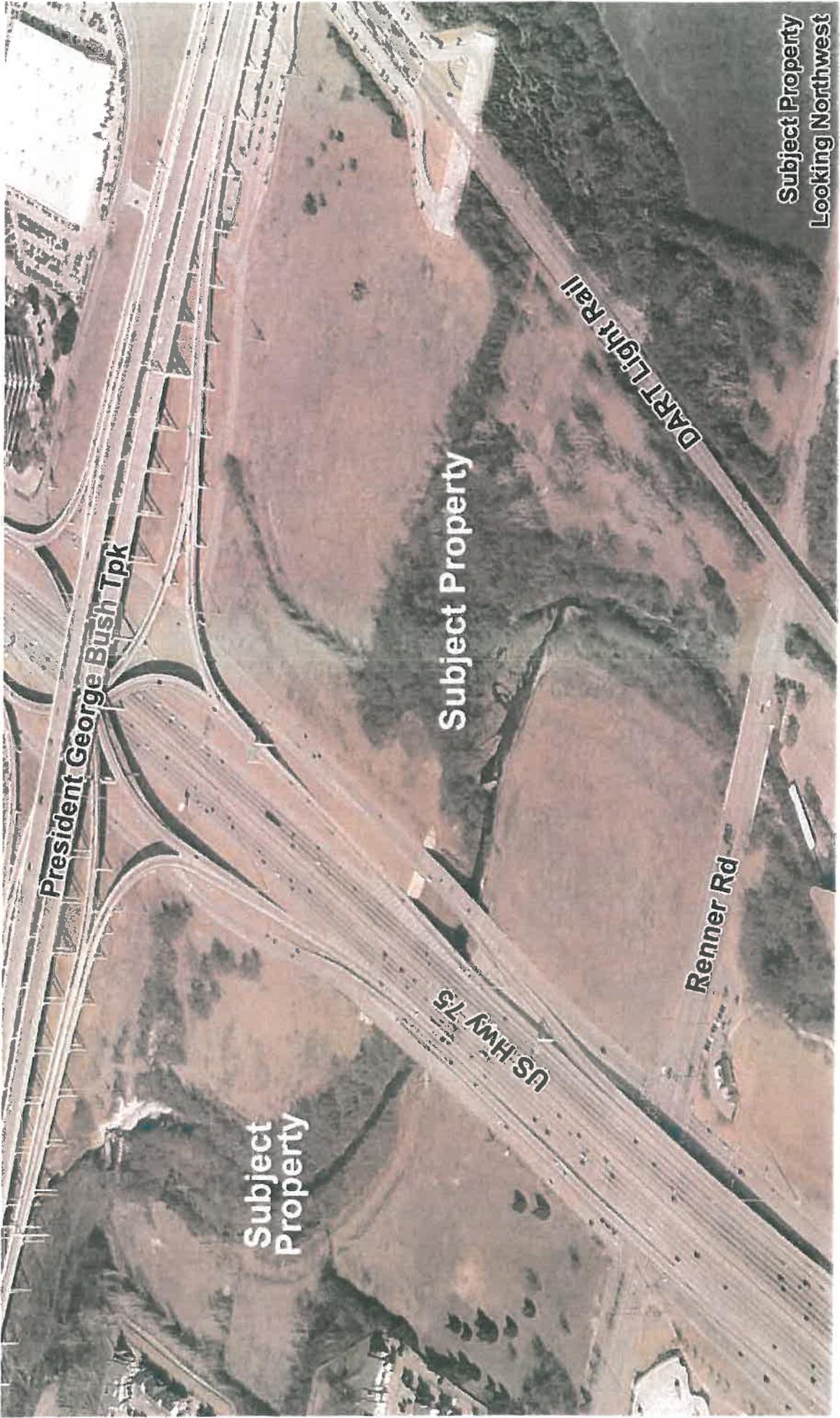
SUBJECT PROPERTY FOR ZONE CHANGE

SUBJECT PROPERTY FOR ZONE CHANGE

ZF 10-20 Aerial Map

Updated By: sbaddley; Update Date: November 17, 2019
File: DSM\mapping\3a\10-20\ZF10-20\ZF1020_ortho.mxd





President George Bush Tpk

Subject Property

US HWY 75

Subject Property

DART Light Rail

Renner Rd

Subject Property
Looking Northwest



GFF Planning

2808 Fairmount Street
Suite 300
Dallas, Texas 75201

214.303.1500, Tel
214.303.1512, Fax
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R Lawrence Good FAIA LEED AP
Brian E Moore AICP
Erin L E Little AIA LEED AP

October 04, 2010

**Mr. Sam Chavez
City of Richardson
Development Services Department
P.O. Box 830309
Richardson, Texas 75083-0309**

RE: Caruth Properties Transit Oriented Planned Development

In January of 2010 the City of Richardson planning and economic development staff approached the consultants for the Caruth properties in an effort to consolidate the zoning and land uses of the properties on each side of the DART LRT Bush Turnpike Station. As initiated by the City's leadership, this consolidation has resulted in a coordinated effort to rezone both tracts of land, which includes two tracts of land west of the station—one on each side of U.S. 75. The expectation for this effort will be to influence the alignment of the future Cotton Belt commuter rail line to travel through the Caruth properties, and link a future transit station with the existing LRT station. This link would make the southeast corner of U.S. 75 and President George Bush Turnpike very unique as one of the premier transit oriented destinations in North Texas.

The purpose of the Caruth Properties Transit Oriented Planned Development as described in the attached submission is to support the development of a pedestrian oriented, mixed-use urban environment, with convenient access to rail transit, shopping, employment, housing, and regional retail services. The goal of this submission is to promote an efficient, compact land use pattern; encourage pedestrian activity; reduce the reliance of private automobiles; and provide a more functional and attractive community through the use of recognized principles of urban design.

The objective of this submission is to foster a major regional employment center with significant regional retail and residential uses within a convenient walking distance from the existing Bush Turnpike transit station and potential transfer station to the Cotton Belt rail line. The existing transit station provides a connection to the twelve (12) member cities of DART, including Dallas. Development within this area would be of high intensity, accommodating large scale office and retail users while providing for transitions to adjoining uses and neighborhoods.

Our team is excited to be presenting this submission to you for your consideration, and look forward to proceeding through the administrative process for this important site that will hopefully be a model for all of North Texas.

Regards,
GOOD FULTON & FARRELL PLANNING

A handwritten signature in black ink, appearing to read "Brian E. Moore". The signature is stylized with a large, sweeping initial "B" and a long horizontal line extending to the right.

Brian E. Moore, AICP

Cc: Pamela A. Spadaro, CCIM
Paul Moser
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CARUTH PROPERTIES TRANSIT ORIENTED PLANNED DEVELOPMENT CODE

January 13, 2011



GFF Planning



Gateway
Planning Group

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Section 1. Purpose and Intent:

The purpose of the Caruth Properties Transit Oriented Planned Development Code (hereinafter, “the C-TOPD Code”) is to support development of a pedestrian-oriented, mixed-use urban development environment, with convenient access to rail transit, shopping, employment, housing, and regional retail services. The goal of the Caruth Properties Transit Oriented Planned Development District (hereinafter, “the C-TOPD District”) is to promote an efficient, compact land use pattern; encourage pedestrian activity; reduce the reliance on private automobiles; and provide a more functional and attractive community through the use of recognized principles of urban design.

- 1.1 Economic Development – The C-TOPD District and corresponding standards are created to support economic development, sustainable tax base, and job creation by establishing adjacency predictability of private development that supports and leverages investment in and around the C-TOPD.
- 1.2 Implement the Design Goals of the Regulating Plan – The objective of the C-TOPD District is to foster a major regional employment center with significant regional retail and residential uses within convenient walking distance from the existing transit station and potential transfer station to the Cotton Belt rail line. The existing transit station provides a connection to the twelve (12) member cities of Dart, including Dallas. Development within this area would be of high intensity, accommodating large scale office and retail users while providing for transitions to adjoining uses and neighborhoods.
- 1.3 Establish Specific Development Standards – The C-TOPD Code implements the vision for Transit Oriented Development along the DART rail corridor as established in the City’s Comprehensive Plan and Caruth Properties Regulating Plan, hereafter known as the Regulating Plan (Appendix A). The Regulating Plan shall provide guidance to property owners, developers, and the City on the form, character, and intensity of future development in the C-TOPD. Creation of different Character Zones within the C-TOPD District enables specific site and locational standards to be enumerated and applied. Clear graphic standards are provided for location, height, and building elements. Such standards promote sustainability, public welfare, walkable mixed use development, housing variety and transportation choice.

Section 2. Components of the Code:

- 2.1 This C-TOPD Code shall apply to the C-TOPD District unless otherwise specified in this Code. Development of property within the C-TOPD shall comply with the respective development standards set forth in the C-TOPD Code. The components of this C-TOPD consist of:
 - 2.1.1 C-TOPD District Regulating Plan: The C-TOPD District Regulating Plan, hereafter known as the “Regulating Plan”, is its official zoning map. It identifies the applicable character zones within the C-TOPD District including:
 - i. Character Zones – The C-TOPD is divided into different “Character Zones”. A Character Zone creates a distinct urban form within that Zone which is different from urban forms in other Character Zones. Each Character Zone shall establish use and development standards including height, bulk, building and parking location, and functional design. The Regulating Plan classifies all lots within the C-TOPD into one of five (5) Character Zones.
 - ii. Special Frontage Standards – The Special Frontage Standards establish exceptions and special conditions for all buildings along designated frontages. These Special Frontage standards shall apply in addition to the underlying Character Zone standards.
 - iii. Street Designations by Street Type – The Street Designations illustrate the design, configurations and development context for all streets within the C-TOPD. The Street

Classification addresses vehicular lane widths, number of lanes, pedestrian accommodation, street tree requirements, on-street parking, and parkway and median standards (streetscape standards). Street designations on the Regulating Plan include Type 'A' and Type 'B' Streets, Mandatory and Non-Mandatory Street Networks. The Mandatory Street Network specifies the future streets needed to implement the C-TOPD Regulating Plan, and shall be required and shall generally meet the locational and connectivity goals of the Regulating Plan. Their design shall be guided by the Street Type Specifications. The Non-Mandatory Street Network indicates locations of suggested, but not required streets within the C-TOPD Regulating Plan. These streets are not mandatory, but at the election of the developer and their design shall be guided by the Street Type Specifications.

- iv. Civic/Open Space Designations – Civic/Open Space Designation include Mandatory and Non-Mandatory Civic/Open Spaces. The Mandatory Civic/Open Space areas shown on the Regulating Plan designate the locations of proposed civic/open spaces (including parks, plazas, greens, and squares). The Non-Mandatory Civic/Open Space designation indicates the locations of desired, but not required civic/open spaces (including parks, plazas, greens, and squares) to implement the Regulating Plan. Recommended Civic/Open Spaces are not mandatory, but at the election of the developer.

- 2.1.2 Development Standards: The C-TOPD Code (the text portion of this Code) enumerates the development standards with text and graphics for Character Zones, building form, civic/open space, landscape, architectural, signage, lighting, and all related standards for all streets, public and private development.

Section 3. Administration

This section sets forth the provisions for reviewing and approving development applications within the C-TOPD District. The intent is to ensure that all development is consistent with the provisions of this Code. All sections of this Code shall be applied during the review process.

- 3.1 The development standards under Articles XIII-A thru XVI and Article XXI-C of the City of Richardson Comprehensive Zoning Ordinance, as amended, shall not apply to the C-TOPD except as specifically referenced herein. Development standards not addressed in the C-TOPD Code shall be governed by the City of Richardson Comprehensive Zoning Ordinance to the extent they are not in conflict with the intent or text of the C-TOPD Code.

- 3.2 Sign Standards under Chapter 18, as amended, of the City of Richardson Code of Ordinances, shall not apply to the C-TOPD except as specifically referenced herein.

3.3 Using this Code:

The following basic steps should be followed to determine the uses and development standards applicable to all properties within the C-TOPD District:

- 3.3.1 Locate the subject property on the Regulating Plan.

3.3.2 Identify:

- i. the Character Zone in which the property is located;
- ii. the Street Type designation along all its street frontages; and,
- iii. any Special Frontage Requirements that may be applicable to the subject property.

- 3.3.3 Review the Schedule of Uses by Character Zone as listed in Table 6.1 to determine allowed uses.

- 3.3.4 Examine the corresponding zone standards in the Building Form and Development Standards in Section 7 to determine the applicable development standards.

- 3.3.5 Refer to Section 5.5 for Special Frontage Standards.

- 3.3.6 Refer to Section 8 for Building Design Standards.

- 3.3.7 Refer to Section 9 for Street Type and Streetscape Standards.

The information from the above listed steps explains where the building will sit on the lot, the limits on its three dimensional form, the range of uses, and the palette of materials that will cover it.

- 3.4 Development within the C-TOPD District that complies with the provisions of this Code shall follow the City's development process as outlined in Chapter 21, Article II of the City of Richardson's Code of Ordinances and shall be approved by the City Manager or designee (see Appendix C for flow chart of the review process). In addition to complying with applicable City regulations that are not in conflict with this Code, the applicant shall provide the information required to adequately show compliance with this Code.
- 3.5 Standard for Approval of development plans: If a development plan conforms to the standards set forth in this Code and applicable City regulations not in conflict with this Code, the development plan shall be approved. Upon request by an applicant, the authority charged with approving the development plan shall certify the reason for an action taken on a development plan.
- 3.6 The City Manager or designee shall be responsible for the following:
- 3.6.1 Reviewing development plan applications for compliance with the requirements of C-TOPD.
 - 3.6.2 Approving development plan applications that are in compliance with the requirements of the C-TOPD Code.
 - 3.6.3 Making determinations on the applications and interpretations of standards in this Code.
 - 3.6.4 Approving revisions to previously approved development plans that comply with this Code and all applicable city ordinances.
 - 3.6.5 Approving any minor modifications to the approved Regulating Plan per Section 3.8.
 - 3.6.6 Recommendations on any SDP applications to the City Plan Commission (CPC) and City Council.
- 3.7 Special Development Plans: A request for a modification to any of the standards of this Code other than minor modifications permitted under Sections 3.8 shall be reviewed and processed as Special Development Plans.
- 3.7.1 Special Development Plans (SDP) are intended to allow applicants development flexibility to address specific market opportunities and/or contexts. An application for a Special Development Plan shall be processed as an amendment to the zoning ordinance under Article XXIX of the City of Richardson Zoning Ordinance and may only be considered by the City Council (CC) after the CPC has made a recommendation. The City Manager or designee shall review, make recommendations on any SDPs, and shall forward all SDP applications to the CPC. In evaluating an SDP, CC may consider the extent to which the application meets any of the following:
- i. provides an alternative "Master Plan" approach by consolidating multiple properties to create a predictable, market responsive development for the area; or
 - ii. fits the adjoining context by providing appropriate transitions.
- 3.8 Minor Modifications to the C-TOPD Code:
- The City Manager or designee shall have the authority to approve a request for minor modifications to C-TOPD Code that:
- 3.8.1 Does not materially change the circulation and building location on the site;
 - 3.8.2 Does not increase the building area permitted under this Code;
 - 3.8.3 Does not change the relationship between the buildings and the street;
 - 3.8.4 Does not allow a use not otherwise authorized in this Code;
 - 3.8.5 Does not allow greater height of any building or reduction of any parking requirement established in this Code; or
 - 3.8.6 Changes to established street cross sections per Table 3.1 below and Section 9 of this Code.

The City Manager or designee shall also have the authority to approve minor modifications outlined in Table 3.1. Any appeals to the decisions of the City Manager on minor modifications shall be heard by the City Council. Any City Council denials of minor modifications or any changes beyond those that meet the criteria above, the thresholds established in Table 3.1, shall be processed as an amendment to this Code under Article XXIX of the City of Richardson Comprehensive Zoning Ordinance.

Table 3.1 Minor Modification Thresholds

<i>Standard</i>	<i>Minor Modification Threshold</i>	<i>Comments</i>
Area/boundary of Character Zones (including any Mandatory Civic/Open Spaces)	No more than a 15% change (increase or decrease) in the area of the TOD Freeway Hi-Rise, TOD Arterial Mixed Use and Urban Neighborhood Zone (aggregate or per block). No more than 25% change (increase or decrease) in the area of the TOD Core and TOD Mixed Use Zone (aggregate or per block).	<ul style="list-style-type: none"> • Shall not eliminate any Character Zone • 15% or 25% measurement shall be based on the total area of that specific Character Zone within the entire C-TOPD District
Location of any Mandatory Street	Location shall not move more than 100' in any direction. (This standard does not apply to the TOD Trail Connector Street Type)	<ul style="list-style-type: none"> • Shall maintain the connectivity intended by the Regulating Plan
Building Form and Development Standards		
<ul style="list-style-type: none"> • Street Setback Line 	No more than a 10% change in the SSL as established in the corresponding Character Zone Building Form and Development Standards	<ul style="list-style-type: none"> • Changes to the Street Setback Lines may only be due to any changes to the street cross sections or change in the width of a sidewalk • In no case shall the sidewalk be less than 6 feet in width.
<ul style="list-style-type: none"> • Build to zones/setbacks 	No more than a 20% change in the maximum or minimum setback.	
<ul style="list-style-type: none"> • Building Frontage 	No more than a 15% reduction in the required building frontage along each block of a Type 'A' Street	<ul style="list-style-type: none"> • Any reduction in the required building frontage shall be to accommodate Porte-cocheres for drop-off and pick-up.
<ul style="list-style-type: none"> • Street screen 	Waiver of Street Screen requirement along a Type 'B' Street	<ul style="list-style-type: none"> • Requirement for a street screen may only be waived along a Type 'B' Street along the frontage of any interim surface parking lot (off-street) that is intended to be in-filled with a parking structure. • In no case shall any portion of the surface parking have frontage along a Type 'A' Street without a required street screen • In no case shall the (off-street) surface parking lot be located at a street intersection for a minimum depth of 30' along each street (regardless of the Street Type).
Street Cross Sections	Cross sections of new streets may be adjusted with respect to number of lanes, lane widths, on-street parking configuration, pedestrian accommodation, and street tree planting	<ul style="list-style-type: none"> • Any changes in the street cross sections shall be based on specific development context such as vegetation, natural features, drainage, and fire access and is subject to approval by the City.

3.9 Requirement for Periodic Review:

3.9.1 60 months after the approval of the first building permit for a structure on the property, and at a frequency of each 36 months thereafter, the City Manager or designee shall convene a meeting of the

owner or owners of all properties within the planned development for the purpose of review and assessment of the effectiveness of the ordinance in achieving intended development forms and patterns. The agenda for this periodic review might include assessment of building appearance, quality of construction, mix of land uses and traffic conditions. It is intended that these reviews would provide an opportunity for the property owners and the City of Richardson to determine if modifications to the ordinance would be mutually beneficial to achieve desired results.

Section 4. Definitions

In addition to Definitions in Article I of the City of Richardson Comprehensive Zoning Ordinance, the following terms shall have the corresponding interpretations.

Arcade means a portion of the main façade of the building that is at or near the Street-Setback Line and a colonnade supports the upper floors of the building. Arcades are intended for buildings with ground floor commercial or retail uses and the arcade may be one or two stories.

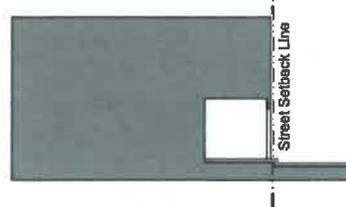


Image of an arcade

Attics/Mezzanines means the interior part of a building contained within a pitched roof structure or a partial story between two main stories of a building.

Auto-Related Sales and Service Uses means establishments that provide retail sales and services related to automobiles including, but not limited to, tires, batteries, gasoline, etc.

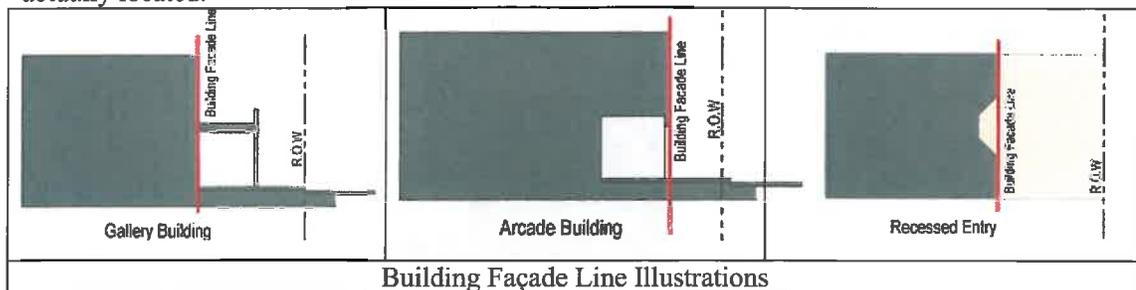
Block means the aggregate of lots, pedestrian passages and rear alleys, circumscribed by streets.

Block Face means the linear dimension of a block along one of its street frontages.

Build-to Line means the line at which the principal building’s front façade shall be built.

Build-to Zone (BTZ) means the area within which the principal building’s front façade is to be built.

Building Façade Line means the horizontal plane along a lot where the building’s front façade is actually located.



Building Form Standards means the standards established for each Character Zone that specify the height, bulk, orientation, and elements for all new construction and redevelopment.

Building Frontage means the percentage of the building’s front façade that is required to be located at the front Build-to Line or Zone as a proportion of the block frontage along that public street. Parks, plazas, squares, improved forecourts, and pedestrian breezeway frontages shall be considered as buildings for the calculation of building frontage.

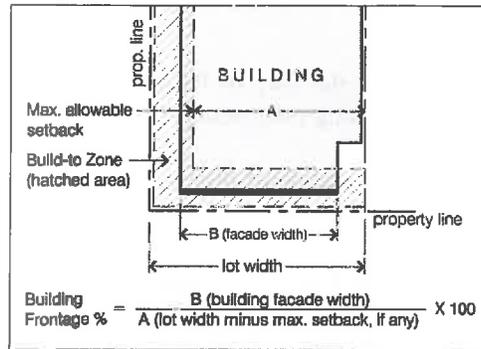


Image showing how a lot's building frontage is calculated .

Character Zone means an area within the C-TOPD that creates a distinct urban form different from other areas within the C-TOPD. Character Zones are identified in the Regulating Plan.

City Manager means the City Manager of the City of Richardson or his/her designee.

Civic/Open Space means publicly accessible open space in the form of parks, courtyards, forecourts, plazas, greens, pocket parks, playgrounds, etc. They may be privately or publicly owned. For all residential uses, privately accessible open spaces such as courtyards, porches, and balconies may also be considered as Civic/Open Space for the purposes of the C-TOPD Code.

Comprehensive Plan means the City of Richardson Comprehensive Plan that establishes the blueprint for the long-term growth and development of the City as adopted on the effective date of this Code.

Encroachment means any structural or non-structural element such as a sign, awning, canopy, terrace, or balcony that breaks the plane of a vertical or horizontal regulatory limit, extending into a Setback, into the Public R.O.W, or above a height limit.

Gallery means an extension of the main façade of the building that is at or near the front property line and the gallery may overlap the public sidewalk.

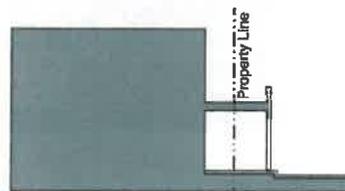


Image of a Gallery

Kiosk means a small, roofed structure, often open on one or more sides, used as a newsstand or booth. This structure could be temporary or permanent.

Live-Work Unit means a dwelling unit that is also used for work purposes, provided that the 'work' component is restricted to the uses of professional office, artist's workshop, studio, or other similar uses and is located on the street level and constructed as separate units under a condominium regime or as a single unit. The 'live' component may be located on the street level (behind the work component) or any other level of the building. Live-work unit is distinguished from a home occupation otherwise defined by the C-TOPD Code in that the work use is not required to be incidental to the dwelling unit, non-resident employees may be present on the premises and customers may be served on site.

Living Screen means a Street Screen composed of landscaping in the form of vegetation.

Minor Modification means any changes to the C-TOPD Code that meets the criteria established in Section 3.8 and Table 3.1.

Master Sign Plan means a unique sign plan to implement a specific vision for a portion or all of the development that meets Section 10.2 of this Code.

Non-Mandatory Civic/Open Space means plaza, green, square, or park area designated on the Regulating Plan which is shown as a suggested feature within the C-TOPD. The Non-Mandatory Plaza is not required and is at the election of the developer.

Non-Mandatory Pedestrian Passage means a pedestrian access area designated on the Regulating Plan which is shown as a suggested feature within the C-TOPD. The Non-Mandatory Pedestrian Passage is not required and is at the election of the developer.

Non-Mandatory Street means a street that is designated on the Regulating Plan which is shown as a suggested street within the C-TOPD. The Non-Mandatory Street is not required and is at the election of the developer.

Park means a civic/open space that is a preserve available for unstructured recreation.

Plaza means a primarily hardscaped civic/open space with formal landscaping, available for civic purposes and commercial activities. A plaza shall be spatially defined by buildings.

Playground means a civic/open space designed and equipped for children's recreation. A playground may be fenced and may include an open shelter. Playgrounds may be located within residential areas and may be placed within a block. They may be included in other civic/open spaces.

Pedestrian Easement means the area between the curb face of the street and the Street-Setback Line (SSL). This area contains the sidewalk, street trees, lighting and pedestrian furniture.

Regulating Plan means the Zoning Map attached hereto as Appendix A that shows the Character Zones, Civic Spaces, location of Streets, maximum height permitted and other Special Requirements applicable to the C-TOPD subject to the standards in the C-TOPD Code. For the purposes of this Code, the Regulating Plan shall also be the Concept Plan for the C-TOPD District.

Retail Ready means space constructed at a minimum interior height of 14 feet which may be used for noncommercial uses and can be converted into retail/commercial use. Prior to the issuance of a certificate of occupancy for a retail/commercial use in a Retail-Ready space, the space must comply with all building and construction codes for that use. The intent of Retail-Ready space is to provide the flexibility of occupying a space in accordance with market demand and allowing the use in such space to change to retail/commercial uses accordingly.

Retail Sales Retail establishments are the final step in the distribution of merchandise. They are organized to sell in small quantities to many customers. Establishments in stores operate as fixed point-of-sale locations, which are designed to attract walk-in customers. Retail establishments often have displays of merchandise and sell to the general public for personal or household consumption, though they may also serve businesses and institutions. Some establishments may further provide after-sales services, such as repair and installation. Included in, but not limited to this category, are durable consumer goods sales and service, consumer goods, other grocery, food, specialty food, beverage, dairy, etc, and health and personal services.

Service Uses means a category for limited personal service establishments which offer a range of personal services that include (but not limited to) clothing alterations, shoe repair, dry cleaners, laundry, health and beauty spas, tanning and nail salons, hair care, etc.

Sign, Building Blade means a pedestrian-oriented sign that is affixed perpendicular to the corner of a building or to the front façade of a building above the ground floor to provide identification for the whole building.



Image of a Building Blade Sign

Sign, Marquee means a sign structure placed over the entrance to a theatre or other public gathering venue. It has signage stating either the name of the establishment or the name of the event, artist, and other details of the event appearing at that venue. The marquee is often identifiable by a surrounding cache of light bulbs, usually yellow or white, that flash intermittently or as chasing lights. Marquee signs may often be combined with Building Blade signs.



Image of a Marquee sign with a Building Blade Sign

Sign, Monument means any sign which is connected to the ground and which has no clear space for the full width of the sign between the bottom of the sign and the surface of the ground. A monument sign may include a sign face and sign structure, and may also include a sign base and sign cap.



Image of a Monument Sign

Sign, Sandwich Board means a portable sign consisting of two panels of equal size, which are hinged at the top or one panel with a support and placed on the ground or pavement so as to be self-supporting.



Images of sandwich board signs.

Sign, Tenant Blade means a smaller pedestrian-oriented sign that is affixed perpendicular to the building façade under a canopy or awning or immediately over a tenant space and provides identification for individual tenants within a building.



Image of a Tenant Blade Signs

Special Development Plan means a development application that meets Section 3.7 of this Code.

Station Platform Frontage: means the special frontage requirement on blocks with frontage along the rail station platform as indicated in the Regulating Plan.

Street Screen means a freestanding wall or living screen built along the BTZ or in line with the building façade line along the street. It may mask a parking lot or a loading/service area from view or provide privacy to a side yard and/or strengthen the spatial definition of the public realm.



Image of a combination masonry and living street screen

Street-Setback Line (SSL) means the imaginary line located at the outside edge of the sidewalk and measured from the center line of the street from which all build-to lines and build-to zones are measured from.

Street Network means the Mandatory and Non-Mandatory network for new and existing streets within the C-TOPD as established in the Regulating Plan.

Street Type means a specific designation for streets that establish a certain character and cross-sections to improve walkability within the C-TOPD.

Tree Well means an unpaved area around the trunk of a tree within the sidewalk area that is either landscaped with ground cover or covered with a tree grate.



Example of a tree well with a tree grate



Example of a tree well with landscaping

Type 'A' Street means the streets identified as such on the Regulating Plan. Type 'A' Streets are the primary pedestrian streets.

Type 'B' Street means the streets identified as such on the Regulating Plan. Type 'B' Streets are intended to primarily accommodate access to parking, service, and other auto-related functions.

Section 5. The Regulating Plan

- 5.1 The Regulating Plan (Appendix A) is hereby adopted as the official zoning map for the C-TOPD District.
- 5.2 Character Zones Established – the following Character Zones are established. The boundaries of the specific Character Zones shall be established in the Regulating Plan.
- 5.2.1 TOD Core: The TOD Core provides the most opportunity for the highest intensity development. It is the area that has significant development impact and the highest pedestrian activity due to its adjacency to the transit station. The TOD Core consists of the highest density, with the greatest variety of uses. Development within the TOD Core Zone shall meet the Building Form and Development Standards in Section 7.1 of this Code.
- 5.2.2 TOD Mixed Use: The TOD Mixed Use is the area adjacent to the TOD Core that is intended for high intensity commercial and residential uses in addition to supporting retail and restaurant uses. Development within the TOD Mixed Use Zone shall meet the Building Form and Development Standards in Section 7.2 of this Code.
- 5.2.3 Arterial Mixed Use: Arterial Mixed Use is intended to provide appropriate transitions to major regional roadways while taking advantage of the arterial frontage for limited auto-oriented sites. The Arterial Mixed Use frontage also acts as a gateway into the C-TOPD by providing an attractive street frontage for the overall development. Development within the Arterial Mixed Use Zone shall meet the Building Form and Development Standards in Section 7.3 of this Code.
- 5.2.4 TOD Freeway Hi-Rise: TOD Freeway Hi-Rise is intended to provide an appropriate transition into the C-TOPD from the President George Bush Turnpike (SH 190) and US 75 access roads. This area is also intended for high intensity development. Development within the TOD Freeway Hi-Rise Zone shall meet the Building Form and Development Standards in Section 7.4 of this Code.
- 5.2.5 Urban Neighborhood: The Urban Neighborhood consists primarily of a residential fabric. The area has a mix of Townhomes and Live-Work units with commercial activity concentrated at street intersections. Development within the Urban Neighborhood Zone shall meet the Building Form and Development Standards in Section 7.5 of this Code.
- 5.3 Street Designations D-1 Street Type Established – The Regulating Plan shall establish the following Street Designations:
- 5.3.1 Type “A” Streets Established – Type “A” Streets are intended to be the primary pedestrian streets. The Type “A” Streets are TOD Main Street, TOD Street Promenade and TOD Station.
- 5.3.2 Type “B” Streets Established – Type “B” Streets are intended to balance pedestrian orientation with automobile orientation (service access, driveways, drive-through lanes, etc.). The Type “B” Streets are TOD Street, Parkway, TOD Trail Connector, and Park Avenue.
- 5.4 Mandatory Streets by Street Type – The Street Network indicates Mandatory and Non-Mandatory streets needed to implement the C-TOPD Regulating Plan. The Regulating Plan designates the type, classification and location of streets. All new streets in the C-TOPD shall meet the street design standards established in Section 9 herein.
- 5.5 Special Frontage Requirements – In order to address specific requirements and transitions based on street frontage and adjacency, the following Special Frontage Requirement as established in the Regulating Plan shall apply:
- 5.5.1 Station Platform Frontage: Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.

5.6 Street Network by Street Type – The Street Network indicates Mandatory and Non-Mandatory streets needed to implement the C-TOPD Regulating Plan. The Regulating Plan designates the type and location of streets. All new streets in the C-TOPD shall meet the street design standards established in Section 9 herein

5.7 Civic/Open Space – The Regulating Plan indicates Mandatory and Non-Mandatory Civic/Open Spaces. The specific standards for Civic/Open Space are established in Section 11.

5.8 Building Height – The Regulating Plan also indicates the maximum building height permitted within each character zone of the C-TOPD District.

Section 6. Schedule of Permitted Uses

6.1 Generally: Due to the emphasis on urban form over land uses in the C-TOPD, general use categories have been identified by Character Zone. Uses not listed in the following schedule, but are substantially similar, may be permitted upon the approval of the City Manager or designee, subject to appeal directly to the City Council.

6.2 Schedule of Uses:

Table 6.1

Character Zone	TOD Core	TOD Mixed Use	Urban Neighborhood	Arterial Mixed Use	TOD Freeway Hi-Rise
Land Use					
Commercial Uses (Office, Retail, Sales & Service Uses)					
Retail Sales or Service with <u>no drive through facility</u> (includes alcohol sales, which shall meet Chapter 4, Alcohol Beverages of the City of Richardson Cod of Ordinances). Excluded from this category are Auto-Retail Sales and Service Uses (see Section 3 of the Code for Definition of Retail, Service uses, and Auto-related Sales and Service)	P	P	P/C	P	P
Finance, Insurance, and Real Estate establishments including banks, credit unions, real estate, and property management services, <u>with no drive through facility</u>	P	P	P/C	P	P
Offices for business, professional, and technical uses such as accountants, architects, lawyers, doctors, etc.	P	P	P/C	P	P
Research laboratory headquarters, laboratories and associated facilities	P	P	NP	P	P
Food Service Uses such as full-service restaurants, cafeterias, bakeries and snack bars with <u>no drive through facilities</u> Included in this category is café seating within a public or private sidewalk area with no obstruction of pedestrian circulation. Also included in this category is the sale of alcoholic beverages which shall meet Chapter 4, Alcoholic Beverages of the City of Richardson Code of Ordinances.	P	P	P/C	P	P
Live-Work Unit	P	P	P	P	P
Any use with a drive through facility	NP	P/C	NP	P/C	NP
Auto-related Sales and Service	NP	NP	NP	P/C	NP
Arts, Entertainment, and Recreation Uses					
Art galleries	P	P	P/C	P	P
Art, antique, furniture or electronics studio (retail, repair or fabrication; excludes auto electronics sales or service)	P	P	P/C	P	P
Games arcade establishments	P	P	NP	P	P
Theater, cinema, dance, music or other entertainment	P	P	NP	P	P

Character Zone	TOD Core	TOD Mixed Use	Urban Neighborhood	Arterial Mixed Use	TOD Freeway Hi-Rise
establishment					
Museums and other special purpose recreational institutions	P	P	NP	P	P
Fitness, recreational sports, gym, or athletic club	P	P	NP	P	P
Parks, greens, plazas, squares, and playgrounds	P	P	P	P	P
Educational, Public Administration, Health Care and Other Institutional Uses					
Business associations and professional membership organizations	P	P	NP	P	P
Child day care and preschools	P	P	P/C	P	P
Schools, libraries, and community halls	P	P	NP	P	P
Universities and Colleges	P/C	P/C	NP	P	P
Hospital	P	P	NP	P	P
Civic uses	P	P	P/C	P	P
Social and fraternal organizations	P	P	NP	P	P
Social services and philanthropic organizations	P	P	NP	P	P
Religious Institutions	P	P	P	P	P
Funeral homes	P	P	NP	P	P
Residential Uses					
Home Occupations	P/A	P/A	P/A	P/A	P/A
Multi-family residential					
Ground Floor	P/C	P	P	NP	P/C
Upper Floors	P	P	P	NP	P
Residential Lofts	P	P	P	P	P
Live-Work Unit	P	P	P	P	P
Single-family residential attached dwelling unit (Townhomes)	NP	P/C	P	NP	NP
Single-family residential detached dwelling unit	NP	NP	P	NP	NP
Accessory residential unit	NA	NA	P/A	NA	NA
Other Uses					
Model homes for sales and promotion**	P	P	P	P	P
Hotels	P	P	P	P	P
Parking, surface (primary use of property)	NP	NP	NP	NP	NP
Parking, surface (accessory use of property)	P	P	P	P	P
Parking, structured	P	P	P	P	P
Private attached garage	NP	P	P	NP	NP
Private detached garage	NP	NP	P	NP	NP
Sales from kiosks	P	P	NP	P	P
Veterinary clinic	NP	P	NP	P	NP
Community garden	P	P	P	P	P
Incidental Outdoor Display (subject to standards in Section 7 of the Code)	P/A	P/A	P/A	P/A	P/A
Antennas including cell, accessory, and mounted on top of buildings.	P/A/C	P/A/C	P/A/C	P/A/C	P/A/C
Utility infrastructure	P/A/C	P/A/C	P/A/C	P/A/C	P/A/C
Rain water harvesting equipment	P/A/C	P/A/C	P/A/C	P/A/C	P/A/C
Wind energy equipment	P/A	P/A	P/A	P/A	P/A
Solar energy equipment	P/A	P/A	P/A	P/A	P/A
Special Event	P	P	P	P	P

P= Permitted by right NP= Not Permitted P/C= Permitted with design criteria per Table 6.2 P/A = Permitted Accessory Use NA= Not applicable

A* = Accessory use to not exceed 25% of the primary use building square footage

** Model homes are limited to a time period until all the homes are sold in the neighborhood.

6.3 Use Criteria: All uses listed as P/C in Table 6.1 shall also meet the following standards in Table 6.2

<i>Use</i>	<i>Zone</i>	<i>Location & Design Criteria</i>
Non-Residential Uses		
Retail Sales & Service (Excluded from this category are Auto-Retail Sales and Service)	Urban Neighborhood	<ul style="list-style-type: none"> Shall be limited to ground floor uses on corner lots in the Urban Neighborhood Zone.
Finance, Insurance, and Real Estate establishments including banks, credit unions, real estate, and property management services (no drive through facility).		
Food Service Uses such as full-service restaurants, cafeterias, bakeries and snack bars (no drive-through facility). Included in this category is café seating within a public or private sidewalk area with no obstruction of pedestrian circulation. Also included in this category is the sale of alcoholic beverages which shall meet Chapter 4 Alcohol Beverages of the City of Richardson Code of Ordinances.		
Offices for business, professional, and technical uses such as accountants, architects, lawyers, doctors, etc. Art, antique, furniture or electronic studio (retail, repair, or fabrication) Art galleries	Urban Neighborhood	<ul style="list-style-type: none"> Shall be limited to ground floor uses on any lot in the Urban Neighborhood Zone.
Any permitted use with a drive through facility	TOD Mixed Use & Arterial Mixed Use	<ul style="list-style-type: none"> All drive through access (driveways) shall be from Type 'B' Streets. Drive through lanes and/or canopies shall not have frontage along on or be located along any Type 'A' Streets. Drive through areas screened by a 4' high Street Screen.
Universities and Colleges	TOD Core & TOD Mixed Use	<ul style="list-style-type: none"> Shall be required to provide structured parking as part of the build-out for the university/college campus
Civic uses	Urban Neighborhood	<ul style="list-style-type: none"> Shall be limited to ground floor uses on corner lots in the Urban Neighborhood.
Child day care and preschools	Urban Neighborhood	<ul style="list-style-type: none"> Shall be located on corner lots only Pre-schools shall be no greater than 15,000 sq. feet Daycares shall be no greater than 10,000 sq. feet
Auto-related Sales and Service	Arterial Mixed Use	<ul style="list-style-type: none"> No more than 50% of a block's frontage along a Type 'B' Street shall be occupied by gas pumps, canopies, and/or service bays. No outdoor storage of vehicles or other products sold shall be permitted. All auto related sales display shall be inside storefronts.
Residential Uses		
Multi-family residential Ground Floor	TOD Core & TOD Freeway Hi-Rise	<ul style="list-style-type: none"> All ground floors along all Type 'A' Streets shall be built to Retail Ready standards.
Single-family residential attached dwelling unit (Townhomes)	TOD Mixed Use	<ul style="list-style-type: none"> Shall not be permitted along Renner Road
Other Uses		
Antennas including cell, accessory and mounted (Excluded from this category are freestanding and commercial antennas and equipment buildings)	All Zones	<ul style="list-style-type: none"> Antennas shall be permitted on rooftops. Antennas shall be screened entirely with a screen of same color as the principal building. Antennas shall not be visible from adjacent Type 'A' Street.

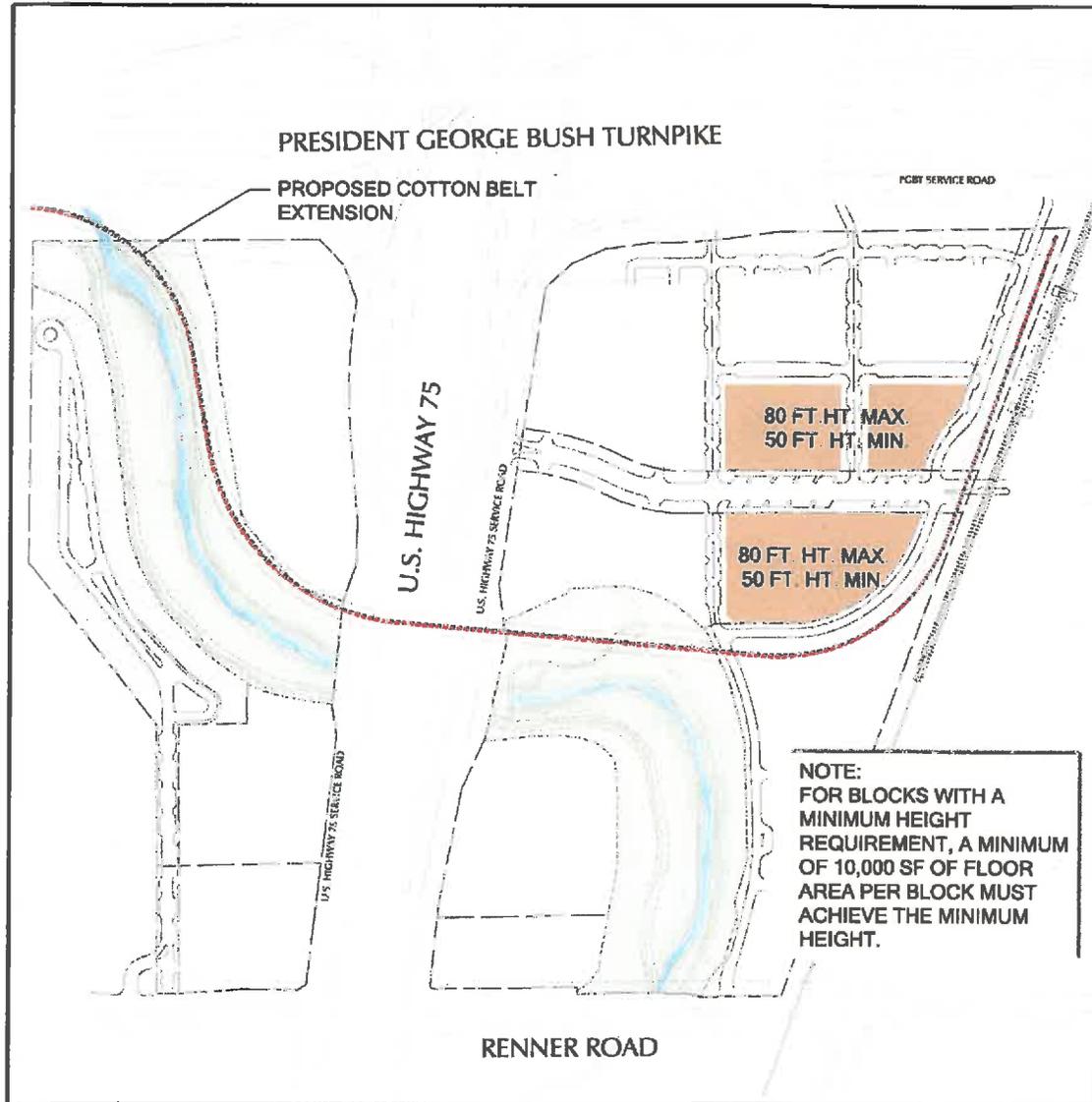
<i>Use</i>	<i>Zone</i>	<i>Location & Design Criteria</i>
Utility equipment (includes electrical transformers, gas meters, etc)	All Zones	<ul style="list-style-type: none"> • Utility equipment shall not be placed along a Type 'A' Street. • On all other frontages, they shall be screened with a Street Screen at least as high as the equipment being screened. • Utility equipment shall be permitted on rooftops. • Utility equipment shall be screened entirely with a screen of same color as the principal building.
Rain water harvesting equipment	All Zones	<ul style="list-style-type: none"> • Rain water harvesting equipment shall be permitted on rooftops. • Rain water harvesting equipment shall be screened entirely with a screen of same color as the principal building.

Section 7. Building Form and Development Standards

The following section establishes the Building Form and Development Standards for all Character Zones within the C-TOPD District. Diagrams and reference letters are used for illustrations purposes only. Reference letters may not be in continuous sequence.

7.1 TOD Core Zone

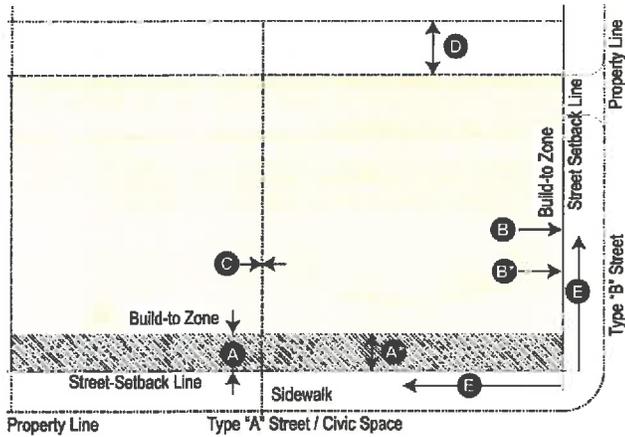
Character Zone: TOD Core



TOD Core Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements.

7.1.1 Building Placement



Legend

-----	Property Line	-----	Setback Line	-----	Street-Setback Line
■	Building Area	▨	Build-to Zone		

Street-Setback Line (SSL)
(Distance from center line of street to edge of the BTZ)

TOD Main Street	61 feet
TOD Street	32 feet min.
TOD Street Promenade	32 feet min.
TOD Station	33.5 feet min.
TOD Trail Connector	24 feet min.

Build-To Zone (BTZ)
(Distance from edge of SSL to edge of the BTZ)

Front (Type "A" Street and Civic Space)	0 – 10 feet	A
Front (Type "B" Street)	0 feet (see #1)	B

Setback

Front (Type "A" Street and Civic Space – distance from SSL)	0 feet (min.) 10 feet (max.)	A*
Front (Type "B" Street – distance from SSL)	0 feet	B*
Side (from property line)	0 feet (see #2)	C
Rear (from property line)	5 feet (see #2)	D

Building Frontage

Building Frontage required along all street BTZ	90% (min.) (see #3 and #8)	E
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7.1.2 Block Standards

Block face dimensions	250 – 500 feet (see #4)
Block perimeter	1500 feet (maximum) (see #4)

7.1.3 Building Height

Principal Building Standards

Building maximum	Shall be established on the Regulating Plan (see #6 and #9)
First floor to floor height	15 feet min. (see #5)
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)
Upper floor(s) height (floor-to-floor)	10 feet min.

7.1.4 Commercial Frontage Requirements

Ground floors of all buildings fronting on Type "A" Streets shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

7.1.5 Station Frontage Requirements

Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.

Notes

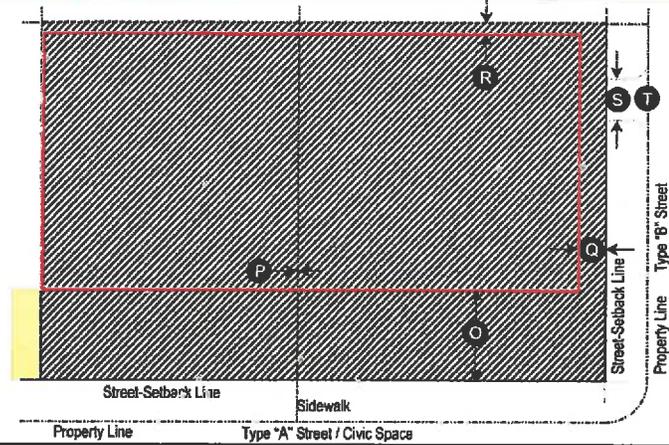
- #1 – The area between the building and the edge of the BTZ at the public sidewalk shall be paved flush with the sidewalk.
- #2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #3 – Corner building street facades shall be built to the BTZ for a minimum of 30' from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to-zone standards.
- #4 – Blocks exceeding the maximum block face and perimeter dimensions may be permitted as follows:
 - (i) No more than two adjacent blocks may be combined based on the Regulating Plan.
 - (ii) An increase in block dimensions shall not eliminate or significantly move a required street or required civic/open space. Required streets and required civic/open spaces may move a maximum of 100 feet
 - (iii) The block shall maintain a continuous Type "A" Street frontage with adjoining blocks.
 - (iv) Ground floor internal pedestrian connectivity shall be provided through the block. Connectivity may be provided inside the building, through a parking garage or outside by way of a Pedestrian Passage.
- #5 – First floor heights shall not apply to parking structures.

#6 – Not used.

#7 – All buildings in the TOD Core Zone shall meet the Building Design Standards in Section 8.

#8 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen higher than the tallest piece of equipment. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen that provides an opaque screen at maturity or a combination of masonry and living screen. Species shall be selected from the C-TOPD Planting List in Appendix B of the C-TOPD Code. The required Street Screen shall be located within the BTZ along the corresponding frontage.

7.1.6 Parking & Service Access



Legend

Property Line	Building Footprint
Surface Parking Area	Street-Setback Line
Above Grade Parking Area	

(i) Parking Location

Surface/At Grade Parking		
Type "A" Street and Civic Space setback	Shall be located behind the principal building	O
Type "B" Street setback	Min. of 3 feet behind the building facade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R
Above Grade Parking		
Setback along Type "A" Street, Type "B" Street, and Civic Space (distance from SSL)	0 feet min.	O
Side and rear setbacks (distance from property line)	0 feet min.	
Upper Floors	May be built up to the building line	

(ii) Required Off-Street Parking Spaces

Non-residential uses	1 space/300 sq. feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	20 feet max. (except when drives may need to be wider to address service access or fire lane standards)	S
Driveways and off-street loading and unloading shall not be located on a Type "A" Streets.		
Porte cocheres may be permitted on Type "A" Streets to provide drop-off and valet service.		
Shared driveways and cross access easements are encouraged between lots to minimize curb cuts.		
If driveway and/or off-street service loading and unloading access is provided from a Type "A" Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.		

7.1.7 Encroachments

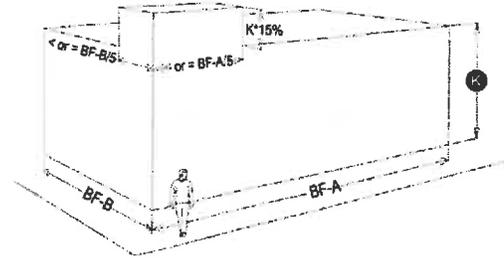
Canopies, signs, awnings and balconies may encroach over the BTZ, setback, and, sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.1.8 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

Notes

#9 - Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



#10 - Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.

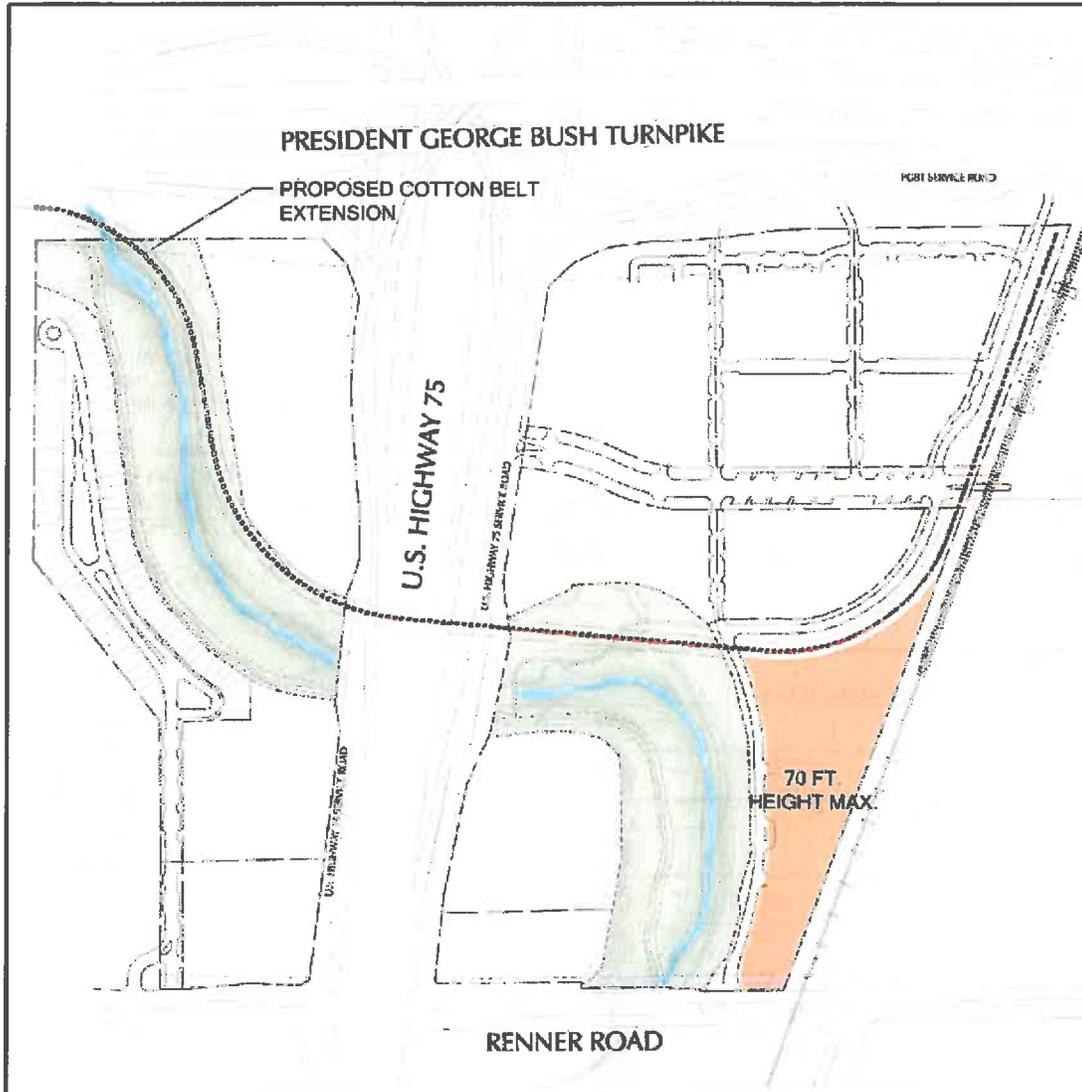
#11 - Setbacks and build-to lines for recessed entry and arcade buildings shall be measured from the building façade line which contains the recessed entry or arcade.

#12 - Required parking may be provided anywhere within the C-TOPD.

#13 - Article III, Chapter 21, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-stre parking areas.

7.2 TOD Mixed Use Zone

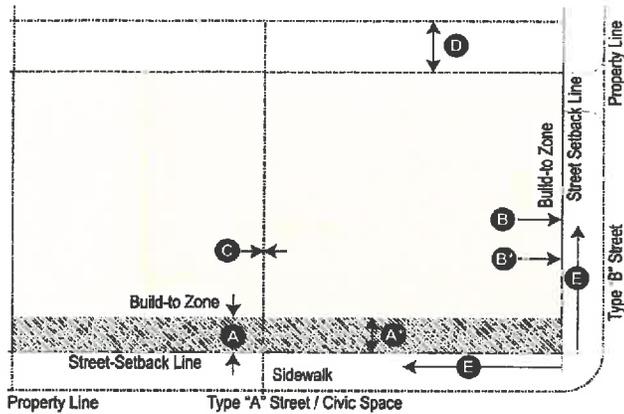
Character Zone: TOD Mixed Use



TOD Mixed Use Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements.

7.2.1 Building Placement



Legend

----- Property Line	----- Setback Line	----- Street-Setback Line
Building Area	Build-to Zone	

Street-Setback Line (SSL)
(Distance from center line of street cross section to edge of the BTZ)

Parkway	38 feet
---------	---------

Build-To Zone (BTZ)
(Distance from edge of SSL to edge of the BTZ)

		(A)
Front (Type "B" Street)	0 - 10 feet (see #1)	(B)

Setback

		(A*)
Front (Type "B" Street – distance from SSL)	0 - 20 feet	(B*)
Side (distance from property line)	0 feet (see #2)	(C)
Rear (distance from property line)	0 feet (see #2)	(D)

Building Frontage

Building Frontage required along all street BTZ	90% (min.) (see #3 and #8)	(E)
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7.2.2 Block Standards

Block face dimensions	250 – 500 feet (see #4)
Block perimeter	1500 feet (maximum) (see #4)

7.2.3 Building Height

Principal Building Standards

Building maximum	Shall be established as on the Regulating Plan (see #6 and #9)
First floor to floor height	10 feet min. (see #5)
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)
Upper floor(s) height (floor-to-floor)	10 feet min.

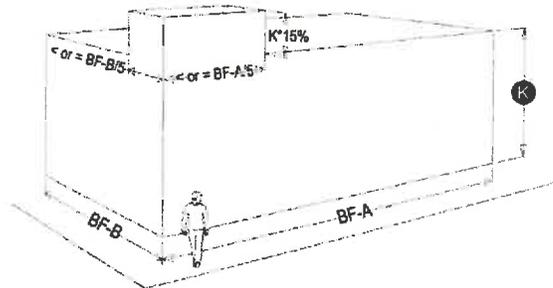
Notes

- #1 – The area between the building and the edge of the BTZ at the public sidewalk shall be paved flush with the sidewalk.
- #2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #3 – Corner building street facades shall be built to the BTZ for a minimum of 30' from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to zone standards.
- #4 – Blocks exceeding the maximum block face and perimeter dimensions may be permitted as follows:
 - i. No more than two adjacent blocks may be combined based on the Regulating Plan.
 - ii. An increase in block dimensions shall not eliminate or significantly move a required street or required civic/open space. Required streets and required civic/open spaces may move a maximum of 100 feet
 - iii. The block shall maintain a continuous Type "A" Street frontage with adjoining blocks.
 - iv. Ground floor internal pedestrian connectivity shall be provided through the block. Connectivity may be provided inside the building, through a parking garage or outside by way of a Pedestrian Passage
- #5 – First floor heights shall not apply to parking structures.
- #6 – Not used.

#7 – All buildings in the TOD Mixed Use Zone shall meet the Building Design Standards in Section 8.

#8 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen higher than the tallest piece of equipment. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen that provides an opaque screen at maturity or a combination of masonry and living screen. Species shall be selected from the C-TOPD Planting List in Appendix B of the C-TOPD Code. The required Street Screen shall be located within the BTZ along the corresponding frontage.

#9 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



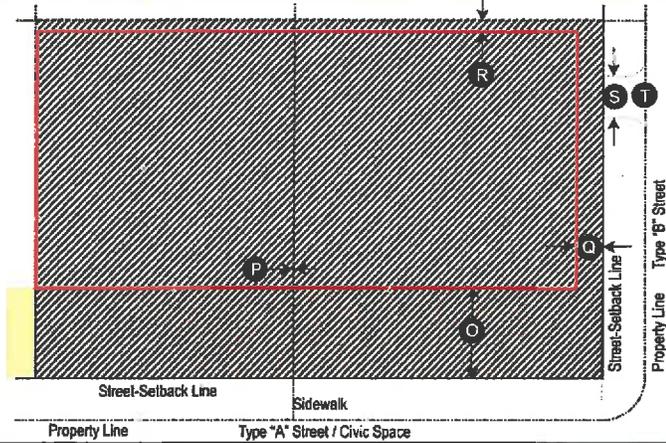
#10 - Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.

#11 – Setbacks and build-to lines for recessed entry and arcade buildings shall be measured from the building façade line which contains the recessed entry or arcade.

#12 – Required parking may be provided anywhere within the C-TOPD.

#13 – Article III, Chapter 21, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas

7.2.5 Parking & Service Access



Legend

- Property Line
- Building Footprint
- Surface Parking Area
- Above Grade Parking Area
- Street-Setback Line

(i) Parking Location

Surface/At Grade Parking

		O
Type "B" Street setback	Min. of 3 feet behind the building facade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R

Above Grade Parking

Setback along Type "B" Street (distance from SSL)	0 feet min.	O
Side and rear setbacks (distance from property line)	0 feet min.	

Upper Floors	May be built up to the building line
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(ii) Required Off-Street Parking Spaces

Non-residential uses	1 space/250 sq. feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	20 feet max. (except service drives which may be a max. of 30 feet wide)	S
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Driveways and off-street loading and unloading shall not be located on a Type "A" Streets. **T**
 Porte cocheres may be permitted on Type "A" Streets to provide drop-off and valet service.
 Shared driveways and cross access easements are encouraged between lots to minimize curb cuts.
 If driveway and/or off-street service loading and unloading access is provided from a Type "A" Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.

7.2.6 Encroachments

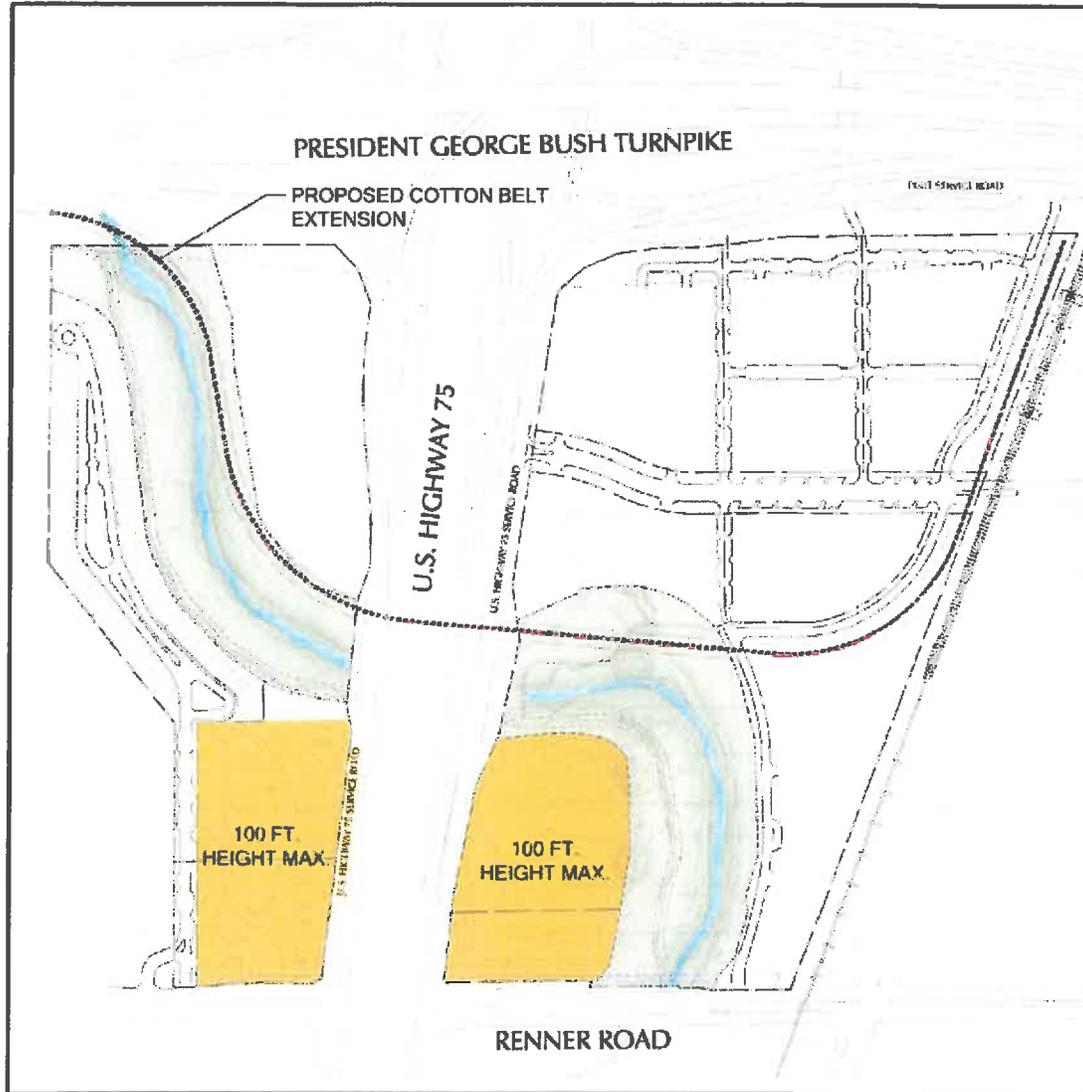
Canopies, signs, awnings and balconies may encroach over the BTZ, setback, and sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.2.7 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

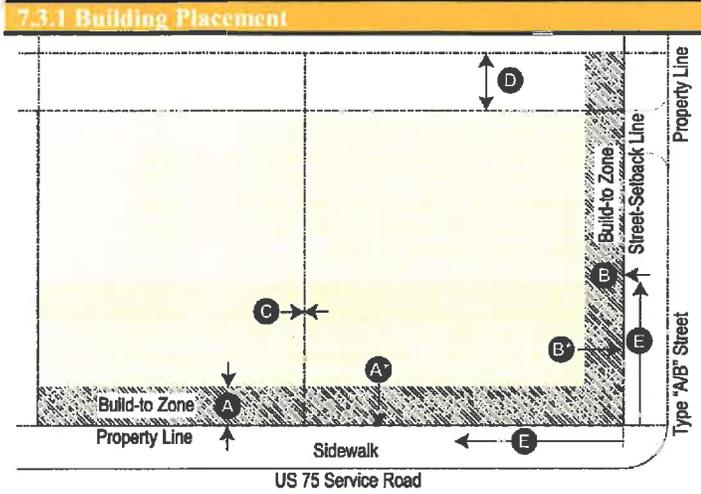
7.3 Arterial Mixed Use Zone

Character Zone: Arterial Mixed Use



TOD Arterial Mixed Use Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all the requirements.



Legend

----- Property Line	----- Setback Line	—— Street-Setback Line
Building Area	▨ Build-to Zone	

Street-Setback Line (SSL)
(Distance from center line of street cross section to edge of the BTZ)

Park Avenue G-2	44 feet
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Build-To Zone (BTZ)
(Distance from Property Line to edge of the BTZ)

Front (U.S. 75 Service Road)	None
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(Distance from SSL to edge of the BTZ)

Front (Civic Space)	0 – 10 feet	A
Front (Type “B” Street)	0 feet (see #1)	B

Setback

Front (U.S. 75 Service Road – distance from property line or edge of utility easement)	15 feet (min.)	
Front (Civic Space – distance from SSL)	0 feet (min.) – 10 feet (max)	A*
Front (Type “B” Street – distance from SSL)	0 feet	B*
Side (distance from property line)	0 feet (see #1)	C
Rear (distance from property line)	0 feet (see #1)	D

Building Frontage

Building Frontage required along all street BTZ	40% (min.) (see #5)	E
Building Frontage required along PGBT and U.S. 75 Service Road	0% (min.) (see #5)	

7.3.2 Block Standards

Block face dimensions	No Maximum
Block perimeter	No Maximum

7.3.3 Building Height

Principal Building Standards

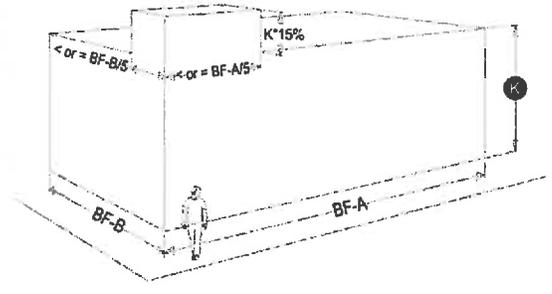
Building maximum	Shall be as established on the Regulating Plan (see #3 and #6)
First floor to floor height	15 feet min. (see #2)
Ground floor finish level	12 inches max. above sidewalk level (for ground floor Retail Ready buildings)
Upper floor(s) height (floor-to-floor)	10 feet min.

7.3.4 Commercial Frontage Requirements

Ground floors of all buildings fronting on Type “A” Streets shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

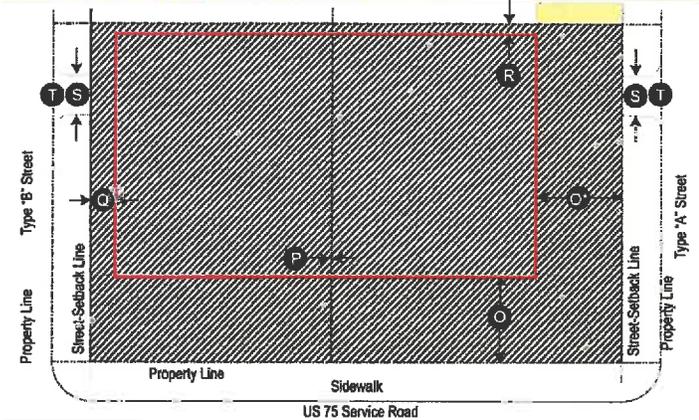
Notes

- #1 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #2 – First floor heights shall not apply to parking structures.
- #3 – Not used.
- #4 – All buildings in the Arterial Mixed Use Zone shall meet the Building Design Standards in Section 8.
- #5 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen higher than the tallest piece of equipment. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen that provides an opaque screen at maturity or a combination of masonry and living screen. Species shall be selected from the C-TOPD Planting List in Appendix B of the C-TOPD Code. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #6 – Corner buildings may exceed the maximum building height by 15% for 20% of the building’s frontage along each corresponding street façade.



- #7 - Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.
- #8 – Setbacks and build-to lines for recessed entry and arcade buildings shall be measured from the building façade line which contains the recessed entry or arcade.
- #9 – Required parking may be provided anywhere within the C-TOPD.
- #10 – Article III, Chapter 21, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas

7.3.5 Parking & Service Access



Legend

Property Line	Building Footprint
Surface Parking Area	Street-Setback Line
Above Grade Parking Area	

(i) Parking Location

Surface/At Grade Parking

U.S. 75 Service Road setback (distance from property line)	Min. 10 feet	
Civic Space setback (distance from SSL)	Min. of 3 feet behind the Building façade line	O
Type "B" Street setback (distance from SSL)	Min. of 3 feet behind the building façade line along that street	Q
Side setback (distance from property line)	0 feet	P
Rear setback (distance from property line)	0 feet	R
Above and Below Grade Parking (Distance from SSL)		
Setback along Type "B" Street, and Civic Space (distance from SSL)	0 feet min.	O
Setback along U.S. 75 Service Road (distance from property line)	0 feet min.	
Side and rear setbacks (distance from property line)	0 feet min.	
Upper Floors	May be built up to the building line	

(ii) Required Off-Street Parking Spaces

Non-residential uses	1 space/200 sq. feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	City standards on Arterial Roadways and 24 feet max on all C-TOPD Streets, except when drives may need to be wider to address service access or fire lane standards.	S
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Driveways and off-street loading and unloading shall not be located on a Type "A" Streets. **T**

Porte cocheres may be permitted on Type "A" Streets to provide drop-off and valet service. Shared driveways and cross access easements are encouraged between lots to minimize curb cuts. If driveway and/or off-street service loading and unloading access is provided from a Type "A" Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.

7.3.6 Encroachments

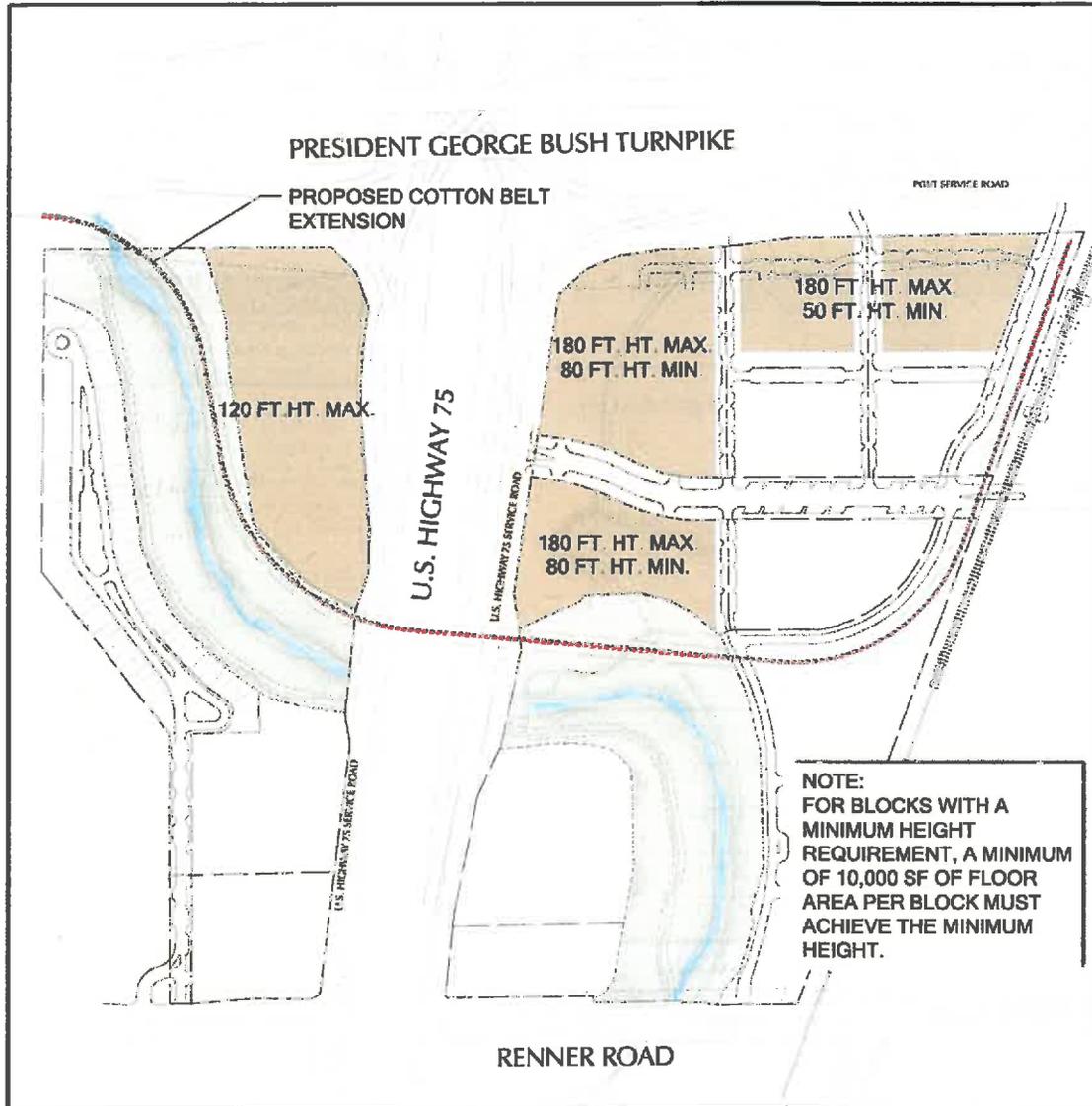
Canopies, signs, awnings and balconies may encroach over the BTZ, setback, and, sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.3.7 Applicability

Building Form and Development Standards in this Section shall apply to all development in this Character Zone.

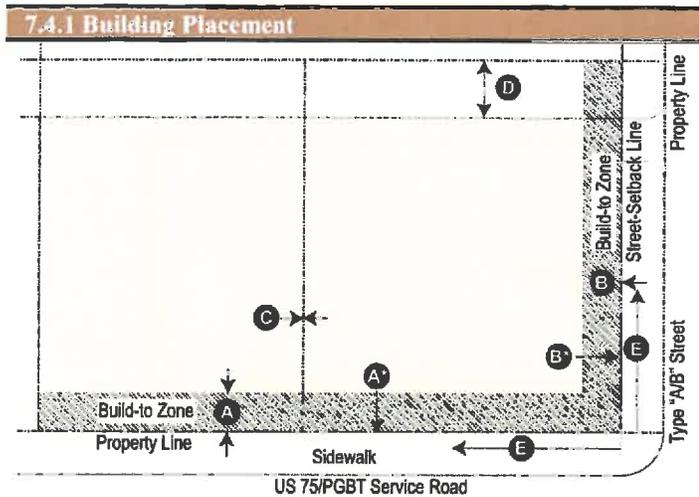
7.4 TOD Freeway Hi-Rise

Character Zone: TOD Freeway Hi-Rise



TOD Freeway Hi-Rise Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements.



Legend

----- Property Line	----- Setback Line	—— Street-Setback Line
▨ Building Area	▨ Build-to Zone	

Street-Setback Zone (SSL)
(Distance from center line of street cross section to edge of the BTZ)

TOD Main Street	61 feet
TOD Street	32 feet min.
TOD Street Promenade	42 feet min.
TOD Station	33.5 feet min.

Build-To Zone (BTZ)
(Distance from Property Line to edge of the BTZ)

Front (PGBT and U.S. 75 Service Road)	None	A
---------------------------------------	------	----------

(Distance from SSL to edge of the BTZ)

Front (Type "A" Street and Civic Space)	0 – 25 feet	B
Front (Type "B" Street)	0 feet	

Setback

Front (PGBT and U.S. 75 Service Road – distance from property line or edge of utility easement)	15 feet (min.)	A*
Front (Type "A/B" Street and Civic Space – distance from SSL)	0 feet	B*
Side (distance from property line)	0 feet (see #1)	C
Rear (distance from property line)	0 feet	D

Building Frontage

Building Frontage required along all street BTZ	90% (min.) (see #2 and #6)	
Building Frontage required along PGBT and U.S. 75 Service Road	0% (min.) (see #2 and #6)	E

7.4.2 Block Standards

Block face dimensions	No Maximum
Block perimeter	No Maximum

7.4.3 Building Height

Principal Building Standards

Building maximum	Shall be as established on the Regulating Plan (see #4 and #7)
First floor to floor height	15 feet min. (see #3)
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)
Upper floor(s) height (floor-to-floor)	10 feet min.

7.4.4 Commercial Frontage Requirements

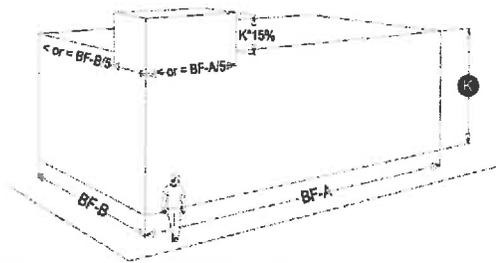
Ground floors of all buildings fronting on Type "A" streets shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

7.4.5 Station Frontage Requirements

Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.

Notes

- #1 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #2 – Corner building street facades shall be built to the BTZ for a minimum of 15' from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to-zone standards.
- #3 – First floor heights shall not apply to parking structures.
- #4 – Not used.
- #5 – All buildings in the TOD Freeway Hi-Rise shall meet the Building Design Standards in Section 8.
- #6 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen higher than the tallest piece of equipment. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen that provides an opaque screen at maturity or a combination of masonry and living screen. Species shall be selected from the C-TOPD Planting List in Appendix B of the C-TOPD Code. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #7 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



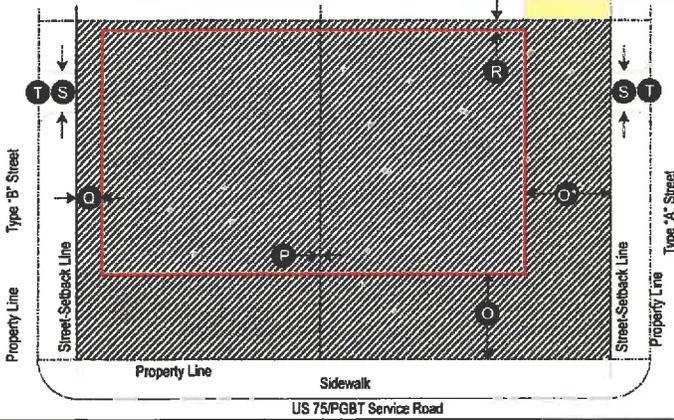
#8 – Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.

#9 – Setbacks and build-to lines for recessed entry and arcade buildings shall be measured from the building façade line which contains the recessed entry or arcade.

#10 – Required parking may be provided anywhere within the C-TOPD.

#11 – Article III, Chapter 21, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-stree parking areas.

7.4.6 Parking & Service Access



Legend

- Property Line
- Surface Parking Area
- Above Grade Parking Area
- Building Footprint
- Street-Setback Line

(i) Parking Location

Surface/At Grade Parking

PGBT Service Road and U.S. 75 setback (see #12)	Shall be located behind the principal building	O
Type "A" Street and Civic Space (see #12)	Shall be located behind the principal building	O*
Type "B" Street setback	Min. of 3 feet behind the building façade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R

Above Grade Parking

Setback along Type "A" Street, Type "B" Street, and Civic Space (distance from SSL)	0 feet min.
PGBT Service Road, U.S. 75 Service Road, side and rear setbacks (distance from property line)	0 feet min.
Upper Floors	May be built up to the building line

(ii) Required Off-Street Parking Spaces

Non-residential uses	1 space/300 sq. feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	TXDOT standards on service road and 24 feet max on all C-TOPD Streets, except when drives may need to be wider to address service access or fire lane standards.	S
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Driveways and off-street loading and unloading shall not be located on PGBT Service Road. **T**
 Porte cocheres may be permitted on Type "A" Streets to provide drop-off and valet service.
 Shared driveways and cross access easements are encouraged between lots to minimize curb cuts.
 If driveway and/or off-street service loading and unloading access is provided from PGBT Service Road, such access shall be deemed as temporary and cross access easements

along the rear of the property shall be required when adjoining properties are undeveloped.

7.4.7 Encroachments

Canopies, signs, awnings and balconies may encroach over the BTZ, setback, and, sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.4.8 Applicability

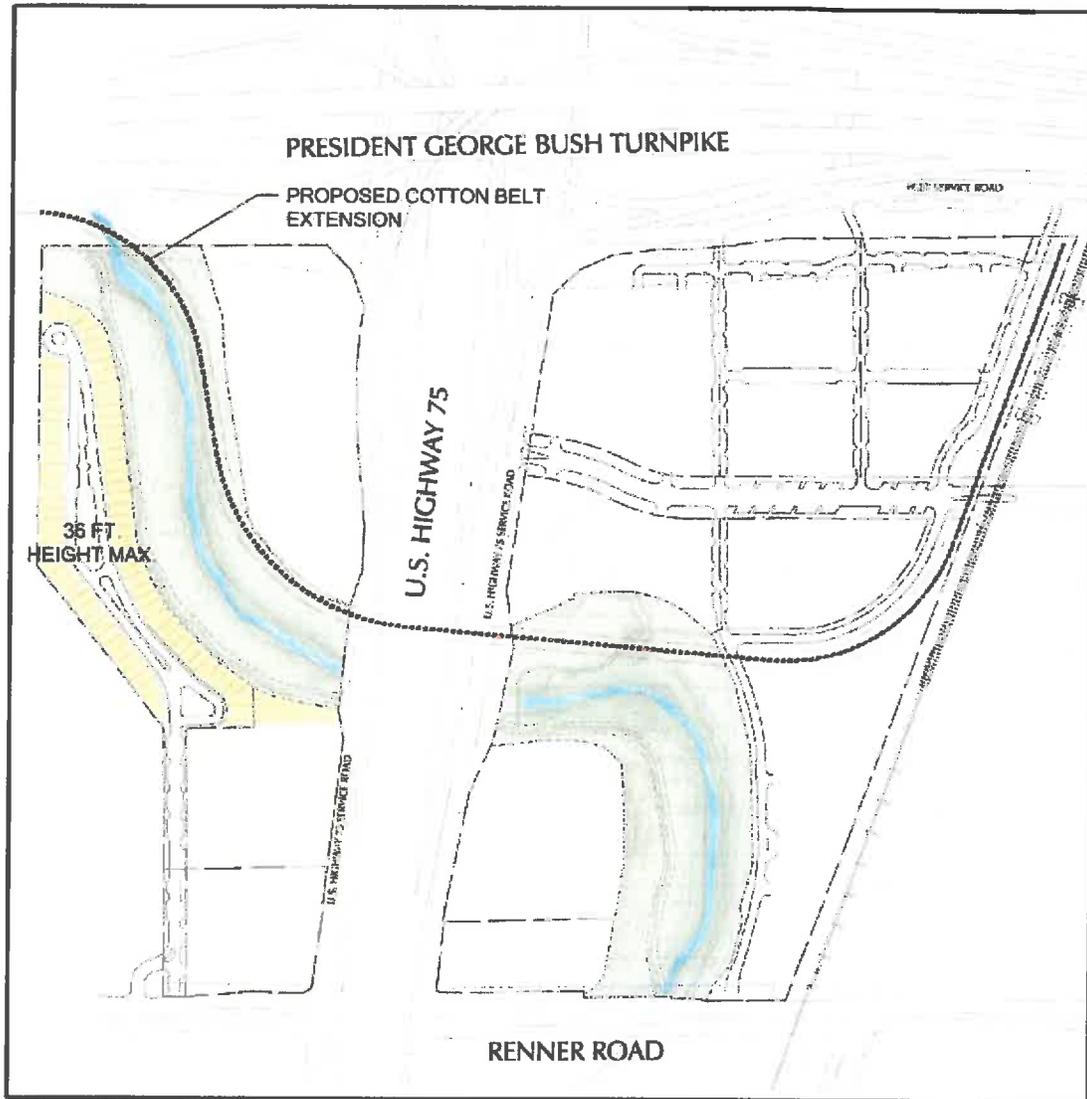
Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

Notes

#12 – Surface parking as an ancillary use with an office motor court shall be permitted with a 20-foot setback.

7.5 Urban Neighborhood Zone

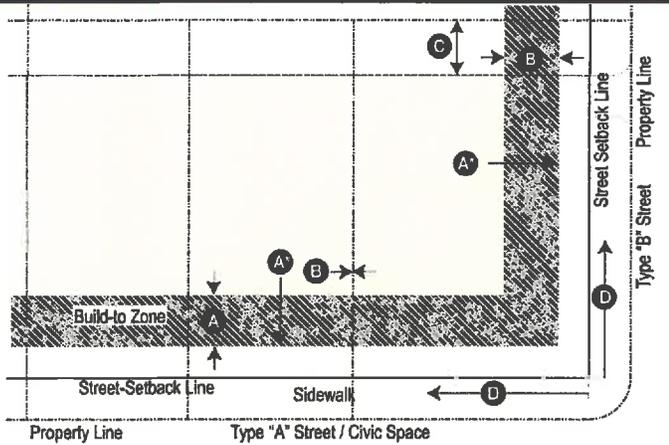
Character Zone: Urban Neighborhood



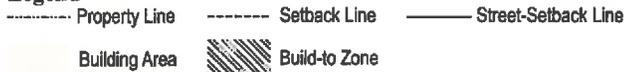
Urban Neighborhood Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements.

7.5.1 Building Placement



Legend



Street-Setback Line (SSL)
(Distance from center line of street cross section to edge of the BTZ)

Park Avenue G-1	26 feet
-----------------	---------

Build-To Zone (BTZ)
(Distance from SSL to edge of the BTZ)

Front (Type Civic Space)	10 – 20 feet	A
Front (Type "B" Street)	10 – 20 feet	B

Setback

Front (distance from SSL)	10 feet	A*
Side (distance from property line)	0 feet (see #1)	B
Rear (distance from property line)	10 feet	C

Building Frontage

Building Frontage required along all streets BTZ	70% (min.) (see #4)	D
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7.5.2 Block Standards

Block face dimensions	No Maximum
Block perimeter	No Maximum

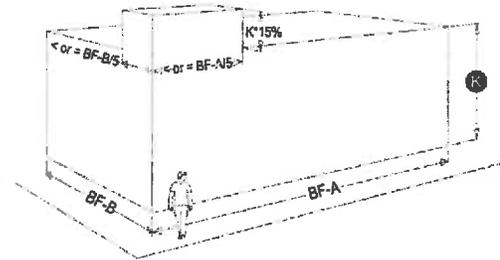
7.5.3 Building Height

Principal Building Standards

Building maximum	Shall be as established on the Regulating Plan (see #2 and #5)
Accessory building maximum	20 feet (see #2)
First floor to floor height	15 feet min. for non-residential 10 feet min for residential
Ground floor finish level	12 in. max. above finished sidewalk (for Retail Ready buildings); 18 in. min. (for residential uses)
Upper floor(s) height (floor-to-floor)	10 feet min.

Notes

- #1 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #2 – Not used.
- #3 – All buildings in the Urban Neighborhood Zone shall meet the Building Design Standards in Section 8.
- #4 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen higher than the tallest piece of equipment. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen that provides an opaque screen at maturity or a combination of masonry and living screen. Species shall be selected from the C-TOPD Planting List in Appendix B of the C-TOPD Code. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #5 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.

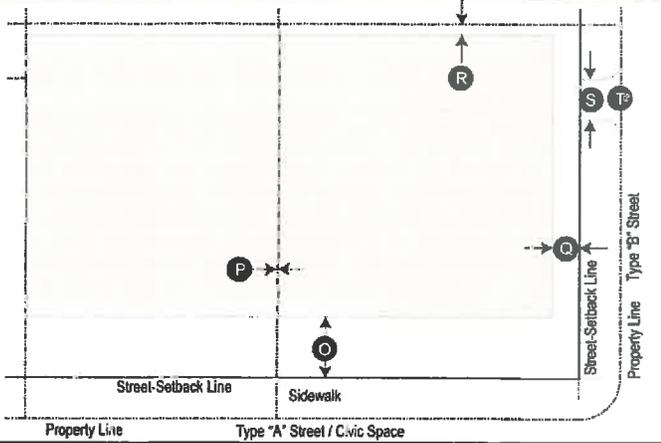


#6 - Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.

#7 – Setbacks and build-to lines for recessed entry and arcade buildings shall be measured from the building façade line which contains the recessed entry or arcade.

#8 – Article III, Chapter 21, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas

7.5.4 Parking & Service Access



Legend

----- Property Line

Parking Area

(iv) Parking Location

Surface/At Grade Parking

Civic Space setback (distance from SSL)	21 feet	O
Type "B" Street setback (distance from SSL)	10 feet	Q
Side setback (distance from property line)	5 feet	P
Rear setback (distance from property line)	5 feet	R

(v) Required Off-Street Parking Spaces

Non-residential uses	1 space/300 sq. feet (gross)
Residential uses	1.5 space/unit

(vi) Driveways and Service Access

Parking driveway width	20 feet max.	S
Shared driveways and cross access easements are encouraged between lots to minimize curb cuts.		T

7.5.5 Encroachments

Porches, stoops, awnings, signs, canopies, balconies, bay windows and other architectural features may encroach into required yards, provided they do not encroach over the front property line.

7.5.6 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

Section 8. Building Design Standards

The Building Design Standards and Guidelines for the C-TOPD shall establish a coherent urban character and encourage enduring and attractive development. Development plans shall be reviewed by the City Manager or designee for compliance with the standards below.

The key design principles establish essential goals for development in the C-TOPD to ensure the preservation, sustainability, and visual quality of this unique environment. Buildings shall be located and designed so that they provide visual interest and create enjoyable, human-scaled spaces. The key design principles are:

- a. New buildings shall utilize building elements and details to achieve a pedestrian-oriented public realm.
- b. Compatibility is not meant to be achieved through uniformity, but through the use of variations in building elements to achieve individual building identity.
- c. Building facades shall include appropriate architectural details and ornament to create variety and interest.
- d. Open space(s) shall be incorporated to provide usable public areas integral to the urban environment.

8.1 General to all Character Zones

8.1.1 Building Orientation

- i. Buildings shall be oriented toward public streets, where the lot has frontage along public streets.
- ii. Primary entrance to buildings shall be located on the street along which the building is oriented. At intersections, corner buildings may have their primary entrances oriented at an angle to the intersection.
- iii. All primary entrances shall be oriented to the public sidewalk for ease of pedestrian access. Secondary and service entrances may be located from internal parking areas or alleys.

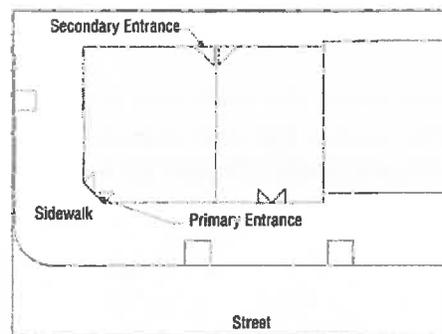


Figure showing required building orientation and location of primary entrances

8.1.2 Design of Parking Structures

- i. All frontages of parking structures located on Type "A" Streets shall not have parking uses on the ground floor to a minimum depth of 30 feet.
- ii. Parking structure facades on all public streets shall be designed with both vertical (façade rhythm of 20 feet to 30 feet) and horizontal (aligning with horizontal elements along the block) articulation.

- iii. Where above ground structured parking is located at the perimeter of a building with frontage along any public street; it shall be screened in such a way that cars on all levels are completely hidden from view.
- iv. When parking structures are located at corners, corner architectural elements shall be incorporated such as corner entrance, signage and glazing.



Images showing appropriate design of Parking Structures

8.1.3 Design of Automobile Related Building Site Elements

- i. Drive-through lanes for commercial uses shall not be located along or visible from any public street within all Character Zones except Arterial Mixed Use and TOD Mixed Use. Drive-through lanes shall be hidden behind a Street Screen within Arterial Mixed Use and TOD Mixed Use.
- ii. All off-street loading, unloading, and trash pick up areas shall be located along alleys or Type 'B' Streets unless permitted in the specific building form and development standards in Section 7. Any off-street loading, unloading, or trash pick up areas shall be screened using a Street Screen that is at least as tall as the trash containers and/or service equipment it is screening at the BTZ. The Street Screen shall be made up of (i) the same material as the principal building or (ii) a living screen or (iii) a combination of the two.

8.2 Standards Specific to the TOD Core, TOD Mixed Use, Arterial Mixed Use and TOD Freeway Hi-Rise Character Zones:

8.2.1 Roof Form

- i. Buildings shall have simple, flat fronts with minimal articulations with flat or low pitched roofs with parapets. Corner hip roof elements and gable accents at the parapet may be permitted. Projecting mansard roofs shall not be permitted.



8.2.2 Façade Composition

- i. Storefronts on facades that span multiple tenants shall use architecturally compatible materials, colors, details, awnings, signage, and lighting fixtures.
- ii. Building entrances may be defined and articulated by architectural elements such as lintels, pediments, pilasters, columns, porticos, porches, overhangs, railings, balustrades, and

others as appropriate. All building elements should be compatible with the architectural style, materials, colors, and details of the building as a whole. Entrances to upper level uses may be defined and integrated into the design of the overall building facade.

- iii. Buildings shall generally maintain the alignment of horizontal elements along the block.
- iv. Corner emphasizing architectural features, pedimented parapets, cornices, awnings, blade signs, arcades, colonnades and balconies may be used along commercial storefronts to add pedestrian interest.



Buildings with architectural features and storefront elements that add interest along the street.

- v. Buildings which are located on axis with a terminating street or at the intersection of streets shall be considered as feature buildings. Such buildings shall be designed with features which take advantage of that location, such as an accentuated entry and a unique building articulation which is off-set from the front wall planes and goes above the main building eave or parapet line.

8.2.3 Windows and Doors

- i. Windows and doors on public street (except alleys) fronting facades shall be designed to be proportional and appropriate to the architectural style of the building. First floor windows shall NOT be opaque, tinted or mirrored glass.
- ii. All ground floor front facades of buildings along public streets or Civic/Open Space shall have transparent storefront windows covering no less than 50% of the façade area.



Images showing appropriate window designs and proportions.

8.2.4 Building Materials

- i. At least 85% of each building's façade (excluding doors and windows) along any public street shall be finished in one of the following materials:
 - Masonry (brick, stone, stucco utilizing a three-step process, cast stone, glass or glass block)
- ii. No more than 15% of each façade along any public street shall use accent materials such as wood, architect metal panel, split-face concrete block, tile, pre-cast concrete panels, or

Exterior Insulating Finishing System (EIFS). EIFS may only be used 8 feet above the ground floor.



Images showing appropriate building materials within TOD Core, TOD Mixed Use, and TOD Freeway Hi-Rise.

- iii. Roofing materials visible from any public right-of-way shall be copper, factory finished standing seam metal, slate, synthetic slate, or similar materials.

8.3 Standards Specific to the Urban Neighborhood Character Zone:

8.3.1 Building Orientation

- i. Garages for Residential Buildings shall be located on alleys at the rear of residential buildings; pull-through garages are allowed if the garage door is set back behind the rear façade of the main structure. If front-loaded garages are utilized for residential uses, the garages shall be no greater than 12 feet wide and set back at least 20 feet measured from the face of the main structure closest to the garage or rotated 90 degrees with windows on the wall facing the street.
- ii. All garage doors shall be divided into single bays separated by at least a 16 in. pier or column.
- iii. Front-loaded garages shall only be permitted on lots that back up to Spring Creek Corridor. Front-loaded garages on residential lots less than 40 feet wide shall not be permitted.

8.3.2 Building Massing and Scale

- i. Commercial and Mixed Use Buildings shall be simple, rectilinear forms with flat or low pitched roofs with parapets.
- ii. Residential buildings shall have few, if any, articulations and simple roofs (gable, hip, combination) with most building wing articulations set at the rear of the structure. Window projections, bay windows, stoops, porches, balconies, and similar extensions shall be exempt from this standard.
- iii. Gable roofs, if provided for residential buildings, shall have a minimum pitch of 5/12. When hipped roofs are used, the minimum pitch shall be 5/12. Other roof types shall be appropriate to the architectural style of the building. Porch roofs may be a minimum pitch of 3/12.
- iv. Projecting mansard roofs shall be prohibited.



Images showing appropriate massing and scale for Residential Buildings

8.3.3 Façade Composition

- i. Buildings shall maintain a façade rhythm of 20 feet to 30 feet along Type 'B' streets. This rhythm may be expressed by changing materials, or color, or by using design elements such as columns and pilasters, or by varying the setback of portions of the building façade.
- ii. Awnings, blade signs, arcades, colonnades, café seating, and balconies should be used along commercial storefronts to add pedestrian interest along all public streets.
- iii. For retail storefronts, a transom, display window area, and bulkhead at the base shall be utilized.
- iv. Buildings shall generally maintain the alignment of horizontal elements along the block.
- v. Porches, stoops, eaves and balconies shall be added along the front residential facades to add pedestrian interest along public streets.
- vi. For residential buildings the grade of the slab or first floor elevation shall be elevated at least 18 inches above the grade of the sidewalk.
- vii. Alley facing facades shall be of finished quality and of the same color that blend with the public facades of the building.



Residential buildings with porches, balconies, and stoops to add interest along the street.

8.3.4 Windows and Doors

- i. Windows and doors shall be designed to be proportional and appropriate to the architectural style of the building.
- ii. Windows may have jack arch, keystone arch, flat arch, or ornamental arches.



Images showing appropriate window designs and proportions.

- iii. All ground floor building facades for commercial and mixed use buildings along public streets shall have transparent storefront windows covering no less than 50% of the façade area. Each upper floor of the same building façades facing a public street or Civic/Open Space shall contain transparent windows covering at least 30% of the façade area.
- iv. All building facades of residential buildings fronting on public streets or civic / open spaces, except alleys, shall have transparent windows covering at least 25% of each façade.

8.3.5 Commercial and Mixed Use Building Materials

- i. The following materials shall NOT be permitted on any façade:
 - Use of lap or shingle siding of any material including wood, vinyl, or painted or corrugated metal, or roofing materials.
- ii. At least 85% of public street facing facades of all new buildings (excluding doors and windows) shall be finished in one or more of the following materials:
 - Masonry (brick, stucco utilizing a three-step process, stone, terra cotta, cast stone, glass block and/or tile).
 - Tilt-up concrete panels that have a grid like appearance
 - Architectural metal panels
- iii. No more than 15% of public street facing facades shall use accent materials such as wood, metal, cementitious-fiber clapboard (not sheet) with at least a 50-year warranty only on upper floors, or Exterior Insulating Finishing System (EIFS). EIFS may only be used 8 feet above the ground floor.
- iv. Side facades and rear facades (that do not front on any streets) shall be of finished quality and of the same color and materials that blend with the front of the building. Building materials for these facades may be any of the primary and accent façade materials listed above. Ground floors of side and rear facades shall not be EIFS.

8.3.6 Residential Building Materials

- i. At least 85% of all public street fronting facades (excluding doors and windows) shall be finished in one or more of the following materials. No more than three different materials shall be used on any single residential façade:
 - Cementitious-fiber clapboard (not sheet) with at least a 50-year warranty.
 - Masonry (brick; stone; cast stone, or stucco utilizing a three-step process).
- ii. The following may only be allowed up to 15% as an accent material:
 - Exterior Insulating Finishing System (EIFS), architectural metal panels or similar material over a cementitious base, rock, glass block and tile. EIFS may only be used 8 feet above the ground floor.
- iii. Rear facades shall be of finished quality and of the same color that blend with the public street facades of the building. Ground floors of all facades shall not be EIFS.
- iv. Roofing materials (visible from any public right-of-way): copper, factory finished painted metal, slate, synthetic slate, terra cotta, and asphalt shingles.
- v. An enclosed garage shall be designed and constructed of the same material as the primary building.

Section 9. Street & Streetscape Design Standards

- 9.1 Generally: Streets in the C-TOPD District need to support the overall goal of a mixed use, compact, pedestrian oriented district. They should balance all forms of mobility while maximizing convenience for residents and visitors.

The Regulating Plan designates the required and recommended street network within the C-TOPD District. This section specifies the typical configuration of streets within the C-TOPD District. The specifications address vehicular lane width, parkway widths, R.O.W widths, number of travel lanes, on-street parking, and pedestrian accommodation. The character of streets in the C-TOPD District will vary based on the location. The service/access roads of President George Bush Turnpike (State Highway 190) and US 75 are under the purview of Texas Department of Transportation (TxDOT) while the remaining streets are city streets.

- 9.2 New Streets: This section specifies standards for all new streets in the C-TOPD District. New streets shall be based on the Mandatory or Non-Mandatory designation on the Regulating Plan.

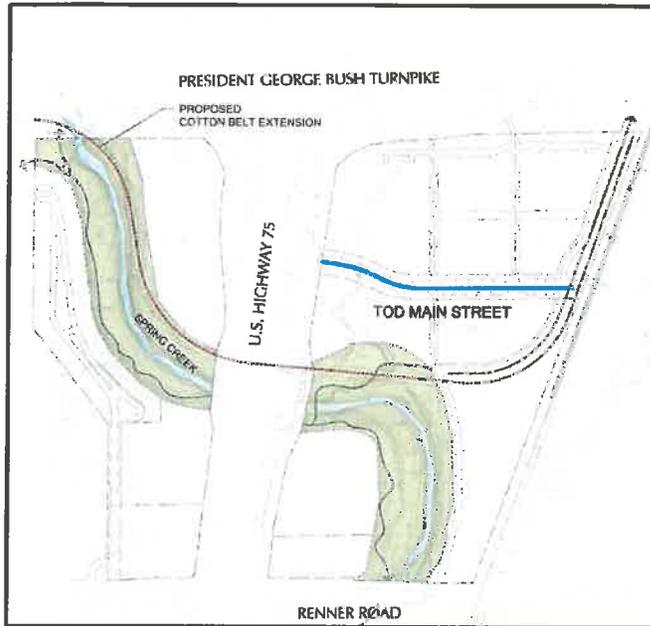
9.3 Street Classifications Established: Table 9.1 and associated cross sections shall establish the cross sections for each street type. The cross sections may be adjusted to fit existing contexts with the approval of the City Engineer. In addition, the proposed cross sections may be adjusted to meet the needs of the Uniform Fire Code as adopted by the City.

Table 9.1

Elements Street Classifications	Street Width (Recommended Minimum)		Number of Vehicular Lanes	Lane Widths for Vehicular Lanes	Number of Bike Lanes	On-Street Parking	Pedestrian Sidewalk Width (min.)	Parkway/ Tree Well
	R.O.W	Pedestrian Easement (both sides)						
TOD Main Street	90 feet	16 feet	4	10.5 feet	2	Yes, both sides, angled	16 feet	Tree well (5 feet X 5 feet)
TOD Street	38 feet	13 feet	2	11 feet		Yes, both sides, parallel	8 feet	Parkway (5 feet) Tree well (5 feet X 5 feet)
Parkway	30 feet	13 feet	2	15 feet		No	8 feet	Parkway (5 feet)
TOD Street Promenade	38 feet	13-23 feet	2	11 feet		Yes, both sides, parallel	8 feet; 10 feet plaza/outdoor dining	Tree well (5 feet X 5 feet) Parkway (5 feet)
TOD Station	Variable	13 feet (west side)	2	10.5 feet		Yes, west side, parallel	13 feet (west)	Tree well (5 feet X 5 feet)
TOD Trail Connector	22 feet	13 feet (north); 14.5 feet (south)	2	11 feet		No	8 feet (north) 14.5 feet (south)	Tree well (5 feet X 5 feet) Parkway (5 feet – north)
Park Avenue (G-1)	Variable	11 feet	2	14 feet		Yes, both sides, parallel	6 feet	Tree well (5 feet X 5 feet) Parkway (5 feet)
Park Avenue (G-2)	57 feet	11 feet	2	15 feet		Yes, both sides, parallel	6 feet	Tree well (5 feet X 5 feet) Parkway (5 feet)

9.4 TOD Main Street

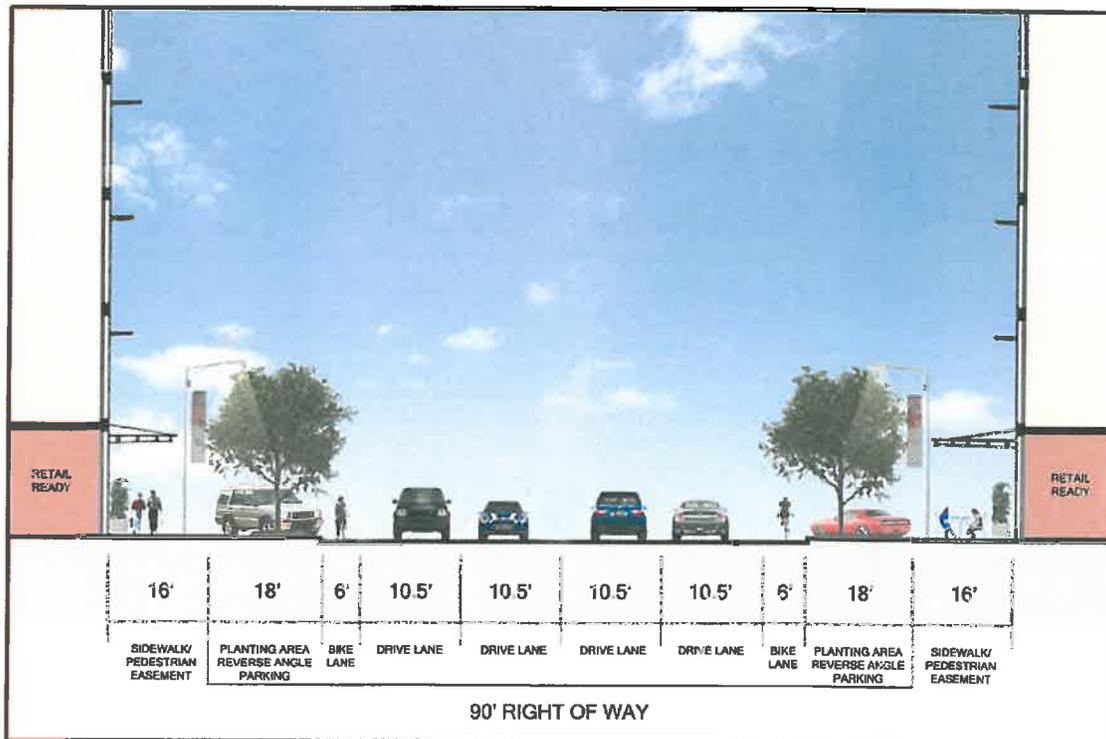
Location Map



TOD Main Street

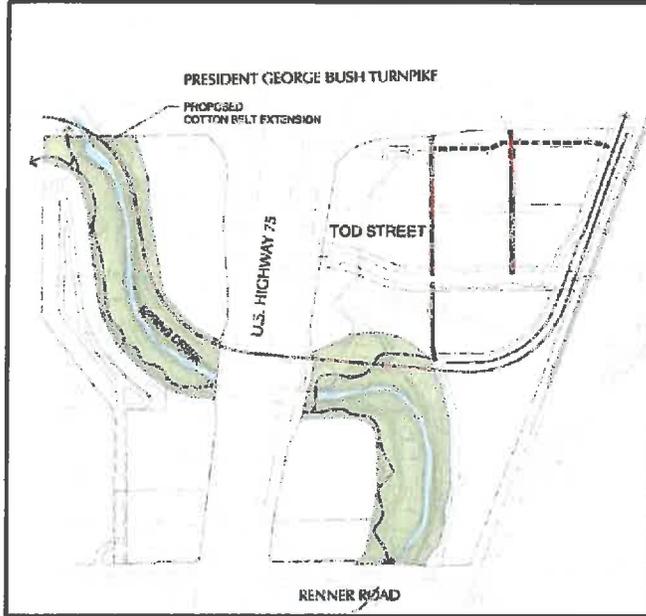
Street Type: A
ROW: 90 ft.
Design Speed: 25 MPH
Parking: Reverse Angle on both sides
Number of Vehicular Lanes: 4
Number of Bike Lanes: 2
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



9.5 TOD Street

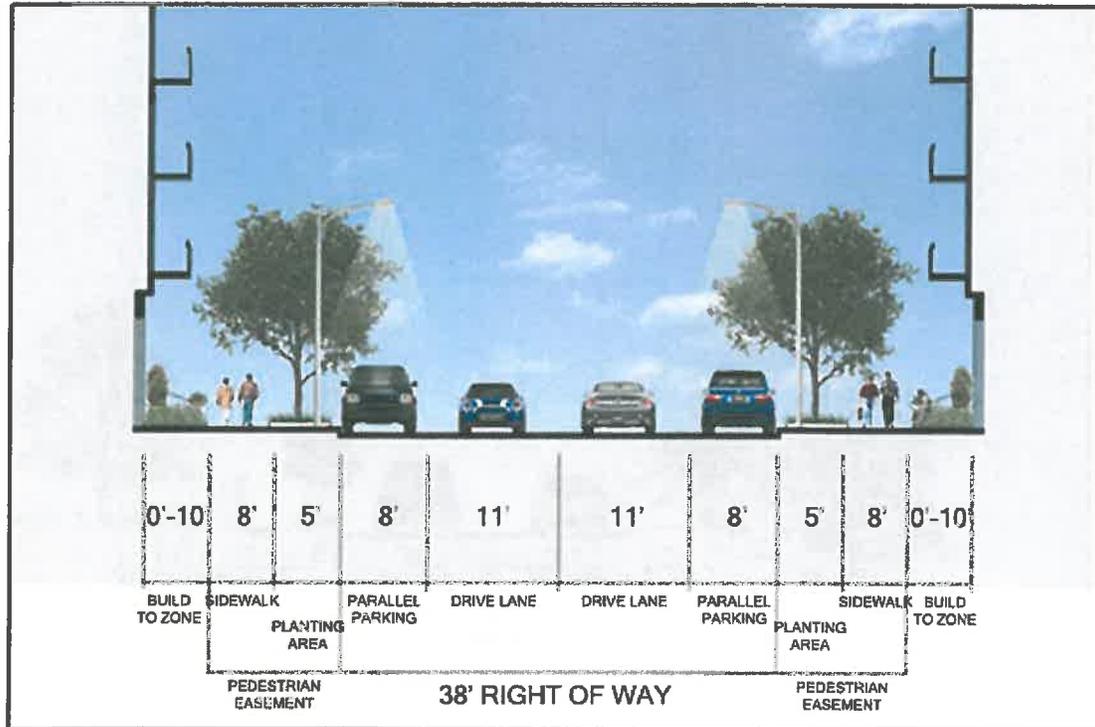
Location Map



TOD Street

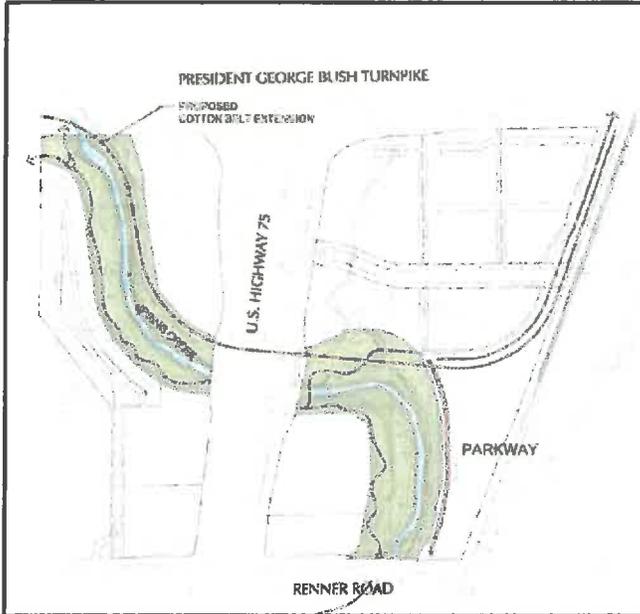
Street Type: B
ROW: 38 ft.
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



9.6 Parkway

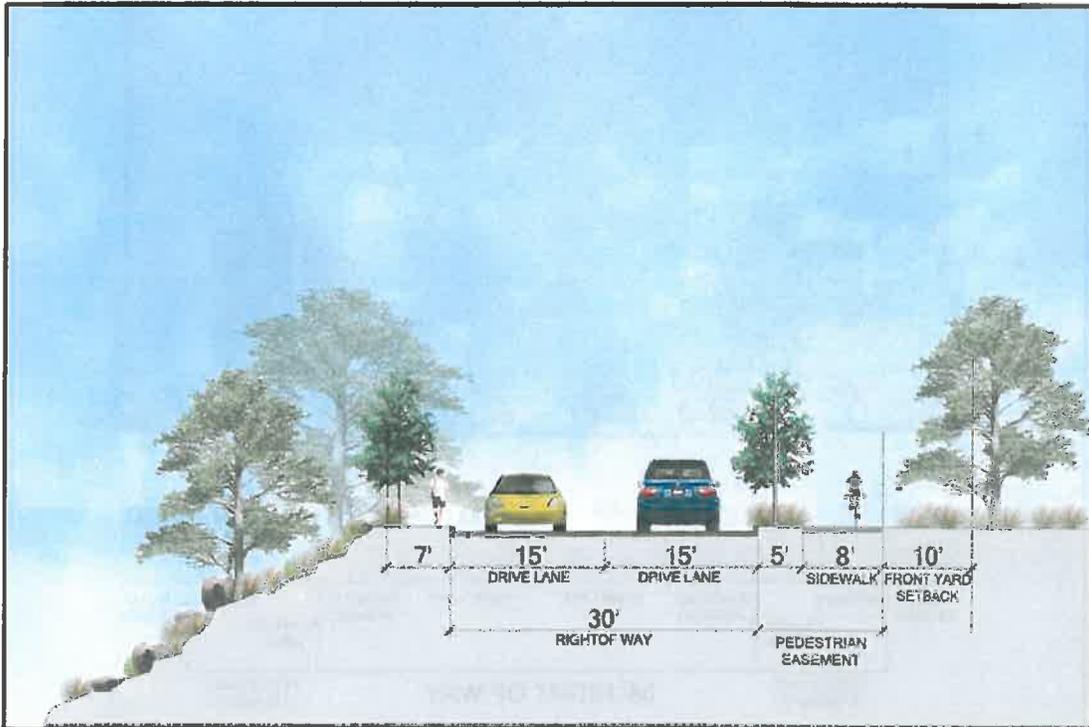
Location Map



Parkway

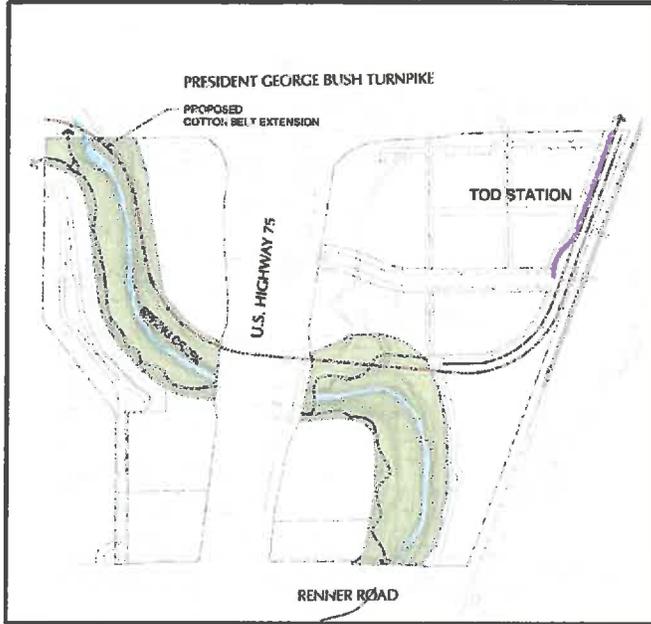
Street Type: B
ROW: 30 ft.
Design Speed: 25 MPH
Parking: None
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



9.8 TOD Station

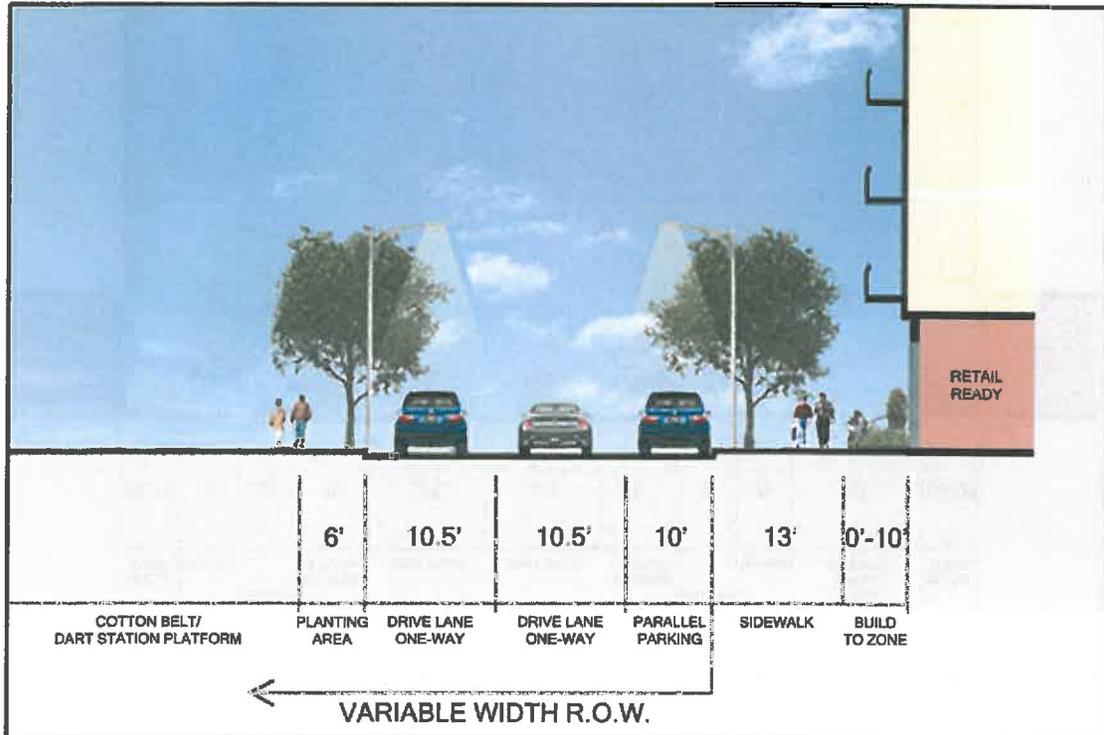
Location Map



TOD Station

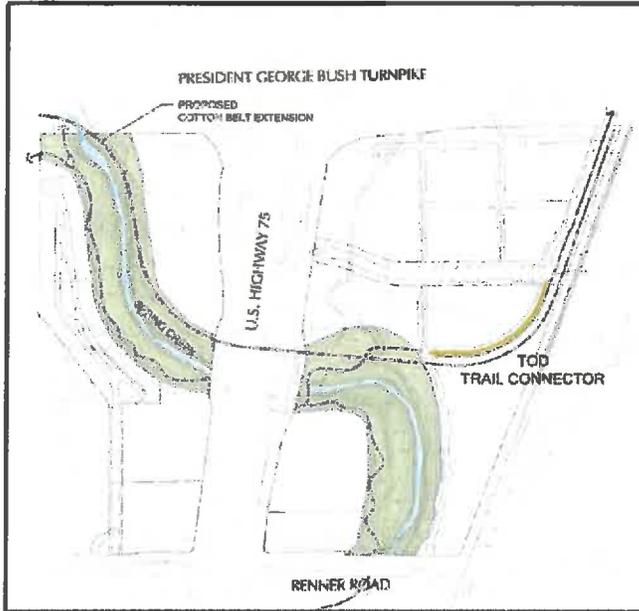
Street Type: A
ROW: Variable
Design Speed: 25 MPH
Parking: Parallel parking on west side
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



9.9 TOD Trail Connector

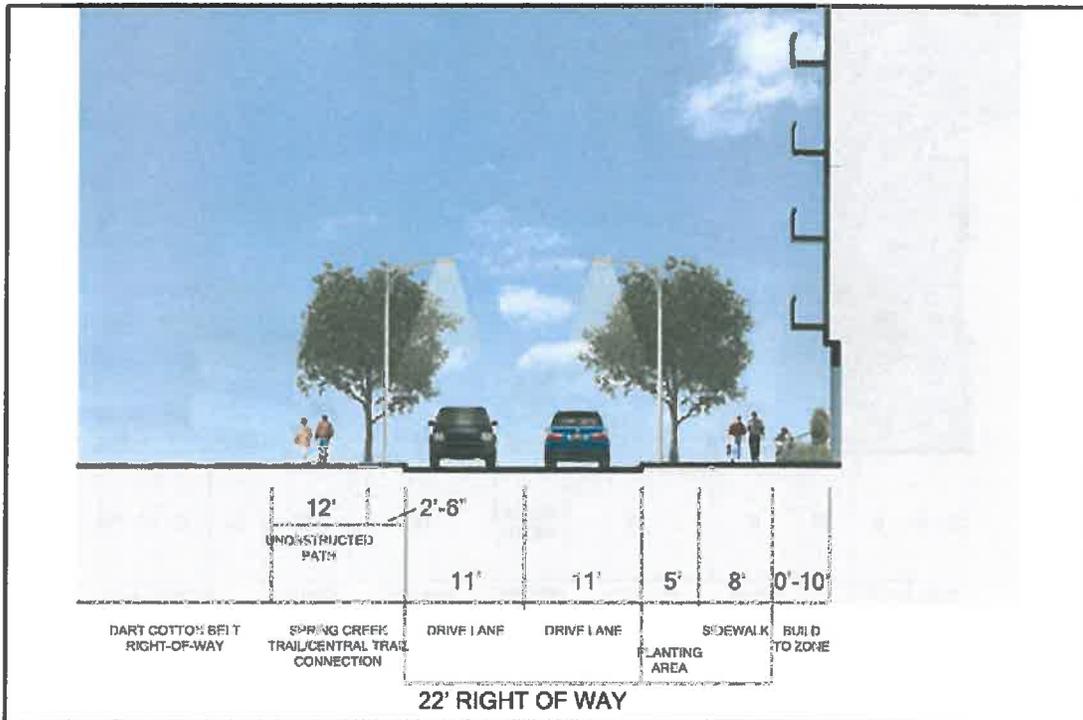
Location Map



TOD Trail Connector

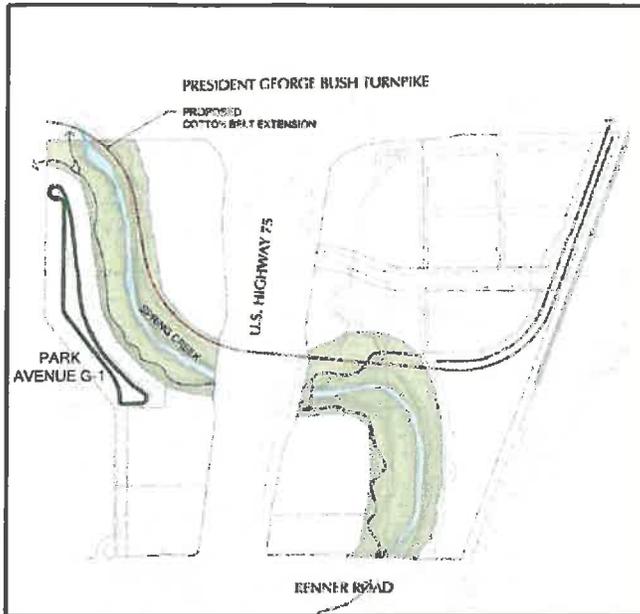
Street Type: B
ROW: 22 ft.
Design Speed: 25 MPH
Parking: None
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



9.10 Park Avenue G-1

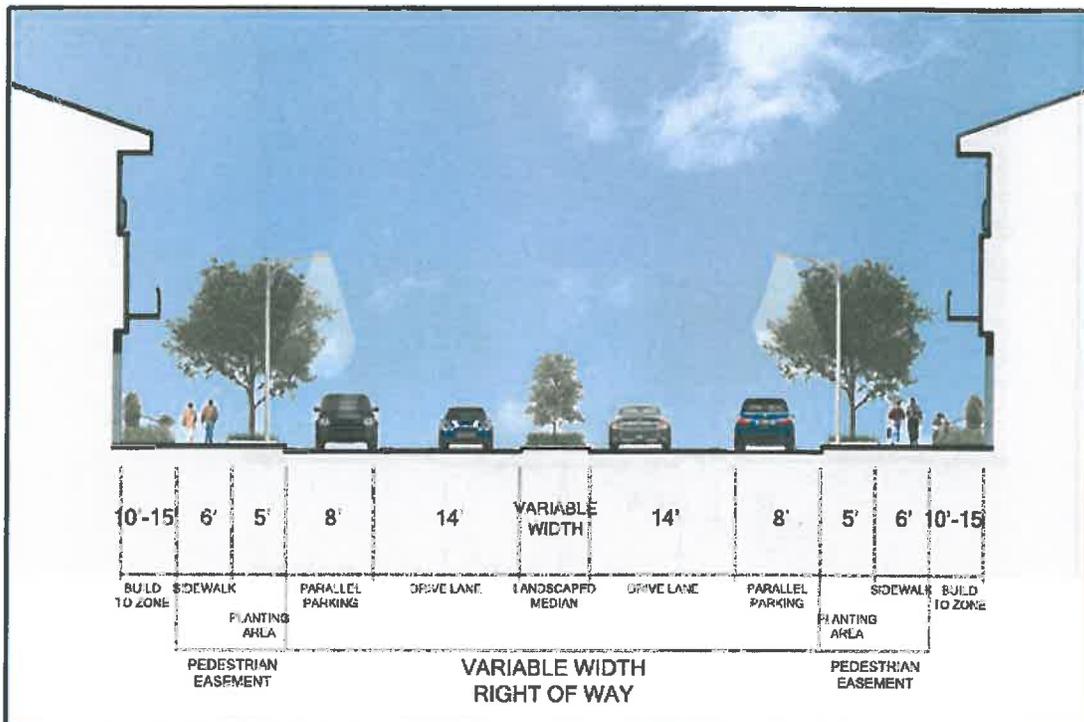
Location Map



Park Avenue G-1

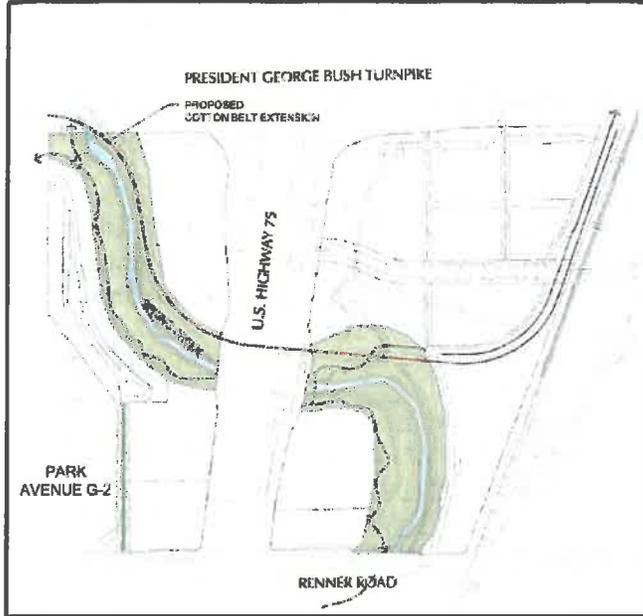
Street Type: B
ROW: Variable
Design Speed: 25 MPH
Parking: Parallel Parking on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



9.11 Park Avenue G-2

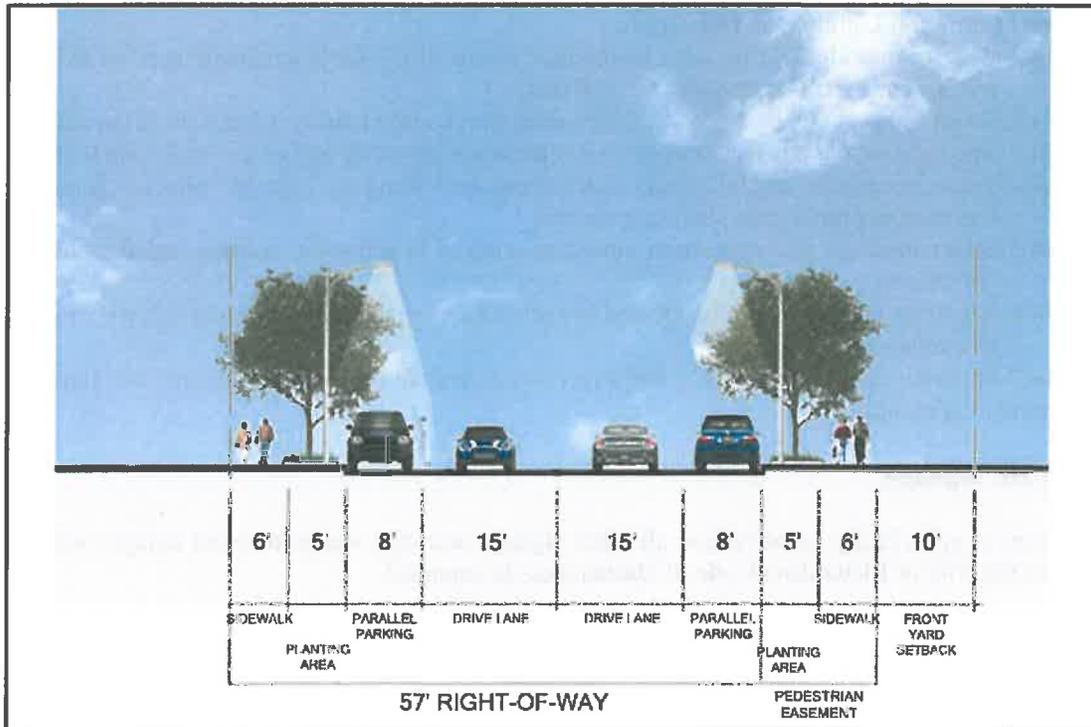
Location Map



Park Avenue G-2

Street Type: B
ROW: 57 ft.
Design Speed: 25 MPH
Parking: Parallel Parking on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



- 9.12 Streetscape & Landscape Standards: Streetscape standards shall apply to all streets within the C-TOPD. Streetscape standards shall address all elements between the building face and edge of the curb. Typical streetscape elements addressed are street trees, lighting, street furniture and pedestrian amenities, and materials. Maintenance of all landscape within the rights-of-way shall be by the property owners association established for the C-TOPD.
- 9.13 Street Trees and Landscaping (within the pedestrian easement):
- 9.13.1 Street trees shall be required on all C-TOPD Streets (except on alleys and PGBT access road.)
 - 9.13.2 Street trees shall be planted approximately 3 feet behind the curb line.
 - 9.13.3 Spacing shall be an average of 40 feet on center (measured per block face) along all streets.
 - 9.13.4 The minimum caliper size for each tree shall be 3 in. and shall be a minimum of 12 feet in height at planting. Each tree shall be planted in a planting area no less than 36 sq. feet; however, the tree well area may be no smaller than 25 sq. feet.
 - 9.13.5 Along the Park Avenue G-1 trees shall be required in the median and spacing and species shall be the same as the trees in the parkway.
 - 9.13.6 Turf and groundcover: When clearly visible from the street and alleys, all unpaved ground areas shall be planted with low growing shrubs or ground cover, ornamental grasses, or a combination thereof. Turf grass must be installed as solid sod and not seeded on.
 - 9.13.7 Species shall be selected from the C-TOPD Planting List in Appendix B of this ordinance.
 - 9.13.8 Maintenance of all landscape materials shall meet the requirements of the City of Richardson Landscape Ordinance Requirements.
 - 9.13.9 Along arterials and highway access roads, street trees shall be planted within the required landscape parkway as per the City of Richardson Landscape Ordinance/Policies.
- 9.14 Street Furniture, Lighting, and Materials:
- 9.14.1 Pedestrian scale lighting shall be required along all C-TOPD streets (except on PGBT access road). They shall be no taller than 20 feet.
 - 9.14.2 Street lights shall be placed at 50 feet on center, approximately 3 feet behind the curb line.
 - 9.14.3 The light standard selected shall be compatible with the design of the street and buildings.
 - 9.14.4 Trash receptacles and bike racks shall be required along all Type 'A' Streets. A minimum of one each per block face shall be required.
 - 9.14.5 Street furniture and pedestrian amenities such as benches are recommended along all Type 'A' Streets.
 - 9.14.6 All street furniture shall be located in such a manner as to allow a clear sidewalk passageway of a minimum of 6 feet.
 - 9.14.7 Materials selected for paving and street furniture shall be of durable quality and require minimal maintenance.

Section 10. Signage

Except as specifically listed below, all other signage and sign standards must comply with Chapter 18 of the City of Richardson Code of Ordinances, as amended.

- 10.1 For new signs, the standards in Table 10.1 shall apply and sign permits shall be approved administratively by the City of Richardson Building Official unless specifically noted in this section.

Table 10.1

Character Zone	TOD Core	TOD MU	Arterial MU	TOD Freeway	Urban Neighborhood	Standard
Sign Type						
Wall (Building) Signs	P	P	P	P	P (comm. uses only)	<ul style="list-style-type: none"> For all ground floor commercial uses (retail, office, and restaurant): One sign per tenant space; area to be calculated at 1.5 sq. feet per linear foot of public street frontage for the tenant space with a maximum of 100 sq. ft per tenant. Second and upper floor commercial uses may also be permitted one second floor wall sign per tenant space per public street frontage; area to be calculated at 1.5 sq. feet per linear foot of second or upper floor frontage along that public street with a maximum of 125 sq. feet Institutional uses (non-profits and churches): One sign per tenant space; area to be calculated at 1.5 sq. feet per linear foot of public street frontage with a maximum of 100 sq. feet Live-Work and Home occupations: One sign limited to an area of 20 sq. feet max. Building sign may encroach a maximum of 12" on to a sidewalk while maintaining a vertical clearance of 8 feet from the finished sidewalk. Building signs may be internally or externally lit. Marquee signs as only permitted as specified below.
Monument Signs	NP	NP	P	P	NP	<ul style="list-style-type: none"> One monument sign per lot per lot street frontage (no more than 2 per lot separated by at least 100 feet) limited to a maximum of 50 sq. feet per sign face and 6 feet in height. Permitted only along PGBT access road, U.S 75 access road, and streets along TOD Freeway Hi-Rise.
Window Signs	P	P	P	P	P (comm. uses only)	<ul style="list-style-type: none"> Limited to 10% of the window area. In the Urban Neighborhood Zone, window signs are only permitted for commercial uses (including the "work" component of live-work uses). <p>The following shall be exempt from this limitation:</p> <ul style="list-style-type: none"> Addresses, closed/open signs, hours of operation, credit card logos, real estate signs, and now hiring signs. Mannequins and storefront displays of merchandise sold. Interior directory signage identifying shopping aisles and merchandise display areas.
Building Blade Signs	P	P	P	P	P (comm. uses only)	<ul style="list-style-type: none"> One per building (commercial and mixed use buildings only), always on separate building face Area = 30 sq. feet maximum per sign face. May encroach a maximum of 6 feet over a sidewalk, but shall not encroach over any parking or travel lane. Building blade signs may be attached to the building at the corners of building or

Character Zone	TOD Core	TOD MU	Arterial MU	TOD Freeway	Urban Neighborhood	Standard
						along any street facing façade above the first floor facade.
Tenant Blade Signs	P	P	P	P	P (comm. uses only)	<ul style="list-style-type: none"> One per commercial tenant space (retail, office, or restaurant use) Area = 16 sq.feet maximum per sign face May encroach a maximum of 4 feet over a public sidewalk, but shall not encroach over any parking or travel lane. Tenant blade signs shall be oriented perpendicular to the building façade and hung under the soffit of an arcade or under a canopy/awning or attached to the building façade immediately over the ground floor tenant space while maintaining a vertical clearance of 8 feet from the finished sidewalk.
Marquee Signs	P	P	P	P	NP	<ul style="list-style-type: none"> Permitted for theatres, auditoriums, and other public gathering venues of 100 persons or more Marquee signs shall be attached to the building or located above or below a canopy only Area = 100 sq.feet maximum Message board may be changeable copy (electronic and non-electronic). Electronic message boards shall be non-flashing. Marquee signs shall not be permitted along Renner Road, PGBT access road and U.S. 75 access road.
For sale/for lease signs	P	P	P	P	P	<ul style="list-style-type: none"> Size is limited to 32 sq. feet per sign face All other standards are the same as City or Richardson Sign Regulations.
Address signs	P	P	P	P	P	Same as City of Richardson Sign Regulations
Temporary construction signs	P	P	P	P	P	One (1) free standing sign per lot during construction only; limited to 32 sq. feet
Banners	P	P	P	P	P	Same as City of Richardson Sign Regulations
Sandwich board signs	P	P	P	P	P	<ul style="list-style-type: none"> Permitted only for retail, service, or restaurant uses Limited to 12 sq. feet per sign face per storefront; Sign may not exceed 4 feet in height. A minimum of 6 feet of sidewalk shall remain clear. Chalkboards may be used for daily changing of messages. Readerboards (electronic and non-electronic) shall be prohibited. Sign shall be removed every day after the business is closed.

Character Zone	TOD Core	TOD MU	Arterial MU	TOD Freeway	Urban Neighborhood	Standard
Light Pole Banners	P	P	P	P	NP	<ul style="list-style-type: none"> Permitted only with approval of the Building Official. Max. 10 sq. feet per sign face. Limited to one per light pole All light pole banners shall be approved by the appropriate utility company prior to consideration by the Sign Control Board. Light pole banners shall be limited to publicize community-wide events, holiday celebrations, public art, and other city sponsored events.
Directory signs	P	P	P	P	P	<ul style="list-style-type: none"> Shall be allowed for all multi-tenant commercial and mixed use buildings only One directory sign per multi-tenant building limited to 12 sq. feet in area Design of the sign shall be integral to the façade on which the sign is to be affixed.
LED signs	P	P	P	P	P	<ul style="list-style-type: none"> Shall be covered by a lens or diffuser. Shall only be permitted as part of a Master Sign Plan.
Pole signs	NP	NP	NP	NP	NP	
Off-premises signs	NP	NP	NP	NP	NP	

10.2 An applicant has the option to establish unique sign standards including size, color, type, design, and location. Such applications shall be reviewed as “Master Sign Plans” by the City of Richardson Building Official and are subject to approval of the Sign Control Board. In evaluating a Master Sign Plan, the Sign Control Board shall consider the extent to which the application meets the proposed Sign Plan:

- 10.2.1 Promotes consistency among signs within a development thus creating visual harmony between signs, buildings, and other components of the property;
- 10.2.2 Enhances the compatibility of signs with the architectural and site design features within a development;
- 10.2.3 Encourages signage that is in character with planned and existing uses thus creating a unique sense of place; and
- 10.2.4 Encourages multi-tenant commercial uses to develop a unique set of sign regulations in conjunction with development standards.

Section 11. Civic/Open Space Standards

11.1 The design of Civic/Open Space shall be regulated by the Civic/Open Space standards herein which shall create a network of open spaces that recognizes the natural qualities of the area while providing a range of both passive and active recreational opportunities. These opportunities may be accommodated in a variety of spaces ranging from large regional parks to neighborhood-scaled plazas. The open space network will be serviced by an interconnected network of trails and paths for pedestrians and bicyclists alike.

11.2 Open Space Standards – Spring Creek Corridor



The required open space, Spring Creek Corridor, as designated on the Regulating Plan, will create an important public space that connects the community within the C-TOPD and allows for active and passive recreation. Spring Creek Corridor shall primarily be naturally landscaped with many places to sit on benches or low walls. Appropriate civic elements, fountains or open shelters may be included. TOD Freeway Hi-Rise, Urban Neighborhood and Arterial Mixed Use buildings shall front onto and activate this space.

Typical Characteristics

General Character

Large, open space

Spatially defined by landscaping and building frontages

Paths, trails, open shelters, lawns, trees and shrubs naturally disposed

May be lineal, following the trajectories of natural corridors

Location and Size

Location and size shall be as shown on the Regulating Plan (Appendix A).

Typical Uses

Passive, and unstructured active recreation

Casual seating/picnicking

11.3 Plaza Standards



Plazas add to the vibrancy of streets within the more urban zones and create formal open spaces available for civic purposes and commercial activity. Building frontages shall define these spaces. The landscape should consist primarily of hardscape. If trees are included, they should be formally arranged and of appropriate scale. Casual seating, along with tables and chairs, should be provided. Plazas typically should be located at the intersection of important streets.

Typical Characteristics

General Character

Formal open space

Primarily hardscape surfaces

Trees and shrubs optional

Spatially defined by building frontages

Location and Size

Location and size shall be as shown on the Regulating Plan (Appendix A).

Typical Uses

Commercial and civic uses

Casual seating

Tables and chairs for outdoor dining

11.4 Pedestrian Passage Standards



Pedestrian passages create intimate passageways through buildings at designated locations. These paths provide direct pedestrian access to residential addresses and create unique spaces for frontages to engage and enter off of. Pedestrian passages allow for social and commercial activity to spill into the public realm. Pedestrian passages should consist of a hardscape pathway activated by frequent entries and exterior stairways. The edges may simply be landscaped with minimal planting and potted plants.

Typical Characteristics

General Character

- Hardscape pathway
- Frequent entries and frontages
- Exterior stairways
- Defined by building frontages
- Minimal planting and potted plants
- Maintain the character of surrounding buildings

Standards

Min. Width 12 feet

Typical Uses

- Pedestrian connection and access
- Casual seating

11.5 Multi-Use Trail Standards



The multi-use trail provides an important place for active recreation and creates a connection to regional paths and biking trails. The multi-use trail will help activate connections between the open spaces and the uses throughout the C-TOPD. The multi-use trail may have different character as it passes through Spring Creek Corridor and as it aligns with public streets. Within Spring Creek Corridor, the trail shall be naturally disposed with low impact paving materials so there is minimal impact to the existing creek bed and landscape. Along public streets, the trail shall have a more formal disposition with a paved trail and regular landscaping.

Typical Characteristics

General Character

Spring Creek Corridor Multi-Use Trail:

Naturally disposed landscape

Low impact paving

Trees lining trail for shade

Appropriately lit for safety

Public Street Multi-Use Trail:

Hardscape Path

Formally disposed pedestrian furniture, landscaping and lighting

Trees lining trail for shade

Standards

Min. Width 12 feet

Location shown on the Regulating Plan (Appendix A).

Typical Uses

Active and passive recreation

Casual seating

11.6 Playground Standards



Playgrounds shall be permitted in parks to provide open space designed and equipped for the recreation of children. These playgrounds should serve as quiet, safe places – protected from the street and typically located where children are not required to cross major roads to access. Playgrounds may be fenced. An open shelter, play structures or interactive art and fountains may be included with landscaping between. Shaded areas and seating shall be provided.

A large playground may be incorporated into the park.

Playground equipment shall serve all ages, based on City of Richardson Parks and Recreation Standards, as amended. Playground equipment and design shall be reviewed and approved by the City of Richardson, including the Parks and Recreation Department.

Typical Characteristics	
General Character	
Focused toward children	
Fenced with minimal exits (non-mandatory)	
Open shelter	
Shade and seating provided	
Play structure, interactive art or fountains	
Standards	
Min. Size	N/A
Max. Size	N/A
As described by civic space type in which playground is located	
Protected from traffic	
No service or mechanical equipment	
Typical Uses	
Active and passive recreation	
Unstructured recreation	
Casual seating	

11.7 Ancillary Structure Standards



Ancillary structures should be formal in character and generally related to but clearly subordinate to surrounding buildings. Each individual structure should keep in character with the style of nearby buildings. Typically, these structures are located at prominent locations within an appropriate civic space. Ancillary structures located in more urban zones may have minor commercial uses, such as small food or news vendors, but may also serve as civic elements for general public use with more passive activities. Other ancillary structures located within the Urban Neighborhood Zone should be more modest in use and character, ranging from a simple, public pavilion or pergola to a neighborhood kiosk or mail pavilion.

Typical Characteristics

General Character

- Formal character
- Relating to style of surrounding buildings
- One or more open sides
- Covered or providing shade
- Small, stand alone structure
- Located within Park, Green, Square or Plaza

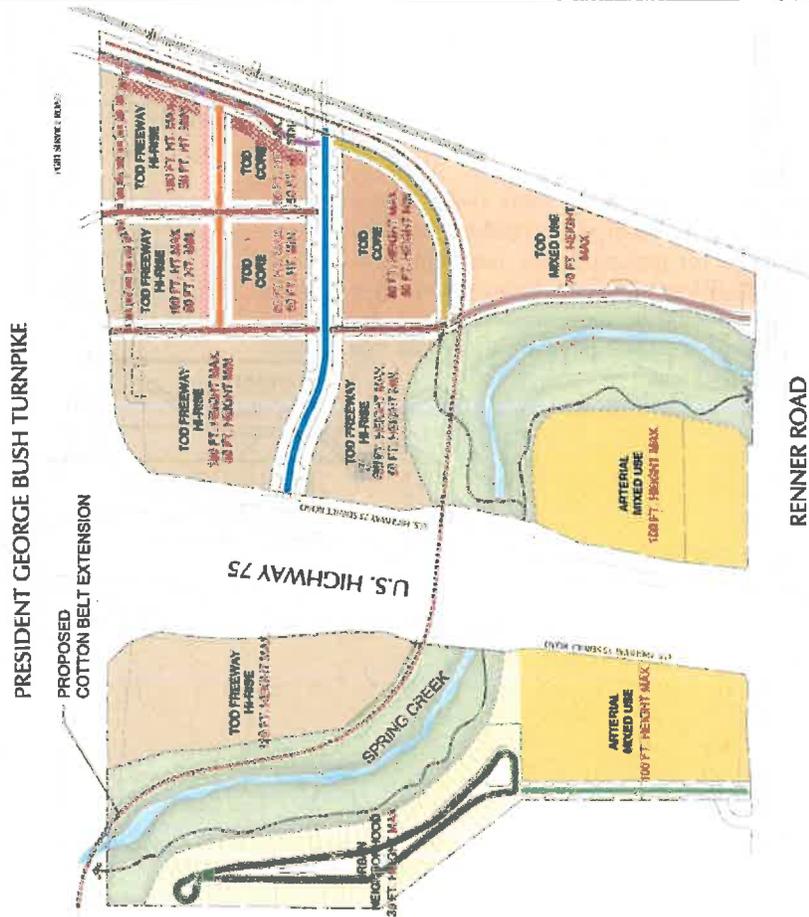
Standards

- Min. Size N/A
- Max. Size N/A

Typical Uses

- Civic purposes
- Minor commercial uses
- Casual seating/picnicking

LEGEND	
CHARACTER ZONES	STREETS - GENERAL
<p>TOD Core - 9 Acres</p> <p>TOD Mixed Use - 5.5 Acres</p> <p>TOD Freeway H-Rise - 21.2 Acres</p> <p>Arterial Mixed Use - 13.1 Acres</p> <p>Urban Neighborhood - 7.0 Acres</p> <p>Note: Acreage's are approximate.</p>	<p>Solid line denotes mandatory street</p> <p>Dashed line denotes non-mandatory street</p>
SPECIAL REQUIREMENTS	STREET TYPES
<p>Station Platform Frontage</p> <p>NOTE: For blocks with a minimum height requirement, a minimum of 10,000 of floor area per block must achieve the minimum height.</p>	<p>Street Type 'A' - TOD Main Street</p> <p>Street Type 'A' - TOD Street Promenade</p> <p>Street Type 'A' - TOD Station</p> <p>Street Type 'B' - Parkway</p> <p>Street Type 'B' - TOD Trail Connector</p> <p>Street Type 'B' - Park Avenue G-1</p> <p>Street Type 'B' - Park Avenue G-2</p> <p>Street Type 'B' - TOD Street</p>
	OTHER
	Multi-Use Trail
	OPEN SPACE TYPES
	<p>Non-Mandatory Plaza</p> <p>Mandatory Open Space</p>



APPENDIX A: REGULATING PLAN

Caruth Properties
Transit Oriented Planned Development



GFF Planning

Appendix B Planting List

The following lists contain all species approved for use in the C-TOPD District. It contains native and acceptable adapted species. Other species that are drought tolerant and adaptive may be used for planting within the C-TOPD District. The use of alternative species may be permitted with the approval of the Zoning Administrator.

CANOPY/STREET TREE LIST

<u>Common Name</u>	<u>Botanical Name</u>
Live Oak	<i>Quercus virginiana</i>
Red Oak	<i>Quercus shumardi</i>
Bald Cypress	<i>Taxodium distichum</i>
Sweetgum	<i>Liquidambar styraciflua</i>
Cedar Elm	<i>Ulmus crassifolia</i>
Lacebark Elm	<i>Ulmus parvifolia</i>
Bigtooth Maple	<i>Acer grandidentatum</i>
Caddo Maple	<i>Acer saccharum 'Caddo'</i>
Texas Ash	<i>Fraxinus texensis</i>
Bur Oak	<i>Quercus macrocarpa</i>
Chinquapin Oak	<i>Quercus muhlenbergii</i>
Escarpment Live Oak	<i>Quercus fusiformis</i>
Ginkgo	<i>Ginkgo biloba</i>

ORNAMENTAL TREE LIST

<u>Common Name</u>	<u>Botanical Name</u>
Yaupon Holly	<i>Ilex vomatoria</i>
Crape Myrtle	<i>Lagerstromia indica</i>
Deciduous Yaupon	<i>Ilex decidua</i>
Southern Crabapple	<i>Malus app.</i>
Chinese Pistache	<i>Pistacia chinensis</i>
Mexican Plum	<i>Prunus Mexicana</i>
Wax Myrtle	<i>Myrica carifera</i>
Chitalpa	<i>Chitalpa tashkentensis</i>
Deciduous Holly	<i>Ilex decidua</i>
Desert Willow	<i>Chilopsis linearis</i>
Eve's Necklace	<i>Sophora affinis</i>

SHRUBS LIST

<u>Common Name</u>	<u>Botanical Name</u>
Dwarf Nandina	<i>Nandina domestica 'nana'</i>
Dwarf Burford Holly	<i>Ilex cornuta 'burfordi nana'</i>
Abelia Grandiflora	<i>Abelia grandiflora</i>
Barberry	<i>Barberry spp.</i>
Yucca (Red, Yellow or Soft Tip)	<i>Hesperaloe parviflora</i>
Texas Sage	<i>Leucophyllum frutescans</i>
Indian Hawthorn	<i>Raphiolepis indica</i>
Dwarf Crape Myrtle	<i>Lagerstromia indica 'nana'</i>

Dwarf Yaupon Holly	<i>Ilex vomitoria 'nana'</i>
Pampas Grass	<i>Cortaderia selloana</i>
Black-Eyed Susan	<i>Rudbeckia hirta</i>
Dwarf Wax Myrtle	<i>Myrica pusilla</i>
Needlepoint Holly	<i>Ilex cornuta 'Needle Point'</i>
Knockout Rose	<i>Rosa 'Knock Out'</i>
Rosemary	<i>Rosmarinus officinalis</i>

GROUND COVER/VINES LIST

<u>Common Name</u>	<u>Botanical Name</u>
Asian Jasmine	<i>Trachelosperum Asiaticum</i>
Big Blue Liriope	<i>Liriope muscari</i>
Mondograss	<i>Ophiopogon japonicus</i>
Purple Winter Creeper	<i>Euonymum coloratus</i>
Santolina	<i>Santolina virens</i>
Trumpet Vine	<i>Campsis radicans</i>
Virginia Creeper	<i>Parthenocissus quinquefolia</i>
Lady Banks Rose	<i>Rosa banksiaw lutea</i>
Confederate Jasmine	<i>Trachelospermum jasminoides</i>
Crossvine	<i>Bignonia capreolata</i>
Evergreen Wisteria	<i>Millettia reticulata</i>
Lantana 'New Gold'	<i>Lantana camara 'New Gold'</i>
Liriope 'Silver Dragon'	<i>Liriope muscari 'Silver Dragon'</i>
Prostrate Rosemary	<i>Rosmarinus officinalis prostrata</i>
Sweet Autumn Clematis	<i>Clematis terniflora</i>

ORNAMENTAL GRASSES LIST

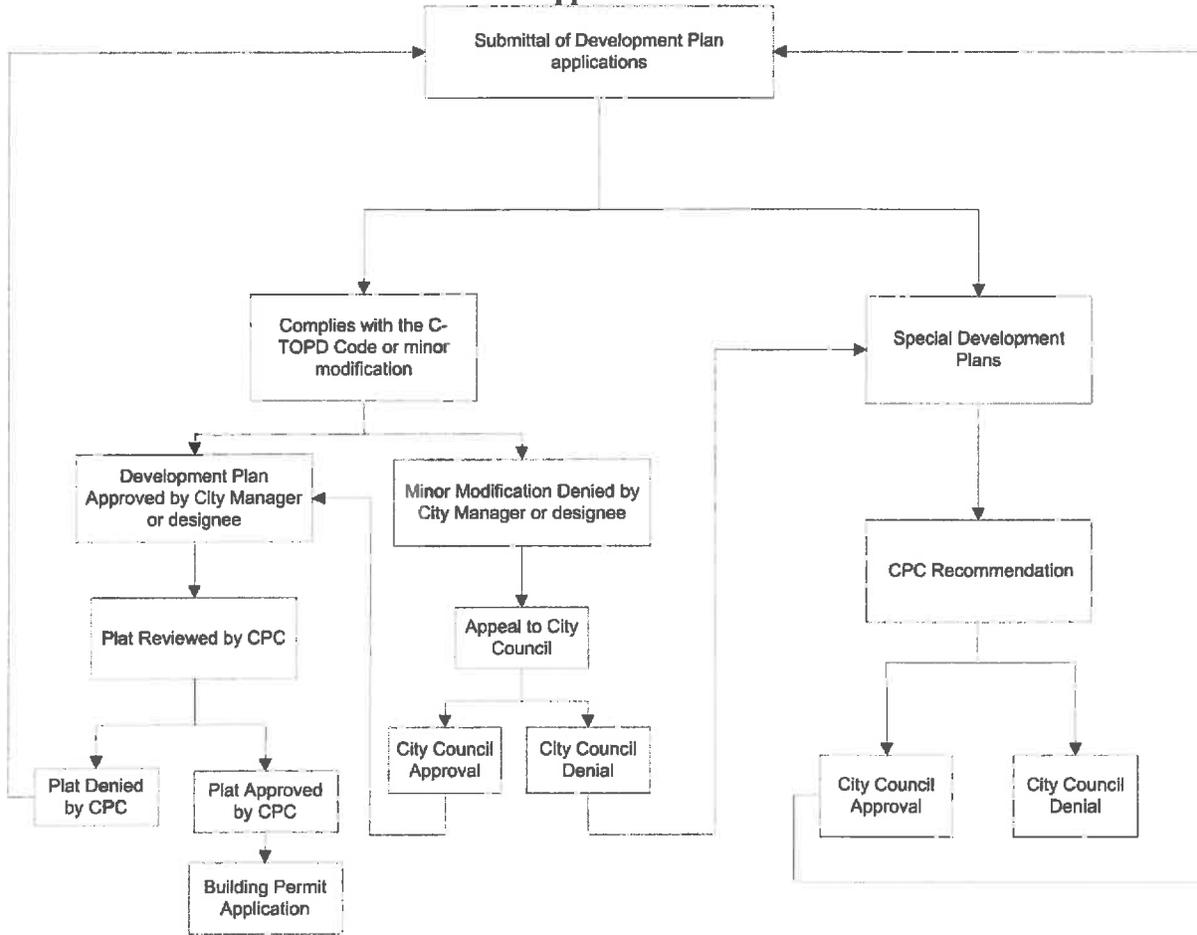
<u>Common Name</u>	<u>Botanical Name</u>
Dwarf Fountain Grass 'Little Bunny'	<i>Pennisetum alopecuroides 'Little Bunny'</i>
Dwarf Maiden Grass	<i>Miscanthus sinensis 'Adagio'</i>
Fountain Grass	<i>Pennisetum alopecuroides</i>
Inland Seoats	<i>Chasmanthium latifolium</i>
Maiden Grass	<i>Miscanthus sinensis 'Gracillimus'</i>
Mexican Feather Grass	<i>Stipa tenuissima</i>
Muhly Grass	<i>Muhlenbergia capillaris</i>
Weeping Lovegrass	<i>Eragrostis curvula</i>

TURF

<u>Common Name</u>	<u>Botanical Name</u>
Bermuda	<i>Cynodon dactylon</i>
St. Augustine	<i>Stenotaphrum secundatum</i>
Zoysia	<i>Zoysia tenuifolia</i>

These plantings may be placed in Civic/Open Spaces or used to meet the private landscaping requirements of the Code. The applicant shall select drought tolerant, low maintenance, and adaptable shrubs and ground cover based on the placement on the site subject to approval by the City.

Appendix C



C-TOPD: Caruth Properties Transit Oriented Planned Development
 CPD: City Plan Commission
 CC: City Council
 SDP: Special Development Plan

Note: Development Plans include site plans, building elevations, landscape plans, civil engineering plans, and all plats

ZF 10-20



RECEIVED
NOV 30 2010
DEVELOPMENT SERVICES

- Phil Dyer*
Mayor
- Lee Dunlap*
Mayor Pro Tem
- Pat Milner*
Deputy Mayor Pro Tem
- Ben Harris*
Place 2
- André Davidson*
Place 3
- Lisa Smith*
Place 4
- Harry LaRosiere*
Place 5
- Jean Carlson*
Place 7
- Thomas H. Muehlenbeck*
City Manager

November 22, 2010

Mr. Sam Chavez
Asst. Director of Development Services
City of Richardson
411 West Arapaho Road
Richardson, Texas 75080

Re: Zoning at Southeast Quadrant of US75 and Spring Creek

Dear Mr. Chavez:

The City of Plano owns an 80' x 80' tract of land surrounded by property owned by the Caruth Foundation. It is our understanding that the Caruth Foundation is in the process of rezoning the property. The City of Plano neither supports nor opposes the proposed zoning.

Sincerely,

Thomas H. Muehlenbeck
City Manager

Is

xc: Frank Turner, Deputy City Manager
Alan Upchurch, Director of Public Works & Engineering
Phyllis Jarrell, Director of Planning

Zoning Case ZF 10-20

Letters in
Support

Re: Zoning: Land Held By Bush/75 Partners LP and Others
Robert Quance
to:
Chris.Shacklett
01/07/2011 04:21 PM
Show Details

My wife and I are in favor of the rezoning request for the referenced property as the land would fall under new regulations to be approved by our City Council, that would be specify requirements to promote quality design and construction.

Robert and Ruth Quance
2107 White Cliff LN
Richardson, TX 75080
Ph 972-231-4065

On Fri, Jan 7, 2011 at 4:01 PM, <Chris.Shacklett@cor.gov> wrote:

Here is my email.

Chris Shacklett
Planner
Department of Development Services
City of Richardson
972.744.4249
chris.shacklett@cor.gov

ZF 10-20

**Letters in
Opposition**

Richardson Apt. development

Caroline Mecom

to:

Chris.Shacklett

12/06/2010 10:09 PM

Show Details

PLEASE let's NOT have 10-story high apt. buildings in Richardson! Smaller, attractive apartment buildings set back from the street and set at angles to each other allowing for green space and trees and some walking room is what will benefit the residents, the neighbors and the city in the long run. PLEASE let's place more restrictive parameters on the developers of both the Caruth and Parliament tracts!

**Thank you! Caroline Mecom
1232 Comanche Dr.
Richardson**



TOD Request North of Renner Road
Cyndi Dupuis to: Chris.Shacklett

12/06/2010 09:24 PM

As an 8 year resident, and business owner in Richardson, I would just like to let you know that I am highly opposed to the proposed construction of the apartment complex at Plano & Renner Road.

I certainly hope that you will share in, and value the opinions of the residents of Richardson. I do not believe that this is a direction that would be good for the city whatsoever.

Thanks you,
Cyndi Dupuis
1316 Chesterton Dr.
Richardson, TX 75080

Re: Request to Richardson to allow the construction of a very large apt complex

Gary Henderson

to:

<rsvp@richardsoncitizensalliance.com>, chris.shacklett@cor.gov

12/06/2010 10:48 PM

Show Details

Chris,

Cannot make the meeting but stand opposed to this complex. If anything we need more single family housing.

Have not heard all the data. But, on the service it doesn't sound like the best type of development for COR.

Gary Henderson

214.282.2222

Sent from my iPhone

On Dec 6, 2010, at 8:26 PM, <rsvp@richardsoncitizensalliance.com> wrote:

This correspondence is intended to make you aware of a request to the City of Richardson to allow the construction of an extremely large apartment complex at the northwest corner of Plano and Renner Roads. The applicant is requesting not to be limited to the number of apartments, but to only be limited to 100 feet in height with the building wall being as close as 6 feet from the road. This approach allows the applicant to build as many apartments on this property as they deem appropriate as long as they don't exceed the height limit which is similar to a ten story building. For planning purposes, City staff has estimated there would be around 3,760 apartments. This would be the largest apartment complex in Richardson and it is only Phase I as the applicant also owns the land across Renner Rd. which is currently approved to have 2,000 additional apartments built on it. There are additional apartments also approved to be built on the southwest corner of this intersection.

The concerns are; a significant increase in traffic and crime, and the over 6,000 apartments at this intersection will cause significant downward pressure on apartment rental rates. This last item will bear an even greater burden to other Richardson apartment complexes like the The Block, Eastside, Brick Row, Galatyn, etc. which have not been able to achieve even reasonable occupancy rates. Reduced rental rates can easily open the door to having West Spring Valley Road crime problems in multiple areas of Richardson. With only 3% left of undeveloped land in Richardson, this is not the direction we need to be taking!

You can help do something to stop this nonsense. Please read the letter below and let your Richardson elected officials know you do not want this happening in Richardson.

The Richardson Citizens Alliance

**7PM, December 7th Plan Commission Hearing
On the TOD Request North of Renner Road**

I just completed studying the 238 page, December 3rd City Staff report on the proposed development north of Renner Road including the Caruth 86 acre tract west of the DART rail and the 57 acre Parliament tract east of the DART rail. Our neighborhood is most concerned about the 100 foot tall buildings planned to house an unlimited number of apartments that would be located east of the rail line and west of Plano Road with almost no setbacks from Renner Road.

The staff's report and traffic impact analysis, based on 3,756 apartments in 100 foot tall buildings and 300 foot tall office buildings, recommends very minimal improvements for the expected traffic and is silent about the resulting level of service on the already congested Renner and Plano Roads and the US75/Renner intersection, which is already heavily congested during the AM and PM travel peak hours.

The staff report contains no concerns or reservations about this development which places no restriction on the actual number of apartments, almost no street setbacks and even allows 15 to 25% increases on building heights if desired by the applicants.

No development in Richardson, even including those along the DART line, currently has more than 528 units. So, these owners are asking for at least seven times the number of apartments in the largest complex in Richardson. The Caruth tract now has no zoning for apartments and the Parliament tract zoning allows 429 units. We already have existing about 2,000 apartments mostly fronting on Renner Road west and south of the Bush Toll Road.

The Neighborhood Protections Alliance of Richardson (NPAR) sharing our concerns created a committee to review the plans and form an official position on the quality of life impact of this development. But time is of the essence and your presence is need Tuesday night in order for the CPC to realize this issue is of concern to the citizens. If you cannot attend please send your comments to Chris.Shacklett@cor.gov.

Of even greater concern to our neighborhood and that of Fairways of Sherrill Park to the east of our Sherrill Park neighborhood is that Parliament owns the property to the east of Plano Road as well and it is already zoned for 2,000 more apartments and the forested area southwest of the Plano/Renner Roads intersection is zoned for somewhere around 300 apartments.

Our only hope to get all apartment zoning along Renner Road down to a reasonable level, is to start with restricting the currently proposed development to a reasonable increase in the allowed number sufficient to support the Transportation Oriented Development desired by our current City Council.

Please plan to attend Tuesday night's City Plan Commission Hearing, and if you have time study the Plan Commission Packet found at <http://www.cor.net> under Plan Commission Agenda. At the Plan Commission Meeting, only a total of 15 minutes is allowed for speakers in opposition to the proposed rezoning. So we need a full house (150+people) at the hearing to see what the Plan Commission plans to do with this application. I fear that if only a few people attend, that the Commission will assume that there is no opposition and will recommend approval to the City Council without any restrictions.

George Human, President, Sherrill Park Neighborhood Association
12/5/2010

RE: Rezoning request for Renner Road

Joel Crisalli

to:

Chris.Shacklett

12/06/2010 09:06 PM

Cc:

jrcrialli

Show Details

I hope this one comes to you.

From: Joel Crisalli [mailto:Joel.crisalli@tx.rr.com]

Sent: Monday, December 06, 2010 9:04 PM

To: 'Chris.Shacklett@cor.gov.'

Cc: 'g.human@sbcglobal.net'; 'jrcrialli@gmail.com'

Subject: Rezoning request for Renner Road

I have been reading some of the material that has been circulated about the proposed mixed use complex on Renner Road. Unfortunately I am unable to attend tomorrow's meeting but would like to express my deep concern about the dramatic effect a project of the size mentioned would have on this area.

We have lived in the Sherrill Park North development for nearly 25 years and have been pleased, until now, with the way that this section of our town has grown, been maintained and prospered. Our City and its government has handled an aging city with a delicate hand and kept it from becoming an urban afterthought. Roads and city resources have been well managed and our planners have earned our confidence.

I am confident that you and the rest of our city government wish to maintain this balance. Unlike the previously undeveloped section of the telecom district which has now become our new urban heart near the Eisemann or the Central/Campbell complex which carefully places a mixed use hub on the Dart Line which is second to none, the current project under consideration adds an element of stress to an already taxed road system and places a residential area, a nature preserve and even our municipal golf course in the shadow of what seems to be a disproportionately large and potentially threatening environment.

In fairness, the Parliament Group should be permitted to submit their proposal for open-minded review and joint discussion. However, I'd like to register that any residential complex which exceeds two stories or, in combination, more than 500 units would not be acceptable to me or my family.

If there is a written proposal published, I should appreciate knowing how to access it.

I repeat my thanks to you and our other City of Richardson staff for your work and regret that I will not be able to be present at this meeting.

Thank you,

Joel R. Crisalli

1511 Ambleside Lane

Richardson, TX 75082

Request to Richardson to allow the construction of a very large apt complex

Ann Nash

to:

Chris.Shacklett

12/07/2010 08:51 AM

Cc:

Betty Morton, Angie Wright, Chrissy Cortez-Mathis, Ed Hassler, Hermayne Merritt, Howard Davis, Ida Hassler, Janice Leventhal, Jim Bates, M ELISE DOHERTY, "Robert 'Trey' Kazee III", Sean Cortez-Mathis, Virginia Costlow, gary.slagel, john.murphy, Amir Omar, bob.townsend, mark.solomon, bob.macy, steve.mitchell

Show Details

I am unable to attend the meeting tonight.

I have received communication (via email from a group called Richardson Citizens Alliance) concerning this proposal for a large apartment complex in east Richardson.

I am totally opposed to adding this number of apartments to Richardson. Short term, I think this creates problems with overcrowded schools and more policing necessary. Long term I think it creates a future situation like we currently have with Spring Valley and the underkept older apartments in east Richardson. Apartments generally do not maintain value over the long term.

We should not squander our last pieces of undeveloped land just to have something on it. This needs to be part of an overall development plan for the city.

Ann Nash

317 Dogwood

Richardson, TX 75080

TOD Development North of Renner Road

ardismgood

to:

Chris.Shacklett

12/07/2010 04:21 PM

Show Details

I will be unable to attend the Council meeting on Dec. 7th but do want to voice my opposition to the Zoning Change Request for the TOD Development.

I feel the zoning change requested is unreasonable and will not enhance the area but, to the contrary, will degrade the lives of those living in the adjoining six neighborhoods.

The traffic impact is of great concern for the neighborhoods already suffering from congested Renner and Plano Roads and the US75/Renner intersection. The area is already heavily congested during the AM and PM travel peak hours - one has to put your life at risk leaving or entering the neighborhood as it is now. The Blue Cross Campus has already added to traffic problems in the area. Additional traffic from such a high density project will make it impossible to leave the neighborhood.

The proposed number of units is unreasonable. No development in Richardson, even including those along the DART line, currently has more than 528 units. So, these owners are asking for at least seven times the number of apartments which would make it the largest complex in Richardson. The Caruth tract now has no zoning for apartments and the Parliament tract is zoning allows 429 units. **The current zoning needs to be left in place**

The City of Richardson has always been known as a good, safe family oriented city and the huge increase in the number of apartments certainly does not preserve the image of family friendly. I would certainly hate to see our area degraded in future years as is what happened with the high density apartment communities along Spring Valley Road in past decades.

Sincerely,

Ardis Good

2909 Ambleside Lane

Richardson

Commission Meeting
Nanci Huskey
to:
Chris.Shacklett
12/07/2010 12:39 PM
Show Details

Chris, I sent this earlier but it didn't go through. I am trying again.

Chris:

Because of family illness, I may not be able to make tonight's meeting. Here are my thoughts on the subject:

1. Councilman Solomon stated in his presentation to the Homeowners Association that one of the features of residing in Richardson was low property taxes for residents because of the high tax on commercial and industrial property.
2. This development would change the nature of the properties from mainly commercial/industrial to largely residential.
3. Has a cost/benefits analysis been undertaken? It seems reasonable to assume that a residential population uses more city services such as, fire department, police department and trash disposal than commercial and industrial.

What will be the affect of this zoning change on our residential tax bills?

If this question cannot be answered directly in the meeting, no action should be taken until it is answered.

Respectfully Submitted

Art Huskey
Nanci Huskey

FW: CPNA
William Kinder
to:
Chris.Shacklett
12/07/2010 04:55 PM
Show Details

Chris,

I am attaching a copy of an email with an attachment I received from one of our active, positive residents, Mr. James Stinson, who has been a party to the creation of the Crowley Park Neighborhood Association representing 1200 homes or about 4,000 Richardson residents.

Mr. Stinson's email attachment clearly and appropriately represents the very strong concerns we have about turning our neighborhood into an apartment cluster with the same potential you are now fighting on Spring Valley, This will create an apartment city for 15,000, a glut which will invariably drive our property values down. Not to mention the traffic snarl it will create for all the neighborhoods that stretch from 75 all the way out Renner Road through the Richardson Panhandle.

We consider the zoning change to permit the construction of this gross number of apartments to be unacceptable.

Your consideration will be appreciated.

***Bill Kinder
President,
Crowley Park Neighborhood Association***

Bill,
I have attached what I would say if I were allowed to speak tonight.
James

Thank you for allowing me to address this body.

I view these developments with great concern. I am guided by thoughts expressed many years ago in one of Patrick Henry's lesser known quotes. He said, "I have no way of judging the future except by looking at the past." That is what I would like to do. Look at the past and you will see that these apartments are a potential and almost certain disaster waiting to happen.

A number of years ago I retired as a teacher in the Richardson ISD and while there I saw apartments ruin two school areas and devastate the communities around them. Along the Spring Valley and Coit Road areas a number of apartments were built back in the late 1970 and early 80's. When folks in this upscale area (at that time) objected they were told not to worry, the apartments would be first class and well maintained. They were for a couple of years. Soon the original owners sold out to new groups and the troubles began. When the apartments began to age and had empty units the owners turned to "public housing" and "government subsistence" to fill them and bring in money. Soon all kind of crime and vice racked the area. The police soon referred to the area as "Crack House North." People moved from the area and the property values dropped. The schools became drug infested and learning became secondary to survival. Northwood Jr. High was closed and turned into the RISD Academy.

The areas near Forest Meadow and Liberty Jr. Highs went through similar experiences. Ask any teacher who ever taught in any of these schools what they think caused the decline and you will hear one answer.....apartments. The Richardson High and Lake Highlands High areas have suffered for many years because of this blight.

Now you want to do the same thing to our area. Rest assured that in the short run everything will look good, but wait a few years and watch it collapse. It is not a case of "if it will happen" it is a case of "when will it happen." Our property values will tumble just like the other areas I mentioned. These good folks requesting the change will tell us it will not hurt the area, but I challenge them to show us where in the Richardson/Plano area that large apartment complexes have not had a negative impact. I don't know any.

The City of Richardson most likely will ignore our objections for the same reason all cities use. They need the money! They are only concerned with "now." What happens to our schools and property values in the future is not one of their concerns. It is a shame that property tax income trumps quality of life. Negative history will repeat itself once again.

I had to at least voice my concerns, even if they fall on deaf ears. After all...Richardson needs the money and that is the bottom line.

Thank You.

James Stinson

Massive Renner Road Development

Martha Valdivia

to:

Chris.Shacklett

12/07/2010 10:28 PM

Show Details

Chris:

We Carlos and Martha Valdivia are very concern about the massive development in Renner Rd. We are not agree with this development. We are worry about the insecurity and a massive traffic in the area. This neighborhood is special for the quiet and family residency where families can live in a peace full area. There is not enough reason to build more construction where there are so many areas in Richardson were there are so many abandon buildings and plenty empty areas in a industrial zones for more commercial buildings. In addition it is not secure to combine small apartments with this type of homes in this area which the price will be down for the contraction apartments that we know in few years the quality of these apartments will not be the same as news. Unfortunately, apartments usually lose their appreciation in a quit time and the neighborhood is always affected with that deterioration.

We really apreacite your attention.

Sincerely,

Carlos and Martha Valdivia

On the TOD Request North of Renner Road

Cary Welch

to:

Chris.Shacklett

12/07/2010 10:36 AM

Show Details

Chris,

My comments, I understand they paid a lot of money for the land and would like them have a fair and reasonable profit if not an excellent return on their investment, but at the same time when I moved out of my parents house as a young man and lived in a number of apartments in my younger years, when I drive by them now all most all of them are an eye sore and a crime ridden area, did not take that many years for it to become that way. Why that happens I do not know, but it does and how to keep that from happening I do not know, but hopefully they do and can show us how this will not happen and back it up with penalty and forfeit ownership of the property to the city if it happens, can they come up with this plan before being approved for any mega project, if they are willing to do this ask them to pretend they live down the street and what would they want in place before it is approved.

Also I am not for Mega projects with out seeing what they want to do build if it is not already zoned for it

Let's have them back it up in writing and have a reasonable way of measuring it. If it has a negative impact on surrounding homeowners they share in that, I would even be willing to say if our home values go up because of their project the city give them a break on some of there taxes, but do not thing a mega apartment complex is going to make our home values go up but would be curious of their thought on this.

Cary

Cary Welch

Accucom

Director

660 N. Glenville Dr

Richardson, Texas 75081

972-265-9758

~ Face your past without regrets, handle your present with confidence
and prepare for the future without fear ~

Planning Meeting for the Renner Road Development

Cathy Jackson

to:

Chris.Shacklett

12/07/2010 12:37 PM

Show Details

Chris Shacklett,

I may not be able to attend the planning meeting reviewing the Renner Road, I-75, George Bush Development, or if I do, it may not be for the entire meeting. So, I am forward my thoughts on this project. I feel the number of apartments are too high; they should be lower, and close to DART and the Cottonbelt (west of Plano Rd.) to reduce traffic. I can't see someone walking from Owens Blvd. past Plano Rd. to the DART/Cottonbelt station to use the rail system everyday to go to work. People like convenience, so I don't think apartments east of Plano Rd. would help reduce the traffic. I feel apartments east of Plano Rd would increase traffic as there will be traffic from the businesses located there as well, and therefore, the land east of Plano Rd. should not be zoned for apartments. The only other area of concern is the massive size of the buildings. It seems more like New York City. I like Richardson as it has the city amenities with a hometown feel, so this area is a concern. Thanks for your time.

Regards,

Cathy

Renner Road Apartments
Christopher Ulrich
to:
Chris.Shacklett
12/07/2010 08:54 AM
[Show Details](#)

We are opposed to the proposed apartment development north of Renner Road.

Thank you.

Chris and Vickie Ulrich

apartments at Renner @ Bush
Patterson, Chris (GE Capital)
to:
Chris.Shacklett
12/07/2010 08:56 AM
Show Details

I personally don't think we need anymore apartments in the city of Richardson. Especially giant complexes like these are proposed to be.
I agree with the concerns over traffic and crime. I also agree that if we already have apartments that are not at least 90% occupied, then we certainly don't need to add more!

Thank you,
Chris Patterson

TOD Request North of Renner Road

Dan Bryan

to:

Chris.Shacklett, gary.slagel

12/07/2010 07:38 PM

Show Details

Gary and Chris - I read the information on the above request in disbelief earlier today. Unfortunately I am out of town and unable to attend tonight's meeting. As a citizen of Richardson for 25 years, I am baffled on how any of our leaders would even consider such a request. The infrastructure is not there, crime will rise, congestion will increase, property values will decrease, school will degrade and quality of life will diminish. I see no value of this complex to the citizens of our city. Please help me understand who is benefiting from this complex and what those benefits are?

Dan Bryan

Proposed Development on Renner Road

Reynolds, Dianna

to:

Chris.Shacklett

12/07/2010 10:56 AM

Show Details

Dear Chris,

As a 20 yr homeowner in the Crowley Park neighborhood, I am distressed to hear about the multifamily development being planned on Renner. As a parent, I am EXTREMELY concerned for this growth. As a homeowner, I am very upset. Our neighborhood and Sherrill Park to our west have been totally ignored by PISD as "red headed stepchildren" and the school originally planned in one end of Crowley Park was abandoned decades ago, never to be readdressed, though at least four brand new elementary schools have been constructed in the last 10 years further to the east of us toward Murphy.

I very concerned that additional multifamily communities will negatively impact the situation, which is already grim. Our kids are already schooled with low income families of old east Plano, which while giving them exposure to some critical social issues, also exposes our children to a high percentage of transient families and significant safety issues.. Instead of the elementary school that was planned in our neighborhood 30 years ago and abandoned, our children have bussed for the last 30 years to Mendenhall Elementary which again, is primarily low income and apartment community families when Aldridge, Boggass, Schell and Stinson are closer. Our children continue to be bussed in middle school to Bowman, again, in the lower income area of PISD, then to Williams HS, another old school in another old area, and finally, to PESH. The fact that our neighborhood has no school that is a reasonable distance for our children and a student body made up of committed homeowners is adversely impacting our home property values and the welfare of our children. Truly, had I anticipated that we would have been raising a child who would feed into the Elementary School and Middle School these neighborhoods are districted in, I would have purchased a home elsewhere in Richardson.

Thank you for your time and attention and your service to our community.

Best Regards,

Dianna H. Reynolds

Senior Claims Specialist

SAFECO INSURANCE

(972) 808-4225; 1-800-332-3226, ext 32 4225;

fax: 1-888-268-8840

Dianna.Reynolds@safeco.com

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My name is George Human. I have lived at 1510 Amesbury Drive in the Sherrill Park Neighborhood for 22 years. I have been President of our Neighborhood Association for four years.

Our neighborhood and others are members of the Neighborhood Protection Alliance of Richardson which currently includes six neighborhoods and other organizations.

I am currently working with the four neighborhoods that are adjacent to Renner Road to develop a consensus for response to the proposed TOD.

I'm a civil engineer by education and have worked with and for Cities for over 50 years, having served as City Planning Director for Fort Worth, City Engineer for Plano, Transportation Director for Richardson and many years in private industry as a consultant to Cities in between those jobs and since.

I am very supportive of the TOD concept and hope a mutually beneficial solution can be worked out that will increase the pride we have for our city, but not be a detriment to our neighborhoods.

Most of the residents living in the neighborhoods along Renner Road have lived there for many years and looked forward to more Cisco and TI type development along Renner.

But they have become outraged at the prospect of 6,000 apartments just in the short distance between US75 and Wyndham Lane just west side of the Shire.

If this apartment zoning were to be approved, it would be 7 times the largest complex ever approved in Richardson. Those of us who have lived in Richardson for many years have seen many apartment complexes become very undesirable to live in and near. We fear adding over 3300 more to the currently zoned almost 3000 approved units would be most detrimental to our neighborhoods and to the City as a whole, since all rents would go down and the complexes would suffer or fail.

The attorney representing Parliament offered me just last night an option which I have not been able to fully appraise the other neighborhoods of or reach a consensus on what we feel we all could live with.

The neighborhood's leadership's responses have ranged from NO MORE APARTMENTS – PERIOD to we need more time to analyze and reach a consensus. And the Alliance needs more time to poll its member neighborhoods leadership to form a consensus after our four neighborhoods on Renner have come to agreement.

We only received the final staff report this morning and the Traffic impact Analysis late yesterday afternoon and have not had time to digest and discuss all of this information much less fully digest the full 238 page staff report put out late Friday night.

I suspect you Plan Commission members haven't had sufficient time either to digest all of this last minute information.

On behalf all of us, I beg you to not make a final decision on the massive development request tonight.

Greg Smith

to:

Chris.Shacklett

12/07/2010 11:34 AM

Show Details

Chris,

One other point. The City staff should be very concerned that if it grants this apartment zoning, the land owner will likely sell as many of these tracts as possible to local apartment developers. There are not many good suburban development tracts available right now so I would expect these tracts will be developed rather quickly. Most apartment developer's want to built, lease and sell the property as fast as possible, leaving the long term operations to investment groups. This is where problems can start if the number of new projects is not restricted. As I stated in the earlier email, the City staff should see how the current land owner develops its existing 2,000 apartment units before granting this zoning request.

Best Regards,

Gregory S. Smith

President

Rockwood Capital, Inc.

Greg Smith
to:
Chris.Shacklett
12/07/2010 11:16 AM
Show Details

Chris,

I own a 442 unit apartment complex in the City of Richardson named the Clearwater Creek Apartments. I agree that the addition of this project would be very detrimental to the overall apartment market in Richardson causing downward pressure on rental rates and making existing projects struggle economically. In addition, our projects were built under stringent lower density requirements so that they would fit in with the overall planning within the City of Richardson. I cannot believe that the Planning Department and the City Council would seriously consider this application. The purchaser of these tracts was fully aware of the existing zoning limitations when they purchased the land tracts. To add this apartment zoning would be a mistake and certainly not equitable to existing land owners that have attempted to secure apartment zoning in the past.

Since they already have zoning for 2,000 apartment units, which is probably the amount the market could absorb over the next 5 years, I am strongly opposed to this request. Once these 2,000 units are constructed and successfully operated, the Planning Department and City Council could consider an additional zoning request at that time. I am sure that the owner of the tracts has planned to zone as much of the land as apartments as possible. Then they will commence to sell the tracts to every apartment developer in town and there will be a gross over building of the market. Land for apartment units will sell around \$10,000-\$12,000 per unit in today's market. Therefore, they have requested zoning that will be worth approximately \$45,000,000 which is probably the same amount they recently paid for the entire tract.

Let me know if I can be of assistance in further discussions. I am not available to attend the hearing on December 7th but you can share my sentiments at the hearing if you desire.

Best Regards,

Gregory S. Smith
President
Rockwood Capital, Inc.

Renner Road Apartments
Kerry Brown
to:
Chris.Shacklett
12/07/2010 11:39 AM
Show Details

I vote AGAINST this. It would greatly impact my quality of life and serenity.

Kerry Brown
1508 Margate Lane
Richardson Texas 75082

Thank you for your consideration.

huge apt. complex issue
Laverne Domel
to:
Chris.Shacklett
12/07/2010 11:32 AM
Show Details

I have been a resident of Richardson since 1958 (or 52 years). My family of 3 children (before growing up and now living elsewhere) and I stayed here after the sudden death of their father, they were 7, 12 and 14 years old; they were born and raised here and the only home they knew. It was a good choice because they kept all their friends and I decided if the children were happy, I'd be happy.

I do not want to live in a city that the most important thing is to grow larger and larger! Our population of near 100,000 with plenty of industry, parks, churches and great schools within a 10 - 15 mile radius, and dart rail to surrounding areas is marvelous! We don't want to get huge apartment complexes in our area because then we'd be like the area around Valley View....just wall to wall concrete and tall buildings. We are a comfortable size for a wonderful suburban town with lots of recreation, theatre and great schools including UTD! Lets keep it that way and let the surrounding areas around us deal with the huge populations! Thank you.

Sincerely LaVerne Domel
1328 Cherokee Dr
Richardson TX 75080
(my address for 47 years and before that at 200 Dublin for 5 years...(Rich. was about 10,000 citizens in Spring of 1958)!

renner road apts
Marilyn Schwartz
to:
Chris.Shacklett
12/07/2010 11:19 AM
Show Details

as a resident of crowley park, i wish to state my great concern and objection to massive apartments proposed on renner road. huge apartment complexes are never a good value to neighborhoods over the test of time.

marilyn schwartz
2556 honeysuckle
richardson tx 75082

Development along Renner Rd.
martin smith
to:
Chris.Shacklett
12/07/2010 07:50 AM
Show Details

Dear Mr. Shacklett,

As a long time resident of Sherril Park I am very dissappointed that our local government would consider over developing the area betwven Renner road and the GB tollway. One of the appeals of living here is the relatively tidyness of the area and the low crime rate, with such a huge developent I am sure that these statistics will be skewed in the negative direction.

I would also like to int out that with the current developemtns that have gone on over the past 10 years towards the east side of Renner the traffic volume has increased drematically. The installation of traffic lights at Wyndham has not been a good solution for us. Traffic continuously flows on Renner and the time wait is excessive for us residents. Also, with it being a 4 way junction when the lights are green all this does is allow traffic from the 190 bypass to get to their destination while we sit and wait.

Also, with the increase in properties how does the city plan to handle waste. The large trucks flowing down Plano road are dangerous, they leave large amounts of litter and cost us in road repairs.

I am very apposed to the proposed plan to change the zoning in this area for these reason.

--

Regards,

Martin smith
Ph 972 664 0042
Cell 214 995 5321



Renner apartments
coconino to: Chris.Shacklett

12/07/2010 11:55 AM

As a 38 year resident of Richardson, it is both discouraging and a little frightening to think that one builder can ruin not only the city atmosphere of Richardson, but the economy as well. It seems apparent to me that by requesting only the one restriction apply and disregarding all other limitations possible on the property, that as many apartments as humanly possible is the goal. The traffic, the crime, the overall congestion to businesses in the area, along with the fact that not all apartments in the same neighborhood are rented is something I do not want in my environment. Greed alone seems to be the objective. Please do not allow this request, and please preserve the integrity of our community. I am completely against these apartments being constructed - on any corner of that intersection. I regret I will not be able to attend the meeting this evening, but wanted to make my concerns known.

Norma-Ree Lueders
406 Crestover Circle

Renner Road Development.

S Howe

to:

Chris.Shacklett

12/07/2010 02:51 PM

Show Details

Chris Shacklett,

My wife and I had intended to be present at the meeting tonight, but due to health, we will not be able to make it

We feel that the current zoning requirements are very liberal and should not be changed. We appreciate the opportunity to have a voice on this issue.

Our thanks go out to those who have led the effort to maintain our neighborhood.

Stewart and Dorothy Howe

1508 Amesbury Dr

Richardson, Tx



Fw: TOD Request North of Renner Road
Pamela Schmidt to: Sam Chavez, Chris Shacklett
Cc: bill.keffler, Cliff Miller, Dan Johnson

12/07/2010 02:43 PM

Pam

Pamela Schmidt
City Secretary
City of Richardson
972-744-4290
972-744-5803 (F)
pamela.schmidt@cor.gov

----- Forwarded by Pamela Schmidt/CH/Cor on 12/07/2010 02:42 PM -----

From: "Suzanne Juliussen" <italia@sbcglobal.net>
To: <Pamela.schmidt@cor.gov>
Cc: <barryhand@netzero.net>, <chrisbdavis@sbcglobal.net>
Date: 12/07/2010 11:15 AM
Subject: Re: TOD Request North of Renner Road

City Secretary: Please forward this to all members of the Plan Commission & the City Council.
Thank you, Suzanne Juliussen

To: Richardson City Plan Commission
Richardson City Council

From: Suzanne Juliussen
908 Dumont Drive
Richardson, TX 75080
972-234-4545
italia@sbcglobal.net
(Cottonwood Heights NA)

Subject: TOD Request North of Renner Road

Date: December 7, 2010

I am unable to come to the Plan Commission meeting this evening. However, I would like to make my wishes be known to those making important decisions for the City of Richardson.

I am 100% against the idea of building humongous apartments at the NW corner of Plano and Renner Roads. My concerns:

- Increase in traffic
- Potential crime
- Affect on other Richardson apartment complexes i.e. Brick Row, Galatyn, etc. which are already having such a difficult time renting
- With 3% left of undeveloped land in Richardson, can't we do a lot better than this?

We need to get apartment zoning along Renner Road down to a reasonable level. I implore those in charge to restrict the currently proposed development to a reasonable increase in the allowed number sufficient to support the TOD desired by our current City council.

apt. complex
Susie Hatley
to:
Chris.Shacklett
12/07/2010 08:40 AM
Show Details

Hello,

I am a resident of Richardson and have been for over 20 years. My family grew up in the Prairie Creek area and that is where my family now resides. My husband and I feel that the building of such a large complex is the wrong choice for Richardson because of the crime it would bring to this area. We also see the nice apartment complexes we already have in Richardson and don't believe they are filled so why build more.

We love living in Richardson because of the care and pride the residents and City Council take in keeping this community a safe and wonderful place to live. Let's keep it that way.

Thank you, Susie Hatley

December 3rd City Staff report

walt barnett

to:

chris.shacklett

12/07/2010 01:21 PM

Cc:

gary.slagel, bill.keffler

Show Details

Complete Subject: December 3rd City Staff report on the proposed development north of Renner Road including the Caruth 86 acre tract west of the DART rail and the 57 acre Parliament tract east of the DART rail.

Attn: Chris Shacklett COR Planning Commission

I oppose the approval of the subject report. After reviewing the situation I can see no reason to put the tracts of land involved to this use. The mixed use concept is acceptable, but the number of potential housing units is not.

This area has numerous apartments that have not achieved their occupancy potential. I see no economic viability for more units. With more units the traffic impact certainly cannot be readily dismissed.

More units will negatively impact the rental rate structure. The West Spring Valley area is an example that should not be repeated. Having worked as a volunteer for Meals on Wheels in the West Spring Valley, I know firsthand the security issues involved and the area. People were wary of strangers with good reason.

Thank you for time time and efforts

Walther Barnett

1114 N Cottonwood Drive

Richardson, TX 75080

Phone 972 231 6038



Renner Apartments

Warren D. Caldwell to: Chris.Shacklett

Cc: "Jason Davidson", "Dad"

12/07/2010 08:08 AM

History:

This message has been replied to.

Hello,

I was unable to attend the meeting last night but wish to briefly air my concerns about this project.

As a lifelong Richardson resident and local real estate broker, I am extremely concerned about the negative impact this development will have on the surrounding neighborhoods. Do we really need another Lake Highlands situation where a wonderful neighborhood is ruined by crime?

It will be peaches and cream for the first 10 years while they are still a shiny penny. It will only be later, long after the developers cash out, when it is sold to a faceless holding company with no ties to our city, who cut expenses to the bone and the riff raff moves in and criminalizes another part of Richardson. This part of town already is bearing an increased crime burden caused by the train station.

Highest and Best Use? I think not.

Respectfully,

Warren D. Caldwell
972-814-0400
onecall@tx.rr.com

"I'd rather wear the Medal of Honor around my neck than be President."
- Harry S. Truman -

Apartment Project
Warren
to:
Chris.Shacklett
12/07/2010 09:59 AM
Show Details

This is the worst idea we could ever have for our city. This many units would indeed be the equivalent of a small city itself. The additional police and fire protection needed would be dramatic and costly beyond what taxes these units would generate. Where would all the kids go to school.?

Richardson was built along the premise of being a residential community--that does not mean that we have to turn down every request for additional buildings--commercial or retail, but it should not include apartment projects of this size. Growth can be good but only if it is the right kind.

How about some department store options or more restaurant options--besides fast food places.?

The general trend is that as apartments age they attract more crime--regardless of the location. The comments that this new project would hurt other near projects that are either recently opened or still in progress --Eastside and Brick Row --are true. Smaller apartment developments are easier to control the original quality/prices/upkeep, etc etc.

I feel approval of this over-sized request for this many units (and office building height) is ridiculous. Land use in this spot should be used for offices, retail, etc ---worst case is down-size to several hundred units --maximum height of 3 stories.

Sincerely

Warren F. Caldwell



FW: Concerns Regarding Renner Road Development

HiTech Marketing to: chris.shacklett

Cc: "HiTech Marketing"

12/07/2010 11:32 AM

From: "HiTech Marketing" <htm1983@sbcglobal.net>
To: <chris.shacklett@cor.gov>
Cc: "HiTech Marketing" <htm1983@sbcglobal.net>

I am Wes Pettinger at 1501 Banbury Ct. My concerns are:

Traffic/accident increase: presently the intersection of Renner/Plano Rd sees many traffic accidents per week. The intersection handles an increasingly higher volume due to 190 access from 75. I do not buy the argument that most apartment dwellers will not use cars. There is not enough local restaurants, shopping, food stores, etc. to maintain the proposed increase in population to promote walking and not using cars.

Noise increase due to tall, high density apartments reflecting car noise from 75 and 190

DART rail loading saturation

Access to Plano and Renner Roads congestion problems from local neighborhood residential roads that do not have traffic signals (Braeburn and Owens Blvd).

Set backs from Plano and Renner Roads

Adverse affect on property values over time due to transient nature of apartment dwellers and the usual poor building maintenance down the road.

Future city costs and taxes to fix traffic congestion (fly-over's, traffic lanes, lights), police loading, utilities loading.

Why does the city jump into projects like this without doing the necessary studies and present the truth to our citizens? Does the staff's study go into any detail of traffic loading, environmental impact of cutting down the forest south of Renner and west of Plano?

Paliament planned development

Christine

to:

Chris.Shacklett, g.human

12/09/2010 11:21 AM

Show Details

Do y'all have a plan for the coyotes and all the other wild animals that call the woods there their home?

Christine Coutu

1101 Pacific Drive

Richardson, TX 75081

Received this 12-8
David Gee
to:
Chris.Shacklett
12/08/2010 01:44 PM
Show Details

Hello Chris, just received this today. This is not good for Richardson, Richardson ISD, Central Expressway and so on. Please bring me up to date on the results of this meeting. Thank you, David Gee

7PM, December 7th Plan Commission Hearing
On the TOD Request North of Renner Road

I just completed studying the 238 page, December 3rd City Staff report on the proposed development north of Renner Road including the Caruth 86 acre tract west of the DART rail and the 57 acre Parliament tract east of the DART rail. Our neighborhood is most concerned about the 100 foot tall buildings planned to house an unlimited number of apartments that would be located east of the rail line and west of Plano Road with almost no setbacks from Renner Road.

The staff's report and traffic impact analysis, based on 3,756 apartments in 100 foot tall buildings and 300 foot tall office buildings, recommends very minimal improvements for the expected traffic and is silent about the resulting level of service on the already congested Renner and Plano Roads and the US75/Renner intersection, which is already heavily congested during the AM and PM travel peak hours.

The staff report contains no concerns or reservations about this development which places no restriction on the actual number of apartments, almost no street setbacks and even allows 15 to 25% increases on building heights if desired by the applicants.

No development in Richardson, even including those along the DART line, currently has more than 528 units. So, these owners are asking for at least seven times the number of apartments in the largest complex in Richardson. The Caruth tract now has no zoning for apartments and the Parliament tract zoning allows 429 units. We already have existing about 2,000 apartments mostly fronting on Renner Road west and south of the Bush Toll Road.

The Neighborhood Protections Alliance of Richardson (NPAR) sharing our concerns created a committee to review the plans and form an official position on the quality of life impact of this development. But time is of the essence and your presence is need Tuesday night in order for the CPC to realize this issue is of concern to the citizens. If you cannot attend please send your comments to Chris.Shacklett@cor.gov.

Of even greater concern to our neighborhood and that of Fairways of Sherrill Park to the east of our Sherrill Park neighborhood is that Parliament owns the property to the east of Plano Road as well and it is already zoned for 2,000 more apartments and the forested area southwest of the Plano/Renner Roads intersection is zoned for somewhere around 300 apartments.

Our only hope to get all apartment zoning along Renner Road down to a reasonable level, is to start with restricting the currently proposed development to a reasonable increase in the allowed number sufficient to support the Transportation Oriented Development desired by our current City Council.

Please plan to attend Tuesday night's City Plan Commission Hearing, and if you have time study the Plan Commission Packet found at <http://www.cor.net> under Plan Commission Agenda. At the Plan Commission Meeting, only a total of 15 minutes is allowed for speakers in opposition to the proposed rezoning. So we need a full house (150+people) at the hearing to see what the Plan Commission plans to do with this application. I fear that if only a few people attend, that the Commission will assume that there is no opposition and will recommend approval to the City Council without any restrictions.

George Human, President, Sherrill Park Neighborhood Association

12/5/2010

J

Re: Renner Apartments
Warren D. Caldwell
to:
Chris.Shacklett
12/08/2010 12:13 PM
Show Details

Thank you for submitting my thoughts. I was unable to attend and am curious about the outcome.

Best,

Warren

— Original Message —

From: Chris.Shacklett@cor.gov
To: Warren D. Caldwell
Sent: Tuesday, December 07, 2010 8:48 AM
Subject: Re: Renner Apartments

Mr. Caldwell,

The meeting for the zoning cases on Renner is actually tonight at 7pm. I will print this email and make sure the Commissioners receive it however.

Chris Shacklett
Planner
Department of Development Services
City of Richardson
972.744.4249
chris.shacklett@cor.gov

"Warren D. Caldwell" ---12/07/2010 08:08:43 AM---Hello, I was unable to attend the meeting last night but wish to briefly air my concerns about this

From: "Warren D. Caldwell" <onecall@tx.rr.com>
To: <Chris.Shacklett@cor.gov>
Cc: "Jason Davidson" <burntorange98@hotmail.com>, "Dad" <warrencaldwell@tx.rr.com>
Date: 12/07/2010 08:08 AM
Subject: Renner Apartments

Hello,

I was unable to attend the meeting last night but wish to briefly air my concerns about this project.

As a lifelong Richardson resident and local real estate broker, I am extremely concerned about the negative impact this development will have on the surrounding neighborhoods. Do we really need another Lake Highlands situation where a wonderful neighborhood is ruined by crime?

It will be peaches and cream for the first 10 years while they are still a shiny penny. It will only be later, long after the developers cash out, when it is sold to a faceless holding company with no ties to our city, who cut expenses to the bone and the riff raff moves in and

criminalizes another part of Richardson. This part of town already is bearing an increased crime burden caused by the train station.

Highest and Best Use? I think not.

Respectfully,

Warren D. Caldwell
972-814-0400
onecall@tx.rr.com

"I'd rather wear the Medal of Honor around my neck than be President."
- Harry S. Truman -

Massive Renner Road Development
Martha Valdivia

to:

Chris.Shacklett

12/07/2010 10:28 PM

Show Details

Chris:

We Carlos and Martha Valdivia are very concern about the massive development in Renner Rd. We are not agree with this development. We are worry about the insecurity and a massive traffic in the area. This neighborhood is special for the quiet and family residency where families can live in a peace full area. There is not enough reason to build more construction where there are so many areas in Richardson were there are so many abandon buildings and plenty empty areas in a industrial zones for more commercial buildings. In addition it is not secure to combine small apartments with this type of homes in this area which the price will be down for the contraction apartments that we know in few years the quality of these apartments will not be the same as news. Unfortunately, apartments usually lose their appreciation in a quit time and the neighborhood is always affected with that deterioration.

We really apreacite your attention.

Sincerely,

Carlos and Martha Valdivia

one more voice on proposition

E Berglund

to:

Chris.Shacklett

12/07/2010 08:11 PM

Show Details

Hello Chris,

Sorry I could not attend tonight but I am just a 40 yr resident of Richardson (2nd generation in same house) who does not want the proposed zillions of apartments near Plano Rd and Renner! Yes I know there are so many still vacant but how about limiting the developer to 500 units with lots of parks! (Maybe some more shops for jobs too....)

Thank you,

Ellie Berglund

Re: Request to Richardson to allow the construction of a very large apt complex

Ann Nash

to:

Gary Slagel

12/07/2010 08:01 PM

Cc:

Chris.Shacklett, gary.slagel, john.murphy, Amir Omar, bob.townsend, mark.solomon, bob.macy, steve.mitchell

Show Details

Gary: thanks for your response. I don't know much about this citizens alliance either and I don't know how they got my email address. I knew I wanted to get a quick response off if all they indicated was a fact. I was in a hurry this morning because I had the crime patrol training class today so I didn't have time to do much research.

Hopefully I answered all my test questions correctly and can now be "Officer Purple Poodle Mom"! (Just kidding, I hope I can do a good job of being observant in my neighborhood.)

Isn't it great to live in Richardson!

Ann Nash

On Tue, Dec 7, 2010 at 5:51 PM, Gary Slagel <gary.slagel@capitalsoft.com> wrote:

Ann,

Thank you for your input on this case. The application is still at the planning level and the meeting tonight is with the City Plan Commission. The City Council should see something early next year. I'm not familiar with the Richardson Citizens Alliance or the email you mentioned, so I don't know what you were told. I do know the Plan Commission and City Planning Department will be diligent.

Best Regards,

Gary

Gary Slagel

Mayor, City of Richardson

411 W. Arapaho Rd

Richardson, TX 75080

ND 214-707-8418

O) 972-220-1563

gary.slagel@cor.gov

From: Ann Nash [mailto:purplepoodlemom@gmail.com]
Sent: Tuesday, December 07, 2010 8:51 AM

To: Chris.Shacklett@cor.gov
Cc: Betty Morton; Angie Wright; Chrissy Cortez-Mathis; Ed Hassler; Hermayne Merritt; Howard Davis; Ida Hassler; Janice Leventhal; Jim Bates; M ELISE DOHERTY; Robert 'Trey' Kazee III; Sean Cortez-Mathis; Virginia Costlow; gary.slagle1@cor.gov; john.murphy@cor.gov; Amir Omar; bob.townsend@cor.gov; mark.solomon@cor.gov; bob.macy@cor.gov; steve.mitchell@cor.gov

Subject: Request to Richardson to allow the construction of a very large apt complex

I am unable to attend the meeting tonight.

I have received communication (via email from a group called Richardson Citizens Alliance) concerning this proposal for a large apartment complex in east Richardson.

I am totally opposed to adding this number of apartments to Richardson. Short term, I think this creates problems with overcrowded schools and more policing necessary. Long term I think it creates a future situation like we currently have with Spring Valley and the underkept older apartments in east Richardson. Apartments generally do not maintain value over the long term.

We should not squander our last pieces of undeveloped land just to have something on it. This needs to be part of an overall development plan for the city.

Ann Nash
317 Dogwood
Richardson, TX 75080

Re: TOD Request North of Renner Road
 Dan Bryan
 to:
 Gary Slagel, Chris.Shacklett
 12/13/2010 07:42 PM
 Show Details

RECEIVED
 DEC 13 2010
 DEVELOPMENT SERVICES

Gary, Let me start off by saying how appreciative I am of your response and explanation. I know you are busy and I do thank you for spending the time to reply. As the old proverb goes "there are always two sides to every story" and your view is certainly different then the 3-4,000 potential apartments story I heard.

With that said, I believe the city should heavily scrutinize any multi-family project. No need to remind you and the council the Spring Valley complexes were once considered luxury units meant for young or mobile professional.

Renner and Central is already a very congested area during peak traffic times and the infrastructure currently in place would be extremely expensive if not impossible to modify. The mixed use developments you described causes localized traffic nightmares. Long and careful consideration should be given to the changes in traffic patterns and any modifications required.

Gary, I have always supported your positions as I feel they have always been for the betterment of the community and its citizens. I believe, as you stated on many occasions, corporate citizen make very good neighbors. I trust the council will maintain the direction that made this community so desirable.

All the Best,

Dan

On Fri, Dec 10, 2010 at 2:21 PM, Gary Slagel <gary.slagel@capitalsoft.com> wrote:

Dan,

I don't know what you saw on this request, but this is not a request for an apartment complex. I do, however, understand your concerns based on what must have been sent to you. I would like to give you an update on the process. Every land owner has the right to make an application and that application must go through a public process with the City Plan Commission and then the City Council. That process is what had just started with the meeting you missed. The land in question is an important piece of property because of the DART rail station, which will likely be our station with a direct connection on the Cotton Belt rail line to DFW Airport. The input I get from residents of all ages is that they would like a high end retail development like Waters Creek in Allen, West Village in Dallas or Legacy Town Center in Plano. The applicant/property owner is proposing such a development, similar to those mentioned, at the location bounded by US 75, Bush, Renner and Plano Rd, with the DART station located in the center. A successful retail developments like those mentioned always have office and housing as part of that mix. The question is, how do we ensure quality over the long term, so that the development enhances the value of property around the development. Second, how do we leverage the major transportation assets of US 75, DART and Bush Turnpike so as to minimize impact on local streets. Those questions and many others will be addressed with a series of public meetings with the property owner. The next City Plan Commission meeting on this application is scheduled for December 21st. I hope you will be able to attend that meeting. I do know the City Plan Commission, City Planning Department and City Council will be very focused on these issues throughout the process. Again, thank you for your interest in our city.

Best Regards,

Gary

From: Dan Bryan [mailto:dan.bryan2@gmail.com]
Sent: Tuesday, December 07, 2010 7:39 PM
To: Chris.Shacklett@cor.gov; gary.slagel@cor.gov
Subject: TOD Request North of Renner Road

Gary and Chris - I read the information on the above request in disbelief earlier today. Unfortunately I am out of town and unable to attend tonight's meeting. As a citizen of Richardson for 25 years, I am baffled on how any of our leaders would even consider such a request. The infrastructure is not there, crime will rise, congestion will increase, property values will decrease, school will degrade and quality of life will diminish. I see no value of this complex to the citizens of our city. Please help me understand who is benefiting from this complex and what those benefits are?

Dan Bryan

Richardson apartments
Carltonie Mosher
to:
Chris.Shacklett
12/12/2010 01:59 PM
Show Details

RECEIVED
DEC 12 2010
DEVELOPMENT SERVICES

I am a resident of Richardson, 2507 West Prairie Creek, and I'm opposed to the zoning change on Renner to build more apartments. Particularly, I don't want 4,000 apts. built between the access road and Plano Rd. When none of the apt. complexes in the area are reporting less than 2% vacancy, another 100-200 might be appropriate. What is the city council even thinking about with posting a zoning request for this type of construction? Please keep me informed of the happenings.

Sincerely,
Caroline Mosher

Bush transit station multi use project
Richard Semock
to:
'Chris.Shacklett@cor.gov'
12/20/2010 03:16 PM
Show Details

History: This message has been forwarded.

Planning Council:

I am impressed that the whole dev team turned up at the NPAR meeting last week giving up their evening to answer questions when they have covered these at public hearings with the planning council. This might be an indication of their dedication and consideration of developing better neighborhoods thru sound building principles. I suspect this might be the case after hearing them speak at the meeting and at city hall. From what I have observed, they are experienced professionals who are competent in what they do and are capable of making responsible decisions in the proper development of the transit station project.

Nevertheless, the council should apply a Reagan principle to its dealings with the developers and Trust but Verify. You can do this by establishing a '16 points' approach that was proposed by the surrounding neighborhoods for the transfer station agreements recently. Some of these points could be:

- 1) Hold them to the high quality construction standards they describe in their presentations
- 2) Put controls in place that prevent the misuse of form based zoning by cramming apartments instead of more expensive townhomes, business, and retail.
- 3) Cap the height of buildings visible from Renner&Plano.
- 4) Cap the number of family dwellings on the site.
- 5) Install stoplights at Owens&Renner and Braeburn&Plano for safe access of the neighborhood due to increased traffic.
- 6) Existing apartments in the area such as the large Prairie Creek complex to the west should be factored into calculations when proposing more. (The complex also has a vested interest in this planning and could be invited to join in the planning)
- 7) ...

Richard Semock - 3206 Owens

From: SPNFANA [<mailto:spnfana@yahoo.com>]
Sent: Monday, December 13, 2010 09:50
To: undisclosed recipients:
Subject: Meeting TONIGHT, Monday, December 13th at 7pm with developers

Notice of Confidentiality: This transmission contains information that may be confidential and that may also be privileged. Unless you are the intended recipient of the message (or authorized to receive it for the intended recipient) you may not copy, forward, or otherwise use it, or disclose its contents to anyone else. If you have received this transmission in error, please notify us immediately and delete it from your system.

CPC Meeting

Prissy Mount

to:

Chris.Shacklett, SPNFANA

12/20/2010 06:03 PM

Please respond to "Prissy Mount"

Show Details

I have lived on Scottsboro Lane for 30 years - Plano Road was a 2 lane black top road, no one had a home computer, very few people had car phones (the size of bricks) and the term "Telecom" had not been imagined. There was a large wooded area across Plano Road, but I - foolishly - believed that Richardson was a wonderful city that put it's residents first and that any development would take the neighborhood into consideration. My husband nor I saw any zoning change signs put up in that area when it was rezoned in the 80s. Now you are planning to give carte blanche to developers to build for any use as long as the streets look like they promise and the buildings are built in a certain way and at a certain height. They show pictures of happy people and nice buildings - but the pictures are not of 10-30 story buildings! They don't talk about dead trees because they can't get any sun due to the towering buildings. They don't talk about Section 8 people filling up apartments that can't be rented. They don't talk about the increased crime in neighborhoods near large apartment communities. Did you see the front page story in the Dallas Morning News about packages stolen from front porches? The residents had a camera put in because they lived near apartments! They don't talk about the slums these buildings can become in 20-40 years when the next "hot" area is built and everyone moves on. How about the neighbors who can hardly leave the neighborhood during rush hour now because of the heavy traffic? The representative from Caruth showed possible development plans for the Caruth section of the property. He showed planned office buildings - but admitted that they could be apartments instead! Would you allow this open ended development to be put in your neighborhood? Can you look at yourself in the mirror if you do it to us?

Priscilla Mount

1404 Scottsboro Lane



Tues meeting
Janice Kummer to: Chris.Shacklett

12/21/2010 10:54 AM

Chris,
Though we may not be stirred up like the masses, this is NOT a reasonable time to have a critical session such as this when many, including us, are already out of town for the holidays. The Bush Turnpike developement can wait a couple of more weeks.

Janice and Stan Kummer
3002 Wren
Sent from my Verizon Wireless Phone

CPC Meeting of 12/21

BHASKAR GHATE

to:

Chris.Shacklett

12/21/2010 12:10 PM

Cc:

Sandy12, rrtmrt, rajiv_r_shah, Kirit Mehta, PBGhate, sushanta106

Show Details

STOP ACHTUNG ACHTUNG

Hello:

Until a few days ago, I thought we were living in a truly Citizen Oriented City. In the last 20 years of my stay in Richardson, I have not experienced such a rapid erosion of my trust in the City's handling of projects of this magnitude and without regard to citizens' interests, as I am experiencing now.

Please postpone this meeting. It is not fair and it is not good governance. Whoever is being lobbied to in the City Administration, they are just rolling over to these developers. We haven't had a decent look at what we are facing and what's in store in the future because of this huge mind boggling development in our neighborhood. We are directly affected by this.

Please stop this project. **Let the citizens get a grip of this situation before you begin further work.**

A very annoyed citizen.

B. B. Ghate
3105 Wyndham Lane
Richardson, TX

SPHOA

Transit Oriented Planned Development @ Renner & Plan Rd

Bronwyn

to:

Chris.Shacklett

12/21/2010 01:18 PM

Show Details

This matter should be put off in its entirety until after the 1st of the year, when all underlying issues, planning code changes, and overall plan changes to the original proposal can be heard, disclosed and reviewed prior to any commitment/vote. Thank you,

Bronwyn MacIsaac

1814 Waterford Lane

Richardson, TX 75082

YOU must be the change you wish to see in the world. -- Gandhi



**City Plan Commission Meets Tonight Concerning Renner /Plano Rd. Planned
Development**

Barbara Baughman to: Chris.Shacklett

12/20/2010 05:06 PM

This huge development deserves more scrutiny by the community. Scheduling community input at such an inconvenient time is equivalent to saying you purposely do not want community input. It won't hurt to wait a few weeks.

--

Barbara Baughman
Systems Analyst
X2157

CPC Meeting Tonight Concerning Renner Road Planned Development

Rowena Graham

to:

Chris.Shacklett

12/21/2010 08:28 PM

Show Details

Chris,

I would just like to let you know that I will not be able to make the meeting tonight and would like to be kept informed as to the out come of the meeting tonight.

I am a resident of Fairways of Sherrill Park and I do like most of the residents have a lot of concerns about the development that is trying to take place on Renner Road.

thanks,

Rowena Graham

rowenag@tx.rr.com

972-898-0596



Scam

Gregory Cavanagh to: Chris.Shacklett

12/21/2010 04:09 PM

Mr. Shacklett,

I am shocked that this meeting was scheduled during the holidays when so many of us are traveling. It seems to have been planned on purpose with the hopes that the attendance would be minimal and therefore benefiting the developers. Which of course, causes greater concern as they seem to be hiding something.

Greg Cavanagh
3207 Westgate Lane

Fw: City Planning Commission meeting Tuesday night at 7pm
George Human
to:
chris.shacklett
12/21/2010 06:01 AM
Cc:
rnune
Show Details

For distribution to Plan Commission for TOD consideration

From: rama nune
Sent: Monday, December 20, 2010 6:18 PM
To: SPNFANA ; g.human@sbcglobal.net
Subject: Re: City Planning Commission meeting Tuesday night at 7pm

I believe, we should seek remedies along the following lines

1. Limit multi-family (apartments) in the 190 corridor (alma to jupiter/ renner)
this could be done by
a) city considering current zoning and all projections and limiting to current zoning or some reasonable amount (given all existing), say 4000
b) city planning commission can then (based on a) above) restrict MF/apt in carruth and parliament requests
to 2500 given
- this can be done in reg. plan by limiting bldg heights to more reasonable 60-70 ft along renner/plano intersection (as galatyn, 15th and other comparable TOD apts are only 5 story or less)

2. Traffic impact analysis

Given that Renner/75 and renner/ plano is bearing the brunt of traffic increase
a) clearly show worst case peak delays at these intersections, with new signals (2?) between renner and routh parkway and between renner and 190 access; is <5 min target between wyndham and 75 ?
b) why not consider alternate access in/out of development from 75 and 190, besides renner/75

3. Request city PC to show all expenses city will incur including projection due to this proposed development
a) any tax incentives
b) include all city street improvements
b) all traffic mitigation costs

Regards,
rama
cell 9728140923

On Dec 20, 2010, at 1:05 PM, SPNFANA wrote:

Morning



Notice of Public Hearing

City Plan Commission • Richardson, Texas

An application has been received by the City of Richardson for a:

TRANSIT ORIENTED PLANNED DEVELOPMENT

File No./Name: ZF 10-20 / Caruth Transit Oriented Planned Development
Property Owners: Pamela A. Spadaro / U.S. Trust, Bank of America
Vester T. Hughes, Jr. / Estate of W.W. Caruth, Jr.
City of Plano
Applicant: Brian E. Moore / GFF Planning
City of Richardson
Location: NEC and NWC of US Hwy 75 and Renner Road (See map on reverse side)
Current Zoning: TO-M Technical Office District
Request: A request to rezone approximately 85.9 acres of land (including an approximate 0.2-acre tract - City of Plano) to a Planned Development District with modified development standards under a Form Based Code.

The City Plan Commission will consider this request at a public hearing on:

TUESDAY, DECEMBER 7, 2010

7:00 p.m.

City Council Chambers

Richardson City Hall, 411 W. Arapaho Road

Richardson, Texas

This notice has been sent to all owners of real property within 200 feet of the request; as such ownership appears on the last approved city tax roll.

Process for Public Input: A maximum of 15 minutes will be allocated to the applicant and to those in favor of the request for purposes of addressing the City Plan Commission. A maximum of 15 minutes will also be allocated to those in opposition to the request. Time required to respond to questions by the City Plan Commission is excluded from each 15 minute period.

Persons who are unable to attend, but would like their views to be made a part of the public record, may send signed, written comments, referencing the file number above, prior to the date of the hearing to: Dept. of Development Services, PO Box 830309, Richardson, TX 75083.

The City Plan Commission may recommend approval of the request as presented, recommend approval with additional conditions or recommend denial. Final approval of this application requires action by the City Council.

Agenda: The City Plan Commission agenda for this meeting will be posted on the City of Richardson website the Saturday before the public hearing. For a copy of the agenda, please go to: <http://www.cor.net/DevelopmentServices.aspx?id=11512>.

For additional information, please contact the Dept. of Development Services at 972-744-4240 and reference Zoning File number ZF 10-20.

Date Posted and Mailed: 11/24/10

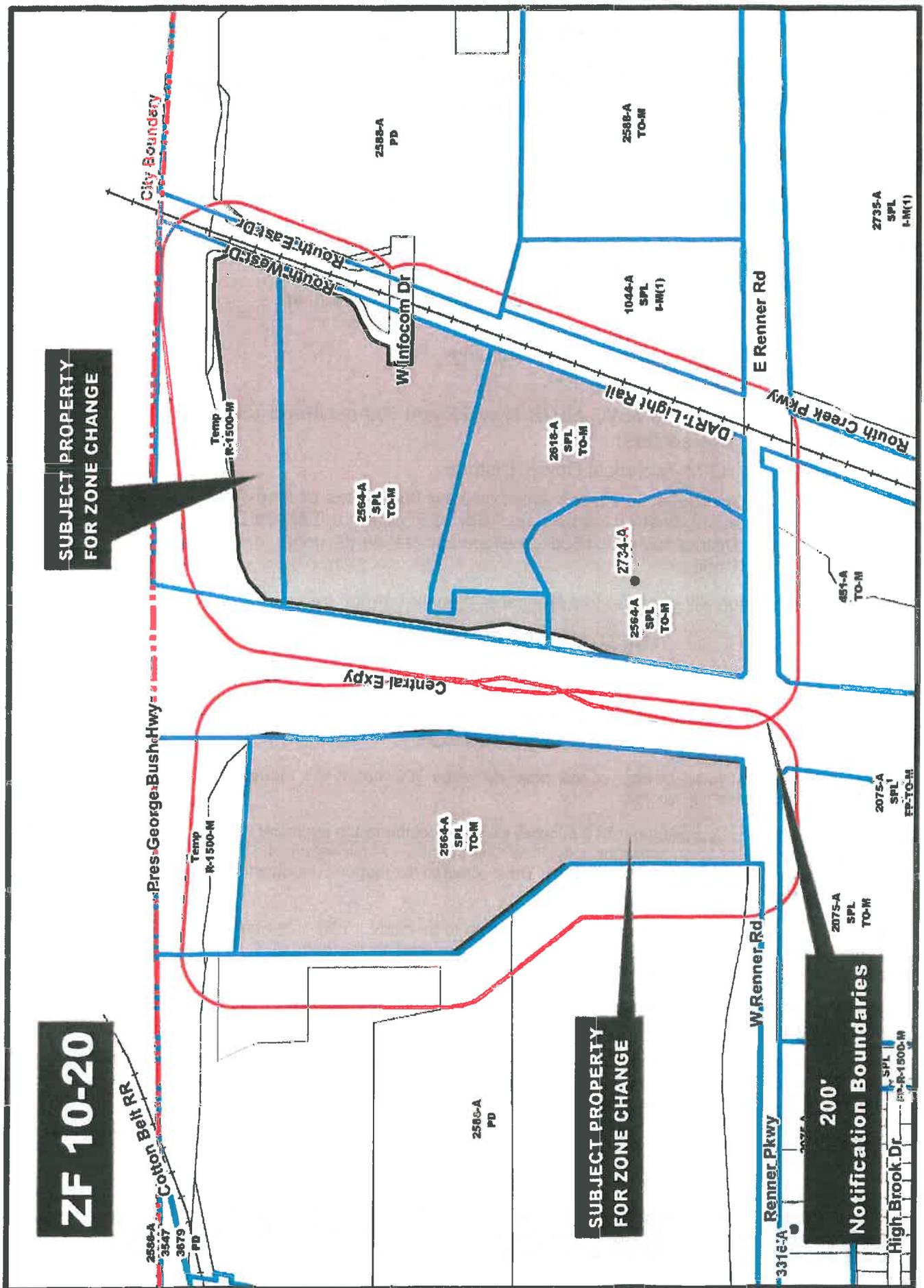


ZF 10-20

SUBJECT PROPERTY FOR ZONE CHANGE

SUBJECT PROPERTY FOR ZONE CHANGE

200'
Notification Boundaries



ZF 10-20 Notification Map

Updated By: shactvec; Update Date: November 17, 2010
 File: D:\Mappr\ZF1020\ZF1020 notification.mxd

M&A TEXAS PRAIRIE LP
7328 VENTURA BLVD STE 188
SAN JOSE, CA 91316-3904

**ONCOR ELECTRIC DELIVERY
COMPANY**
PO BOX 219071
DALLAS, TX 75221-9071

VESTER T. HUGHES, JR.
ESTATE OF WW CARUTH, JR.
1717 MAIN STREET, STE 2800
DALLAS, TX 75201

CARUTH W W JR
5803 GREENVILLE AVE
DALLAS, TX 75206-2916

PLANO CITY OF
PO BOX 860358
PLANO, TX 75086-0358

CARUTH W W JR FOUNDATION
PO BOX 831500
DALLAS, TX 75283-1500

OTR
275 E BROAD ST
COLUMBUS, OH 43215-3703

GATEWAY PIONEER INC #1
1406 HALSEY WAY STE 110
CARROLLTON, TX 75007-6444

**RICHARDSON HEIGHTS
BAPTIST CHURCH**
201 W RENNER RD
RICHARDSON, TX 75080-1318

CARUTH W W FOUNDATION
NATIONSBANK OF TX-TRUST
PO BOX 831500
DALLAS, TX 75283-1500

BUSH/75 PARTNERS LP
4801 W LOVERS LN
DALLAS, TX 75209-3137

DALLAS AREA RAPID TRANSIT
1401 PACIFIC AVE
DALLAS, TX 75202-2732

DALLAS AREA RAPID TRANSIT
PO BOX 660163
DALLAS, TX 75266-0163

RICHARDSON I S D
400 S GREENVILLE AVE
RICHARDSON, TX 75081-4181

CITY OF PLANO
PHYLLIS JARRELL, PLANNING
1520 AVE K
PLANO, TX 75074

BRIAN E. MOORE
GFF PLANNING
2808 FAIRMOUNT ST, ST 300
DALLAS, TX 75201

PAMELA A. SPADARO
U.S. TRUST, BANK OF AMERICA
901 MAIN STREET, 16TH FLOOR
DALLAS, TX 75202

ZF 10-20



**City of Richardson
City Council Meeting
Agenda Item Summary**



Meeting Date: Monday, January 17, 2011

Agenda Item: Items of Community Interest

Staff Resource: Bill Keffler, City Manager

Summary: The City Council will have an opportunity to address items of community interest, including:

Expressions of thanks, congratulations, or condolence; information regarding holiday schedules; an honorary or salutary recognition of a public official, public employee, or other citizen; a reminder about an upcoming event organized or sponsored by the City of Richardson; information regarding a social, ceremonial, or community event organized or sponsored by an entity other than the City of Richardson that was attended or is scheduled to be attended by a member of the City of Richardson or an official or employee of the City of Richardson; and announcements involving an imminent threat to the public health and safety of people in the City of Richardson that has arisen after the posting of the agenda.

Board/Commission Action: NA

Action Proposed: No action will be taken.