

RICHARDSON CITY COUNCIL
JANUARY 31, 2011
6:00 P.M.
CIVIC CENTER/CITY HALL, 411 W. ARAPAHO, RICHARDSON, TX

1. **INVOCATION**
 2. **PLEDGE OF ALLEGIANCE: U.S. AND TEXAS FLAGS**
 3. **MINUTES OF THE JANUARY 17, 2011 MEETING**
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4. VISITORS. (THE CITY COUNCIL INVITES CITIZENS TO ADDRESS THE COUNCIL ON ANY TOPIC NOT ALREADY SCHEDULED FOR PUBLIC HEARING. PRIOR TO THE MEETING, PLEASE COMPLETE A "CITY COUNCIL APPEARANCE CARD" AND PRESENT IT TO THE CITY SECRETARY. THE TIME LIMIT IS FIVE MINUTES PER SPEAKER.)
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5. CONTINUATION OF PUBLIC HEARING, ZONING FILE 10-21: A REQUEST BY SCOTT POLIKOV, GATEWAY PLANNING GROUP, INC., REPRESENTING BUSH/75 PARTNERS LP, FOR A PD PLANNED DEVELOPMENT FOR APPROXIMATELY 57 ACRES LOCATED ON THE NORTH SIDE OF RENNER ROAD BETWEEN THE DART LIGHT RAIL AND PLANO ROAD. (CONTINUED FROM JANUARY 17, 2011, CITY COUNCIL MEETING)

ACTION TAKEN:

6. CONTINUATION OF PUBLIC HEARING, ZONING FILE 10-20: A REQUEST BY BRIAN E. MOORE, GOOD FULTON & FARRELL PLANNING AND THE CITY OF RICHARDSON, REPRESENTING THE ESTATE OF W.W. CARUTH, JR., US TRUST, BANK OF AMERICA AND THE CITY OF PLANO, FOR A PD PLANNED DEVELOPMENT FOR APPROXIMATELY 86 ACRES LOCATED AT THE NORTHWEST AND NORTHEAST CORNERS OF US HWY 75 AND RENNER ROAD. (CONTINUED FROM JANUARY 17, 2011, CITY COUNCIL MEETING)

ACTION TAKEN:

7. REPORT ON ITEMS OF COMMUNITY INTEREST.
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THE RICHARDSON CITY COUNCIL WILL MEET AT 5:30 P.M. ON MONDAY, JANUARY 31, 2011, IN THE HUFFHINES ROOM OF THE CIVIC CENTER/CITY HALL, 411 W. ARAPAHO, RICHARDSON, TEXAS. AS AUTHORIZED BY SECTION 551.071(2) OF THE TEXAS GOVERNMENT CODE, THIS MEETING MAY BE CONVENED INTO CLOSED EXECUTIVE SESSION FOR THE PURPOSE OF SEEKING CONFIDENTIAL LEGAL ADVICE FROM THE CITY ATTORNEY ON ANY AGENDA ITEM LISTED HEREIN. THIS BUILDING IS WHEELCHAIR ACCESSIBLE. ANY REQUESTS FOR SIGN INTERPRETIVE SERVICES MUST BE MADE 48 HOURS AHEAD OF THE MEETING. TO MAKE ARRANGEMENTS, CALL 972-744-4000 VIA TDD OR CALL 1-800-735-2989 TO REACH 972-744-4000.

I CERTIFY THE ABOVE AGENDA WAS POSTED ON THE BULLETIN BOARD AT THE CIVIC CENTER/CITY HALL ON FRIDAY, JANUARY 28, 2011, BY 5:00 P.M.


CITY SECRETARY

MINUTES OF A SPECIAL MEETING OF THE CITY COUNCIL
January 17, 2011
City of Richardson, Texas

A Regular Meeting of the City Council was held at 6:00 p.m., Monday, January 17, 2011 with a quorum of said Council present, to-wit:

Gary Slagel	Mayor
Bob Townsend	Mayor Pro Tem
Mark Solomon	Council member
John Murphy	Council member
Bob Macy	Council member
Steve Mitchell	Council member
Amir Omar	Council member

City staff present:

Bill Keffler	City Manager
Dan Johnson	Deputy City Manager
Michelle Thames	Assistant City Manager Administrative Services
David Morgan	Assistant City Manager Community Services
Cliff Miller	Assistant City Manager Development Services
EA Hoppe	Assistant to the City Manager
Pamela Schmidt	City Secretary
Sam Chavez	Asst. Dir. of Development Services – Planning
Dave Carter	Asst. Dir. of Development Services – Traffic

1. **INVOCATION**
2. **PLEDGE OF ALLEGIANCE: U.S. AND TEXAS FLAGS**
3. **MINUTES OF THE DECEMBER 6, 2010, DECEMBER 20, 2010, AND JANUARY 10, 2011 MEETINGS**

ACTION TAKEN: Mr. Macy moved approval of the minutes; second by Mr. Mitchell and the motion was approved with a unanimous vote.

4. VISITORS.

Mayor Slagel advised the audience that the Council has already received a lot of input from those in favor, in opposition and those who are concerned. He explained the typical process for managing public hearing items, which included hearing from staff and the applicant followed by the people who want to speak. He stated that the Visitors item is for anyone to speak about items not listed as a public hearing item and asked the City Secretary to announce the visitors.

Mark Smithhisler, 2201 Victoria Lane, Board member of the Neighborhood Protection Alliance addressed the Council about the process of the agenda. He noted that residents have spent hours observing Plan Commission and joint Council/Plan Commission meetings on the public hearing topic as well as spent time researching the topic. He stated they spent time drafting

objectives and reasonable questions with the goal to make sure that the City Council and staff are considering all aspects of the development to insure the most successful outcome. He voiced objection and disappoint that residents would have to wait until after the presentations to ask their questions, which he noted have already been provided to Council. He questioned the City's motives and sincerity and asked Council to follow the intent of the posted and approved agenda, and allow the NPA of Richardson to speak during the Visitors items.

Darrell Day, 1303 Chickasaw, asked about the wisdom of adding potentially 7,000 new apartments and asked if the City had conducted any specific survey of Richardson residents to determine what they feel is needed or wanted in the area as well as requested the survey be made public. He began to address the posted public hearing item and Mayor Slagel asked that he hold his comments until the appropriate time during the public hearing.

Ms. Schmidt advised that Mr. Bassuk stated he was interested in speaking regarding Item 5.

With regard to the comments made by Mr. Smithhisler, Mayor Slagel replied that the process that would be followed has been followed for many meetings and years, and again explained the process. He further noted that he had described the process to Mrs. Smithhisler the previous week and received a request from her on Sunday suggesting it be handled the way described by Mr. Smithhisler. He reiterated that it is not the process or practice used in the past. He stated the goal is to get public input from the Council and the residents. He stated they would talk through the application because the applicant has made some modifications since the Plan Commission meeting. He confirmed receiving the questions referred to by Mr. Smithhisler as he received the questions late in the afternoon. He assured the audience that the City would address the questions, but would not be able to address it this evening. He felt it was very important for the audience to listen to the application, to try to understand what the application is about and then form a decision. He stated the Council would not take action this evening and will continue discussing the application in two weeks. He announced that the public hearings would be held at the same time to expedite the process but would discuss them separately.

5. PUBLIC HEARING, ZONING FILE 10-21: A REQUEST BY SCOTT POLIKOV, GATEWAY PLANNING GROUP, INC., REPRESENTING BUSH/75 PARTNERS LP, FOR A PD PLANNED DEVELOPMENT FOR APPROXIMATELY 57 ACRES LOCATED ON THE NORTH SIDE OF RENNER ROAD BETWEEN THE DART LIGHT RAIL AND PLANO ROAD.

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Mr. Keffler explained that the City of Plano owns some property within the Caruth holdings and based on State statute, they cannot have their property reviewed in a zoning file and have assigned that responsibility to the City of Richardson inherently as a part of this process. He stated that this has been a very moving process even since the City Plan Commission (CPC) took their action on December 7, 2011. He explained that since that time period, staff has had

multiple meetings with the property owners in response to an ongoing number of issues that have been raised by the public, the City staff and comments raised by the applicants, and is the reason why it is so important to review what some may feel is tired information. He noted the Council's interest in transit oriented development over 14 years ago and stated that the dozens of public meetings since that time reflect the Council's intent of trying to maximize the value of the DART investment and to take advantage of the properties around the rail stations. He advised that in 1997, the Comprehensive Planning Guide first noted the Bush Turnpike Station as a location in the Comp Plan and with it identified TOD for the first time in the 2000 Comp Plan Guide that followed that work as a planning initiative that the City wanted to continue to pursue. He stated that the history is important, as reviewed during the November 9th Joint Meeting with the CPC, and stated that the history demonstrates that it is a product of vision. He noted that many City Councils, Plan Commission members and the public have been a part of the vision. He stated that in 2000, the Council commissioned the Urban Land Institute to look at all rail stations including the Bush site. In 2009, amplification was made on transit villages with a live, work and play theme. He provided highlights of the study, particularly regarding the site in question. He informed Council that the Parliament Group purchased the property near the DART station on September 1, 2009 and the purchase was followed by a kick-off meeting on March 22, 2010 with groups representing the two property owners as well as a myriad of other staff participation. He referred to the June 7, 2010 Joint Work Session with the City Plan Commission and a second joint meeting on November 9, 2010, both of which underscore the public process and discussions. He stated that Richardson has been an active participant on a Regional context as well. He felt it important to note that Mike Eastland, Executive Director of the North Central Texas Council of Governments, met with the Council in June of 2010 and discussed the 2050 implementation of the Vision North Texas and underscored that Richardson is a part of the bigger whole. Mr. Keffler stated the study identified that people want more than a home; they want a community that is walkable with character identity, a healthy lifestyle, and choices as lives and interests change. He noted Mr. Eastland's comments that the neighborhoods, jobs, services and amenities that North Texans want and need in 2030 or 2060 will not be the same as in 2000 or earlier. He also referred to *Dallas Morning News* articles talking about the areas of the future. Mr. Keffler stated the Telecom Corridor is the second largest employment center in DFW. He stated that more people work here than in the Central Business District of Fort Worth, Las Colinas or in Legacy; all very well regarded workplaces. He noted that Richardson is conscious about the need to compete with other communities as it tries to hold on to jobs that has allowed Richardson to be the community that it is. He stated the City will continue to focus on other key areas and study areas that are perceived to be desired areas such as Waters Creek, West Village or Legacy Town Center. He read from an email he received addressed to the Mayor and Council encouraging the Council to use the expertise to put into place all of the safeguards and caps that will guide the City of Richardson into becoming a true leader of this area for growth in the 21st Century. He asked Sam Chavez, Asst. Director of Development Services – Planning, to brief the Council on the two requests.

Mr. Chavez stated that the property in question is located on the south side of George Bush Tollway just north of Renner Road between Plano Road on the west side of US 75. The current zoning on the property for the Bush Central Station is about 57 acres of land that allows for office, research, hospital, hotel, light manufacturing, limited retail and residential. The Caruth property is about 85 acres and allows similar type uses, but does not allow residential use. He also noted the zoning districts surrounding the properties. He stated that the 2009 Comprehensive Plan designates the area for Transit Village and Regional Employment. Mr. Chavez explained that the request is to rezone the properties to PD Planned Development District through the use of form-based elements. He assured the Council that subject properties would be required to comply with all City codes and ordinances, specifically the zoning

ordinance when not in conflict with the proposed PD, Subdivision and Development Code, Building codes, Fire codes, Drainage and Engineering Design Standards, Landscape Standards, the Thoroughfare Plan and the Sign Code. He reviewed a PD Comparison table explaining the difference between conventional zoning and form based zoning, noting that form based code provides more detail than a typical PD District. Key components of the requested PD are the regulating plan (zoning map) and the development standards using text & graphics. The Bush Central Station character zones include TOD Core, TOD Mixed Use, Highway Mixed Use, and Arterial Mixed use, and the Caruth character zones include TOD Core, TOD Mixed Use, Arterial Mixed Use, TOD Freeway Hi-Rise and Urban Neighborhood. He brought attention to the fact that the developable Caruth property included 52.5 acres and the Bush Central Station of developable properties included 28.6 acres. A lot of the acreage is used for the open space requirements and the infrastructure and street network system. Mr. Chavez stated that the street designations establish a certain character and design to improve walkability with the PD. He underscored that the development standards provide for the setbacks, building heights and those sorts of elements and it establishes a coherent urban character and encourages and attractive development. He provided a map reflecting the proposed building heights along with the building heights currently allowed and noted that based on the exhibit, the maximum height would be 300 ft., which equates to a 27-28 story building. He reviewed charts with development standards for exterior materials for the Bush Central Station and the Caruth area and noted that both proposals exceed the minimum requirements for exterior materials in a residential setting. He referred to the proposed street and streetscape, streetscapes and landscape, and signage standards and noted that the public realm is widened to allow for street cafes. He advised that the standards prohibit any sort of residential on the first floor. The PD includes sign standards for the types of signs and includes design, development and installation criteria. He compared the development process included in the PD to the City's development process. He stated that if the plan complies with the PD requirements or requires minor modifications as defined in the PD, it would be reviewed and approved by the administrative staff. The subdivision plat is then forwarded to the Plan Commission. Mr. Chavez explained that if staff determines that a minor modification does not meet the threshold, the applicant can appeal to the City Manager and if denied by the City Manager or his designee, the request for modification would be heard by the City Council, illustrating that there are checks and balances in the PD. Any major modifications would go through to the Plan Commission and Council as a special development plan through the zoning process. He described the steps that staff would take in reviewing an application under the PD. He noted that one of the unique items about the proposed PD is that it requires street trees and landscaping and noted the standards such as being planted 40' on center and 12' in height and he also noted the requirements for pedestrian scale lighting and bike racks along Type "A" streets. Mr. Chavez relinquished the floor to Dave Carter to present the Traffic Impact Analysis, which was conducted by Kimley Horn & Associates.

Mr. Mitchell asked for number of allowed apartments. Mr. Chavez replied that there are no multi-family units allowed in the Caruth property and the northern half of the Bush Central Station has zoning entitlements for 426 units. Mr. Mitchell asked how many apartments would be allowed on the tract further to the east of Plano Road that is not part of the zoning request and Mr. Chavez replied just under 2000 units. He stated the property also allows office and retail development. Mr. Mitchell asked what would keep it from being developed as a large apartment community and not having the kind of amenities that residents want to have. Mr. Chavez stated that the development standards eliminate the possibility of what would be considered a traditional garden type style apartment complex. Mr. Mitchell asked if there was anything in the proposed ordinance that would assure the incremental amenities as multi family is being constructed. Mr. Mitchell defined amenities such as restaurants and retail and again asked if there are guarantees. Mr. Keffler interjected that it is the applicant's responsibility to

explain and describe the application on the points raised by Mr. Mitchell. Mr. Chavez stated there are development standards that prohibit residents from occupying the first floor of any building.

Mr. Omar asked if the proposed park and other green space would be considered an amenity and Mr. Chavez replied affirmatively.

Mayor Slagel asked staff to continue with the Traffic Impact Analysis.

Mr. Carter advised that the applicant hired Kimley Horn & Associates, Inc., (KHA) to prepare a single combined TIA and the study included all three properties together to get a good overview of the entire development process. The study utilized the City's standard guidelines for developing TIA's. Mr. Carter noted that Kimley Horn has performed previous studies in the area including signal timing and design projects, which allowed them to be familiar with the area. They conducted new traffic counts and turning movements at all surrounding roadways and at signalized intersections. He reported that the City hired Lee Engineering to review the TIA to have an unbiased person and a firm to review all of the details. Following the Plan Commission meeting and subsequent meetings with the public, City staff requested that the applicants have Kimley Horn & Associates conduct analysis of additional scenarios. He explained the review requirements of the TIA and the additional scenarios. Using an aerial photograph, Mr. Carter reviewed the existing daily traffic volume counts in the area of Bush and US 75. He advised that Kimley Horn utilized the land use assumptions that were provided by the applicants. He reviewed and described the table providing land use assumptions and zoning comparisons as well as a trip generation table, with existing and proposed zoning. He noted that the proposed TOD mixed use zoning results in more daily traffic than the existing zoning, but is spread out over the day. He also noted that peak hour traffic volumes would be lower with a better distribution of inbound and outbound traffic. Mr. Carter stated that congestion is generally associated with morning and evening peak hours and pointed out that the TIA calls for a 15% decrease in morning peak hours and a 17% decrease for the evening peak hours, so there would be less traffic during the peak hours but more spread out throughout the day. He further noted that the TIA provides that 30% of the traffic would be to/from the North; 15% to/from the East; 37% to/from the South and 18% to/from the West. He talked about the current level of service showing Renner Road at Level D, which is considered acceptable. He reviewed the TIA recommended developer mitigation measures for Renner Road, Plano Road, US 75 frontage roads and other transportation recommendations.

Renner Road

- WB auxiliary Lane – Plano road to Rough Creek Parkway
- WB deceleration lanes at all other driveways
- Median opening with turn lanes and traffic signal at drive P2
- EB left turn to Routh Creek Parkway
- Improved WB right lane with island and auxiliary lane at US 75

Plano Road

- SB auxiliary / right turn lanes – Bush Turnpike to Renner Road
- NB left turn lanes at Infocom and Drive P5
- Traffic signals at Infocom Drive and Drive P5
- Improve right lane with island at Renner Road

US 75 Frontage Roads

- Deceleration lanes at all driveways
- Satisfy TxDOT access management requirements

Other Transportation Recommendations

- Cotton Belt Transit Line connection to Bush Turnpike Station
- Plano Road at Renner Road intersection to remain at-grade intersection

He also reviewed the TIA recommended potential future CIP improvements that could be considered by the City and TxDOT.

Renner Road at US 75 Frontage Roads

- Modify the existing 5-lane SBFR approach to Renner Road to provide two left turn lanes (one shared with the U-turn lane), two through lanes and one right turn lane.
- Modify the existing 5-lane NBFR approach to Renner Road to provide two left turn lanes (one shared with the U-turn lane), two through lanes, and one right turn lane.
- Convert the existing northbound right turn lane to be a free right turn by the addition of an eastbound auxiliary lane on Renner Road between the NBFR and the eastbound right turn bay at Routh Creek Parkway.

Renner Road at Plano Road

- Convert the existing northbound right turn lane to be a free right turn by the addition of an acceleration lane on eastbound Renner Road.

He noted that significant levels of roadway capacity enhancements including additional turn bays and auxiliary lanes are proposed as part of the regulating plan to maximize the efficiency of the roadway network. He pointed out that the current Master Transportation Plan calls for the Plano Road at Renner Road intersection to be grade separated and the TIA concludes that it could remain at-grade.

Mr. Mitchell asked about the timeline for the improvements and funding responsibility. Mr. Carter stated the developer's mitigation would be implemented as the development takes place, meaning the time line is based on the development. The auxiliary lane further to the south might go in at the beginning. The improvement at Plano and Renner might be constructed when a significant development, but wouldn't necessarily have to construct it all at once.

Mr. Murphy asked if the volume count was based upon existing road conditions outside the City remaining the same and asked if they took into account that Bush would eventually connect to the east to I-30. Mr. Carter explained that the TIA adds in the background traffic first; evaluate that and then add the development to the background traffic. He stated there would be some growth that would not be attributable to the development.

Mr. Townsend asked about the development process with regard to minor modifications. Mr. Chavez responded that minor modifications are defined in the proposed code and provided examples of a minor modification. He likened it to an appeal of a building code.

Mr. Chavez continued the presentation providing a comparison chart of development standards to other similar types of developments; Legacy Town Center, Waters Creek and West Village. He noted that there are no maximums within the West Village or Waters Creek developments. He stated the proposed development seemed to be in line with the comparable sites. He noted

the major element in the subject sites is the rail station. Mayor Slagel asked about a comparison of retail and Mr. Chavez stated it is within the planned commercial, which includes retail and hotel. Mr. Mitchell asked about heights at the comparable sites and Mr. Chavez stated there were no minimums. Mr. Solomon asked if the 1.5/1 FAR was a ratio for multi family and other uses and Mr. Chavez replied affirmatively. Mr. Murphy asked Mr. Chavez to address the minimum height standard. Mr. Chavez pointed out that West Village, Waters Creek and the Legacy Town Center building heights are written into the standard zoning ordinance and were not planned development districts. He reviewed the Plan Commission recommendations and described the applicant's proposed revisions since the December 21st Plan Commission meeting.

Applicant Proposed Revisions since the City Plan Commission meeting

Bush Central Station

- Changed the building material requirements along Plano and Renner Road to be the same as for a Type A street
- Added "curtain" before glass under building materials
- Added Plano, Renner and PGBT access road building frontages within specific character zones to require buildings to be built to Retail Ready Standards

Caruth

- Added a periodic review provision
- Excluded multi-family from Arterial Mixed Use Character Zone
- Added a minimum 50' building height to the TOD Core Character Zone
- Excluded the CPC recommended minimum 80' building height from TOD Freeway HI-Rise on the west side of US 75
- Amended the minimum building height from 80' to 50' in the TOD Freeway Hi-Rise adjacent to the rail line
- Increased the maximum building height from 100' to 180' in the TOD Freeway Hi-Rise zone, south of future Infocom
- Added retail ready depiction to Street Sections

He noted the following key points of the proposal: both are planned development district; proposed standards equal to and/or greater than current City standards; all other codes and ordinances still apply; developable acreage is significantly less than gross acreage; normal staff review procedure is in place; there is CPC and City Council oversight; and proposed standards inhibit first floor residential. He also reviewed PD standards that drive high quality mixed use. He concluded the presentation with topics for refinement which included the need for a periodic status review, multi-family caps, construction quality, and location; minimum building heights and final mix of land uses. Mr. Keffler underscored that the topics of refinement have been discussed at length with the applicants. Mayor Slagel noted phasing is another topic that people are concerned about and needs to be discussed.

Mr. Omar asked about floor to floor height and asked what it meant for ceiling inside the floor. Mr. Chavez replied that it creates a 10' air space, which would be the typical office or live-work unit and definitely a residential unit and would provide for a minimum 8' ceiling height.

Mayor Slagel invited the applicant for Item 5 to present the application.

Bill Dahlstrom, 901 Main Street, Ste. 6000, Dallas, on behalf of Bush/75 Partners, LP, stated they are really excited about the project and have property owners who has invested in the community and is in it for the long haul. He stated the project is a unique opportunity to make a real statement for the rest of the country; to provide a quality development to help recruit some of the talented employment for the blue chip companies in the City of Richardson. He stated his role is to address some of the questions heard. He voiced their agreement of a cap of 2,400 multi-family rental units that does not include townhomes or condominiums. He agreed that buildings on no less than 50% of the net acreage north of Infocom Drive must be constructed in accordance with the proposed standards for structures with over five stories in height, which is a way to say with concrete and steel. With regard to the tract of land east of Plano Road, he offered deed restrictions prohibiting multi family units within 600 ft of the north boundary of Renner Road into the 150 acres. He voiced agreement that a property owners association for the Bush Central property would appear before the Council in a work session every three years with a report on the progress of the development to include the square footage of the buildings that have actually been completed; the number of urban dwelling units/residential units that have been completed; photographs showing compliance with the design guidelines; and a confirmation plan that shows the extent of the development within the Bush Central 57 acre development. He agreed that they would prohibit two story apartments within the development, which effectively addresses the minimum height in the multi family units. He felt the TIA addressed the issues of concern for traffic congestion. With regard to green space, he stated that the proposal requires open space; a large green area along Routh Creek Parkway; a mandatory Plaza; a mandatory green; a mandatory square and the streetscape itself includes public realms. With regard to the park space along Routh Creek Parkway, he noted that only a small portion of it is in the flood plain so the majority would be developable. He relinquished the floor to Scott Polikov to address the design issues.

Mr. Mitchell asked about the offered deed restrictions on the property east of Plano Road that was not part of the request as it relates to the number of apartments it would still allow. Mr. Dahlstrom stated the deed restrictions were to provide a buffer but had not focused on the use of the property because it was not part of the proposed zoning case.

With regard to the area south of Infocom, Mr. Omar asked about minimums and Mr. Dahlstrom replied it would be the minimums as set forth in the code.

Scott Polikov stated the form based code would be the regulating plan for Bush Central Station and presented an illustrative plan that is not part of the ordinance, but is what was developed based on the market analysis, based on the shape of the property, based on leveraging the park, and based on the relationship to the Caruth properties. He stated the plan embraces the entire area as one organism. He began the presentation with a concept drawing reflecting the type of development that could be expected and noted that the 15 story building conceived of as a hotel could include some condominium units and would be constructed with steel and concrete. With regard to what guarantees the quality of the construction is the level of detail in the form based code, which exceeds the minimum standards in the City's current code. He felt it is the totality of the ordinance that will insure the quality. With regard to the floor to floor for the second floor, he stated it was a 10' minimum and would likely be exceeded because many people like airier residential areas. Mr. Polikov noted that the Comprehensive Plan calls for a transit oriented development and sustainability, which the proposal includes. He referred to the study by the Urban Land Institute, which is a fairly conservative organization primarily consisting of developers and investors. He reiterated that development of the Bush/75 area was a prime opportunity in the region and the Country. He stated that Richardson is poised to really take advantage of providing an area for an influx of people over the next 40 to 50 years. Referring to

the Regional Transportation Council, he stated the approved 2035 program has deprogrammed 50 billion of new highway capacity, most of it in the fringes of the Metroplex, which means that the Cotton Belt corridors and other future rail corridors that will connect to the Cotton Belt and the connections to the DART corridors, becomes the place to attract the high quality development. This makes the opportunity at Bush Central more important. It's significant because the young professionals, as reported by the *Wall Street Journal*, the Gen Ys represent a higher demographic and noted the key finding that they want to walk everywhere. He stated that stick construction can withstand decades and can be reinvented. He felt that providing a mix and having the kind of proposed quality, it provides a full range of housing opportunities. He stated that much of West Village is stick construction. He stressed that form based code is not an open checkbook as some have characterized it. He stated that form based code focuses on form and design and the conventional zoning focuses on use. He stated it is literally impossible to do all apartments and underscored that garden style apartments will not be allowed. He gave Craig Ranch in McKinney as a good example of a form based code with almost 3 million square feet of non-residential constructed in 8 years with no minimum mix. He addressed protections that were provided in the code such as the prohibition of residential units on the ground floor for the first 30' of Main Street frontage. The reason to not prescribe a minimum mix in every building is to let the designers be creative and figure out exactly where the mix should be. He stated the streets are the most important part of the neighborhood and stated that most of America was based on form based code, but it was called that at the time. He stated most of the great downtowns were not prescribed a use mix from a building site to building site but were prescribing the development outcome and street types. He provided photos of buildings in major cities that reflect the strict standards proposed. Mr. Polikov pointed out that the code provides very stringent standards on hiding structured parking so that the parking structures don't overwhelm the project. He reviewed the checks and balances imposed in the code to ensure compliance the extensive protections in terms of building and design standards. He stated that no other ordinance at the zoning level in this region or the United States has the same level of prescription at zoning for the street design. He stated the proposed code includes very clear identification of the types of things that would be considered minor modifications. With regard to minor modifications being denied, Mr. Polikov explained that it wasn't that the minor modifications would be heard by Council, it was whether an appeal of a minor modification would be heard by the CPC or Council and the decision was for it to go to Council. He provided the following regulations:

Minor modifications are minor changes to the Bush Central Station Code within specified limits to the following elements of the Code:

- Area of Character zones $\pm 15\%$ change only
- Location of any mandatory street may move 100' in any direction
- 10% change in any street setback line
- No more than 20% change in any build-to line or zone
- No more than 15% reduction in the building frontage standard
- Waiver of street screen required along Type 'B' streets (under limited conditions only)
- Minor adjustments to street cross sections

Mr. Polikov stated that if something doesn't come in that meets the defined block patterns, development characteristics, uses, or building placements, it must go to Council for approval.

Roy Wilshire, Kimely Horn & Associates, pointed out that the plan proposes a very rich network of internal streets that bring traffic to the adjacent roadways. In addition, he referred to the auxiliary lanes and additional lanes to be built on Plano Road and Renner Road, and other

proposed lanes. He emphasized proposed traffic volumes compared to existing. He stated that staff limited the TIA to a 10% transit reduction and stated that he felt the transit reduction could be significantly more, which would drive the other numbers down.

Mr. Polikov noted that Mr. Dahlstrom, Mr. Willshire and the applicants have big ties to Richardson and would not have met with staff and the community as much as they have if they were just trying to get a zoning entitlement to flip the property. He reiterated that appeals to minor modifications will go to Council and reinforced that if the neighborhood character zone is done and is optional on the southern half with lower intensity with the townhomes; we would not require that 'A' street to be retail ready. He recapped the responsive code changes as follows:

- Increased the masonry requirements to higher than the City standards in some cases;
- Established the appeals process
- Effectively turn Plano and Renner Road into Type 'A' streets with about 80% of all streets are Type 'A' streets – retail ready streets
- At least 50% of the development by net land area north of Infocom Drive shall be constructed in accordance with standards in the City's building code for structures exceeding five stories regardless of height
- Cap of 2400 units for multi family and doesn't include attached single family or townhome
- One and two story multi family is prohibited
- The property owners association shall present a detailed report of the status of the development every three years to the City Council

In closing, Mr. Polikov felt this was the most exciting project in the United States and thought it would be a tremendous legacy. He felt the proposed code would build on the TOD strategy of the Telecom Corridor. He noted that they met with representatives of the blue chip companies who said the urban residential neighborhood component is critical to differentiating the area to recruit the best employees.

Mr. Murphy voiced a concern about the other 50% of construction that does not include concrete and steel and asked why the proposal did not go totally top quality; he also questioned the use of EIFS. Mr. Polikov stated that some of the highest rents are in areas without concrete and steel construction. If stick construction is eliminated, Council would not see the kind of urban residential and the mix move into the area. He felt that some of the highest quality projects in the region have the combination and speak for themselves. The location and the fact that the buildings will reinvent themselves is the assurance that the buildings will be maintained over time. He felt the proposal strikes the balance of being able to deliver to the market housing for the new graduates without being low quality. He felt the Spring Valley apartment example is off base and stressed the code prohibits anything like the development on Spring Valley. Mr. Murphy stated his appreciation for providing a cap on the number of multi family units. Mr. Polikov stated that they arrived at the 2,400 units in the TIA because it was a conglomeration of five or six factors including the market study, design and the location. He felt what would be developed is retail, hotel, office and entertainment.

Mr. Omar agreed that the development area is a crown jewel and stated that he has bought in to the concept of the form based code to provide flexibility and to allow a building to be reinvented. He felt that 10' ceilings should be minimum, rather than 8', which would make the floor to floor closer to 12 ft. He also suggested the first floor minimum should be 20' rather than 15' and felt that most would come in at more than 15 ft. Mr. Polikov felt that the 10 ft floor to floor was worth talking about, but felt a minimum of 20' would result in a malicious obedience with lower quality.

He felt that 15' is a threshold that will make it likely that you'll have accidents in almost all cases. Mr. Omar voiced a concern with the stick structure being able to be reinvented and stated he is worried that a 50% threshold would make it less than a crown jewel. Mr. Polikov stated it isn't true that stick construction can't be reinvented, but if it is a problem, even in a great area, it can be taken down. He further stated that places that are all steel frame are sterile, unimaginative and not reinventable.

Mr. Solomon stated he liked the 2400 unit cap on multi-family, including condominiums and excluding townhomes. Mr. Solomon stated he preferred using the term residential units rather than apartments because it was a broader context that means that we can have condos in the cap. Mr. Polikov stated that the term apartment is not in the ordinance and multi family is the term that should be used to capture rental and condominiums. Mr. Solomon stated he like the ownership factor better than the rental. He was concerned about a concentration of multi family without the mix being developed. Mr. Polikov replied that it is impossible to build 1200 units of residential on the site given the prohibition of one or two story apartments, the parking requirements, the vertical nature of the project and the requirement to build commercial ready ground floor. He stated that the first phase of the development is likely to be along Routh Creek and Infocom. Everything along Infocom on the ground floor has to be at a 30' depth, non residential. Everything along Routh Creek has to be a minimum 15 ft; flush entries for ADA; the building has to be up to the sidewalk; it has to be commercial code standard, which increases the per square foot cost tremendously, and is the power of the form based code. He stated that it is a system of elements that come together that assure that it's not going to be all urban residential.

Mayor Slagel asked about sustainability and talked about the concern with degradation of EIFS exterior material and concern about required phasing to make sure it is developed as proposed. Mr. Polikov stated that EIFS is not allowed on 'A' streets and not allowed on the ground floor of 'B' streets. He stated the market did a great job of developing Uptown and Legacy even without the development standards included in the proposal and did not have phasing requirements. He stated that the 'retail ready' requirement on all of the 'A' streets in blue and no residential allowed on the core street, by definition will require a phasing of mixes of uses. He felt there would be more mix and more aggressive phasing with the form based zoning. Mayor Slagel underscored that Council would like to see office, quality retail and enough residential to drive retail. Mr. Polikov stated it's the cool factor that makes the area attractive to the kind of retail desired rather than the number of units and stated it is the quality of the residential that creates the cool place.

Mr. Mitchell asked about the level of commitment to the proposed streets and elevations. Mr. Polikov stated that the streets illustrated were included in the code and further noted that every street has to be constructed as described in the code. The regulating plan guarantees the types of streets shown. Mr. Polikov stated that they believe that the details matter in the project and that the details will require the elevations of the constructed building to look something like the illustrations provided. He further stated, however, that the style and architectural application could change but the type of building frontage as depicted would occur. He stated that the probability of having a significant amount of building frontage along Routh Creek above five stories is very likely, but could not be guaranteed. Mr. Mitchell stated he would like to see more emphasis on the office component and less emphasis on the multi family piece. He stated he would like to see more buildings like BCBS and other types of office buildings and asked how to reserve areas for those things. Mr. Polikov stated the Council should consider all of the property in both cases and noted that there is plenty of capacity for millions of square feet of office. He felt the 2400 cap helps in that regard and as well as the required concrete and steel

construction creates the reserve. Mr. Mitchell asked again for an estimate of the number of apartments that could be constructed on the property east of Plano Road. Mr. Polikov stated he wanted to be very careful that they are not crossing over into contract zoning; he stated they have not tried to calculate a number; it could be a big number. He underscored that the deed restriction on the property was not contingent on the zoning case before the Council.

Mr. Macy stated he is impressed by all of the people involved and asked about LEED design. Mr. Polikov asked that Mr. Good be allowed to respond to the environmental question. He stated that it was the applicant's opinion that the 10% transit capture was low and noted that every street is bike friendly whether includes a bike lane, and noted that the carbon footprint on a per household or per person basis relative to the rest of the region, it would be very low.

Mr. Townsend stated he would really like to see them submit a zoning request for something other than multi-family on the property east of Plano Road. He stated his concern about the commercial to residential ratio and asked what the ratio was for West Village. Mr. Polikov stated it was a floor to area ratio rather than a use ratio and stated that the West Village should be viewed with the thousands of urban units and offices around it.

Mr. Solomon asked for the amount of developable acreage and Mr. Polikov replied that about 29 of the Caruth property is net development area, with a lot of it being streets, sidewalks, green space and parks. He stated a very little bit of it is in the flood plain.

Mr. Omar expressed a concern about not getting the very best result around the park and Mr. Polikov stated the developers don't have any development pressures on them; no interest clock ticking and the longer they wait, the better off they are in terms of the development potential and are aligned with the Council's interest. He felt that all of the other factors add up to the cost of delivering a building and will drive good quality.

Mr. Murphy asked about deliveries, trash pickup and other issues. Mr. Polikov stated that they worked those issues with Mr. Wilshire to reflect a very good deployment of those needs. In answer to Mr. Mitchell regarding the apartments on the property on the east side of Plano Road not included in the current zoning application, Mr. Polikov stated that approximately 500 units would be reduced in the 600' buffer, so it would move to approximately 1500 possible units.

Mayor Slagel opened the floor to the applicant for Item 6.

Larry Good, Good Fulton & Ferrell, advised that Gateway Planning wrote the proposed code. He stated that they could not write a zoning ordinance or prepare a planned development district ordinance until a master plan has been developed that they feel can be delivered so the illustrative plan provides that. He stated their experience allows them to be pretty accurate with their predictions with how the property would develop and how it has to develop to match the economics of the site and the Council's vision. He reminded everyone that the route for the Cotton Belt to get to this station comes about because his clients have agreed to dedicate that property and make it available. He stated that the Cotton Belt would be an elevated rail over Central that runs through the property. He stated that approximately 25 acres is set aside as part of the Spring Creek greenbelt. In addition to the existing trail system that is under construction, the regulating plan obligates them to cross the creek and bring the trail up to the Station. The streets shown in the plan are highly defined and they must build it. He expected that all of the Central Expressway/Bush Turnpike intersection tracts are going to be most appealing to office users. The residential units with ground floor retail will occur in the TOD area immediately adjacent to the Station. He stated that to create a TOD that is pedestrian friendly,

that does feel good to people living there and shopping there is the building do have to have the mix of scale. He used photos of similar areas to illustrate his comments. He talked about the character zones and the mixture of buildings. He agreed to a cap of 1,500 total multi family units; Multi family will not be permitted in the arterial mixed use character zone; agreed to a minimum height of 80 ft on several blocks; and agreed that 53% of street frontages on the east tract require ground floor retail. With regard to construction quality, he stated that the minimum height forces concrete or steel construction types; EIFS limited to 15% of any façade, and only above 8 feet in height; and considerable detail of design standards. He stated that they don't want to lose the case over EIFS. He stated the benefit of using EIFS is for those cornice moldings and trims that are in shapes that are too expensive to create in stone. He stated they would be amenable to changing the 15% to language that allows its use for decorative moldings would be acceptable. He reiterated the comments by Mr. Polikov that the details in the code of the buildings assure the quality of the development. Lastly Mr. Good included language in the ordinance to address periodic assessment of 60 months after the approval of the first building permit and at a frequency of each 36 months thereafter.

Mr. Mitchell asked about the proposed height of 50 ft rather than 80 ft. Mr. Good stated the core blocks and the two blocks north against Bush Turnpike are proposed to be 50 ft and 80 ft on the blocks fronting Central on the east side of Central. He referred to the need to have sunlight on the streets in order for the right atmosphere to be created. Mr. Mitchell voiced a concern with the PGBT overwhelming the proposed buildings and Mr. Good stated it wasn't about PGBT, it was about the environment for the people within the development. He clarified that the minimum height on the access challenged southwest corner of US 75 and Bush is something that they cannot lived with. He stated the tract must be accessed either by exiting before Alma and continuing on the frontage road of Bush or existing north of Plano Parkway is you are heading south. He stated it would be an office site and it would be very difficult to entice something larger than a three story user.

Mayor Slagel asked about the type of construction on the remaining 47% of the property and Mr. Good stated that to make ground floor functional a concrete podium is constructed, which can have as much as four stories of wood frame above that podium. So it is likely to have a five story building with a parking garage. He stated the Bush the frontage is not a nice environment so the master plan reflects the parking garage pushed up against the Turnpike and wrap around it and conceal it with residential and retail. Mr. Good explained that wrap style residential is developed frequently in seven story concrete frame so concrete and steel construction is not prohibited. He reminded Council that the parking garages can't be exposed to view on certain streets.

Mr. Omar voiced a concern about lowering the minimum height from 80 to 50 feet and felt it would not maintain the level of standard. He asked him to talk about where apartments and townhouses would be allowed. Mr. Good felt the land was too expensive for townhouses anywhere and the difference between townhouses at 8-12 unit per acre range and the proposed development, which is 100 units per acre if it is five stories, is already reflecting land value with what it's been pointed toward even with the 50 ft minimum height. Mr. Omar stated that many have embraces the concept of multi family close to rail but if he feels the most logical use is going to be office, he would prefer it be formalized on paper. He reiterated his desire to have 10' ceilings to insure a quality development. Mr. Good stated the market is driving 10 ft ceilings and to get it, one customarily has an 11'8" floor to floor. Mr. Omar stated he just wants to insure the quality that has been stated and it is his opinion that 10' ceiling is indicative of a quality unit. With regard to the proposed 15 ft ceilings for commercial / retail development, Mr. Good said

that generally retailers are looking for 14 – 16 ft ceilings and though the 15 ft ceilings were in the right range.

Mr. Murphy voiced his preference of 10 ft ceilings as well and was not opposed to regulating the height. He questioned the 50 ft minimum along the toll road. Mr. Good stated they could not support it because it removes some flexibility for the scale of the blocks, which is crucial to the success. He stated that this is a neighborhood with different scales of buildings and different uses that creates a sense of place.

With regard to comments from the audience, Mayor Slagel noted there were three categories; in favor, in opposition and neutral, and stated speakers in opposition would speak first. He also noted that the applicants would be given an opportunity to respond to the comments. He reiterated that Council would not make a decision this evening. He asked the City Secretary to begin calling speakers in opposition to the podium. Ms. Schmidt noted there were a total of 37 cards submitted with the majority being neutral.

Jim Shepherd, 1811 Normandy Drive, spoke in opposition to the request and to form based zoning. He encouraged the Council to zone the property for use desired.

David Clelland, 2706 Foxboro Drive, presented various numbers regarding density, population and population growth. He questioned the impact on the school district, on the quality of life and urged the Council to give the request due consideration. He spoke in opposition to the request.

Larry Bassuk, 2907 Wyndham Lane, stated he was looking for assurances that the property would be developed as proposed and stated his understanding that it was the ordinance that would provide the assurances. He stated there were good points raised and was in favor of the request.

Darrell Day, 1303 Chickasaw, spoke against the request due to the number of apartments and spoke about the need to have assurances from the developer.

Karl Voigtsberger, 411 Canyon Ridge Drive, voiced concern about the density of the apartment housing. He questioned if all of the facts were being presented. He felt there were too many unanswered questions and encouraged the Council to take a much closer look at what was being proposed.

Rama Nune, 3204 Owens Blvd., voiced concern about the proposed increase in multi-family units. He wanted to support the development aspect but felt the density was more than the neighborhood would support. He also voiced concern about increased traffic congestion.

Evelyn Roberson, 2217 Victoria Lane, felt the form based code allows the developers to set their own development standards; voiced concern about a lack of oversight; was concerned with extremely dense capacity and felt that density breeds decline and crime. She also questions the costs of widening Plano Road and Renner Road and was opposed to the requests.

Maitri Smithhisler, 2201 Victoria, Chair of the Neighborhood Protection Alliance of Richardson (NPAR), stated that she felt the residents should have been allowed to speak before the staff and applicant presented the application as requested. She felt the Plan Commission did not adequately vet the requests. She stated the residents want a successful, truly mixed development but it needs to have the greatest possible due diligence to make sure it happens.

She stated that other speakers would present questions and she felt the answers would generate more questions and discussions.

Nancy Husky, 1408 Ridgemoor Lane, Vice President of the Sherrill Park HOA, asked if a comparison had been done to the example developments to identify other less visible yet material differences. She also asked about setbacks.

Katherine Cargile, 1605 Woodoak Drive, Vice President of Owens Park NA, asked if an analysis of the urban forest had been completed and if there had been an analysis of the creation of a recreation area using the vacant 23 acre lot of undeveloped land at the southeast corner of Lookout Drive and Plano Road.

Shelley McCall, 538 Highland Blvd., President of Highland Terrace, asked for the resume and credentials of the third party expert engaged to provide an ongoing objective perspective of the development.

Robert Pavelick, 1627 Aurora Drive, Co President of Yale Park HOA, asked what would happen if the Cotton Belt goes through Plano rather than Richardson.

Chuck Johnson, 2304 Primrose Drive, Vice Chair for the Civic and Environmental Committee for Crowley Park, talked about the need for an analysis of successful form based zoning projects; asked if the staff thoroughly evaluated the form based code to insure that it exceeds the City's building requirements and public safety codes; and asked if an analysis of additional resources compared to projected revenue had been completed.

David Ruffino, 2103 Briarwick Court, Past President of the University Estates North HOA, asked questions regarding the ULI study.

Bill Kinder, 2662 Daffodil, President of the Crowley Park NA, stated that a thoughtful, well done TOD could be beneficial but was concerned about the proposed density and the traffic impact on Renner Road. He asked if Council has had any interaction with the people who would develop the property.

Darryl Grant, 4420 Binley Drive, President of Creek Hollow HOA, asked if it is correct that the City has offered an incentive to finance internal street infrastructure for the development; asked if it was common for the City to offer tax abatements to encourage economic development. He also asked about the amount of parking spaces that would be available and if the development would really would be pedestrian friendly.

Diana Clawson, 800 Westminster Drive, President of the Duck Creek HOA, asked if an analysis has been conducted to determine whether the plan meets the City's standard for park service. She asked about the flood plain and green space and asked if a floodplain and storm water management analysis had been completed. She also felt that the form based code would increase the staff workload and minimizes transparency at the Plan Commission and Council level. She voiced concern about density, traffic, and air quality.

John Wallberg, 1609 Woodoak Drive, President of Owens Park NA, talked about the level of disposable income that residents would need to sustain the development and also asked if an analysis had been completed on which major or local retailers would be interested in the location.

David Leister, 1904 Drew Lane, NPAR liaison for University Estates North, asked if the analysis and case studies that support 10% transit reduction be provided; asked if there minutes of meeting held to document the evolution of the project and asked if an analysis on realigning Renner Road between Plano Road to 190 had been conducted.

George Human, 1510 Amesbury Drive, Immediate Past President of Sherrill Park NA, stated he and his neighborhood had concerns of density, building heights, number of apartments and related traffic congestion.

Charlie Dorris, 2220 Shannon Lane, asked what would happen if the Cotton Belt does not run through Richardson; asked about control of the development; setbacks off Renner Road and Plano Road as well as height restrictions.

Verly Throckmorton, 1602 Pickwick, on behalf of Sue Oldham of the Sherrill Park NA, talked about concerns of increased crime, and asked about upstream impacts of flooding due to increased rooftops. He asked if there had been an analysis between the planner, the City and TxDOT for long term solutions.

Chip Pratt, 2700 W. Prairie Creek, Director of Community Relations for the Canyon Creek HOA, stated the Board is in favor of a high quality TOD development on the site that includes retail, restaurants, office and residential uses and is developed in a manner and quality similar to other high end mixed use developments. He stated their belief that the proposal does not adequately assure that the end result will live up to its vision. He urged the Council to set a maximum on the amount of density permitted for each type of use to insure an adequate mix of uses and a cap on the number of apartments. He noted concerns of traffic congestion and lowered levels of service.

Dennis Stewart, 5616 McKinley, stated that although not opposed to the project as presented, he asked if the city had committed its taxpayers to contribute \$25 million to the projects and if so, asked where the funds would come from.

Rick Wilder, 1614 Villanova Drive, asked if an environmental impact study was completed and stated a desire to see the study. He stated they would like to see the land developed in a responsible and realistic manner that will compliment the surrounding communities. He requested that responses to all of the questions asked be posted on cor.net at least two weeks prior to the next public hearing and stated he looked forward to an iterative and transparent process.

Cathy Jackson, 3303 Foxcreek Drive, asked if about the traffic analysis stated that she thought the inbound and out bound numbers would be closer and asked how many people the City expected would use DART. She also felt that many retail places would not want 20' ceiling heights because of the utility costs.

Jim Strand, 411 Belle Grove Drive, Chair of the Richardson Chamber of Commerce, stated the Chamber was in favor of the project and listed the positive aspects of the proposed development. He voiced concerns of assurances of a quality development; asked for requirements for phasing of construction to insure that it would not be strictly multi family; talked about the need for periodic review; traffic congestion and maintaining public safety.

Don Dillard, Galatyn Park Corporation, stated the request is visionary and spoke in support of the request.

Mayor Slagel stated the Council would continue discussion at the January 31 meeting. He stated staff would work to respond to the questions asked and would get as much information as possible posted on the City's website.

ACTION TAKEN ITEM 5 AND 6: Mr. Murphy moved to adjourn the meeting and continue on January 31; second by Mr. Mitchell and the motion was approved with a unanimous vote.

7. REPORT ON ITEMS OF COMMUNITY INTEREST.

None

The meeting was adjourned at 11:25 p.m.

MAYOR

ATTEST:

CITY SECRETARY



**City of Richardson
City Council Meeting
Agenda Item Summary**



Meeting Date: Monday, January 31, 2011

Agenda Item: *Visitors (The City Council invites citizens to address the Council on any topic not already scheduled for public hearing.)*

Staff Resource: Pamela Schmidt, City Secretary

Summary: Members of the public are welcome to address the City Council on any topic not already scheduled for public hearing. Speaker Appearance Cards should be submitted to the City Secretary prior to the meeting. Speakers are limited to 5 minutes and should avoid personal attacks, accusations, and characterizations.

In accordance with the Texas Open Meetings Act, the City Council cannot take action on items not listed on the agenda. However your concerns will be addressed by City staff, may be placed on a future agenda, or by some other course of resolution.

Board/Commission Action: N/A

Action Proposed: Receive comments by visitors.



**City of Richardson
City Council Meeting
Agenda Item Summary**



City Council Meeting Date: Monday, January 31, 2011

Agenda Item: ZF 10-21 – Bush Central Station -PD

Staff Resource: Sam Chavez, Assistant Director of Development Services SC

Summary: This request was continued by the City Council at their January 17, 2011 meeting.

The applicant's request is to rezone approximately 57.1 acres of land to a Planned Development District with modified development standards under Form Based elements. The site is located at the northwest corner of Plano Road and Renner Road. Of the 57.1 acres, approximately 28.6 acres of land is designated for development due to the preservation of open space.

Numerous citizens spoke in opposition to the proposed request.

Board/Commission Action: The request was considered by the City Plan Commission on December 7, 2010. The Commission voted 5-2 to recommend approval of the request with amendments.

Action Proposed: The City Council may approve the request as presented, approve with conditions, or deny the request.

Staff Update – Bush Central Station

Applicant's Code Revisions as of December 21, 2010

1. Added an appeals procedure to the City Managers decisions on minor modifications (Page 5, Section 3.8.6) and modified the associated Development Review Process flow chart accordingly (Page 60). **(CPC Recommendation)**
2. Changed the building material requirements along Plano and Renner Road to be the same as for a Type A Street (Page 31, Section 8.1.7, i. and ii. and Page 33 iii.). **(Applicant Proposed)**
3. Changed the masonry requirement along Type A streets to 85% from 80% and accent material to 15% from 20% (Page 32, Section 8.17, I and ii). **(CPC Recommendation)**
4. Added Plano, Renner and PGBT access road building frontages within specific character zones to require buildings to be built to Retail Ready Standards (Page 18, Section 7.1.4, Page 21, Section 7.2.4 and Page 27, Section 7.4.4). **(Applicant Proposed)**

Proposed by Applicant at City Council's January 17, 2011 Meeting

1. Limit the number of multi-family units to 2,400.
2. Added a periodic review provision – 3 year review.
3. Eliminate two-story multi-family in all Character Zones.
4. Add language to Section 5.6 (Page 13) to clarify that the ground floor retail ready requirement along Type 'A' Streets frontage will not apply if the Neighborhood Frontage is developed as townhomes.
5. Require all building on 50% of the net area, north of Infocom, to be constructed of concrete and steel.

Applicant's Additional Proposed Revisions Following City Council's January 17, 2011 Meeting

1. Change the minimum floor height for upper floors from 10 feet floor-to-floor to 10 feet minimum floor-to-ceiling for a major in any given floor plate in all Character Zones.
2. Restrict use of EIFS for decorative molding only.

Items Pending Clarification – Bush Central Station

1. Establishing the amount of masonry on non-public street facing building elevations to 80%, to conform with the City's standards for the percentage of masonry on each elevation.
2. Extending the requirement that all buildings on 50% of the net area be constructed of concrete and steel to the TOD Core Character Zone located south of Infocom.
3. Minimum first floor heights-Podium (minimum 15 feet floor-to-floor).
4. Establishment of 600 foot wide "no multi-family unit buffer" along Renner Road (Deed Restriction).



Staff Report

TO: City Council
FROM: Sam Chavez, Assistant Director of Development Services **SC**
DATE: January 31, 2011
RE: **Zoning File 10-21: Bush Central Station - PD**

REQUEST:

To rezone approximately 57.1 acres of land to a Planned Development District with modified development standards under Form Based elements. This request was continued at the City Council's January 17, 2011 meeting.

The "Staff Update" outlines current and applicant proposed amendments subsequent to the Commission's December 21, 2010 meeting.

APPLICANT:

Scott Polikov / Gateway Planning Group, Inc.

PROPERTY OWNER:

Joe Altemore / Bush/75 Partners LP

TRACT SIZE AND LOCATION:

Approximately 57.1 acres, north side of Renner Rd. between the DART Light Rail and Plano Rd.

EXISTING DEVELOPMENT:

The subject tract is undeveloped.

ADJACENT ROADWAYS:

PGBT: Freeway/Turnpike; 69,000 vehicles per day on all lanes, eastbound and westbound, at the Shiloh Road Toll Plaza; 155,000 vehicles per day on all lanes, eastbound and westbound at the Coit Road toll Plaza (December 2009).

Renner Road: Six-lane, divided arterial; 27,900 vehicles on all lanes, eastbound and westbound between Plano Road and DART Light Rail (March 2009).

Plano Road: Six-lane, divided arterial; 26,300 vehicles per day on all lanes, northbound and southbound between PGBT and Renner Road (March 2009).

SURROUNDING LAND USE AND ZONING:

North: City of Plano
South: Undeveloped; I-M(1) Industrial
East: Undeveloped; PD Planned Development and C-M Commercial
West: Undeveloped and existing Bush Turnpike Station; R-1500-M Temp and TO-M Technical Office

FUTURE LAND USE PLAN:

Transit Village and Regional Employment

Mixed or multiple land uses built around small-scale pedestrian blocks located at the City's rail stations. Uses include medium- to high-density residential, retail, entertainment, hospitality and offices.

Higher density development is appropriate with the primary use being high-rise office. Secondary uses include retail centers and entertainment venues.

Future Land Uses of Surrounding Area:

North: City of Plano
South: Regional Employment
East: Regional Employment
West: Transit Village and Regional Employment

EXISTING ZONING:

Planned Development and TO-M Technical Office District (Ord. No. 2588-A, February 1987) and I-M(1) Industrial (Ord. No. 1044-A, October 1976)

APPLICANT'S STATEMENT

(Please refer to the complete Applicant's Statement.)

TRAFFIC IMPACTS:

Please see Exhibit B

STAFF COMMENTS:

Applicant's Proposal:

The applicant's request is to rezone approximately 57.1 acres of land to a Planned Development District with modified development standards under Form Based elements.

The table below represents the current zoning entitlements for the subject property and the land use assumptions used for the purpose of analyzing traffic impacts associated with the request:

Current Zoning Entitlement Rights		TIA Land Use Assumptions		Difference
General Office	2,640,632 SF	General Office	600,000 SF	(2,040,632 SF)
Shopping Center/Retail	293,403 SF	Shopping Center/Retail	200,000 SF	(93,403 SF)
Hotel	0 Rooms	Hotel	400 Rooms	400 Room
Multi-Family	426 units	Multi-Family	2,400 Units	1,974 Units
Townhome	0 units	Townhome	100 Units	100 Units

In summary, the amount of square footage for non-residential uses anticipated in the subject request is reduced by a 2,134,035 square feet, and the number of multi-family units is increased by 1,974 units over what is currently allowed in the current zoning entitlements. In addition, 400 hotel rooms and 100 townhome units are being added.

The Traffic Impact Analysis (TIA) submitted by the applicant examined the subject request and the Caruth TODPD application (Zoning File 10-20) in a single analysis. The table below represents the current zoning entitlements for the subject properties and land use assumptions for both requests.

All Tracts Combined	Unit	Current Entitlement	TIA Land Use Assumptions	Difference
General Office	SF	5,172,584	2,207,000	(2,965,584)
Shopping Center/Retail	SF	293,403	386,000	92,597
Hotel	Rooms	0	400	400
Multi-Family	Units	426	3,765	3,339
Townhome	Units	0	175	175
Bush Central Station				
General Office	SF	2,640,632	600,000	(2,040,632)
Shopping Center/Retail	SF	293,403	200,000	(93,403)
Hotel	Rooms	0	400	400
Multi-Family	Units	426	2,400	1,974
Townhome	Units	0	100	100
East Caruth Tract				
General Office	SF	1,507,006	767,500	(739,506)
Shopping Center/Retail	SF	0	161,000	161,000
Multi-Family	Units	0	1,365	1,365
West Caruth Tract				
General Office	SF	1,024,946	839,500	(185,446)
Shopping Center/Retail	SF	0	25,000	25,000
Townhome	Units	0	75	75

In summary, the amount of square footage for non-residential uses anticipated in the proposed overall zoning scenarios is reduced by a total of 2,872,987 square feet and the number of multi-family units is increased by 3,340 units over what is currently allowed in the existing zoning entitlements. In addition, 400 hotel rooms and 175 townhome units are being added.

The proposed PD includes the following elements:

- Administration
- Definitions
- Regulating Plan
- Schedule of Permitted Uses
- Building Form Standards
- Architectural Standards
- Street and Streetscape Design Standards
- Signage Standards
- Civic Space Standards
- Appendix A: Regulating Plan
- Appendix B: Planting List
- Appendix C: Development Process Flow Chart

Administration (Section 3) establishes the provisions for reviewing and approving development applications to ensure that all development is consistent with the PD. *In addition to the PD requirements, all development plans are also subject to all other applicable City of Richardson Codes and ordinances, such as the Building Code, Fire Code, Engineering Design Standards, Thoroughfare Plan, Subdivision and Development Code and Sign Code.*

The PD outlines the basic steps for review of a development application. The outline serves as a checklist to ensure that the development plan complies with the applicable PD requirements. The following steps are to be taken:

- Locate the property on the Regulating Plan
- Identify
 - Character Zone
 - Street Type
 - Special Frontage Standards
- Review the Schedule of Uses by Character Zone
- Examine the Building Form and Development Standards
- Refer to Building Design Standards
- Refer to Street Type and Streetscape Standards

If the development plan does not comply with the PD, the plan is deferred to the City Manager or designee for interpretation, for review as a minor modification as prescribed in the PD, if appealed-forwarded to City Council for consideration or forwarded to the City Plan Commission and City Council for consideration as a Special Development Plan.

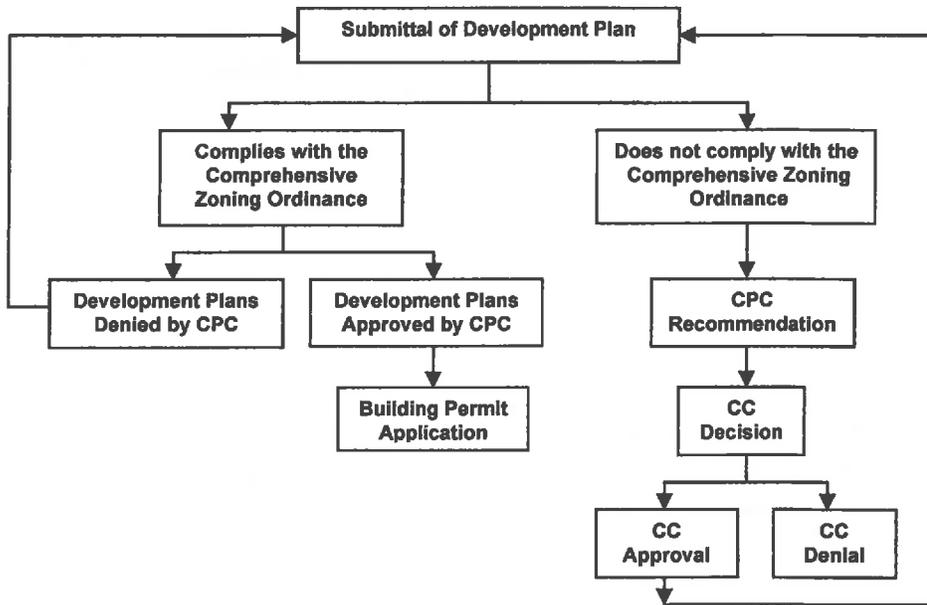
Minor Modifications (Subsection 3.8) designates the approving authority with regard to minor modifications to the PD and establishes thresholds (Table 3.1) for specific allowed modifications, including an appeal process for uses not listed in the PD.

- Minor Modifications (approving authority City Manager or designee)
 - Does not materially change the circulation and building location
 - Does not increase the building area permitted
 - Does not change the relationship between the building and streets
 - Does not allow a prohibited use
 - Does not increase building height or reduce required parking
 - Limited changes to a street cross section and Street and Streetscape Design Standards
 - Uses not listed, but are substantially similar (appeals are forwarded directly to the City Council)

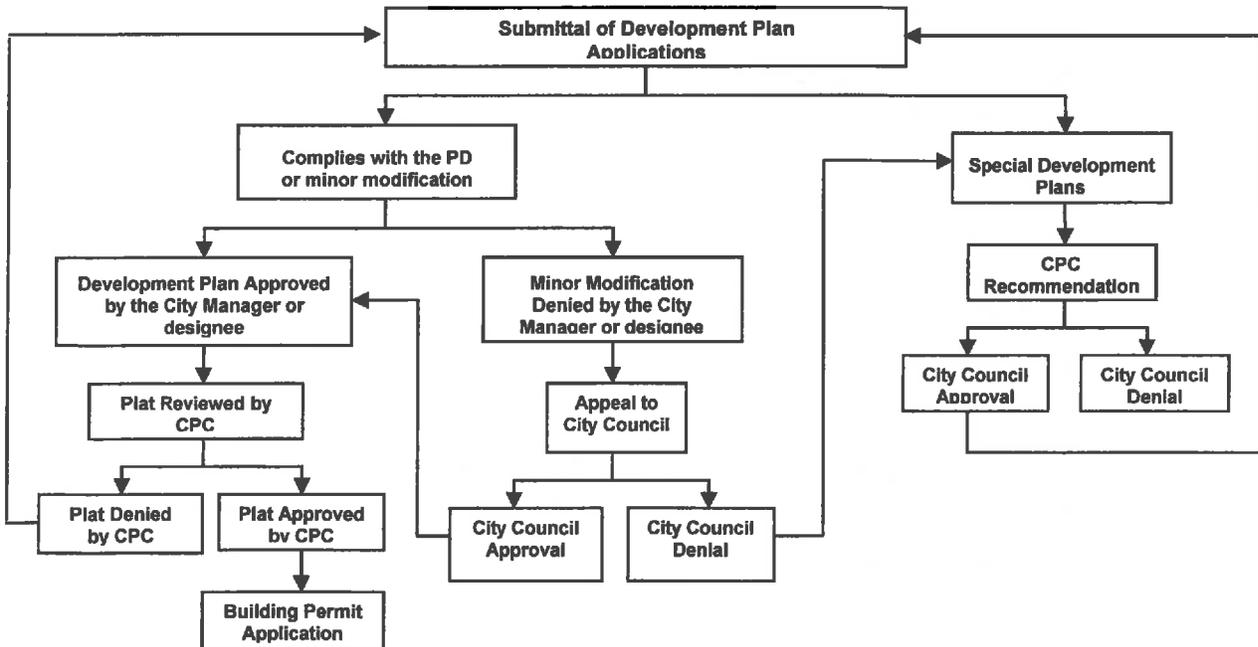
Special Development Plans (Subsection 3.7) establishes the approving authority for any proposed modification to the PD; other than those permitted as minor modification, and are consider major modifications and thus treated as Special Development Plans. Special Development Plans require the request to be reviewed and considered by the City Plan Commission and City Council.

The flow chart below depicts the City’s current development review process as a comparison with what is proposed by the applicant.

City of Richardson Development Review Process



Proposed Development Review Process



The proposed review/approval process defers from the City's current process in that site plans, building elevations, landscape plans, and civil engineering will be approved at staff level if the development application conforms with the PD, while development plats will continue to be approved by the Commission.

The PD

The Form Based elements include a Regulating Plan and the Development Standards.

Regulating Plan (Appendix A) represents the zoning map for the subject site and creates distinct character zones which are different from other areas within the subject site. Other elements on the Regulating Plan include Street Designations, Special Frontages, Civic/Open Space and Other Destinations. The four (4) Character Zones and a general description of each and allowable uses are indentified below. For a detailed list of proposed allowed uses in each Character Zone please refer to the Schedule of Permitted Uses (Section 6, Table 6.1) in the PD.

- **Character Zones**

- TOD Core (High Pedestrian Activity, Highest Density, Greatest Variety of Uses)
- TOD Mixed Use (High Intensity Commercial and Residential with supporting retail uses)
- Arterial Mixed Use (Transition to Regional Streets for Auto-Oriented Sites, Gateway to Development)
- Highway Mixed Use (Transition from PGBT and US 75, High Intensity Development)

General Allowable Uses (by Character Zone)

TOD Core, TOD Mixed, Arterial Mixed, Highway Mixed

- Retail Sales or Service, Office, Research, Food Service
- Arts, Entertainment, and Recreation
- Education, Public Administration, Health Care, Institutions
- Home Occupation, Multi-Family, Residential Lofts, Live-Work Units
- Hotel, Surface Parking (accessory), Structured Parking, Sales from Kiosk, Community Garden, Roof-Mounted Antenna, Special Event, Equipment (utility, wind, solar, rain harvesting)

Additional Uses (by Character Zone)

TOD Mixed, Arterial Mixed

- Drive-Thru
- Townhome

Arterial Mixed, Highway Mixed

- Auto-Related Sales and Service
- Surface Parking (primary)

Highway Mixed

- Drive-Through

- **Street Designations** (Mandatory and Non-Mandatory Streets) illustrates the design, configuration and standards of all streets and include the following street designations:

- TOD Main Street
- Parkway
- TOD Avenue

- Park Avenue
- TOD Street Type A
- TOD Street Type B
- Court Street
- Plano/Renner Slip Road
- Commercial Alley
- **Special Frontages** (Mandatory and Non-Mandatory) are applied to certain blocks to address specific requirement and transitions, and include the following designations:
 - Station Platform
 - Main Street
 - Neighborhood (Non-Mandatory) Only applicable if residential is developed
 - Special Designation
- **Civic/Open Space** (Mandatory and Non-Mandatory) creates a network of open spaces that provide passive and active recreational opportunities, and include the following designations:
 - Mandatory Park
 - Mandatory and Non-Mandatory Plazas
 - Mandatory Green
 - Mandatory Square
- **Other Destinations** (Mandatory and Non-Mandatory) indicates an area for a specific use with applicable standards, and include the following designations:
 - Mandatory Multi-Use Trail
 - Non-Mandatory Alley, Pedestrian Passage, Existing Trees, Gateway Element and Vista Terminus

Development Standards are established in text and graphic form for each Character Zone, and include Building Form and Development Standards, Building Design Standards, Street & Streetscape Standards, Signage, Civic/Open Space and related standards for all streets, public and private development. Development plans will be reviewed for compliance with each standard.

Building Form and Development Standards (Section 7) are established in text and graphic form for each Character Zone and include the following standards:

- Building Placement
 - Street-Setback Line
 - Build-To Zone
 - Setback
 - Building Frontage
- Block Standards
 - Block Face Dimension
 - Block Perimeter
- Building Height
 - TOD Core (maximum 300 feet)

- TOD Mixed Use (maximum 225 feet)
- Arterial Mixed Use (maximum 100 feet)
- Highway Mixed Use (maximum 300 feet)
- Special Destination Area (maximum 2 stories)
- Commercial Frontage Requirements
 - TOD Core (Ground floors of all buildings fronting on Type ‘A’ Streets and Plano Road built to Retail Ready Standards)
 - TOD Mixed Use (Ground floors of all buildings fronting on Type ‘A’ Streets and Renner Road built to Retail Ready Standards)
 - Arterial Mixed Use (Ground floors of all buildings fronting on Type ‘A’ Streets, Plano Road and Renner Road built to Retail Ready Standards)
 - Highway Mixed Use (Ground floors of all buildings fronting on PBPT service road and Plano Road built to Retail Ready Standards)
- Special Frontage Requirements
 - TOD Core (Main Street Frontage and Station Frontage requirements)
 - TOD Mixed Use (Neighborhood Frontage requirements)
 - Arterial Mixed Use (None)
 - Highway Mixed Use (Station Frontage requirements)
- Parking & Service Access
 - Parking Location
 - Off-Street Parking Standards
 - Driveways and Services
- Encroachments
 - Allowed for specific elements, require 8’ vertical clearance
- Applicability
 - Additional development standards

Building Design Standards (Section 8) are used to establish a coherent urban character, and includes the following standards:

- Building Orientation
- Design of Parking Structure
- Design of Automobile Related Building Site Elements
- Roof Form
- Façade Composition
- Windows and Doors
- Commercial and Mixed Use Building Materials
- Standards Specific to the Urban Neighborhood

The tables below depict the proposed building materials for the proposed development.

TOD Core, TOD Mixed Use, Arterial Mixed Use, Highway Mixed Use Building Materials

Proposed Building Material	Permitted by City PD	Bush Central Station
Ground Floor (Base)		
Masonry (brick, stone, stucco, cast stone, rock, marble, granite, curtain glass, glass block,)	Min. 85% of entire façade area (min. 80% per elevation) <ul style="list-style-type: none"> Proposed materials EIFS not permitted below a height of 8' 	<ul style="list-style-type: none"> Min. 85% along a Type 'A' Street, Plano & Renner Masonry or Accent when not along a Type 'A' Street, Plano & Renner EIFS not permitted along Type 'A' Street, Plano or Renner
Accent material (wood, architect metal, split-face concrete block, pre-cast concrete panel)	Max. 15% of entire façade area (max. 20% per elevation) <ul style="list-style-type: none"> Proposed materials EIFS not below a height of 8' 	<ul style="list-style-type: none"> Max. 15% along a Type 'A' Street, Plano & Renner EIFS not permitted along Type 'A' Street, Plano or Renner
Upper Floors (Middle and Cap)		
Masonry (brick, stone, stucco, cast stone, rock, marble, granite, curtain glass, glass block,)	Min. 85% of entire façade area (min. 80% per elevation) <ul style="list-style-type: none"> Proposed materials 	<ul style="list-style-type: none"> Permitted along streets and alleys EIFS limited to 10% on Type "B" Streets and alleys
Accent material (wood, architect metal, split-face concrete block pre-cast panel, cementitious-fiber clapboard with 50 year warranty and EIFS)	Max. 15% of entire façade area (max. 20% per elevation) <ul style="list-style-type: none"> Proposed materials EIFS not permitted below a height of 8' 	<ul style="list-style-type: none"> Permitted along streets and alleys EIFS limited to 10% on Type "B" Streets and alleys, not allowed on ground floors Clapboard limited to 15% along any street and alley, not allowed on ground floors
Roof material (copper, standing seam metal, slate, synthetic slate or similar materials)	Class 'C' Fire Classification (asphalt shingles, copper, standing seam metal roof, slate, synthetic slate or similar materials)	<ul style="list-style-type: none"> When visible from streets and alley (- asphalt shingles)

Neighborhood Frontage Building Materials (Residential)

Proposed Building Material	Permitted by City PD	Bush Central Station
Building Facade		
Masonry (brick, stone, man-made stone, stucco, cementitious-fiber clapboard with 50 year warranty)	Min. 85% of entire façade area (min. 80% per elevation) <ul style="list-style-type: none"> brick, stone, man-made stone, rock, marble, granite, glass block, split-face concrete block, 	<ul style="list-style-type: none"> Min. 85% along Type 'A' Streets EIFS not allowed on Type 'A' Streets or on ground floors
Accent material (architectural metal, rock, glass block, tile)	Max. 15% of entire façade area (max. 20% per elevation) <ul style="list-style-type: none"> Proposed EIFS not below a height of 8' 	<ul style="list-style-type: none"> Max. 15% architect metal panels or similar along streets EIFS not permitted along Type 'A' Streets or on ground floor Max. 10% EIFS on upper floors of side or rear facades
Roof material (asphalt shingles, copper, standing seam metal roof, slate, synthetic slate, terra cotta)	Class 'C' Fire Classification (asphalt shingles, copper, standing seam metal roof, slate, synthetic slate or similar materials)	<ul style="list-style-type: none"> When visible from streets and alley

- ❖ The sides and rear facades in the Urban.Neighborhood are to be of finished quality and the same color that blend with the public street facades of the building.

Street & Streetscape Design Standards (Section 9) specify typical street configurations for the development in text and graphic form. The standards include specifications for:

- Right-of-way widths
- Vehicular lane width
- Parkway widths
- Number of travel lanes
- On-street parking
- Pedestrian accommodations

Signage Standards (Section 10) are established to ensure compatibility throughout the development. Except as specially listed in the proposed Sign Standards section of the PD, all other signage shall comply with the City's Sign PD. For a detail list, please refer to Table 10.1 of the proposed PD. In general the following types of sign are proposed:

- Wall (Building)
- Monument
- Window
- Building Blade
- Tenant Blade
- Marquee
- For Sale/For lease
- Address
- Temporary
- Banner
- Sandwich Board
- Light Pole Banner
- Directory
- LED Signage (requires lens covers or diffuser)

Civic/Opens Space (Section 11) creates a network of open spaces and standards that recognizes the natural qualities of the area while providing a range of both passive and active recreational opportunities.

- Park
- Green
- Square
- Plaza
- Pedestrian Passage
- Multi-Use Trail
- Playground
- Ancillary Structure

On November, 7, 2010, the Commission and City Council met at a Joint Work Session to receive a presentation from staff and the applicant's on the proposed project. As a result of the work session, the following items were discussed:

The following were addressed and the PD has been amended accordingly.

- Require major modifications to be reviewed by the City Council (CC), after a City Plan Commission (CPC) recommendation...*The PD and Development Process Flow Chart been amended to reflect required action.*
- Prohibit EIFS as an allowed building material...*The PD has been amended to prohibit along Type 'A' streets and ground floors and limit EIFS to 10% on upper floors.*
- Permit LED lighting for lighting and signage...*The PD has been amended to require LED's to be covered by a lens or diffuser.*
- "Supergraphics" allowance...*The City's Sign Ordinance which does not allow "supergraphics" will supersede.*

The following were discussed with the applicant; however, they remain as items for discussion:

- Require minor modification to be approved by CPC
- Incorporate "green" design standards. (*Applicant Response: appropriate for CC&Rs*)
- Incorporate iconic elements/branding for the development. (*Applicant Response: appropriate for CC&Rs*)
- Incorporate additional architectural/design controls. (*Applicant Response: appropriate for CC&Rs*)

The following elements have been identified by staff that may require further refinement:

- Development Rights as they relate to the TIA
The Regulating Plan and the PD do not designate density and/or intensity values, which is common for a Form Based PD not to include. However, the appropriateness of the roadway network for the development is based on the land use assumption used in the TIA's findings. Establishing maximum number of units and maximum non-residential square footage could be considered as assumed in the applicant's TIA.
- Civic/Open Space Standards - Park Standards
The applicant intends to utilize the area located adjacent to and along the DART rail line; designated on the Regulating Plan as "Park", to meet the need of the residents of the development for active recreation opportunities. The subject site, with the exception of a multi-use trail that will be construction on the site is designated to be preserved. In addition, the Regulating Plan designates a 0.38-0.75 acre "Mandatory Green" located north of Renner Road, which may be impacted with the preservation of mature trees.

Throughout the development passive recreational opportunities will be provided, which will be in the forms of urban plazas/opens space. The details of either active or passive recreational areas will be further defined at the time of development.

Conclusion

Based on staff's review of the applicant's request, the proposal of a PD with Form Based elements appears to be appropriate for the site. The PD achieves a predictable community vision through its regulatory nature. It achieves a predictable physical result by its concentration on the visual aspect of the development through building heights, façade treatment and the relationship of the building to the street (pedestrian friendly) through compact, walkable urbanism. If applied appropriately, it could provide the community with the opportunity to respond to market demands in an expedient and predictable manner.

Motion: On December 21, 2010, the Commission on a vote of 5-2 recommended approval of the request, subject to the following special conditions:

1. The subject site shall be zoned PD Planned Development and shall be developed in accordance with the Bush Central Station Planned Development PD attached hereto as Exhibit "D".
2. The minimum percentage of masonry shall be 85% and maximum percentage of accent material shall be 15%.

Exhibit B

TRAFFIC IMPACT:

As part of the application, a Traffic Impact Analysis (TIA) was submitted by the applicants. The TIA was conducted by Kimley-Horn & Associates, Inc., a traffic engineering and planning consulting firm hired by the applicants to evaluate the combined impact of the developments on the roadway system. As part of the review process, the City hired an independent traffic engineering consultant, Lee Engineering, to perform a comprehensive evaluation of the TIA, its assumptions, and results.

The TIA examined both the Bush Central Station and Caruth TOPD requests in a single analysis. The applicant's consultant utilized the City's standard TIA guidelines and prior Kimley-Horn studies conducted in Richardson. As part of the study, new traffic counts were conducted at signalized intersections and on all surrounding arterial roadway and frontage roads.

The following scenarios were initially analyzed per the City TIA guidelines:

- 2020 Background Traffic (existing 2010 plus typical growth)
- 2020 Background Traffic plus Full Site build-out Traffic
- 2035 Background Traffic (existing 2010 plus typical growth)
- 2035 Background Traffic plus Full Site build-out Traffic

At the December 9, 2010 City Planning Commission meeting, members of the commission requested additional information pertaining to the existing traffic conditions of the roadway network so the public could better understand how the proposed development would potentially impact the roadway network. Following the CPC meeting and subsequent meetings with the public, City staff requested that Kimely-Horn conduct analysis of additional scenarios including the following:

- 2010 Existing Traffic
- 2010 Existing Traffic plus Full Site build-out Traffic
- 2035 Traffic plus Full Site build-out without any of the applicant's proposed mitigation
- 2035 Traffic with other potential intersection improvements at the Renner and US75 intersections which could be entertained by the City and TxDOT to combat the congestion generated by background traffic and future background traffic growth not associated with the development.
- 2035 Traffic plus Full Site build-out with additional background improvements and the developer's mitigation at the Renner and US75 intersections

The results of all these studies were be presented at the January 17, 2011 City Council meeting.

The existing 2010 traffic data collected by Kimley-Horn and the historical daily traffic volume counts conducted by the City are attached to this exhibit for review. The bar graph shows historical and current traffic volume counts for Renner Road and Plano

Road in the area of the development, and includes traffic volume counts for other arterial roadways in Richardson for comparison purposes.

To assess traffic impacts associated with the proposed development on the adjacent roadway network, basic land use assumptions were established by the applicants. The table below depicts land use intensity assumptions utilized in the TIA for the proposed zoning, as well as currently allowed land use intensities allowed by the existing zoning.

TIA Land Use Assumptions and Existing vs Proposed Zoning Comparison

All Tracts Combined		Units	Existing	Proposed	Difference
General Office	SF		5,172,584	2,207,000	(2,965,584)
Shopping Center Retail	SF		293,403	386,000	92,597
Hotel	Rooms		0	400	400
Apartment/Multifamily	DU		426	3,765	3,339
Townhome	DU		0	175	175
Bush Central Station Tract		Units	Existing	Proposed	Difference
General Office	SF		2,640,632	600,000	(2,040,632)
Shopping Center/Retail	SF		293,403	200,000	(93,403)
Hotel	Rooms		0	400	400
Apartment/Multifamily	DU		426	2,400	1,974
Townhome	DU		0	100	100
East Caruth Tract		Units	Existing	Proposed	Difference
General Office	SF		1,507,006	767,500	(739,506)
Shopping Center/Retail	SF		0	161,000	161,000
Apartment/Multifamily	DU		0	1,365	1,365
West Caruth Tract		Units	Existing	Proposed	Difference
General Office	SF		1,024,946	839,500	(185,446)
Shopping Center/Retail	SF		0	25,000	25,000
Townhome	DU		0	75	75

In summary, the amount of square footage for non-residential uses anticipated in the proposed zoning scenario is reduced by a total of 2,965,584 square feet and the number of multi-family units is increased by 3,339 units over what is currently allowed in the existing zoning entitlements. In addition, 400 hotel room and 175 townhome units are being added.

The table below depicts new automobile trip generations associated with the proposed development and trip generations associated with the currently allowed land uses.

Trip Generation Table (existing and proposed zoning)

New Auto Trips - Proposed Zoning

Proposed Tract	Daily Total	AM Peak Hour			PM Peak Hour		
		IN	OUT	TOTAL	IN	OUT	TOTAL
Bush Central Station	28,255	1,066	1,103	2,169	1,363	1,435	2,798
Caruth - East	19,237	966	634	1,600	866	1,232	2,098
Caruth - West	9,082	898	166	1,064	287	917	1,204
Totals	56,574	2,930	1,903	4,833	2,516	3,584	6,100

New Auto Trips - Existing Zoning

Proposed Tract	Daily Total	AM Peak Hour			PM Peak Hour		
		IN	OUT	TOTAL	IN	OUT	TOTAL
Bush Central Station	27,004	2,484	592	3,076	1,321	3,275	4,596
Caruth - East	9,696	1,301	177	1,478	270	1,320	1,590
Caruth - West	7,606	1,009	138	1,147	199	967	1,166
Totals	44,306	4,794	907	5,701	1,790	5,562	7,352

28%	-39%	110%	-15%	41%	-36%	-17% ²
Increase	Decrease	Increase	Decrease	Increase	Decrease	Decrease

Based on the findings of the TIA, the following roadway improvements were recommended as mitigation measures to be implemented by the developer. The figure attached graphically displays the location of each improvement.

- Renner Road:
 1. Westbound Auxiliary Lane – Plano Road to Routh Creek Parkway
 2. Westbound Deceleration Lanes at All Other Driveways
 3. Westbound to Northbound free-flow right-turn lane and auxiliary lane at US75 leading north at least to the first driveway
 4. Median Opening with Turn Lanes at Drive P2 (proposed street designated as “Park Avenue” on the Bush Central Station Regulating Plan, located between Plano Road and proposed Routh Creek Parkway)
 5. Traffic Signal at Drive P2
 6. Eastbound Left Turn to Routh Creek Parkway

- Plano Road:
 1. Southbound Right Turn bays / Auxiliary Lane – Bush Turnpike to Renner Road
 2. Northbound Left Turn Lanes at Infocom and Drive P5
 3. Traffic Signals at Infocom Drive and Drive P5 (proposed street designated as “TOD Street” on the Bush Central Station Regulating Plan, located on Plano Road, between Infocom and Renner Road)
 4. Improved Right Turn Lane to Renner Road with free-flow into auxiliary lane on Renner Road

- US-75 Frontage Roads
 1. Deceleration Lanes at All Driveways (All driveways and right turn bays to satisfy TxDOT Access Management Requirements)

- Other Transportation Recommendations:
 1. Cotton Belt Transit Line Connection to Bush Turnpike Station
 2. Plano Road at Renner Road Intersection to remain as an At-Grade Intersection in the City’s Master Transportation Plan.

While the TIA identifies the measures which should be the responsibility of the developer to return traffic operations to the background conditions, the following modifications are also recommended if the City desires to improve the background operating conditions shown in the 2035 background scenarios. With these measures in place, there are no LOS F conditions in 2035. These recommendations would not be the responsibility of the developers and are also shown in an attached figure.

Renner Road at US 75 Frontage Roads

1. Modify the existing five-lane SBFR approach to Renner Road to provide two left-turn lanes (one shared with the U-turn lane), two through lanes, and one right-turn lane.
2. Modify the existing five-lane NBFR approach to Renner Road to provide two left-turn lanes (one shared with the U-turn lane), two through lanes, and one right-turn lane.

3. Convert the existing northbound right-turn lane to be a free right turn by the addition of an eastbound auxiliary lane on Renner Road between the NBFR and the eastbound right-turn bay at Routh Creek Parkway.

Renner Road at Plano Road

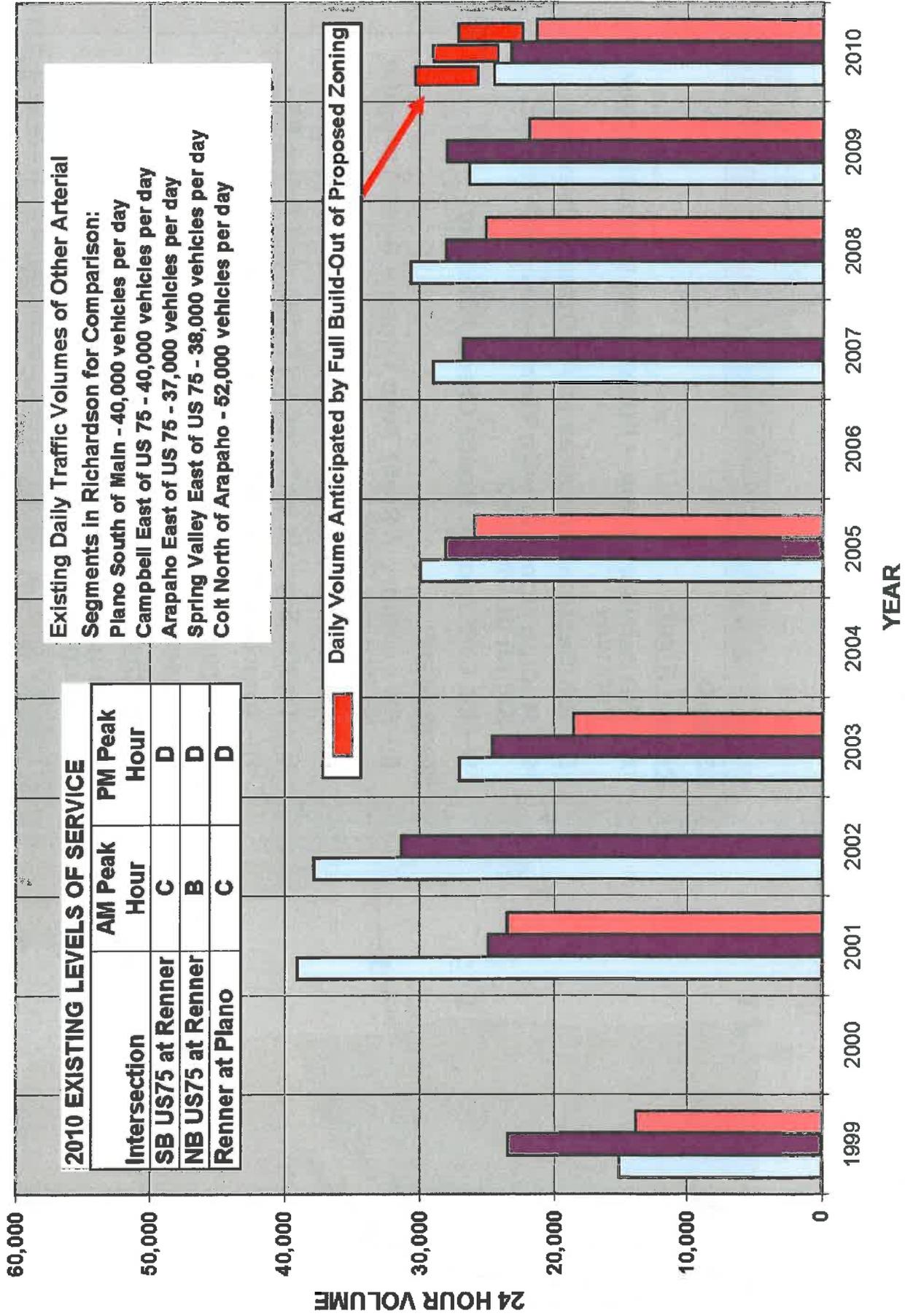
1. Convert the existing northbound right-turn lane to be a free right turn by the addition of an acceleration lane on eastbound Renner Road.

After a thorough review of the TIA and its recommendations, City staff and staff's traffic consultant (Lee Engineering) were able to conclude the following:

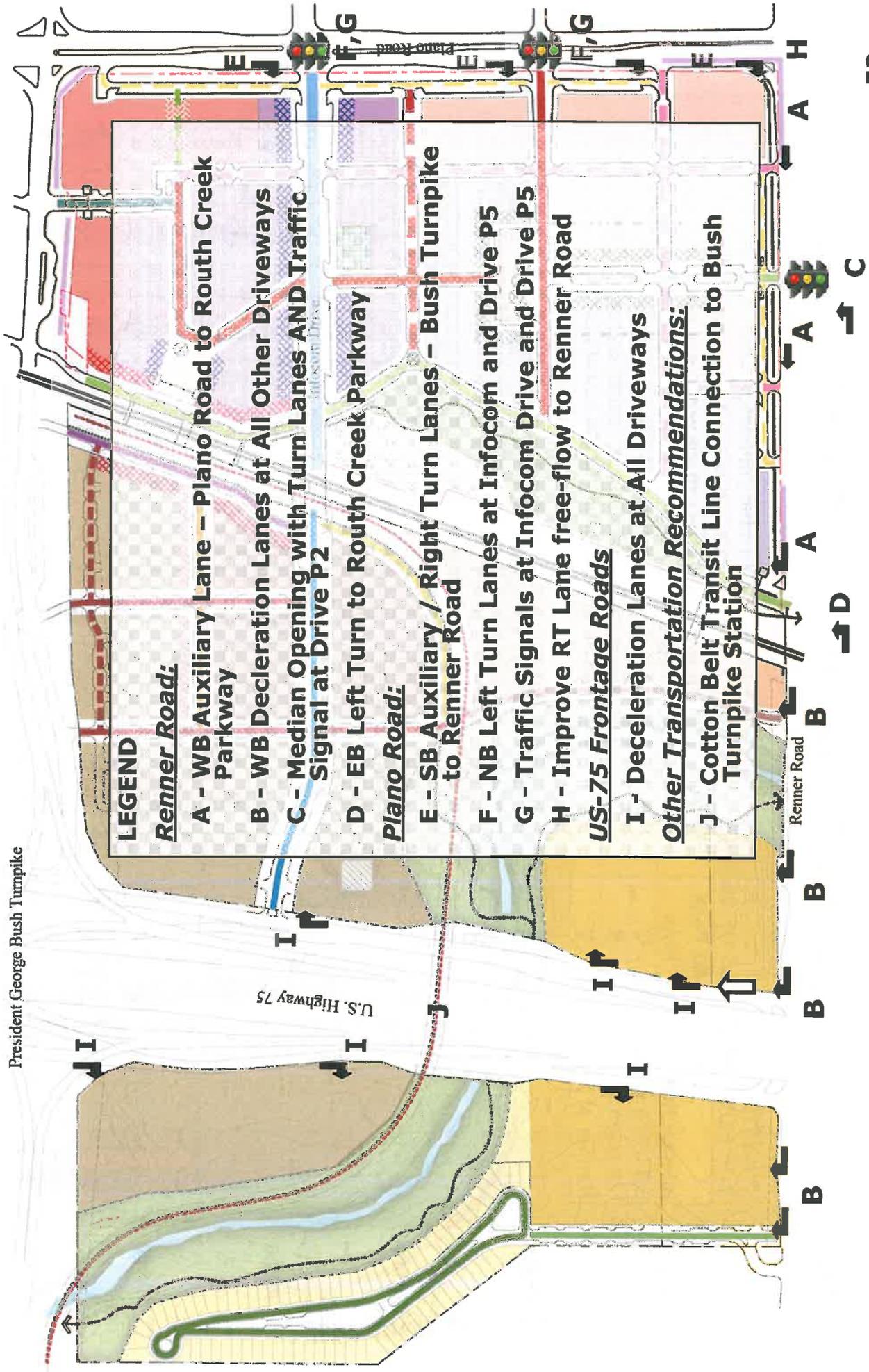
- The proposed TOD Mixed Use Zoning results in more daily traffic than the existing zoning entitlements, but it is more spread out over the entire day due to the mixture of proposed uses and the Peak Hour traffic volumes will be lower than the existing zoning allows with a better distribution of inbound and outbound traffic.
- Adequate provision of access and circulation drives will evenly distribute the traffic to the Arterial and Freeway frontage road system minimizing the impact to any specific link on the roadway network.
- The Cotton Belt Transit Line Connection to Bush Turnpike Station will greatly benefit the development and reduce the impact on traffic even more than depicted in the results of the TIA because staff limited the study's assumption of Transit usage to only a 10% reduction of trip capture rate.
- Significant levels of roadway capacity enhancements including additional turn bays and auxiliary lanes on the US 75 Frontage Roads, Plano Road and Renner Road are proposed by the applicant as part of the regulating plan to maximize the efficiency of the roadway network.
- The Plano Road at Renner Road Intersection can remain as an At-Grade Intersection. The Thoroughfare Plan will need to be amended to reflect this change at a future time.

The above conclusions were based on the land use assumptions utilized in the TIA and the applicants have agreed to cap the intensity of some landuses; however, if a significant increase in land use intensity or a major modification to the proposed mix of land use are proposed in the future, staff recommends that a revised TIA be required prior to any future PD amendment similar to the process in which other major modifications must be submitted through the City Council.

2010 Traffic Analysis and Development Traffic with Historical Data

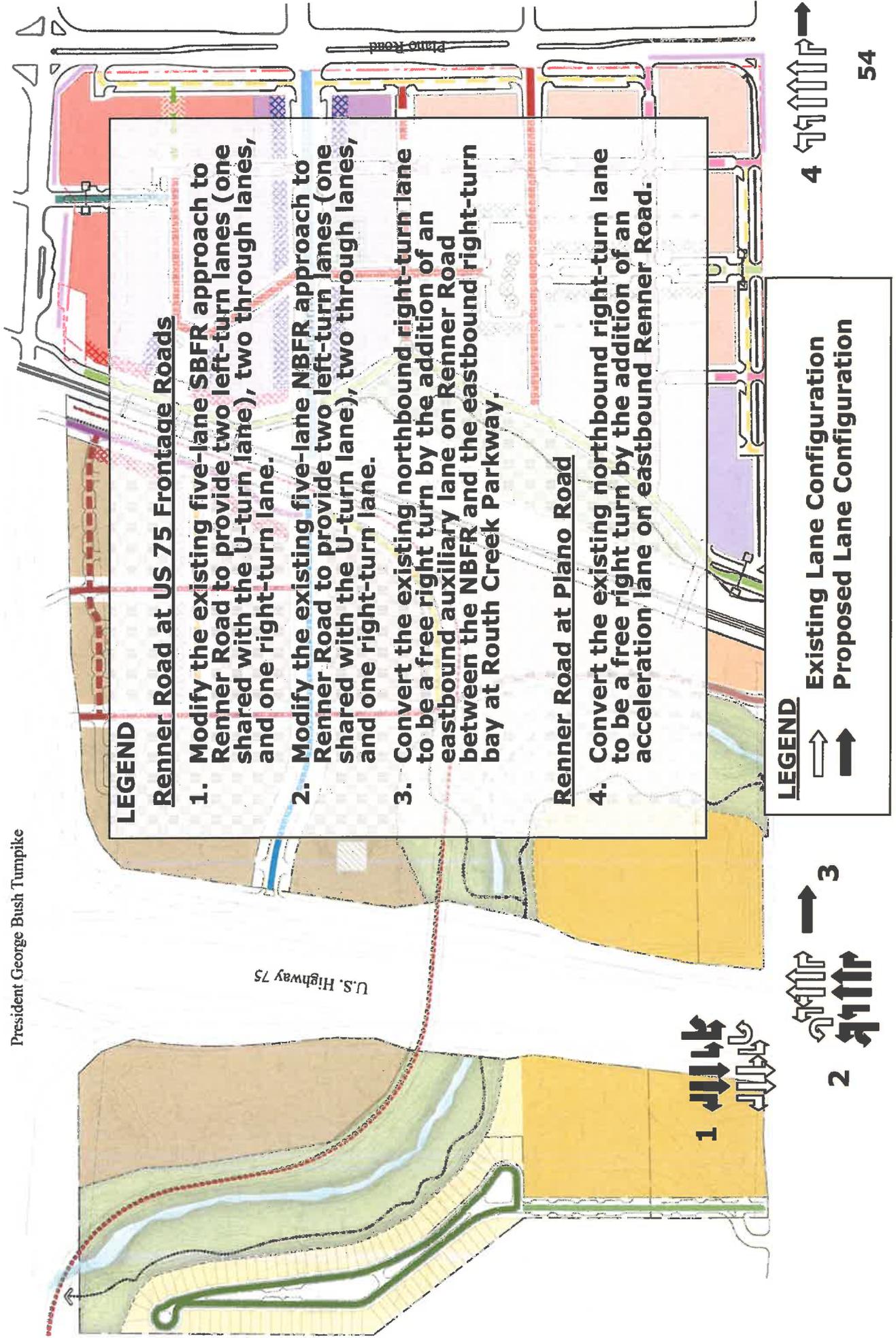


Traffic Impact Analysis Recommended Developer Mitigation Measures



Traffic Impact Analysis Recommended Potential Future CIP Improvements

President George Bush Turnpike



Explanation and Description of Request

The purpose of the Bush Central Station and the corresponding Planned Development Code is to support development of the area into a pedestrian-oriented, mixed-use urban development environment, with convenient access to rail transit, shopping, employment, housing, and regional retail services.

The Bush Central Station will foster the development of a major regional employment center with significant regional retail and residential uses within convenient walking distance from the existing transit station and potential transfer station to the Cotton Belt rail line. Development within this area would be of high intensity, accommodating large scale office and retail users while providing for moderate scale mixed use and residential within portions of the Bush Central Station.

The Bush Central Station and Planned Development Code are created to support economic development, sustainable tax base, and job creation by establishing adjacency predictability of private development that supports and leverages investment in and around the existing transit stop. The standards established provide a high level of prescription on the building form and design with flexibility on the mix of uses.

Bush Central Station implements the vision for the transit-oriented development in the City's Comprehensive Plan and the Regulating Plan (Appendix A). The Regulating Plan provides guidance to property owners, developers, and the City on the form, character, and intensity of future development within Bush Central Station. Creation of different Character Zones within Bush Central Station enables specific site and locational standards to be enumerated and applied. Clear graphic standards are provided for location, height, and building elements. Such standards promote sustainability, public welfare, walkable mixed use development, housing variety and transportation choice.

ZF 10-21



**SUBJECT PROPERTY
FOR ZONE CHANGE**



ZF 10-21 Aerial Map

Updated By: shacklett, Update Date: November 17, 2010
File: DSI\mapping\Cases\ZF10-21\ZF1021\ZF1021_ortho.mxd



President George Bush Tpk

Plano Rd

Subject Property

Renner Rd

DART Light Rail

Routh Creek Pkwy

Subject Property
Looking Northwest

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Section 1. Purpose and Intent:

The purpose of the Bush Central Station Planned Development Code, hereafter known as the BCS-PD Code, is to support development of the Bush Central Station into a pedestrian-oriented, mixed-use urban development environment, with convenient access to rail transit, shopping, employment, housing, and regional retail services. The goal of the Bush Central Station is to promote an efficient, compact land use pattern; encourage pedestrian activity; reduce the reliance on private automobiles; and provide a more functional and attractive community through the use of recognized principles of urban design.

- 1.1 Economic Development – The Bush Central Station and corresponding standards are created to support economic development, sustainable tax base, and job creation by establishing adjacency predictability of private development that supports and leverages investment in and around the Bush Central Station.
- 1.2 Implement the Design Goals of the Bush Central Station – The objective of the Bush Central Station is to foster a major regional employment center with significant regional retail and residential uses within convenient walking distance from the existing transit station and potential transfer station to the Cotton Belt rail line. The existing transit station provides a connection to 12 member cities of Dallas Area Rapid Transit (DART) including Dallas. Development within this area would be of high intensity, accommodating large scale office and retail users while providing for moderate scale mixed use and residential within portions of the Bush Central Station.
- 1.3 Establish Specific Development Standards – The BCS-PD Code implements the vision for the Bush Central Station as established in the City’s Comprehensive Plan and the Bush Central Station Regulating Plan, hereafter known as the Regulating Plan (Appendix A). The Regulating Plan shall provide guidance to property owners, developers, and the City on the form, character, and intensity of future development in the Bush Central Station. Creation of different Character Zones within Bush Central Station enables specific site and locational standards to be enumerated and applied. Clear graphic standards are provided for location, height, and building elements. Such standards promote sustainability, public welfare, walkable mixed use development, housing variety and transportation choice.

Section 2. Components of the Code:

- 2.1 This BCS-PD Code shall apply to the Bush Central Station unless otherwise specified in this Code. Development of property within the Bush Central Station shall comply with all applicable development standards set forth in this Code. The components of this BCS-PD Code consist of:
 - 2.1.1 Bush Central Station Regulating Plan: The Bush Central Station Regulating Plan, hereafter known as the “Regulating Plan”, is its official zoning map. It identifies the applicable character zones within the Bush Central Station including:
 - i. Character Zones – The Bush Central Station is divided into different “Character Zones”. A Character Zone creates a distinct urban form within that Zone which is different from urban forms in other Character Zones. Each Character Zone shall establish use and development standards including height, bulk, building and parking location, and functional design. The Regulating Plan classifies all lots within the Bush Central Station into one of four Character Zones.
 - ii. Special Frontage Standards – The Special Frontage Standards establish exceptions and special conditions for all buildings along designated frontages. Special Frontage Standards may be Mandatory or Non-Mandatory. Mandatory Special Frontage Standards shall apply in addition to the underlying Character Zone standards. Non-Mandatory Special Frontage Standards may be applied at the election of the developer and are not required.

- iii. Street Designations by Street Type– The Street Designations illustrate the design, configurations, and development context for all streets within the Bush Central Station. The street classification addresses vehicular lane widths, number of lanes, pedestrian accommodation, street tree requirements, on-street parking, and parkway and median standards (streetscape standards). In addition, streets are distinguished by the appropriate development context by denoting them on the Regulating Plan as Type ‘A’ or Type ‘B’ Streets.
- iv. Mandatory Street Network – The Mandatory Street Network specifies the future streets needed to implement the Bush Central Station Regulating Plan. The Mandatory Streets shall be required and shall generally meet the locational and connectivity goals of the Regulating Plan. Their design shall be guided by the Street Type Specifications.
- v. Mandatory Civic/Open Space – The Mandatory Civic/Open Space areas shown on the Regulating Plan designate the locations of proposed civic/open spaces (including parks, plazas, greens, and squares).
- vi. Non-Mandatory Street Network – The Non-Mandatory Street Network indicates locations of suggested, but not required streets within the Bush Central Station Regulating Plan. These streets are not mandatory, but at the election of the developer and their design shall be guided by the Street Type Specifications.
- vii. Non-Mandatory Civic/Open Space – The Non-Mandatory Civic/Open Space designation indicates the locations of desired, but not required civic/open spaces (including parks, plazas, greens, and squares) to implement the Regulating Plan. Non-Mandatory Civic/Open Spaces are not required, but at the election of the developer.
- viii. Special Destination – The Special Destination indicates an area on the Regulating Plan identified for specific uses with applicable standards as established in Section 5.7 of this Code.

2.1.2 Development Standards: The BCS-PD Code (the text portion of this Code) enumerates the development standards with text and graphics for Character Zones, Special Frontage, building form, civic open space, landscape, building design, signage, lighting, and all related standards for all streets, public and private development.

Section 3. Administration

This section sets forth the provisions for reviewing and approving development applications within the Bush Central Station. The intent is to ensure that all development is consistent with the provisions of this Code. All sections of this Code shall be applied during the review process.

3.1 The development standards under Articles XIII-A thru XVI and Article XXI-C of the City of Richardson Comprehensive Zoning Ordinance, as amended, shall not apply to the Bush Central Station except as specifically referenced herein. Development standards not addressed in this ordinance shall be governed by the City of Richardson Comprehensive Zoning Ordinance to the extent they are not in conflict with the intent or text of the BCS-PD Code.

3.2 Sign Standards under Chapter 18, as amended, of the City of Richardson Code of Ordinances, shall not apply to the Bush Central Station except as specifically referenced herein.

3.3 Using this Code:

The following basic steps should be followed to determine the uses and development standards applicable on property within the Bush Central Station:

3.3.1 Locate the subject property on the Bush Central Station Regulating Plan.

3.3.2 Identify:

- i. the Character Zone in which the property is located;
 - ii. the Street Type designation along all its street frontages; and,
 - iii. any Special Frontage Requirements that may be applicable to the subject property.
- 3.3.3 Review the Schedule of Uses by Character Zone as listed in Table 6.1 to determine allowed uses.
- 3.3.4 Examine the corresponding zone standards in the Building Form and Development Standards in Section 7 to determine the applicable development standards.
- 3.3.5 Refer to Section 5.5 and 5.6 for Special Frontage Standards.
- 3.3.6 Refer to Section 8 for Building Design Standards.
- 3.3.7 Refer to Section 9 for Street Type and Streetscape Standards.

The information from the above listed steps explains where the building will sit on the lot, the limits on its three dimensional form, the range of uses, and the palette of materials that will cover it.

- 3.4 Development within the Bush Central Station that complies with the provisions of this Code shall follow the City's development process as outlined in Chapter 21, Article II of the City of Richardson's Code of Ordinances and shall be approved by the City Manager or designee (see Appendix C for flow chart of the review process). In addition to complying with applicable City regulations that are not in conflict with this Code, the applicant shall provide the information required to adequately show compliance with this Code.
- 3.5 Standard for Approval of development plans: If a development plan conforms to the standards set forth in this Code and applicable City regulations not in conflict with this Code, the development plan shall be approved. Upon request by an applicant, the authority charged with approving the development plan shall certify the reason(s) for an action taken on a development plan.
- 3.6 The City Manager or designee shall be responsible for the following:
- 3.6.1 Reviewing development plan applications for compliance with the requirements of BCS-PD Code.
 - 3.6.2 Approving development plan applications that are in compliance with the requirements of the BCS-PD Code.
 - 3.6.3 Making determinations on the applications and interpretations of standards in this Code.
 - 3.6.4 Approving revisions to previously approved development plans that comply with this Code and all applicable city ordinances.
 - 3.6.5 Approving any minor modifications to the approved Regulating Plan per Section 3.8.
 - 3.6.6 Recommendations on any SDP applications to the City Plan Commission (CPC) and City Council.
- 3.7 Special Development Plans: A request for a modification to any of the standards of this Code other than minor modifications permitted under Sections 3.8 shall be reviewed and processed as Special Development Plans.
- 3.7.1 Special Development Plans (SDP) are intended to allow applicants development flexibility to address specific market opportunities and/or contexts. An application for a Special Development Plan shall be processed as an amendment to the zoning ordinance under Article XXIX of the City of Richardson Zoning Ordinance and may only be considered by the City Council (CC) after the CPC has made a recommendation. The City Manager or designee shall review, make recommendations on any SDPs, and shall forward all SDP applications to the CPC. In evaluating an SDP, CC may consider the extent to which the application meets any of the following:
 - i. provides an alternative "Master Plan" approach by consolidating multiple properties to create a predictable, market responsive development for the area; or

ii. fits the adjoining context by providing appropriate transitions.

3.8 Minor Modifications to the BCS-PD Code:

The City Manager or designee shall have the authority to approve a request for minor modifications to BCS-PD Code that:

- 3.8.1 Does not materially change the circulation and building location on the site;
- 3.8.2 Does not increase the building area permitted under this Code;
- 3.8.3 Does not change the relationship between the buildings and the street;
- 3.8.4 Does not allow a use not otherwise authorized in this Code;
- 3.8.5 Does not allow greater height of any building or reduction of any parking requirement established in this Code; or
- 3.8.6 Changes to established street cross sections per Table 3.1 below and Section 9 of this Code.

The City Manager or designee shall also have the authority to approve minor modifications outlined in Table 3.1. Any appeals to the decisions of the City Manager on minor modifications shall be heard by the City Council. Any City Council denials of minor modifications or any changes beyond those that meet the criteria above, the thresholds established in Table 3.1, shall be processed as an amendment to this Code under Article XXIX of the City of Richardson Comprehensive Zoning Ordinance.

Table 3.1 Minor Modifications Allowed

<i>Standard</i>	<i>Minor Modification Allowed</i>	<i>Comments</i>
Area/boundary of Character Zones (including any Mandatory Civic/Open Spaces)	No more than a 15% change (increase or decrease) in the area of any Character Zone (aggregate or per block)	<ul style="list-style-type: none"> • Shall not eliminate any Character Zone • 15% measurement shall be based on the total area of that specific Character Zone within the entire Bush Central Station • For the purposes of this code, any increase in any Character Zone that results from the elimination of a non-mandatory street shall not count towards the 15% threshold.
Location of any Mandatory Street	Location shall not move more than 100' in any direction	<ul style="list-style-type: none"> • Shall maintain the connectivity intended by the Regulating Plan
Building Form and Development Standards		
<ul style="list-style-type: none"> • Street Setback Line 	No more than a 10% change in the SSL as established in the corresponding Character Zone Building Form and Development Standards	<ul style="list-style-type: none"> • Changes to the Street Setback Lines may only be due to any changes to the street cross sections or change in the width of a sidewalk • In no case shall the sidewalk be less than 6 feet in width.
<ul style="list-style-type: none"> • Build to zones/setbacks 	No more than a 20% change in the maximum or minimum setback.	
<ul style="list-style-type: none"> • Building Frontage 	No more than a 15% reduction in the required building frontage along each block of a Type 'A' Street	<ul style="list-style-type: none"> • Any reduction in the required building frontage shall be to accommodate Porte-cocheres for drop-off and pick-up.
<ul style="list-style-type: none"> • Street screen 	Waiver of Street screen requirement along a Type 'B' Street	<ul style="list-style-type: none"> • Requirement for a street screen may only be waived along a Type 'B' Street along the frontage of any interim surface parking lot (off-street) that is intended to be in-filled with a parking structure. • In no case shall any portion of the surface parking have frontage along a Type 'A' Street without a required street screen • In no case shall the (off-street) surface parking lot be located at a street intersection for a minimum depth of 30' along each street (regardless of the Street Type).
Street Cross Sections	Cross sections of new streets may be adjusted with respect to number of lanes, lane widths, on-street parking configuration, pedestrian accommodation, and street tree planting	<ul style="list-style-type: none"> • Any changes in the street cross sections shall be based on specific development context such as vegetation, natural features, drainage, and fire access and is subject to approval by the City.

Section 4. Definitions

In addition to Definitions in Article I of the City of Richardson Comprehensive Zoning Ordinance, the following terms shall have the corresponding interpretations.

Arcade means a portion of the main façade of the building that is at or near the Street-Setback Line and a colonnade supports the upper floors of the building. Arcades are intended for buildings with ground floor commercial or retail uses and the arcade may be one or two stories.

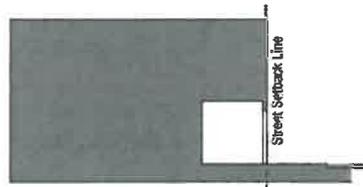


Image of an arcade

Attics/Mezzanines means the interior part of a building contained within a pitched roof structure or a partial story between two main stories of a building.

Auto-Related Sales and Service Uses means establishments that provide retail sales and services related to automobiles including, but not limited to, cars, tires, batteries, gasoline, etc.

Block Face Dimensions means the linear dimension of a block along one of its street frontages.

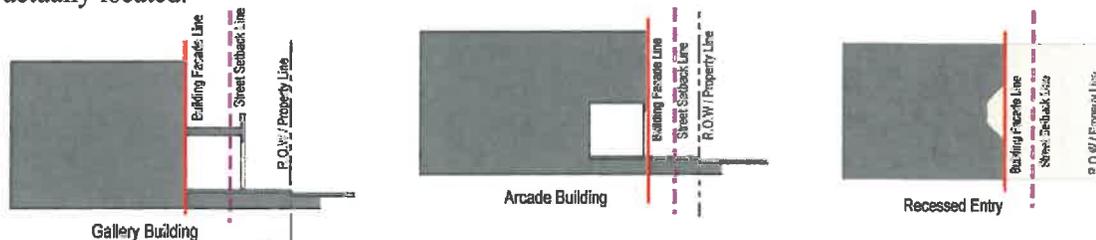
Block Perimeter means the aggregate dimension of a block along all of its street frontages.

Block means the aggregate of lots, pedestrian passages and rear alleys, circumscribed on all sides by streets.

Build-to Line means the line at which the principal building's front façade shall be built.

Build-to Zone (BTZ) means the area within which the principal building's front façade is to be built.

Building Façade Line means the vertical plane along a lot where the building's front façade is actually located.



Building Façade Line Illustrations

Building Form Standards means the standards established for each Character Zone that specify the height, bulk, orientation, and elements for all new construction and redevelopment.

Building Frontage means the percentage of the building's front façade that is required to be located at the front Build-to Line or Zone as a proportion of the block frontage along that public street. Parks, plazas, squares, improved forecourts, and pedestrian breezeway frontages shall be considered as buildings for the calculation of building frontage.

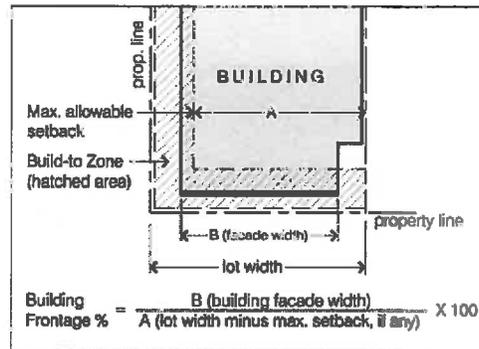


Image showing how a lot's building frontage is calculated.

Character Zone means an area within the Bush Central Station that creates a distinct urban form different from other areas within the Bush Central Station. Character Zones are identified in the Regulating Plan.

City Manager means the City Manager of the City of Richardson or his/her designee.

Civic/Open Space means publicly accessible open space in the form of parks, courtyards, forecourts, plazas, greens, pocket parks, playgrounds, etc. They may be privately or publicly owned. For all residential uses, privately accessible open spaces such as courtyards, porches, and balconies may also be considered as Civic/Open Space for the purposes of this ordinance. Building façades facing a Civic/Open Space shall be treated as a Type 'A' Street frontage.

Green means a civic/open space intended for unstructured recreation, spatially defined by landscaping rather than building frontages.

Park means a civic/open space that is a preserve largely available for unstructured recreation.

Plaza means a primarily hardscaped civic/open space with formal landscaping, available for civic purposes and commercial activities. A plaza shall be spatially defined by buildings.

Playground is a civic/open space designed and equipped for children's recreation. A playground may be fenced and may include an open shelter. Playgrounds may be located within residential areas and may be placed within a block. They may be included in other civic/open spaces.

Comprehensive Plan means the City of Richardson Comprehensive Plan that establishes the blueprint for the long-term growth and development of the City as adopted on the effective date of this Code.

Development Plan means any submittal as defined by Chapter 21, Article II of the City of Richardson's Code of Ordinances.

Encroachment means any structural or non-structural element such as a sign, awning, canopy, terrace, or balcony that breaks the plane of a vertical or horizontal regulatory limit, extending into a Setback, into the Public R.O.W, or above a height limit.

Gallery means an extension of the main façade of the building that is at or near the front property line and the gallery may overlap the public sidewalk.

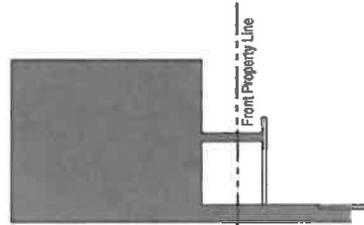


Image of a Gallery

Kiosk means a small temporary or permanent structure often open on one or more sides used for sales in civic/open spaces.

Live-Work Unit means a dwelling unit that is also used for work purposes, provided that the 'work' component is restricted to the uses of professional office, artist's workshop, studio, or other similar uses and is located on the street level and constructed as separate units under a condominium regime or as a single unit. The 'live' component may be located on the street level (behind the work component) or any other level of the building. Live-work unit is distinguished from a home occupation otherwise defined by this ordinance in that the work use is not required to be incidental to the dwelling unit, non-resident employees may be present on the premises and customers may be served on site.

Living Screen means a Street Screen composed of landscaping in the form of vegetation.

Main Street Frontage means the special frontage requirement along identified Type A Street frontages as indicated in the Regulating Plan.

Master Sign Plan means a unique sign plan to implement a specific vision for a portion or all of the development that meets Section 10.2 of this Code.

Minor Modification means any changes to the BCS-PD Code that meet the threshold criteria established in Section 3.8 and Table 3.1.

Non-Mandatory Civic/Open Space means plaza, green, square, or park area designated on the Regulating Plan which is shown as a suggested feature within the Bush Central Station. The Non-Mandatory Plaza is not required and is at the election of the developer.

Non-Mandatory Neighborhood Frontage means the special frontage option to maintain a neighborhood compatible frontage as indicated in the Regulating Plan. The Non-Mandatory Neighborhood Frontage is not required and is at the election of the developer.

Non-Mandatory Pedestrian Passage means a pedestrian access area designated on the Regulating Plan which is shown as a suggested feature within the Bush Central Station. The Non-Mandatory Pedestrian Passage is not required and is at the election of the developer.

Non-Mandatory Street means a street that is designated on the Regulating Plan which is shown as a suggested street within the Bush Central Station. The Non-Mandatory Street is not required and is at the election of the developer.

Pedestrian Easement means a grant of use of private property for pedestrian access and use.

Regulating Plan means the Zoning Map attached hereto as Appendix A that shows the Character Zones, Civic Spaces, location of Special Frontages, Streets, and other Special Requirements applicable to the Bush Central Station subject to the standards in this Code. For the purposes of this Code, the Regulating Plan shall also be the Concept Plan for the Bush Central Station.

Residential Loft means a residential unit that is built to Retail Ready standards and adapted for residential uses.

Retail Ready means space constructed at a minimum interior height of 14 feet which may be used for noncommercial uses and can be converted into retail/commercial use. Prior to the issuance of a certificate of occupancy for a retail/commercial use in a Retail-Ready space, the space must comply with all building and construction codes for that use. The intent of Retail-Ready space is to provide the flexibility of occupying a space in accordance with market demand and allowing the use in such space to change to retail/commercial uses accordingly.

Retail Sales Retail establishments are the final step in the distribution of merchandise. They are organized to sell in small quantities to many customers. Establishments in stores operate as fixed point-of-sale locations, which are designed to attract walk-in customers. Retail establishments often have displays of merchandise and sell to the general public for personal or household consumption, though they may also serve businesses and institutions. Some establishments may further provide after-sales services, such as repair and installation. Included in, but not limited to this category, are durable consumer goods sales and service, consumer goods, other grocery, food, specialty food, beverage, dairy, etc, and health and personal services.

Service Uses means a category for limited personal service establishments which offer a range of personal services that include (but not limited to) clothing alterations, shoe repair, dry cleaners, laundry, health and beauty spas, tanning and nail salons, hair care, etc.

Sign, Building Blade means a pedestrian-oriented sign that is affixed perpendicular to the corner of a building or to the front façade of a building above the ground floor to provide identification for the whole building.



Image of a Building Blade Sign

Sign, Marquee means a sign structure placed over the entrance to a theatre or other public gathering venue. It has signage stating either the name of the establishment or the name of the event, artist, and other details of the event appearing at that venue. The marquee is often identifiable by a surrounding cache of light bulbs, usually yellow or white, that flash intermittently or as chasing lights. Marquee signs may often be combined with Building Blade signs.



Image of a Marquee sign with a Building Blade Sign

Sign, Monument means any sign which is connected to the ground and which has no clear space for the full width of the sign between the bottom of the sign and the surface of the ground. A monument sign may include a sign face and sign structure, and may also include a sign base and sign cap.



Image of a Monument Sign

Sign, Sandwich Board means a portable sign consisting of two panels of equal size, which are hinged at the top or one panel with a support and placed on the ground or pavement so as to be self-supporting.



Images of sandwich board signs.

Sign, Tenant Blade means a smaller pedestrian-oriented sign that is affixed perpendicular to the building façade under a canopy or awning or immediately over a tenant space and provides identification for individual tenants within a building.



Image of a Tenant Blade Sign

Slip Road means a drive that extends within the property and parallel to either Renner Road or Plano Road. The Slip Road is generally located within close proximity to these main roads as identified on the Regulating Plan.

Slip Road Frontage means the frontage along a Slip Road.

Special Development Plan means a development application that meets Section 3.7 of this Code.

Special Frontage Requirements means standards applied to certain blocks as indicated in the Regulating Plan in order to address specific requirements and transitions based on street frontage and adjacency in addition to the underlying Character Zone standards.

Station Platform Frontage means the special frontage requirement on blocks with frontage along the rail station platform as indicated in the Regulating Plan.

Street-Setback Line means the imaginary line located at the outside edge of the sidewalk and measured from the center line of the street from which all build-to lines and build-to zones are measured from.

Street Screen means a freestanding wall or living screen built along the BTZ or in line with the building façade line along the street. It may mask a parking lot or a loading/service area from view or provide privacy to a side yard and/or strengthen the spatial definition of the public realm.



Image of a combination masonry and living street screen

Street Type means a specific designation for streets in the Bush Central Station that establishes a certain character and cross-sections to improve walkability within the Bush Central Station.

Street Network means the Mandatory and Non-Mandatory network for new and existing streets within the Bush Central Station as established in the Regulating Plan.

Transition Zones means the areas with specific adjacencies within which certain limitations on building heights apply as established for each Character Zone.

Tree Well means an unpaved area around the trunk of a tree within the sidewalk area that is either landscaped with ground cover or covered with a tree grate.



Example of a tree well with a tree grate



Example of a tree well with landscaping

Type 'A' Street means the streets identified as such on the Regulating Plan. Type 'A' Streets are the primary pedestrian streets and buildings along Type 'A' Streets shall be held to the highest standard of pedestrian-oriented design.

Type 'B' Street means the streets identified as such on the Regulating Plan. Type 'B' Streets are intended to primarily accommodate access to parking, service, and other auto-related functions.

Section 5. The Regulating Plan

5.1 The Regulating Plan (Appendix A) is hereby adopted as the official zoning map for the Bush Central Station.

5.2 Character Zones Established – the following Character Zones are established. The boundaries of the specific Character Zones shall be established in the Regulating Plan.

5.2.1 TOD Core: The TOD Core provides the most opportunity for the highest intensity development. It is the area that has significant development impact and the highest pedestrian activity due to its adjacency to the transit station. The TOD Core consists of the highest density and height, with the greatest variety of uses. Development within the TOD Core Zone shall meet the Building Form and Development Standards in Section 7.1 of this Code.

5.2.2 TOD Mixed Use: The TOD Mixed Use is the area adjacent to the TOD Core that is intended for high intensity commercial and residential uses in addition to supporting retail and restaurant uses. Development within the TOD Mixed Use Zone shall meet the Building Form and Development Standards in Section 7.2 of this Code.

5.2.3 Arterial Mixed Use: Arterial Mixed Use is intended to provide appropriate transitions to major regional roadways while taking advantage of the arterial frontage for limited auto-oriented sites. The Arterial Mixed Use frontage also acts as a gateway into the Bush Central Station by providing an attractive street frontage for the overall development. Development within the Arterial Mixed Use Zone shall meet the Building Form and Development Standards in Section 7.3 of this Code.

5.2.4 Highway Mixed Use: Highway Mixed Use is intended to provide an appropriate transition into the Bush Central Station from the President George Bush Turnpike (SH 190) access road. This area is also intended for high intensity development. Development within the Highway Mixed Use Zone shall meet the Building Form and Development Standards in Section 7.4 of this Code.

- 5.3 Street Designations By Street Type Established – The Regulating Plan shall establish the following Street Designations.
- 5.3.1 Type ‘A’ Streets Established – Type ‘A’ Streets are intended to be the primary pedestrian streets and buildings along Type ‘A’ Streets shall be held to the highest standard of pedestrian-oriented design. The Type ‘A’ Streets are TOD Main Street, TOD Street Type A, Parkway, Park Avenue and Court Street as identified in the Regulating Plan.
- 5.3.2 Type ‘B’ Streets Established – Type ‘B’ Streets are intended to balance pedestrian orientation with automobile orientation. Buildings along Type ‘B’ Streets may be permitted to accommodate some service and auto-related functions. The Type ‘B’ Streets are TOD Avenue and TOD Street Type B as identified in the Regulating Plan. In addition, Slip Roads provide a functional transition from the suburban arterials that bound the Bush Central Station to the urban street network offered within the development. The Slip Roads are designated along Plano Road and Renner Road and shall be considered as Type ‘B’ Streets for the purpose of development frontage standards.
- 5.4 Mandatory Streets by Street Type – The Street Network indicates Mandatory and Non-Mandatory streets needed to implement the Bush Central Station Regulating Plan. The Regulating Plan designates the type, classification, and location of streets. All new streets in the Bush Central Station shall meet the street design standards established in Section 9 herein.
- 5.5 Mandatory Special Frontage Requirements – In order to address specific requirements and transitions based on street frontage and adjacency, the following Special Frontage Requirements as established in the Regulating Plan shall apply:
- 5.5.1 Main Street Frontage: Ground floors of all buildings designated as Main Street Frontage on the Regulating Plan shall not be occupied by residential units and/or lodging rooms in hotels to a minimum depth of 30 feet as measured from the front building line.
- 5.5.2 Station Platform Frontage: Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.
- 5.6 Non-Mandatory Neighborhood Frontage Requirements – In order to allow the development of a unique neighborhood enclave of brownstones, townhomes and live-work units within the Bush Central Station, the blocks along the Park Avenue have an optional Neighborhood Frontage designation for a minimum depth of 30 feet from the front building façade line. All standards of the underlying Character Zone shall apply to the blocks designated as Neighborhood Frontage with the exception of height which shall be limited to a maximum of sixty (60) feet along this frontage. If the Non-Mandatory Neighborhood Frontage is developed on any block or portion of a block designated as such, it shall then become Mandatory for those blocks that face the developed blocks and designated as Neighborhood Frontage on the Regulating Plan.
- 5.7 Special Destination – The following building and site standards shall apply to areas designated as Special Destination on the Regulating Plan:
- 5.7.1 Maximum permitted building height shall be two (2) stories.
- 5.7.2 Only “Destination” uses shall be permitted. Destination uses shall include restaurants, cafes, entertainment venues and accessory retail with no drive through service.
- 5.7.3 Off-street parking shall not be permitted between the building and any Type ‘A’ Street. However, driveway access to parking may be permitted off Type ‘A’ or ‘B’ Streets.
- 5.7.4 Building pad and surface parking shall be located to maximize the preservation of trees.
- 5.7.5 Off-street parking may be met through a combination of on-site surface parking and shared parking within the Bush Central Station.
- 5.7.8 Valet parking circulation drives may be permitted from Type ‘A’ Streets.

- 5.7.9 Service areas shall not have frontage along Type ‘A’ Streets. Service areas shall be located along the back or side of the principal building.
- 5.7.10 All restaurant uses shall provide outdoor seating/patio.
- 5.7.11 The area occupied by any impervious surfaces (buildings or parking) shall not exceed the area identified on the regulating plan as Special Destination.
- 5.7.12 Uses permitted within the Special Destination area are:
 1. Accessory Use only - Retail Sales or Service with no drive through facility (includes alcohol sales which shall meet Chapter 4, Alcoholic Beverages of the City of Richardson Code of Ordinances). Excluded from this category are Auto-Retail Sales and Service Uses (see Section 4 of the Code for Definition of Retail, Service uses, and Auto-related Sales and Service).
 2. Food Service Uses such as full-service restaurants, cafeterias, bakeries and snack bars with no drive through facilities Included in this category is café seating within a public or private sidewalk area with no obstruction of pedestrian circulation. Also included in this category is the sale of alcoholic beverages which shall meet Chapter 4, Alcoholic Beverages of the City of Richardson Code of Ordinances.
 3. Theater, cinema, dance, music or other entertainment establishment
 4. Sales from kiosks
 5. Incidental Outdoor Display (subject to standards in Section 7 of the Code)
 6. Special Events

5.8 Civic/Open Space – The Regulating Plan indicates Mandatory and Non-Mandatory Civic/Open Spaces. The specific standards for Civic/Open Space are established in Section 11.

Section 6. Schedule of Permitted Uses

6.1 Generally: Due to the emphasis on urban form over land uses in the Station Areas, general use categories have been identified by character zones. Uses not listed in the following schedule, but are substantially similar, may be permitted upon the approval of the City Manager or designee, subject to appeal directly to the City Council.

6.2 Schedule of Uses:

Table 6.1

Character Zone	TOD Core	TOD Mixed Use	Arterial Mixed Use & Highway Mixed Use
Land Use			
Commercial Uses (Office, Retail, Sales & Service Uses)			
Retail Sales or Service with <u>no drive through facility</u> (includes alcohol sales which shall meet Chapter 4, Alcoholic Beverages of the City of Richardson Code of Ordinances). Excluded from this category are Auto-Retail Sales and Service Uses (see Section 4 of the Code for Definition of Retail, Service uses, and Auto-related Sales and Service)	P	P	P
Finance, Insurance, and Real Estate establishments including banks, credit unions, real estate, and property management services, <u>with no drive through facility</u>	P	P	P
Offices for business, professional, and technical uses such as accountants, architects, lawyers, doctors, etc.	P	P	P
Research laboratory headquarters, laboratories and associated facilities	P	P	P
Food Service Uses such as full-service restaurants, cafeterias, bakeries and snack bars with <u>no drive through facilities</u> Included in this category is café seating within a public or private sidewalk area with no obstruction of pedestrian circulation. Also included in this category is the sale of alcoholic	P	P	P

Character Zone	TOD Core	TOD Mixed Use	Arterial Mixed Use & Highway Mixed Use
beverages which shall meet Chapter 4, Alcoholic Beverages of the City of Richardson Code of Ordinances.			
Any use with a drive through facility	P/C	P/C	P/C
Auto-related Sales and Service	NP	NP	P/C
Arts, Entertainment, and Recreation Uses			
Art galleries	P	P	P
Art, antique, furniture or electronics studio (retail, repair or fabrication; excludes auto electronics sales or service)	P	P	P
Games arcade establishments	P	P	P
Theater, cinema, dance, music or other entertainment establishment	P	P	P
Museums and other special purpose recreational institutions	P	P	P
Fitness, recreational sports, gym, or athletic club	P	P	P
Parks, greens, plazas, squares, and playgrounds	P	P	P
Educational, Public Administration, Health Care and Other Institutional Uses			
Business associations and professional membership organizations	P	P	P
Child day care and preschools	P	P	P
Schools, libraries, and community halls	P	P	P
Universities and Colleges	P/C	P/C	P
Hospital	P	P	P
Civic uses	P	P	P
Social and fraternal organizations	P	P	P
Social services and philanthropic organizations	P	P	P
Religious Institutions	P	P	P
Funeral homes	P	P	P
Residential Uses			
Home Occupations	P/A	P/A	P/A
Multi-family residential			
Ground Floor	P/C	P	P/C
Upper Floors	P	P	P
Residential Lofts	P	P	P
Single-family residential attached dwelling unit (Townhomes)	P/C	P/C	P/C
Live-work unit	P	P	P
Other Uses			
Model homes for sales and promotion**	P	P	P
Hotels	P	P	P
Parking, surface (primary use of property)	NP	NP	P
Parking, surface (accessory use of property)	P	P	P
Parking, structured	P	P	P
Private attached garage	NP	NP	NP
Private detached garage	NP	NP	NP
Sales from kiosks	P	P	NP
Veterinary clinic	NP	P	P
Community garden	P	P	P
Incidental Outdoor Display (subject to standards in Section 7 of the Code)	P/A	P/A	P/A
Antennas including cell, accessory, and mounted on top of buildings.	P/A/C	P/A/C	P/A/C
Wind energy equipment	P/A	P/A	P/A
Solar energy equipment	P/A	P/A	P/A
Special Event	P	P	P
Rain harvesting equipment	P/A/C	P/A/C	P/A/C
Utility equipment (includes electrical transformers, gas meters, etc)	P/A/C	P/A/C	P/A/C

** Model homes are limited to a time period until all the homes are sold in the neighborhood.

P= Permitted by right NP= Not Permitted P/C= Permitted with design criteria per Table 6.2 P/A = Permitted Accessory Use NA= Not applicable
A = Accessory use to not exceed 25% of the primary use building square footage

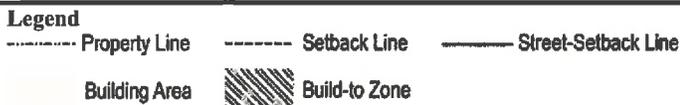
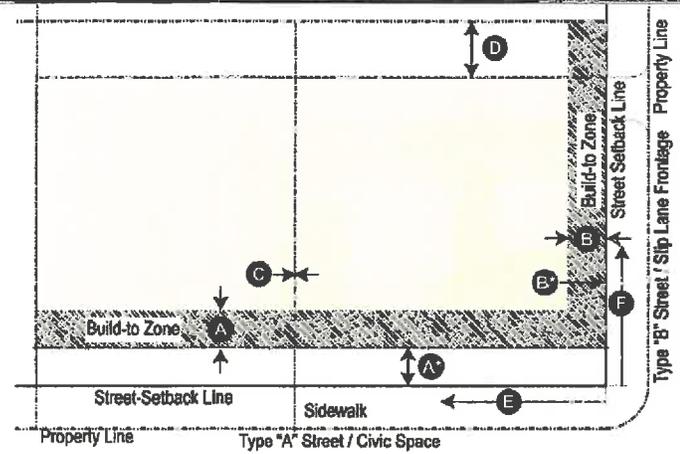
6.3 Use Criteria: All uses listed as P/C in Table 6.1 shall also meet the following standards in Table 6.2

Table 6.2 – Use Criteria		
<i>Use</i>	<i>Zone</i>	<i>Location & Design Criteria</i>
Non-Residential Uses		
Any permitted use with a drive through facility	TOD Core, TOD Mixed Use & Arterial Mixed Use	<ul style="list-style-type: none"> All drive through access (driveways) shall be from Type 'B' Streets (including Slip Roads). Drive through lanes and/or canopies shall not have frontage along on or be located along any Type 'A' Streets. Drive through areas screened by a 4' high Street Screen.
Universities and Colleges	TOD Core & TOD Mixed Use	<ul style="list-style-type: none"> Shall be required to provide structured parking as part of the build-out for the university/college campus
Auto-related Sales and Service	Arterial Mixed Use & Highway Mixed Use	<ul style="list-style-type: none"> Gas pumps, canopies, and/or service bays shall not be located along any Type 'A' Street frontage. No more than 50% of a block's frontage along a Type 'B' Street shall be occupied by gas pumps, canopies, and/or service bays. Any buildings associated with the use shall also have a pedestrian entrance at a Type 'A' Street. No outdoor storage of vehicles or other products sold shall be permitted. All auto-related sales display shall be inside storefronts.
Residential Uses		
Multi-family residential Ground Floor	TOD Core and Arterial Mixed Use	<ul style="list-style-type: none"> All ground floors along all Type 'A' Streets and Slip Road frontages shall be built to Retail Ready standards. Ground floors may be occupied by residential uses unless designated as Main Street Frontage. Ground floors of all buildings designated as Main Street Frontage on the Regulating Plan <u>shall not</u> be occupied by residential units and lodging rooms to a minimum depth of 30 feet as measured from the front building line.
Single-family residential attached dwelling unit (Townhomes)	TOD Core, TOD Mixed Use, Arterial Mixed Use	<ul style="list-style-type: none"> Shall be permitted along Type 'B' Streets or along Type 'A' Streets which are also designated for Neighborhood Frontage on the Regulating Plan Frontages along street intersections shall be built to Retail Ready standards for a minimum of 30' along each street or the width of the lot, whichever is less.
Other Uses		
Antennas including cell, accessory and mounted (Excluded from this category are freestanding and commercial antennas and equipment buildings)	All Zones	<ul style="list-style-type: none"> Antennas shall be permitted on rooftops. Antennas shall be screened entirely with a screen of same color as the principal building. Antennas shall not be visible from adjacent Type 'A' Street.
Rain water harvesting equipment	All Zones	<ul style="list-style-type: none"> Rain water harvesting equipment may not be installed along Type 'A' Streets. On all other frontages, they shall be screened with a Street Screen at least as high as the equipment being screened.
Utility equipment (includes electrical transformers, gas meters, etc)	All Zones	<ul style="list-style-type: none"> Utility equipment shall not be installed with frontage on Type 'A' Streets. On all other frontages, they shall be screened with a Street Screen at least as high as the equipment being screened.

Section 7. Building Form and Development Standards

The following section establishes the Building Form and Development Standards for all Character Zones within the Bush Central Station. Diagrams and reference letters are used for illustrations purposes only. Reference letters may not be in continuous sequence.

7.1.1 Building Placement



Street-Setback Line (SSL)
(Distance from center line of street to edge of the BTZ)

TOD Main Street	61 feet
TOD Street Type A	35 feet
TOD Street Type B	31 feet
TOD Avenue	44 feet
Parkway	36 feet
Slip Road Frontage	85 feet (measured from the R.O.W of Plano Road)

Build-To Zone (BTZ)
(Distance from edge of SSL to edge of the BTZ)

Front (Type 'A' Street and Civic Space)	5 – 10 feet	A
Front (Type 'B' Street and Slip Road Frontage)	0 – 10 feet (see #1)	B

Setback

Front (Type 'A' Street and Civic Space – distance from SSL)	5 feet (min.) – 10 feet (max)	A*
Front (Type 'B' Street and Slip Road Frontage – distance from SSL)	0 feet (min.) – 10 feet (max.)	B*
Side (from property line)	0 feet (see #2)	C
Rear (from property line)	5 feet	D

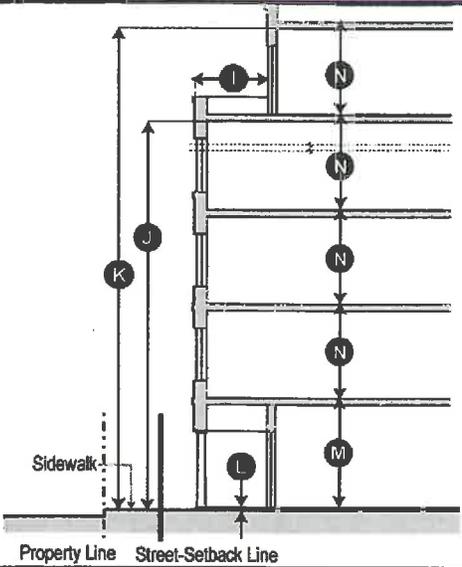
Building Frontage

Building Frontage required along Type 'A' Street/civic space BTZ	90% (min.) (see #3 and #7)	E
Building Frontage required along Type 'B' Street BTZ	0% (min.) (see #3 and #7)	F

7.1.2 Block Standards

Block face dimensions	250 – 400 feet (see #4)
Block perimeter	1400 feet (maximum) (see #4)

7.1.3 Building Height



Principal Building Standards

Building maximum	300 feet (see #8)	K
First floor to floor height	15 feet (min.) (see #5)	M
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)	L
Upper floor(s) height (floor-to-floor)	10 feet min.	N
Stepback height	Maximum 6 stories then stepback (see #13)	J
Stepback distance	10 feet min. (see #13)	I

7.1.4 Commercial Frontage Requirements

Ground floors of all buildings fronting on Type 'A' Streets and Plano Road shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

7.1.5 Special Frontage Requirements

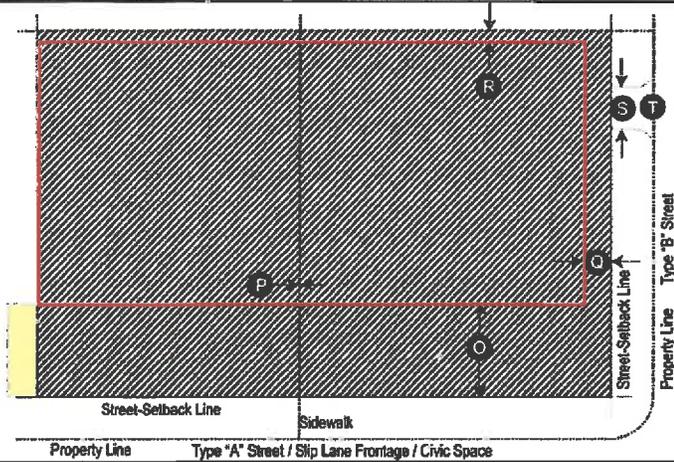
Requirements Specific to Main Street Frontage
Ground floors of all buildings designated as Main Street Frontage on the Regulating Plan shall not be occupied by residential units and lodging rooms to a minimum depth of 30 feet as measured from the front building line.

Requirements Specific To Station Frontage
Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.

Notes

- #1 – The area between the building and the edge of the BTZ at the public sidewalk shall be paved flush with the sidewalk.
- #2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.

7.1.6 Parking & Service Access



Property Line Type 'A' Street / Slip Lane Frontage / Civic Space

Legend

- Property Line
- Building Footprint
- Surface Parking Area
- Above Grade Parking Area
- Street-Setback Line

(i) Parking Location

Surface/At Grade Parking

Type 'A' Street, Slip Road Frontage and Civic Space setback	Shall be located behind the principal building	O
Type 'B' Street setback	Min. of 3 feet behind the building facade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R

Above Grade Parking

Setback along Type 'A' Street, Type 'B' Street, Slip Road Frontage and Civic Space (distance from SSL)	0 feet min.	O
Side and rear setbacks (distance from property line)	0 feet min.	
Upper Floors	May be built up to the building line	

(ii) Required Off-Street Parking Spaces (see #11 and #12)

Non-residential uses	1 space/300 square feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	20 feet max. (except when drives may need to be wider to address service access or fire lane standards)	S
Driveways and off-street loading and unloading shall not be located on a Type 'A' Streets.		T
Porte cocheres may be permitted on Type 'A' Streets to provide drop-off and valet service.		
Shared driveways and cross access easements are encouraged between lots to minimize curb cuts.		
If driveway and/or off-street service loading and unloading access is provided from a Type 'A' Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.		

7.1.7 Encroachments

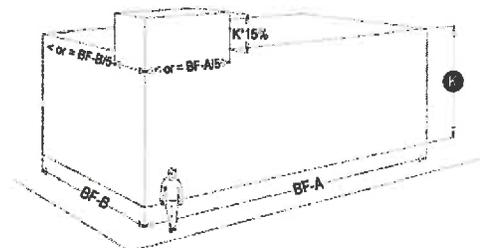
Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.1.8 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

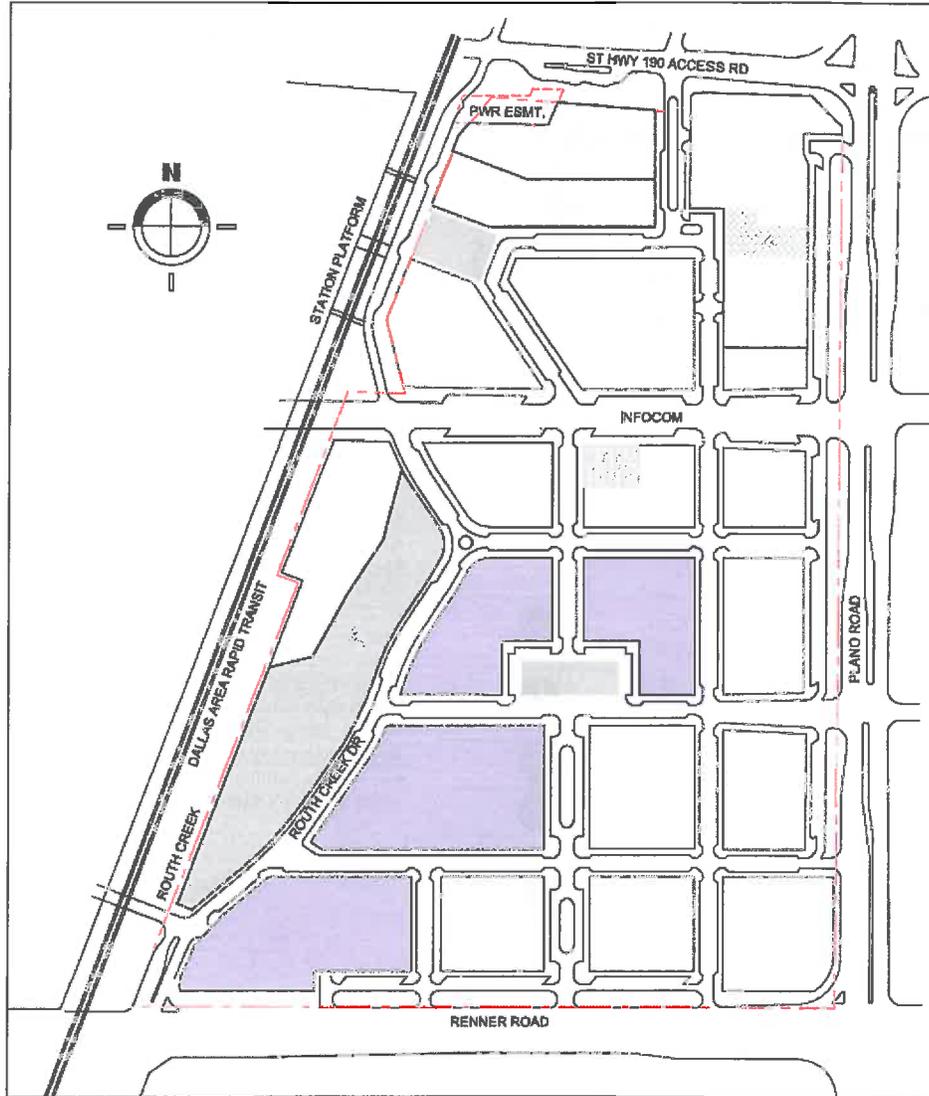
Notes

- #3 - Corner building street facades shall be built to the BTZ for a minimum of 30' from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to zone standards.
- #4 - Blocks exceeding the maximum block face and perimeter dimensions may be permitted as follows:
 - (i) No more than two adjacent blocks may be combined based on the Regulating Plan.
 - (ii) An increase in block dimensions shall not eliminate or significantly move a required street or required civic/open space. Required streets and required civic/open spaces may move a maximum of 100 feet.
 - (iii) The block shall maintain a continuous Type 'A' Street frontage with adjoining blocks.
 - (iv) Ground floor internal pedestrian connectivity shall be provided through the block. Connectivity may be provided inside the building, through a parking garage or outside by way of a pedestrian breezeway.
- #5 - First floor heights shall not apply to parking structures.
- #6 - All buildings in the TOD Core Zone shall meet the Building Design Standards in Section 8.
- #7 - Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen that is at least as high as the service equipment being screened. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Bush Central Station Planting List in Appendix B of this ordinance. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #8 - Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street facade.



- #9 - Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.
- #10 - Setbacks and build-to lines on recessed entries and arcade buildings shall be measured from the building facade line.
- #11 - Required parking may be provided anywhere within the Bush Central Station.
- #12 - Article III, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas.
- #13 - Stepback requirement shall not apply to any perimeter streets of the BCS-PD including S.H. 190 access road, Plano Road, Renner Road, Routh Creek Parkway, and DART R.O.W frontage.

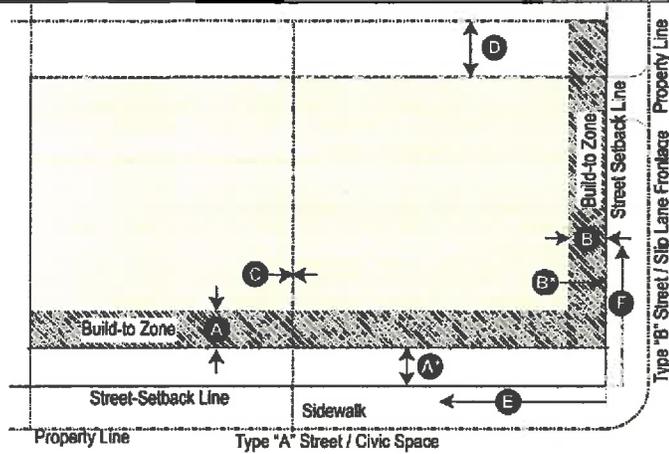
7.2 TOD Mixed Use Zone



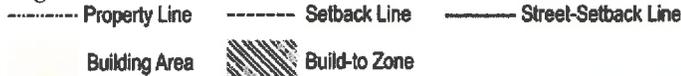
TOD Mixed Use Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements

7.2.1 Building Placement



Legend



**Street-Setback Line (SSL)
(Distance from center line of street cross section to edge of the BTZ)**

TOD Street Type A	35 feet
TOD Street Type B	31 feet
Parkway	36 feet
Court Street	26 feet
Slip Road Frontage	85 feet (measured from the R.O.W of Renner Road)

**Build-To Zone (BTZ)
(Distance from edge of SSL to edge of the BTZ)**

Front (Pedestrian Priority 'A' Street, Pedestrian Priority 'B' and Civic Space)		
Front (Type 'A' Street and Civic Space)	5 – 10 feet	A
Front (Type 'B' Street and Slip Road Frontage)	0 – 10 feet (see #1)	B

Setback

Front (Type 'A' Street and Civic Space – distance from SSL)	5 feet (min.) – 10 feet (max)	A*
Front (Type 'B' Street and Slip Road Frontage – distance from SSL)	0 feet (min.) – 10 feet (max)	B*
Side (distance from property line)	0 feet (see #2)	C
Rear (distance from property line)	0 feet (see #2)	

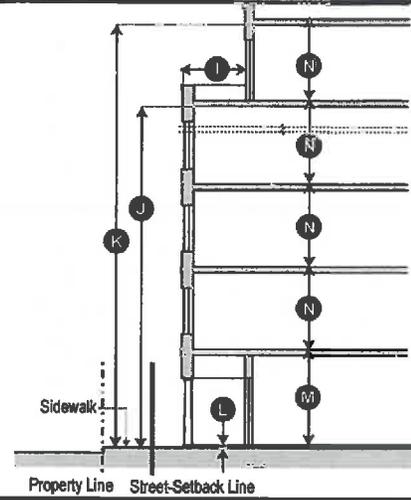
Building Frontage

Building Frontage required along Type 'A' Street/civic space BTZ	80% (min.) (see #3 and #7)	E
Building Frontage required along Type 'B' Street BTZ	0% (min.) (see #3 and #7)	F

7.2.2 Block Standards

Block face dimensions	250 – 400 feet (see #4)
Block perimeter	1500 feet (maximum) (see #4)

7.2.3 Building Height



Principal Building Standards

Building maximum	225 feet (see #8)	K
First floor to floor height	15 feet min. (see #5)	M
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)	L
Upper floor(s) height (floor-to-floor)	10 feet min.	N
Stepback height	Maximum 6 stories then stepback (see #13)	J
Stepback distance	10 feet min (see #13)	I

7.2.4 Commercial Frontage Requirements

Ground floors of all buildings fronting on Type 'A' Streets and Renner Road shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

7.2.5 Special Frontage Requirements

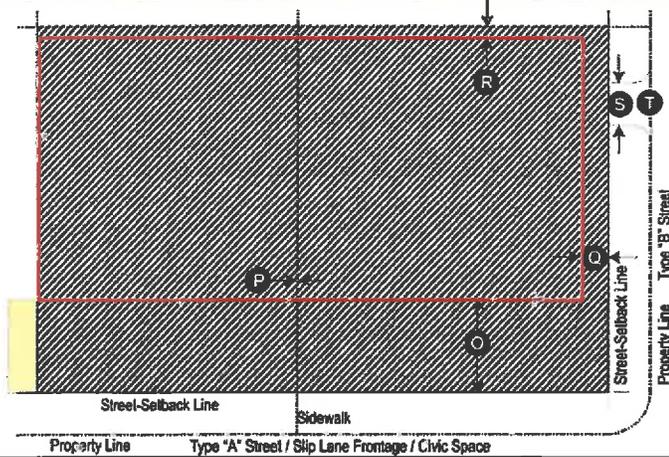
Requirements Specific to Neighborhood Frontage

All buildings designated Neighborhood Frontage on the Regulating Plan shall have a building height maximum of 4 stories to a minimum depth of 30 feet.

Notes

- #1 – The area between the building and the edge of the BTZ at the public sidewalk shall be paved flush with the sidewalk.
- #2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #3 – Corner building street facades shall be built to the BTZ for a minimum of 30 feet from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to zone standards.
- #4 – Blocks exceeding the maximum block face and perimeter dimensions may be permitted as follows:
 - i. No more than two adjacent blocks may be combined based on the Regulating Plan.
 - ii. An increase in block dimensions shall not eliminate or significantly move a required street or required civic/open space. Required streets and required civic/open spaces may move a maximum of 100 feet.
 - iii. The block shall maintain a continuous Type 'A' Street frontage with adjoining blocks.
 - iv. Ground floor internal pedestrian connectivity shall be provided through the block. Connectivity may be provided inside the building, through a parking garage or outside by way of a pedestrian breezeway.

7.2.6 Parking & Service Access



- Legend**
- Property Line
 - Surface Parking Area
 - ▨ Above Grade Parking Area
 - Building Footprint
 - Street-Setback Line

(i) Parking Location

Surface/At Grade Parking		
Type 'A' Street, Slip Road Frontage and Civic Space setback	Shall be located behind the principal building	○
Type 'B' Street setback	Min. of 3 feet behind the building facade line along that street	○
Side setback (distance from property line)	0 feet min.	○
Rear setback (distance from property line)	0 feet min.	○
Above Grade Parking		
Setback along Type 'A' Street, Type 'B' Street, Slip Road Frontage and Civic Space (distance from SSL)	0 feet min.	○
Side and rear setbacks (distance from property line)	0 feet min.	○
Upper Floors	May be built up to the building line	

(ii) Required Off-Street Parking Spaces (see #10 and #11)

Non-residential uses	1 space/300 square feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	20 feet max. (except when drives may need to be wider to address service access or fire lane standards))	○
Driveways and off-street loading and unloading shall not be located on a Type 'A' Streets.		
Porte cocheres may be permitted on Type 'A' Streets to provide drop-off and valet service.		
Shared driveways and cross access easements are encouraged between lots to minimize curb cuts.		
If driveway and/or off-street service loading and unloading access is provided from a Type 'A' Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.		

7.2.7 Encroachments

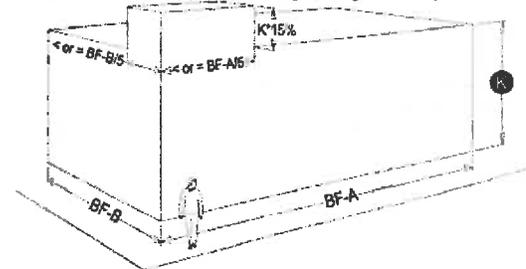
Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.2.8 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

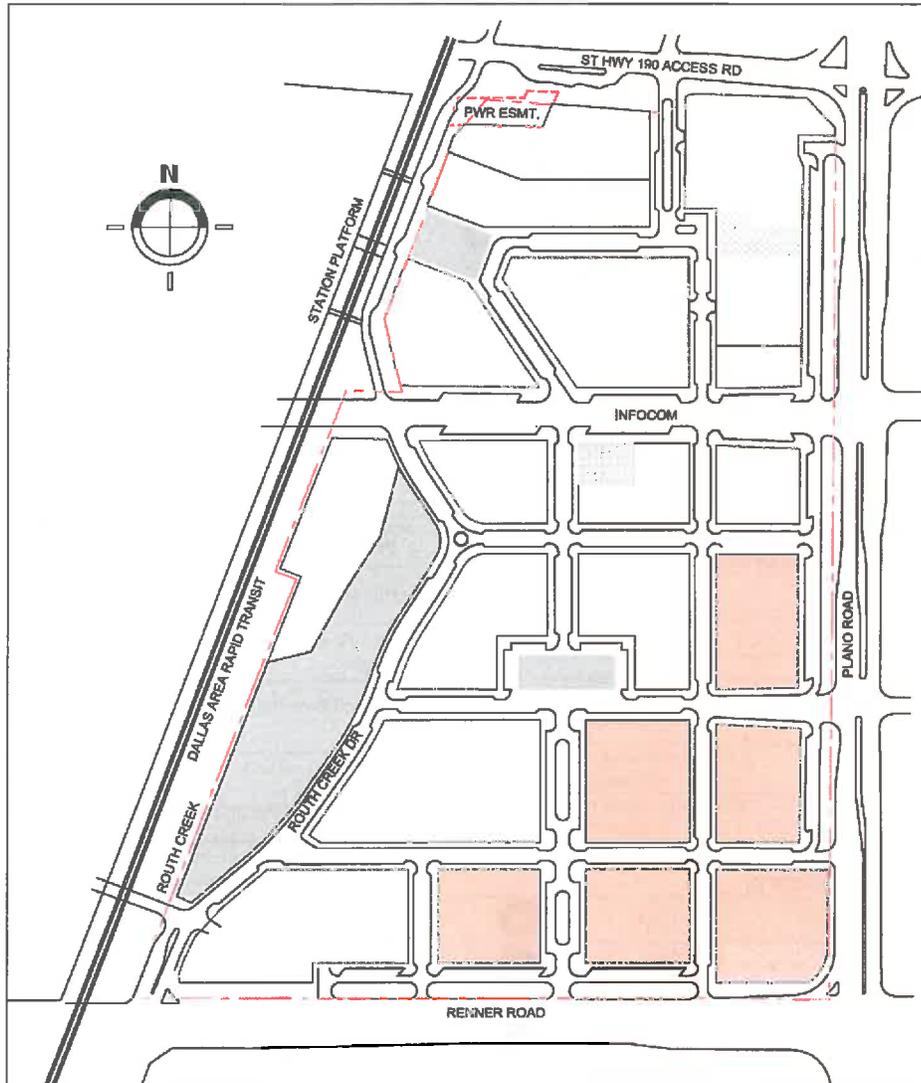
Notes

- #5 – First floor heights shall not apply to parking structures.
- #6 – All buildings in the TOD Mixed Use Zone shall meet the Building Design Standards in Section 8.
- #7 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen that is at least as high as the service equipment being screened. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Bush Central Station Planting List in Appendix B of this ordinance. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #8 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



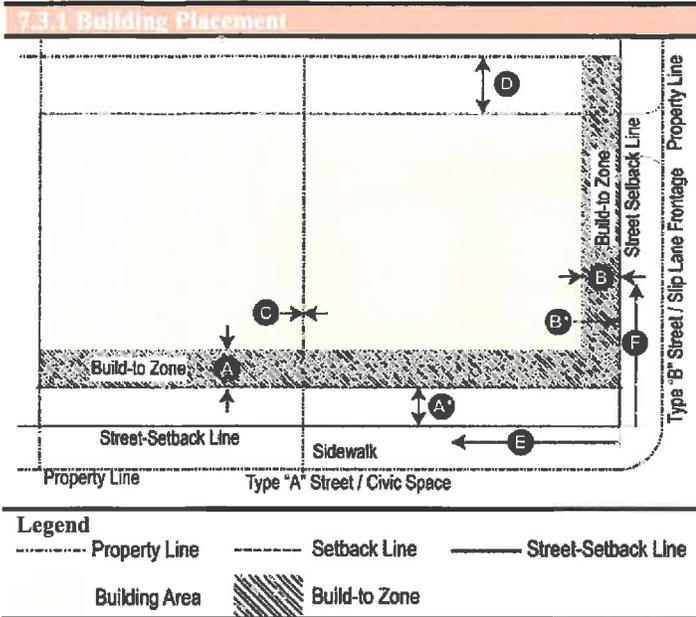
- #9 - Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.
- #10 – Setbacks and build-to lines on recessed entries and arcade buildings shall be measured from the building façade line.
- #11 – Required parking may be provided anywhere within the Bush Central Station.
- #12 – Article III, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas.
- #13 – Stepback requirement shall not apply to any perimeter streets of the BCS-PD including S.H. 190 access road, Plano Road, Renner Road, Routh Creek Parkway, and DART R.O.W frontage.

7.3 Arterial Mixed Use Zone



Arterial Mixed Use Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements



Street-Setback Line (SSL)
(Distance from center line of street cross section to edge of the BTZ)

TOD Street Type A	35 feet
TOD Street Type B	31 feet
TOD Main Street	61 feet
Park Avenue	51 feet
Slip Road Frontage (Plano Road)	85 feet (measured from the R.O.W of Plano Road)
Slip Road Frontage (Renner Road)	85 feet (measured from the R.O.W of Renner Road)

Build-To Zone (BTZ)
(Distance from SSL to edge of the BTZ)

Front (Type 'A' Street and Civic Space)	5 – 10 feet	A
Front (Type 'B' Street and Slip Road Frontage)	0 – 10 feet (see #1)	B

Setback

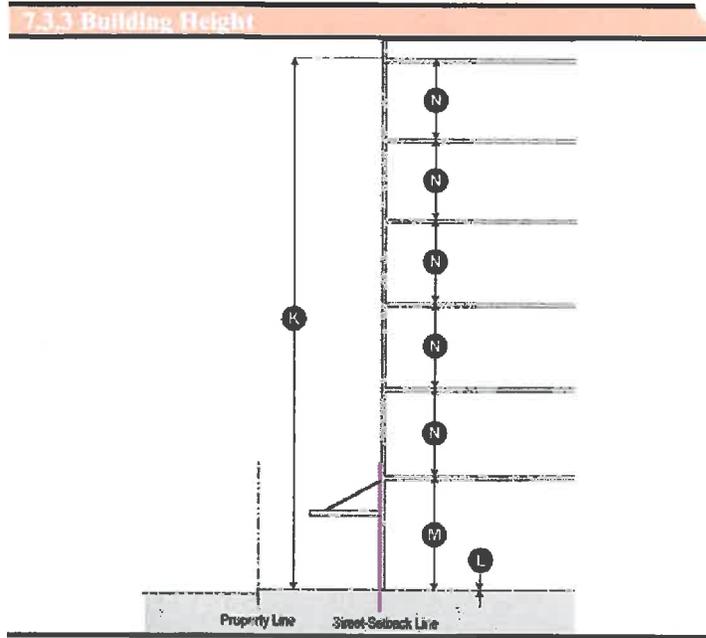
Front (Type 'A' Street and Civic Space – distance from SSL)	5 feet (min.) – 10 feet (max)	A*
Front (Type 'B' Street and Slip Road Frontage – distance from SSL)	0 feet (min.) – 10 feet (max)	B*
Side (distance from property line)	0 feet (see #2)	C
Rear (distance from property line)	0 feet	D

Building Frontage

Building Frontage required along Type 'A' Street/civic space BTZ	50% (min.) (see #3 and #7)	E
Building Frontage required along Type 'B' Street BTZ	0% (min.) (see #3 and #7)	F

7.3.2 Block Standards

Block face dimensions	250 – 400 feet (maximum) (see #4)
Block perimeter	1100 feet (maximum) (see #4)



Principal Building Standards

Building maximum	100 feet (see #8)	K
First floor to floor height	15 feet (min.) (see #5)	M
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)	L
Upper floor(s) height (floor-to-floor)	10 feet min.	N

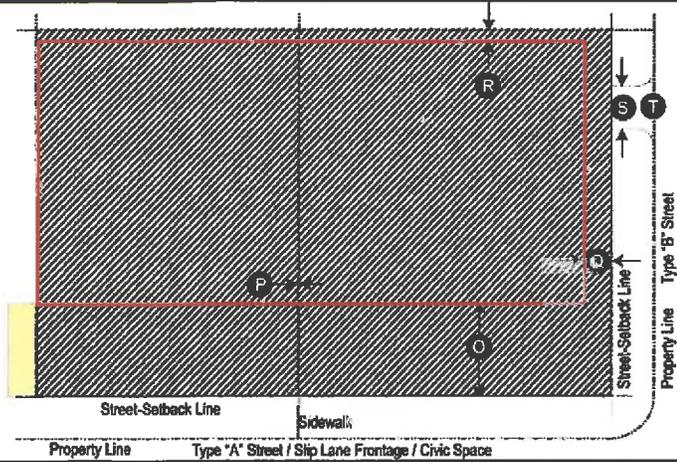
7.3.4 Commercial Frontage Requirements

Ground floors of all buildings fronting on Type 'A' Streets, Plano Road and Renner Road shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

Notes

- #1 – The area between the building and the edge of the BTZ at the public sidewalk shall be paved flush with the sidewalk.
- #2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #3 – Corner building street facades shall be built to the BTZ for a minimum of 15' from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to-zone standards.
- #4 – Blocks exceeding the maximum block face and perimeter dimensions may be permitted as follows:
 - i. Combined blocks shall be adjacent to one another based on the Regulating Plan.
 - ii. An increase in block dimensions shall not eliminate or significantly move a required street or required civic/open space. Required streets and required civic/open spaces may move a maximum of 100 feet.
 - iii. The block shall maintain a continuous Type 'A' Street frontage with adjoining blocks.
 - iv. Ground floor internal pedestrian connectivity shall be provided through the block. Connectivity may be provided inside the building, through a parking garage or outside by way of a pedestrian breezeway.

7.3.5 Parking & Service Access



- Legend**
- Property Line
 - Building Footprint
 - Surface Parking Area
 - Above Grade Parking Area
 - Street-Setback Line

(i) Parking Location

Surface/At Grade Parking		
Type 'A' Street, Slip Road Frontage and Civic Space setback	Shall be located behind the principal building	O
Type 'B' Street setback	Min. of 3 feet behind the building facade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R
Above and Below Grade Parking (Distance from SSL)		
Setback along Type 'A' Street, Type 'B' Street, Slip Road Frontage and Civic Space (distance from SSL)	0 feet min.	O
Side and rear setbacks (distance from property line)	0 feet min.	
Upper Floors	May be built up to the building line	

(ii) Required Off-Street Parking Spaces (see #11 and #12)

Non-residential uses	1 space/300 square feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	City standards on Arterial Roadways and 24 feet max. on all Bush Central Station Streets (except when drives may need to be wider to address service access or fire lane standards)	S
Driveways and off-street loading and unloading	shall not be located on a Type 'A' Streets.	T
Porte cocheres	may be permitted on Type 'A' Streets to provide drop-off and valet service.	
Shared driveways and cross access easements	are encouraged between lots to minimize curb cuts.	
Driveway and/or off-street service loading and unloading access	is provided from a Type 'A' Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.	

7.3.6 Encroachments

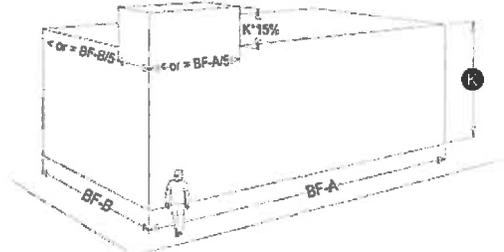
Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.3.7 Applicability

Building Form and Development Standards in this Section shall apply to all development in this Character Zone.

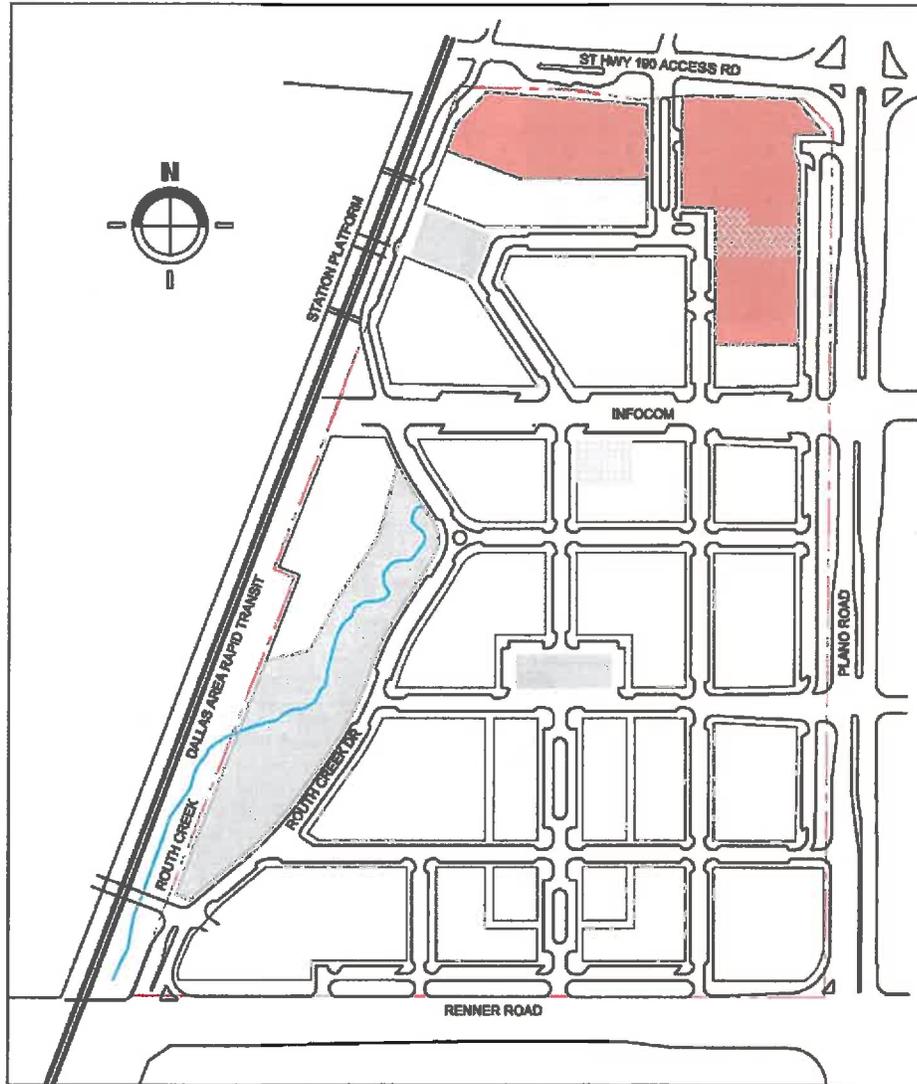
Notes

- #5 – First floor heights shall not apply to parking structures.
- #6 – All buildings in the Arterial Mixed Use Zone shall meet the Building Design Standards in Section 8.
- #7 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen that is at least as high as the service equipment being screened. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Bush Central Station Planting List in Appendix B of this ordinance. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #8 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



- #9 - Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.
- #10 – Setbacks and build-to lines on recessed entries and arcade buildings shall be measured from the building façade line.
- #11 – Required parking may be provided anywhere within the Bush Central Station.
- #12 – Article III, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas.

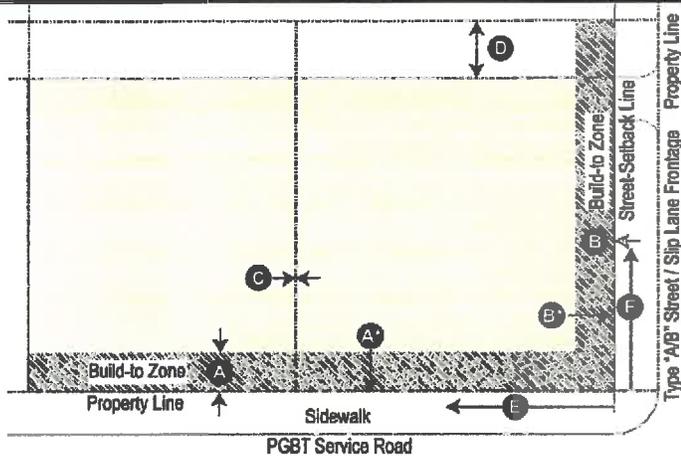
7.4 Highway Mixed Use



Highway Mixed Use Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements

7.4.1 Building Placement



Legend



**Street-Setback Zone (SSL)
(Distance from center line of street cross section to edge of the BTZ)**

TOD Avenue	44 feet
TOD Street Type B	31 feet
Slip Road Frontage	85 feet (measured from the R.O.W of Plano Road)

**Build-To Zone (BTZ)
(Distance from Property Line to edge of the BTZ)**

Front (PGBT Service Road)	None	A
---------------------------	------	----------

(Distance from SSL to edge of the BTZ)

Front (Type "A/B" Street and Slip Road Frontage)	0 – 10 feet (see #1)	B
--	----------------------	----------

Setback

Front (PGBT Service Road – distance from property line or edge of utility easement)	15 feet (min.)	A*
Front (Type "A/B" Street and Slip Road Frontage – distance from SSL)	0 feet (min) – 10 feet (max)	B*
Side (distance from property line)	0 feet (see #2)	C
Rear (distance from property line)	0 feet	D

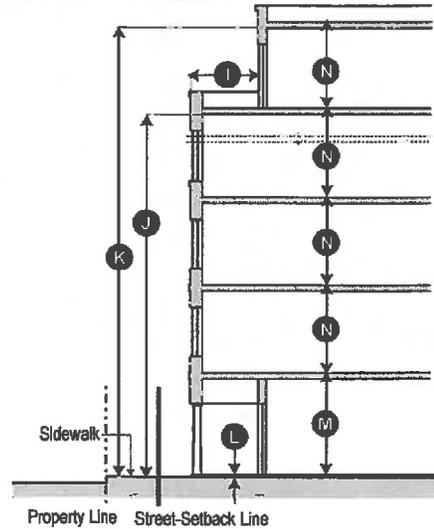
Building Frontage

Building Frontage required along Type 'A' Street BTZ	90% (min.) (see #3 and #7)	
Building Frontage required along PGBT Service Road	0% (min.) (see #3 and #7)	E
Building Frontage required along Type 'B' Street BTZ	0% (min.) (see #3 and #7)	F

7.4.2 Block Standards

Block face dimensions	250 – 400 feet (maximum) (see #4)
Block perimeter	1400 feet (maximum) (see #4)

7.4.3 Building Height



Principal Building Standards

Building maximum	300 feet (see #8)	K
First floor to floor height	15 feet min. (see #5)	M
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)	L
Upper floor(s) height (floor-to-floor)	10 feet min.	N
Stepback height	Maximum 10 stories then stepback (see #13)	J
Stepback distance	10 feet min.	I

7.4.4 Commercial Frontage Requirements

Ground floors of all buildings fronting on President George Bush Turnpike (PGBT) service road and Plano Road shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

7.4.5 Special Frontage Requirements

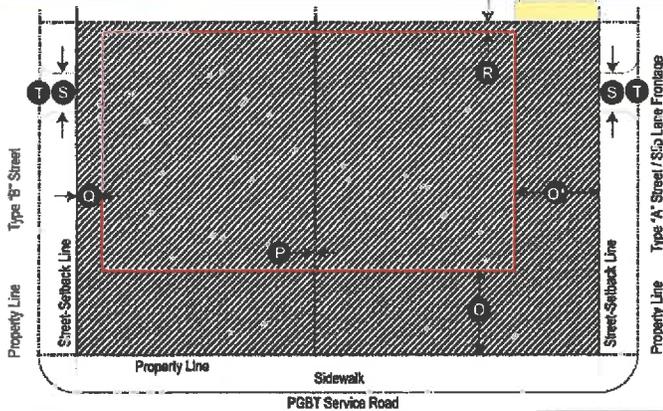
Requirements Specific To Station Frontage

Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.

Notes

- #1 – The area between the building and the edge of the BTZ at the public sidewalk shall be paved flush with the sidewalk.
- #2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #3 – Corner building street facades shall be built to the BTZ for a minimum of 15 feet from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to zone standards.

7.4.6 Parking & Service Access



Legend

Property Line	Building Footprint
Surface Parking Area	Street-Setback Line
Above Grade Parking Area	

(i) Parking Location

Surface/At Grade Parking

PGBT Service Road setback	Shall be located behind the principal building	O
Type 'A' Street and Slip Road Frontage	Shall be located behind the principal building	O
Type 'B' Street setback	Min. of 3 feet behind the building façade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R

Above Grade Parking

Setback along Type 'A' Street, Type 'B' Street, Slip Road Frontage and Civic Space (distance from SSL)	0 feet min.	O
PGBT Service Road, side and rear setbacks (distance from property line)	0 feet min.	

Upper Floors	May be built up to the building line	
--------------	--------------------------------------	--

(ii) Required Off-Street Parking Spaces (see #11 and #12)

Non-residential uses	1 space/300 square feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	TXDOT standards on service road and 24 feet max on all Bush Central Station Streets (except when drives may need to be wider to address service access or fire lane standards)	S
Driveways and off-street loading and unloading shall not be located on PGBT Service Road. Porte cocheres may be permitted on Type 'A' Streets to provide drop-off and valet service. Shared driveways and cross access easements are encouraged between lots to minimize curb cuts. If driveway and/or off-street service loading and unloading access is provided from PGBT Service Road, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.		T

7.4.7 Encroachments

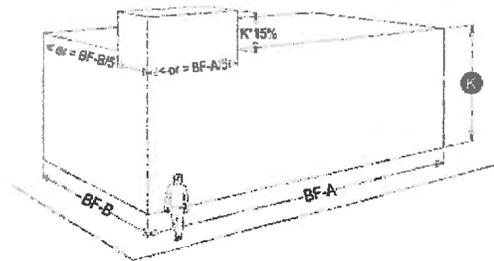
Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.4.8 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

Notes

- #4 – Blocks exceeding the maximum block face and perimeter dimensions may be permitted as follows:
 - i. No more than two adjacent blocks may be combined based on the Regulating Plan.
 - ii. An increase in block dimensions shall not eliminate or significantly move a required street or required civic/open space. Required streets and required civic/open spaces may move a maximum of 100 feet.
 - iii. The block shall maintain a continuous Type 'A' Street frontage with adjoining blocks.
 - iv. Ground floor internal pedestrian connectivity shall be provided through the block. Connectivity may be provided inside the building, through a parking garage or outside by way of a pedestrian breezeway.
- #5 – First floor heights shall not apply to parking structures.
- #6 – All buildings in the Highway Mixed Use shall meet the Building Design Standards in Section 8.
- #7 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen that is at least as high as the service equipment being screened. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Bush Central Station Planting List in Appendix B of this ordinance. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #8 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



- #9 – Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.
- #10 – Setbacks and build-to lines on recessed entries and arcade buildings shall be measured from the building façade line.
- #11 – Required parking may be provided anywhere within the Bush Central Station.
- #12 – Article III, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas.
- #13 – Stepback requirement shall not apply to any perimeter streets of the BCS-PD including S.H. 190 access road, Plano Road, Renner Road, Routh Creek Parkway, and DART R.O.W frontage.

Section 8. Building Design Standards

The Building Design Standards and Guidelines for the Bush Central Station shall establish a coherent urban character and encourage enduring and attractive development. Development plans shall be reviewed by the City Manager or designee for compliance with the standards below.

The key design principles establish essential goals for development in the Bush Central Station to ensure the preservation, sustainability, and visual quality of this unique environment. Buildings shall be located and designed so that they provide visual interest and create enjoyable, human-scaled spaces. The key design principles are:

- a. New buildings shall utilize building elements and details to achieve a pedestrian-oriented public realm.
- b. Compatibility is not meant to be achieved through uniformity, but through the use of variations in building elements to achieve individual building identity.
- c. Building facades shall include appropriate architectural details and ornament to create variety and interest.
- d. Open space(s) shall be incorporated to provide usable public areas integral to the urban environment.

8.1 General to all Character Zones

8.1.1 Building Orientation

- i. Buildings shall be oriented towards Type 'A' Streets, where the lot has frontage along Type 'A' Streets. All other buildings shall be oriented towards Type 'B' Streets or Civic Spaces.
- ii. Primary entrance to buildings shall be located on the street along which the building is oriented. At intersections, corner buildings may have their primary entrances oriented at an angle to the intersection.
- iii. All primary entrances shall be oriented to the public sidewalk for ease of pedestrian access. Secondary and service entrances may be located from internal parking areas or alleys.

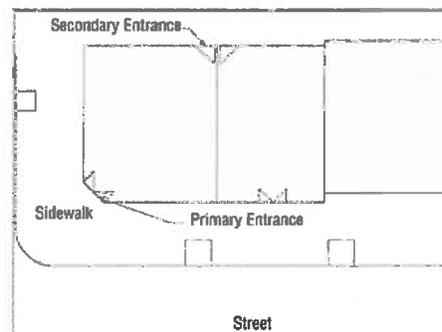


Figure showing required building orientation and location of primary entrances

8.1.2 Design of Parking Structures

- i. All frontages of parking structures located on Type 'A' Streets shall not have parking uses on the ground floor to a minimum depth of 30 feet along the Type 'A' Street frontage. If the frontage is along a designated Main Street Frontage, then the Main Street Frontage requirement shall supersede.
- ii. The amount of Type 'A' Street frontage devoted to a parking structure shall be minimized by placing the shortest dimension(s) along the Type 'A' Street edge(s).

- iii. Parking structure facades on all Type 'A' Streets shall be designed with both vertical (façade rhythm of 20 feet to 30 feet) and horizontal (aligning with horizontal elements along the block) articulation.
- iv. Where above ground structured parking is located at the perimeter of a building with frontage along a Type 'A' Street; it shall be screened in such a way that cars on all parking levels are completely hidden from view from all adjacent public streets. Parking garage ramps shall not be visible from any public street. Ideally, ramps should not be located along the perimeter of the parking structure. Architectural screens shall be used to articulate the façade, hide parked vehicles, and shield lighting.
- v. When parking structures are located at corners, corner architectural elements shall be incorporated such as corner entrance, signage and glazing.
- vi. Parking structures and adjacent sidewalks shall be designed so pedestrians are clearly visible to entering and exiting automobiles.



Images showing appropriate design of Parking Structures

8.1.3 Design of Automobile Related Building Site Elements

- i. Drive-through lanes for commercial uses shall not be located along any Type 'A' Street. Drive-through lanes shall be hidden behind a Street Screen along the Type 'B' Street frontage.
- ii. All off-street loading, unloading, and trash pick-up areas shall be located along alleys or Type 'B' Streets only unless permitted in the specific building form and development standards in Section 7. Any off-street loading, unloading, or trash pick-up areas shall be screened using a Street Screen that is at least as tall as the trash containers and/or service equipment it is screening at the BTZ. The Street Screen shall be made up of (i) the same material as the principal building or (ii) a living screen or (iii) a combination of the two.

8.1.4 Roof Form

- i. Buildings shall have simple, flat fronts with minimal articulations with flat or low pitched roofs with parapets. Corner hip roof elements and gable accents at the parapet may be permitted. Projecting mansard roofs shall not be permitted.



8.1.5 Façade Composition

- i. Buildings shall maintain a façade rhythm of 20 feet to 30 feet along all Type 'A' Streets.

- ii. This rhythm may be expressed by changing materials, or color, or by using design elements such as fenestration, columns and pilasters, or by varying the setback of portions of the façade.
- iii. Buildings shall be designed and built in tri-partite architecture so that they have a distinct Base, Middle and Cap.



Image of Tri-Partite Architecture

- iv. An expression line or equivalent architectural element shall delineate the Base and Cap of all buildings. A Cornice shall delineate the caps of facades that do not utilize a pitched roof.
- v. For retail storefront buildings, a transom, display window area, and bulkhead at the base shall be utilized.

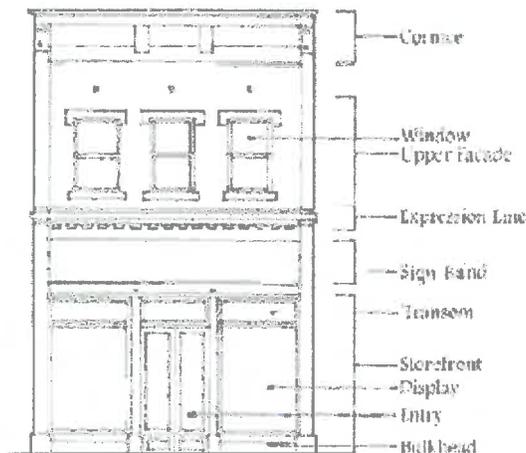


Image of a typical storefront

- vi. Storefronts on facades that span multiple tenants shall use architecturally compatible materials, colors, details, awnings, signage, and lighting fixtures.
- vii. Building entrances may be defined and articulated by architectural elements such as lintels, pediments, pilasters, columns, porticos, porches, overhangs, railings, balustrades, and others as appropriate. All building elements should be compatible with the architectural style, materials, colors, and details of the building as a whole. Entrances to upper level uses may be defined and integrated into the design of the overall building facade.
- viii. Buildings shall generally maintain the alignment of horizontal elements along the block.

- ix. Corner emphasizing architectural features, pedimented gabled parapets, cornices, awnings, blade signs, arcades, colonnades and balconies may be used along commercial storefronts to add pedestrian interest.



Buildings with architectural features and storefront elements that add interest along the street.

- x. Buildings which are located on axis with a terminating street or at the intersection of streets shall be considered as feature buildings. Such buildings shall be designed with features which take advantage of that location, such as an accentuated entry and a unique building articulation which is off-set from the front wall planes and goes above the main building eave or parapet line.

8.1.6 Windows and Doors

- i. Windows and doors on street (except alleys) fronting facades shall be designed to be proportional and appropriate to the specific architectural style of the building. First floor windows shall NOT be opaque, tinted or mirrored glass.
- ii. All ground floor front facades of buildings along Type 'A' Streets or Civic/Open Space shall have transparent storefront windows covering no less than 50% of the façade area. Each upper floor of the same building facades facing a Type 'A' Street or Civic/Open Space shall contain transparent windows covering at least 30% of the façade area. All other street facing facades (except alleys) shall have transparent windows covering at least 15% of the façade area for all floors.



Images showing appropriate storefront display and transparency

8.1.7 Commercial and Mixed Use Building Materials

- i. At least 85% of each building's Base façade (excluding doors and windows) along any Type 'A' Street, Plano or Renner Roads shall be finished in one of the following materials:
- Masonry (brick, stone, stucco utilizing a three-step process, cast stone, rock, marble, granite, curtain glass, or glass block)
- ii. No more than 15% of each Base façade along any Type 'A' Street, Plano, or Renner Road shall use accent materials such as wood, architect metal panel, split-face concrete block,

tile, or pre-cast concrete panels. EIFS shall not be permitted along any Type 'A' Street or Plano or Renner Road facades.



Images showing appropriate building materials within TOD Core, TOD Mixed Use, Arterial and Highway Mixed Use.

- iii. A building's Middle and Cap facades along Type 'A' Streets, Plano, or Renner Roads and all facades along Type 'B' Streets or alleys shall be of a similar finished quality and color that blend with the front of the building. Building materials for these facades may be any of the primary and accent façade materials listed above. EIFS shall not be permitted along any Type 'A' Street façades, ground floor facades along Type 'B' Streets and ground floor facades of alleys. EIFS on upper floors of Type 'B' Street and alley facades are limited to no more than 10% of the respective façade areas. Cementitious-fiber clapboard (not sheet) with at least a 50-year warranty may only be used on the upper floors only of any commercial frontage on any street or alley façade and are limited to no more than 15%.
- iv. Roofing materials visible from any public right-of-way shall be copper, factory finished standing seam metal, slate, synthetic slate, or similar materials.

8.2 Standards Specific to Development with Neighborhood Frontage Designation:

8.2.1 Building Orientation

- i. Garages and carport for Residential Buildings shall be located on alleys at the rear of residential buildings.

8.2.2 Building Massing and Scale

- i. Residential buildings shall have few, if any, articulations and simple roofs (gable, hip, combination) with most building wing articulations set at the rear of the structure. Window projections, bay windows, stoops, porches, balconies, and similar extensions shall be exempt from this standard.
- ii. Gable roofs, if provided for residential buildings, shall have a minimum pitch of 5/12. When hipped roofs are used, the minimum pitch shall be 5/12. Other roof types shall be appropriate to the architectural style of the building. Porch roofs may be a minimum pitch of 3/12.
- iii. Projecting mansard roofs shall be prohibited.



Images showing appropriate massing and scale for Residential Buildings

8.2.3 Façade Composition

- i. Buildings shall maintain a façade rhythm of 20 feet to 30 feet along Type 'A' Streets. This rhythm may be expressed by changing materials, or color, or by using design elements such as columns and pilasters, or by varying the setback of portions of the building façade.
- ii. At least one of the following -- porches, stoops, eaves or balconies --shall be added along the front residential facades to add pedestrian interest along the street.
- iii. For residential buildings the grade of the slab or first floor elevation shall be elevated at least 18 inches above the grade of the sidewalk.
- iv. Alley and/or Type 'B' Street facing facades shall be of finished quality and of the same color that blend with the front of the building.



Residential buildings with porches, balconies, and stoops to add interest along the street.

8.2.4 Windows and Doors

- i. Windows and doors shall be designed to be proportional and appropriate to the architectural style of the building.
- ii. Windows may have jack arch, keystone arch, flat arch, or ornamental arches.
- iii. All building facades of residential buildings fronting on all streets or civic / open spaces, except alleys, shall have transparent windows covering at least 15% of each façade.



Images showing appropriate window designs and proportions.

8.2.5 Residential Building Materials

- i. At least 85% of the Type 'A' Street facing facades of all buildings (excluding doors and windows) shall be finished in one or more of the following materials. No more than three different materials shall be used on any single residential façade:
 - Cementitious-fiber clapboard (not sheet) with at least a 50-year warranty.
 - Masonry (brick; stone; man-made stone, or stucco utilizing a three-step process).
- ii. The following may only be allowed up to 15% as an accent material:
 - Architectural metal panels or similar material over a cementitious base, rock, glass block and tile.
- iii. EIFS shall not be permitted along any Type 'A' Street facades.

- iv. Side and rear facades shall be of finished quality and of the same color that blend with the front of the building. Ground floor side and rear facades shall not be EIFS. On upper floors of side and rear facades, EIFS is limited to no more than 10% of the façade area.
- v. Roofing materials (visible from any public right-of-way): copper, factory finished painted metal, slate, synthetic slate, terra cotta, and asphalt shingles (laminated and classified as being at least 300 pounds/100 sf).
- vi. An enclosed garage or carport shall be designed and constructed of the same material as the primary building.

Section 9. Street & Streetscape Design Standards

9.1 Generally: Streets in the Bush Central Station need to support the overall goal of a mixed use, compact, pedestrian oriented district. They should balance all forms of mobility while maximizing convenience for residents and visitors.

The Regulating Plan designates the required and recommended street network within the Bush Central Station. This section specifies the typical configuration of streets within the Bush Central Station. The specifications address vehicular lane width, parkway widths, R.O.W widths, number of travel lanes, on-street parking, and pedestrian accommodation. The character of streets in the Bush Central Station will vary based on the location. The service road of President George Bush Turnpike (State Highway 190) is under the purview of TxDOT while the remaining streets are city streets.

9.2 New Streets: This section specifies standards for all new streets in the Bush Central Station. New streets shall be based on the Mandatory or Non-Mandatory designation on the Regulating Plan.

9.3 Street Classification Established: Table 9.1 and associated cross sections shall establish the cross sections for each street type. The cross sections may be adjusted to fit existing contexts with the approval of the City Engineer. In addition, the proposed cross sections may be adjusted to meet the needs of the Uniform Fire Code as adopted by the City.

Table 9.1

Elements Street Classification	Street Width (Recommended minimum)		Number of Vehicular Lanes	Number of Bike Lanes	Vehicular Lane Widths	Bike Lane Width	On-Street Parking	Pedestrian Sidewalk Width (min.)	Parkway/ Tree Well
	R.O.W	Pedestrian Easement (on both sides)							
TOD Main Street	90 feet	16 feet	4	2	10.5 feet	5 feet	Yes, both sides, reverse angled	16 feet	Tree Well, 5 X 5 feet min.
Parkway	40 feet	16-18 feet	2	N/A	12 feet	N/A	Yes, both sides, parallel	16-18 feet	Tree Well, 5X5 feet
TOD Avenue	64 feet	12 feet	2	N/A	14 feet	N/A	Yes, both sides, parallel	12 feet	Tree Well, 5 X 5 feet min.
Park Avenue	74 feet	14 feet	2	N/A	14 feet	N/A	Yes, both sides, parallel	8 feet	Tree Well, 5 x 5 feet min. or Parkway, 6 feet min.
TOD Street Type 'A'	38 feet	16 feet	2	N/A	11 feet	N/A	Yes, both sides, parallel	16 feet	Tree Well, 5 X 5 feet min.
TOD Street Type 'B'	38 feet	12 feet	2	N/A	11 feet	N/A	Yes, both sides, parallel	12 feet	Tree Well, 5 X 5 feet min.
Court Street	20 feet	16 feet	1	N/A	12 feet	N/A	Yes, one side, parallel	16 feet	Tree Well, 5 X 5 feet min.
Plano Road Slip Road Frontage	60 feet	15 feet	1	N/A	16 feet One Way	N/A	Yes, one side, angled; other side parallel	15 feet	Tree Well, 5 X 5 feet min.

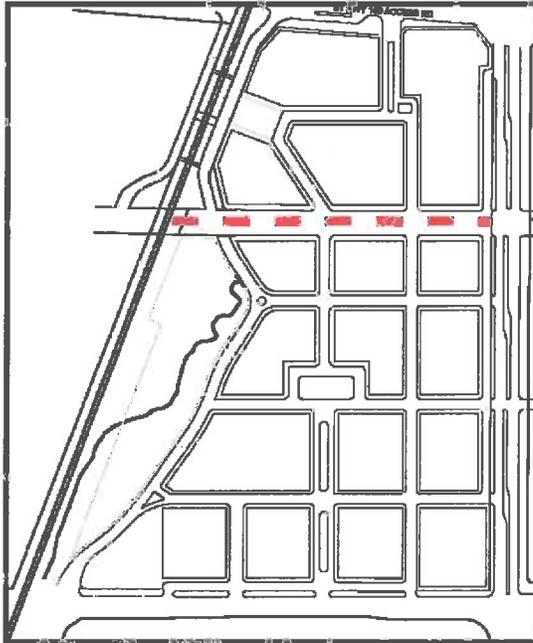
**BUSH CENTRAL STATION
PLANNED DEVELOPMENT CODE**

Elements Street Classification	Street Width (Recommended minimum)		Number of Vehicular Lanes	Number of Bike Lanes	Vehicular Lane Widths	Bike Lane Width	On-Street Parking	Pedestrian Sidewalk Width (min.)	Parkway/ Tree Well
	R.O.W	Pedestrian Easement (on both sides)							
Remmer Road Slip Road Frontage	60 feet	15 feet	1	N/A	16 feet One Way	N/A	Yes, one side, angled; other side parallel	15 feet	Tree Well, 5 X 5 feet min.
Commercial Alley	26 feet (16 feet of paving)	None	N/A	None	N/A	N/A	None	None	None

9.4 Street Cross Sections

9.4.1 TOD Main Street

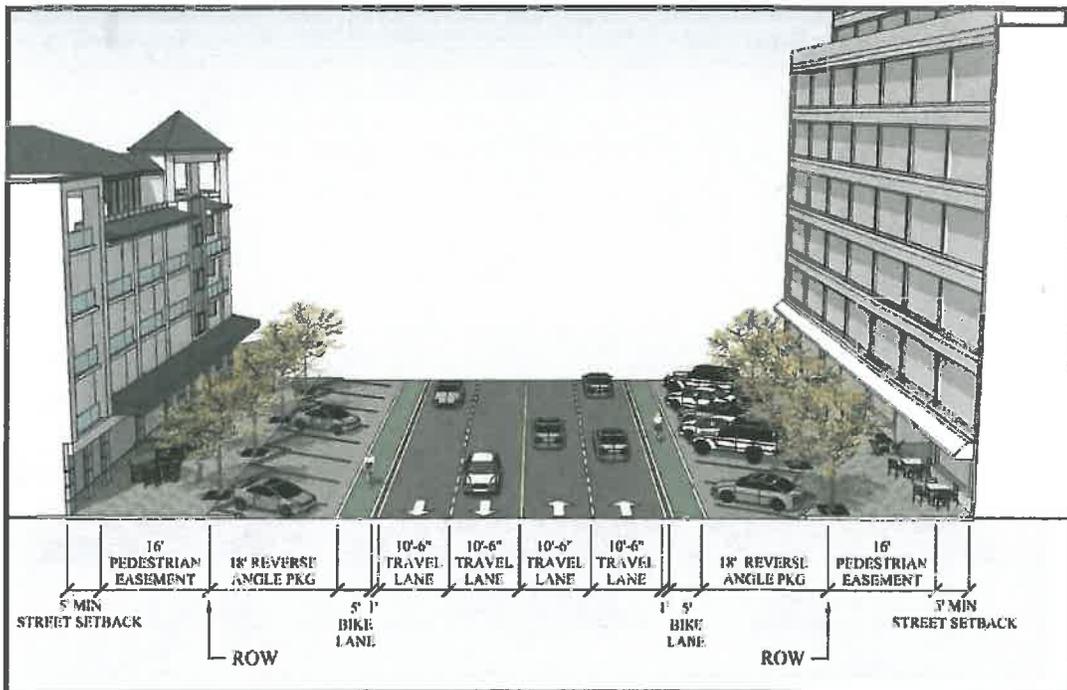
Locator Map



TOD Main Street

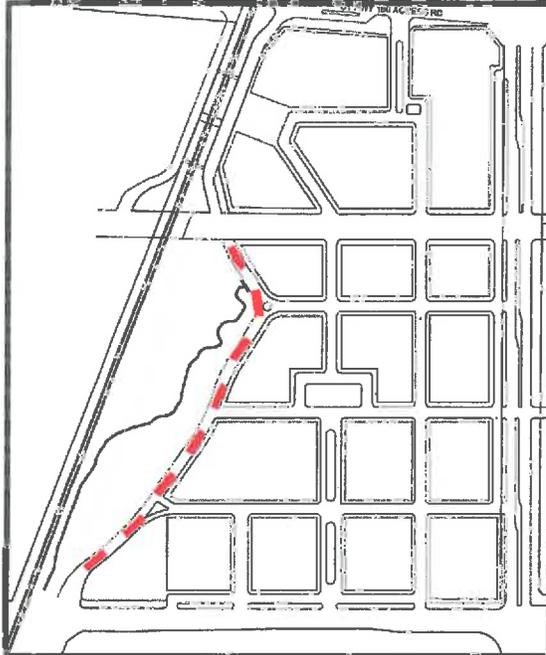
Street Type: A
ROW: 90'
Design Speed: 25 MPH
Parking: Reverse Angle on both sides
Number of Vehicular Lanes: 4
Number of Bike Lanes: 2
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees (@ 40' OC avg)

Street Section



9.4.2 Parkway

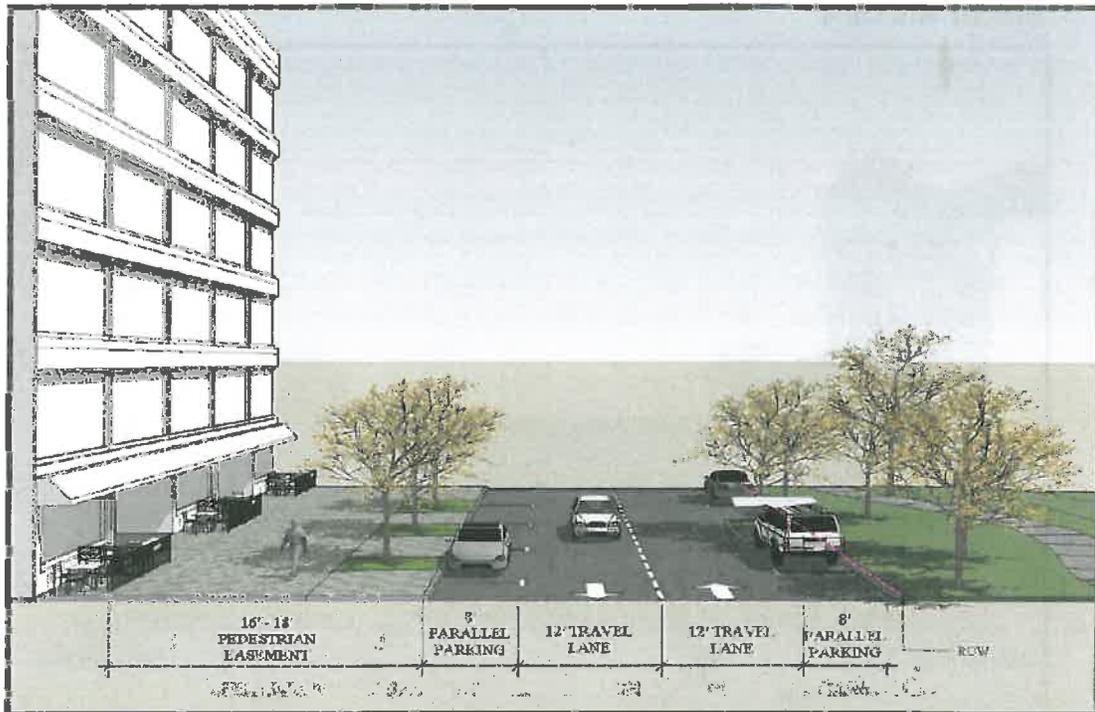
Locator Map



Parkway

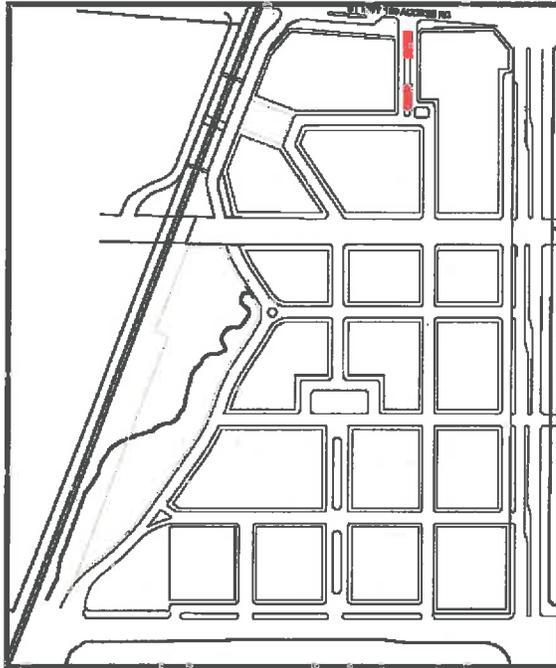
Street Type: A
ROW: 40'
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.3 TOD Avenue

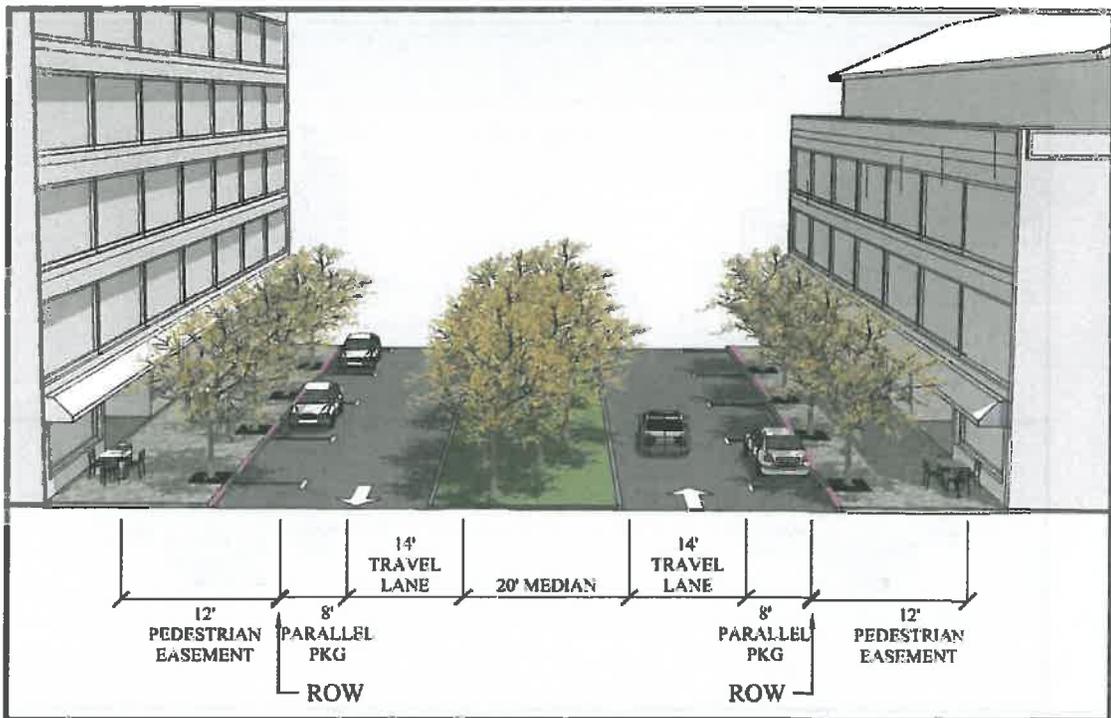
Locator Map



TOD Avenue

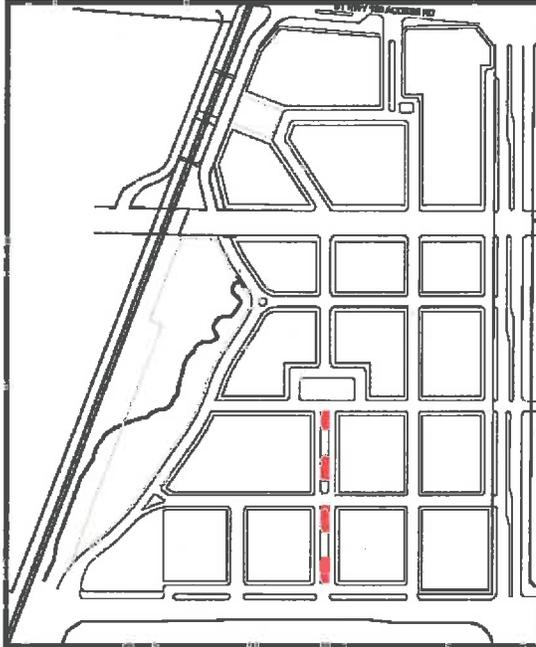
Street Type: B
ROW: 64'
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.4 Park Avenue

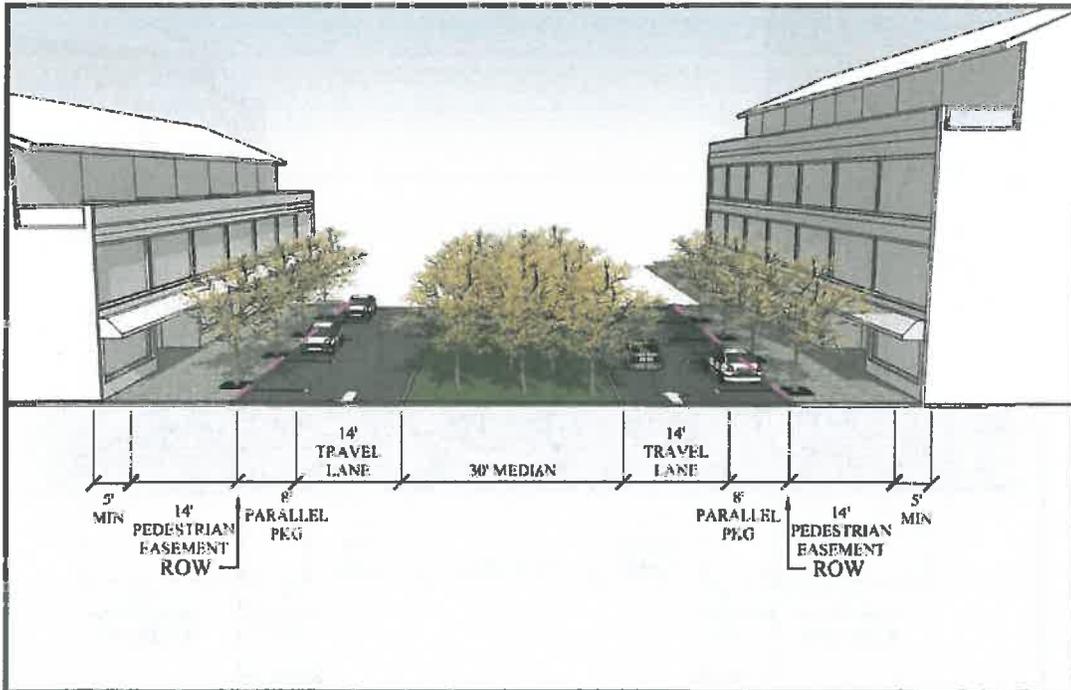
Locator Map



Park Avenue

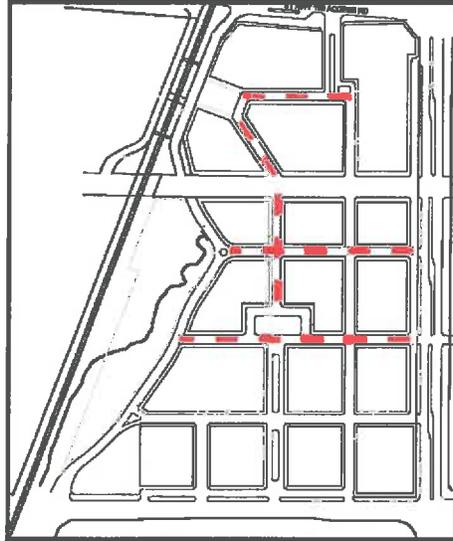
Street Type: A
ROW: 74'
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.5 TOD Street Type A

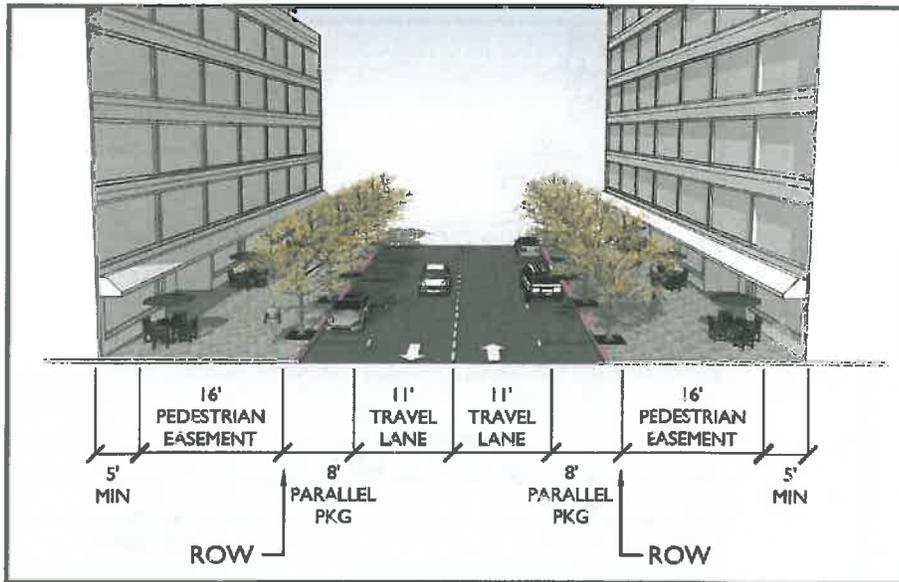
Locator Map



TOD Street Type A

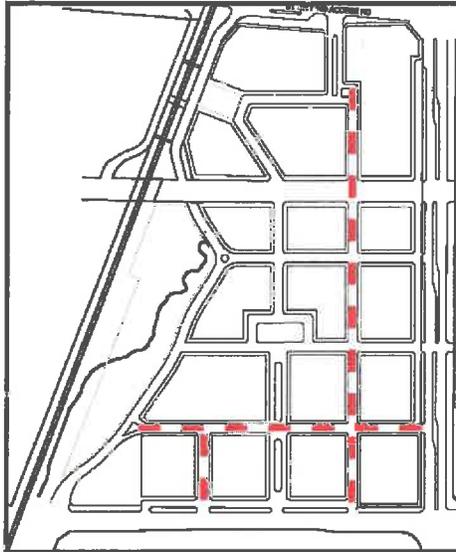
Street Type: A
ROW: 38'
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.6 TOD Street Type B

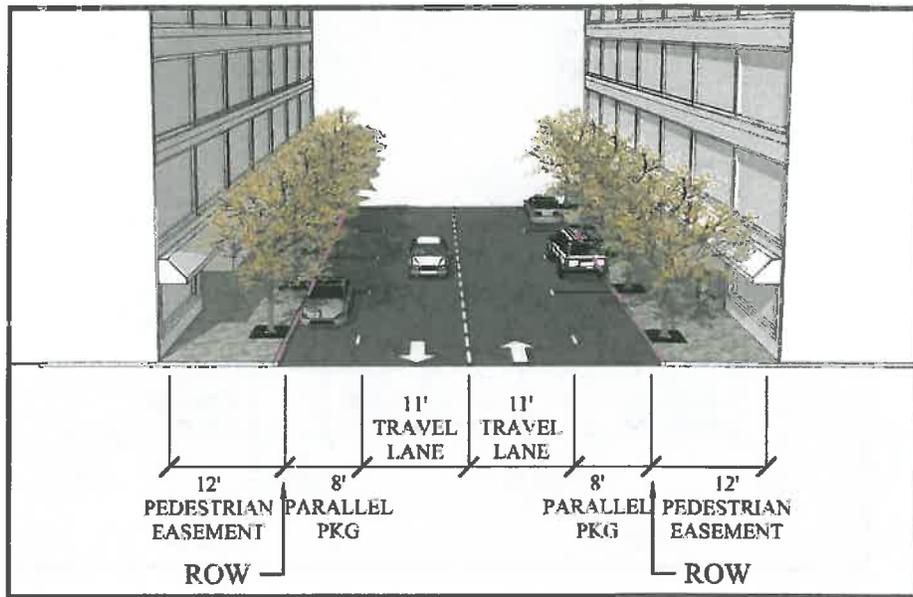
Locator Map



TOD Street Type B

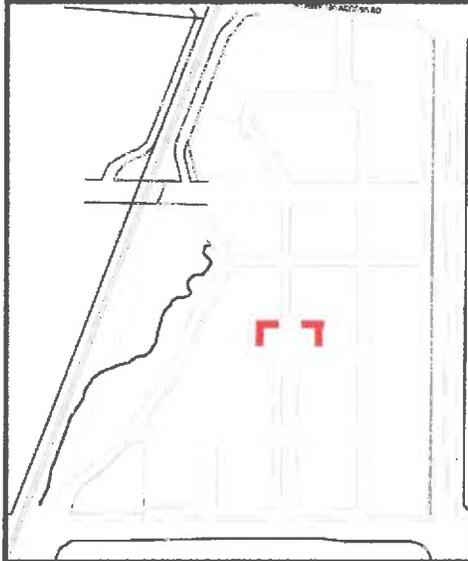
Street Type: B
ROW: 38'
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.7 Court Street

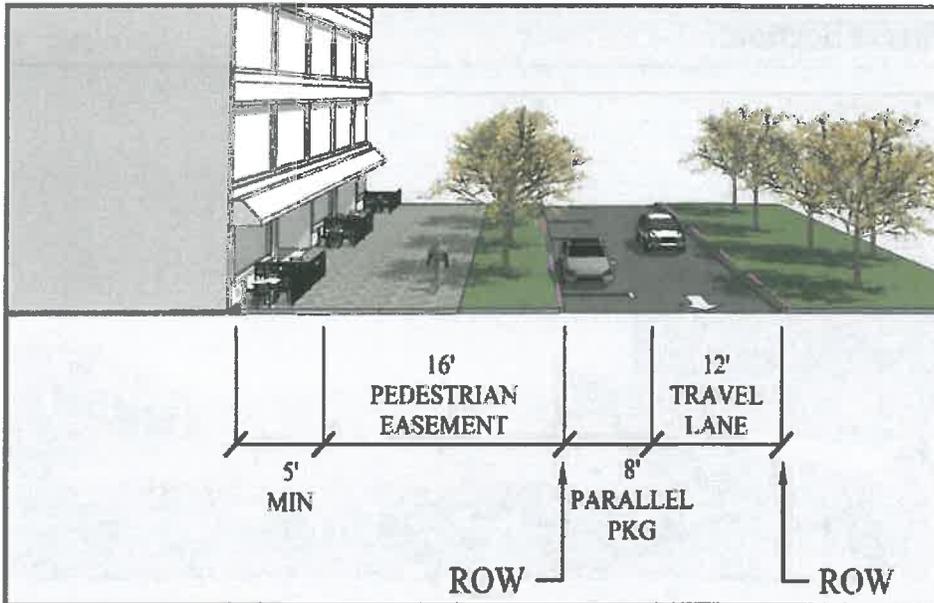
Locator Map



Court Street

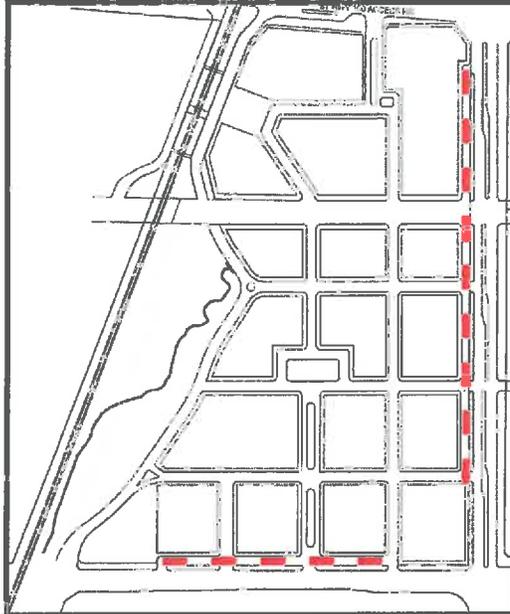
Street Type: A
ROW: 20'
Design Speed: 25 MPH
Parking: Parallel on one side
Number of Vehicular Lanes: 1
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.8 Plano/Renner Slip Road Frontage

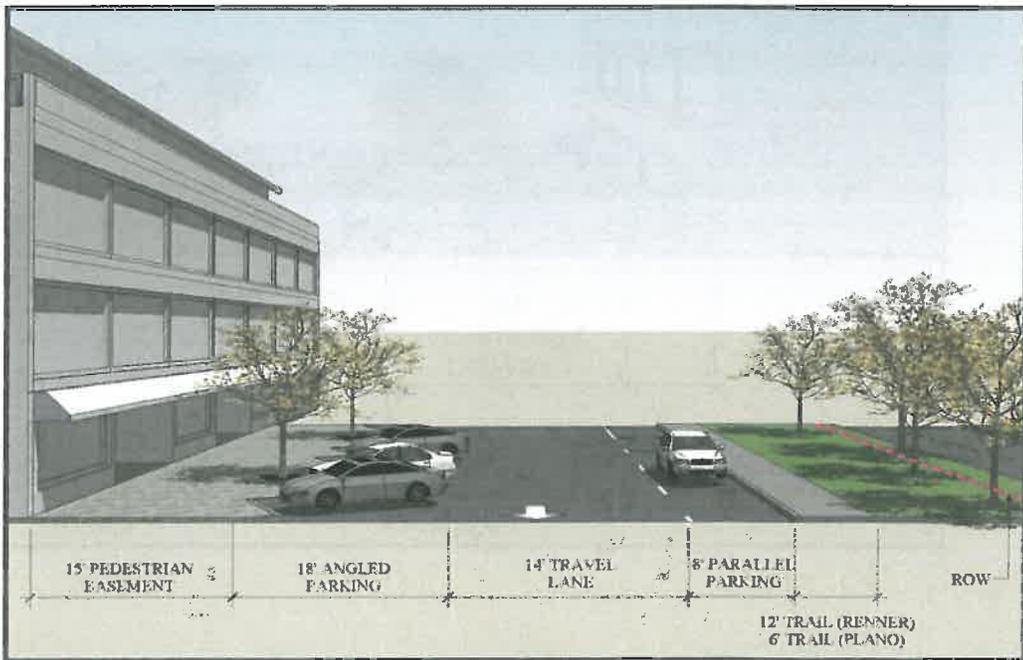
Locator Map



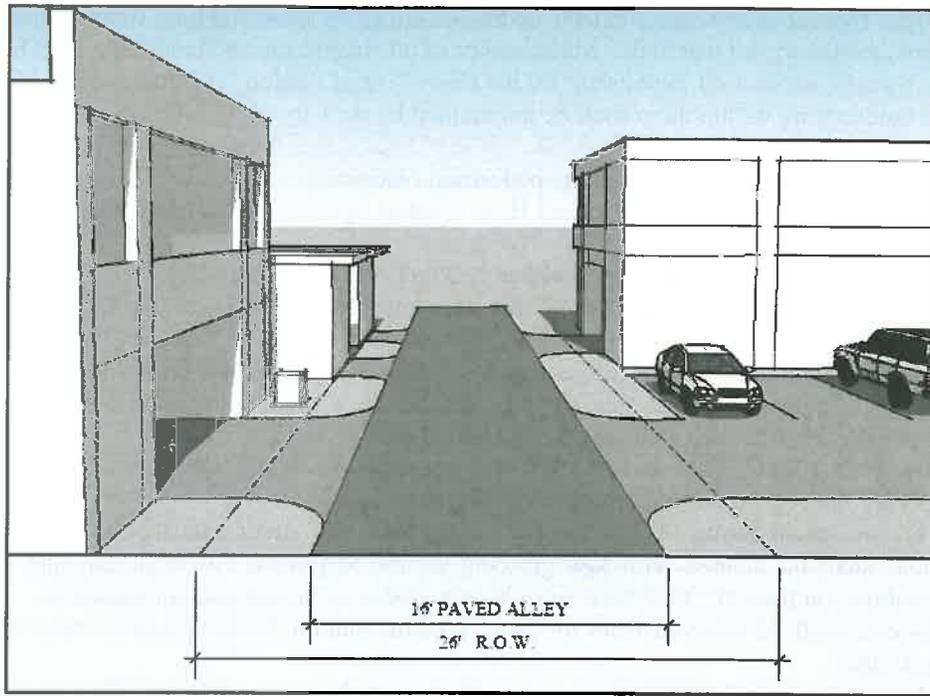
Slip Road (Plano / Renner)

Street Type: B
Slip Road Width: 40'
Design Speed: < 25 MPH
Parking: Head-in angled and parallel
Number of Vehicular Lanes: 1
Number of Bike Lanes: N/A
Curb Radius: 20'
Walkway Type: Pedestrian Easement / Trail
Landscape Type: Street Trees @ 40' OC avg

Street Section



9.4.9 Commercial Alley



- 9.5 Streetscape & Landscape Standards: Streetscape standards shall apply to all streets within the Bush Central Station. Streetscape standards shall address all elements between the building face and edge of the curb. Typical streetscape elements addressed are street trees, lighting, street furniture and pedestrian amenities, and materials. Maintenance of all streetscape and landscape shall be by the property owners association established for the Bush Central Station. All dedicated public rights-of way and landscaping within them shall be maintained by the City of Richardson.
- 9.6 Street Trees and Landscaping (within the pedestrian easement):
- 9.6.1 Street trees shall be required on all Bush Central Station Streets (except on alleys and PGBT access road.)
 - 9.6.2 Street trees shall be planted approximately 3 feet behind the curb line.
 - 9.6.3 Spacing shall be an average of 40 feet on center (measured per block face) along all streets except alleys.
 - 9.6.4 The minimum caliper size for each tree shall be 3 in. and shall be a minimum of 12 feet in height at planting. Each tree shall be planted in a planting area no less than 36 sq. feet. However, the tree well area may be no smaller than 25 sq.ft.
 - 9.6.5 Along the TOD Avenue and Park Avenue trees shall be required in the median and spacing and species shall be the same as the trees in the parkway.
 - 9.6.6 Turf and groundcover: When clearly visible from the street and alleys, all unpaved ground areas shall be planted with low growing shrubs or ground cover, ornamental grasses, or a combination thereof. Turf grass must be installed as solid sod and not seeded on.
 - 9.6.7 Species shall be selected from the Bush Central Station Planting List in Appendix B of this ordinance.
 - 9.6.8 Maintenance of all landscape materials shall meet the requirements of the City of Richardson Landscape Ordinance Requirements.
 - 9.6.9 Along arterials and highway access roads, street trees shall be planted within the required landscape parkway as per the City of Richardson Landscape Ordinance/Policies.
- 9.7 Street Furniture, Lighting, and Materials:
- 9.7.1 Pedestrian scale lighting shall be required along all Bush Central Station streets (except on alleys and PGBT access road). They shall be no taller than 20 feet.
 - 9.7.2 Street lights shall be placed at 50 feet on center, approximately 3 feet behind the curb line.
 - 9.7.3 The light standard selected shall be compatible with the design of the street and buildings.
 - 9.7.4 Trash receptacles and bike racks shall be required along all Type 'A' Streets. A minimum of one each per block face shall be required.
 - 9.7.5 Street furniture and pedestrian amenities such as benches are recommended along all Type 'A' Streets.
 - 9.7.6 All street furniture shall be located in such a manner as to allow a clear sidewalk passageway of a minimum of 6 feet.
 - 9.7.7 Materials selected for paving and street furniture shall be of durable quality and require minimal maintenance.

Section 10. Signage

Except as specifically listed below, all other signage and sign standards must comply with Chapter 18 of the City of Richardson Code of Ordinances, as amended.

- 10.1 For new signs, the standards in Table 10.1 shall apply and sign permits shall be approved administratively by the City of Richardson Building Official unless specifically noted in this section.

Table 10.1

Character Zone	TOD Core & Special Destination	TOD MU	Arterial MU	Highway MU	Standard
Sign Type					
Wall (Building) Signs	P	P	P	P	<ul style="list-style-type: none"> For all ground floor commercial uses (retail, office, and restaurant): One sign per tenant space; area to be calculated at 1.5 sq. feet per linear foot of public street frontage for the tenant space with a maximum of 100 sq. ft per tenant. Second and upper floor commercial uses may also be permitted one second floor wall sign per tenant space per public street frontage; area to be calculated at 1.5 sq. feet per linear foot of second or upper floor frontage along that public street with a maximum of 125 sq. feet. Institutional uses (non-profits and churches): One sign per tenant space; area to be calculated at 1.5 sq. feet per linear foot of public street frontage with a maximum of 100 sq. feet. Live-Work and Home occupations: One sign limited to an area of 20 sq. feet max. Building sign may encroach a maximum of 12" on to a sidewalk while maintaining a vertical clearance of 8 feet from the finished sidewalk. Building signs may be internally or externally lit. Marquee signs as only permitted as specified below.
Monument Signs	NP	NP	P	P	<ul style="list-style-type: none"> One monument sign per lot per lot street frontage (no more than 2 per lot separated by at least 100 feet) limited to a maximum of 50 sq. feet per sign face and 6 feet in height. Permitted only along PGBT access road, Type 'B' Streets and Slip Road Frontages.
Window Signs	P	P	P	P	<ul style="list-style-type: none"> Limited to 10% of the window area. <p>The following shall be exempt from this limitation:</p> <ul style="list-style-type: none"> Addresses, closed/open signs, hours of operation, credit card logos, real estate signs, and now hiring signs; Mannequins and storefront displays of merchandise sold; and Interior directory signage identifying shopping aisles and merchandise display areas.
Building Blade Signs	P	P	P	P	<ul style="list-style-type: none"> One per building (commercial and mixed use buildings only). Area = 30 sq. feet maximum per sign face. May encroach a maximum of 6 feet over a sidewalk, but shall not encroach over any parking or travel lane. Building blade signs may be attached to the building at the corners of building or along any street facing façade above the first floor facade.
Tenant Blade Signs	P	P	P	P	<ul style="list-style-type: none"> One per commercial tenant space (retail, office, or restaurant use). Area = 16 sq. feet maximum per sign face. May encroach a maximum of 4 feet over a public sidewalk, but shall not encroach over any parking or travel lane. Tenant blade signs shall be oriented perpendicular to the building façade and hung under the soffit of an arcade or under a canopy/awning or attached to the building façade immediately over the ground floor tenant space while maintaining a vertical clearance of 8 feet from the finished sidewalk.
Marquee Signs	P	P	P	P	<ul style="list-style-type: none"> Permitted for theatres, auditoriums, and other public gathering venues of 100 persons or more. Marquee signs shall be attached to the building or located above or below a canopy only. Area = 100 sq. feet maximum. Message board may be changeable copy (electronic and non-electronic). Electronic message boards shall be non-flashing.
For sale/for lease signs	P	P	P	P	<ul style="list-style-type: none"> Size is limited to 32 sq. feet per sign face. All other standards are the same as City or Richardson Sign Regulations.
Address signs	P	P	P	P	Same as City of Richardson Sign Regulations.
Temporary construction signs	P	P	P	P	1 free standing sign per lot during construction only; limited to 32 sq. feet.
Banners	P	P	P	P	Same as City of Richardson Sign Regulations.

Character Zone	TOD Core & Special Destination	TOD MU	Arterial MU	Highway MU	Standard
Sign Type					
Sandwich board signs	P	P	P	NP	<ul style="list-style-type: none"> Permitted only for retail, service, or restaurant uses. Limited to 12 sq. feet per sign face per storefront. Sign may not exceed 4 feet in height. A minimum of 6 feet of sidewalk shall remain clear. Chalkboards may be used for daily changing of messages. Readerboards (electronic and non-electronic) shall be prohibited. Sign shall be removed every day after the business is closed.
Light Pole Banners	P	P	P	P	<ul style="list-style-type: none"> Permitted only with approval of the City. Max. 10 sq. feet per sign face. Limited to one per light pole. All light pole banners shall be approved by the appropriate utility company prior to consideration by the City. Light pole banners shall be limited to publicize community-wide events, holiday celebrations, public art, and other city sponsored events.
Directory signs	P	P	P	P	<ul style="list-style-type: none"> Shall be allowed for all multi-tenant commercial and mixed use buildings only. One directory sign per multi-tenant building limited to 12 sq. feet in area. Design of the sign shall be integral to the façade on which the sign is to be affixed.
Pole signs	NP	NP	NP	NP	<ul style="list-style-type: none"> May only be permitted as part of a Master Sign Plan.
LED Signs	NP	NP	NP	NP	<ul style="list-style-type: none"> Shall be covered by a lens or diffuser. May only be permitted where electronic message boards are permitted within Bush Central Station. Or, may only be permitted as part of a Master Sign Plan.
Off-premise signs	NP	NP	NP	NP	

10.2 An applicant has the option to establish unique sign standards including size, color, type, design, and location. Such applications shall be reviewed as “Master Sign Plans” by the City of Richardson Building Official and are subject to approval of the Sign Control Board. In evaluating a Master Sign Plan, the Sign Control Board shall consider the extent to which the application meets the proposed Sign Plan:

- 10.2.1 Promotes consistency among signs within a development thus creating visual harmony between signs, buildings, and other components of the property;
- 10.2.2 Enhances the compatibility of signs with the architectural and site design features within a development;
- 10.2.3 Encourages signage that is in character with planned and existing uses thus creating a unique sense of place; and
- 10.2.4 Encourages multi-tenant commercial uses to develop a unique set of sign regulations in conjunction with development standards.

Section 11. Civic/Open Space Standards

11.1 The design of Civic/Open Space shall be regulated by the Civic/Open Space standards herein which shall create a network of open spaces that recognizes the natural qualities of the area while providing a range of both passive and active recreational opportunities. These opportunities may be accommodated in a variety of spaces ranging from large regional parks to neighborhood-scaled greens to urban squares and plazas. The open space network will be serviced by an interconnected network of trails and paths for pedestrians and bicyclists alike. Standards in Article 13-A, Section 5 of the City of Richardson Zoning Ordinance shall not apply to any development in the Bush Central Station.

11.2 Park Standards



The required park, as designated on the Regulating Plan, will create an important public space that connects the community within the Bush Central Station and allows for passive recreation. Parks shall primarily be naturally landscaped with many places to sit on benches or low walls. Passive recreation activities in parks may include grassy lawns for unstructured and informal active recreational activities. Appropriate civic elements, fountains or open shelters may be included. TOD Core and TOD Mixed Use buildings shall front onto and activate this space.

Typical Characteristics

General Character

Large, open space

Spatially defined by landscaping and building frontages

Paths, trails, open shelters, lawns, trees and shrubs naturally disposed

May be lineal, following the trajectories of natural corridors

Location and Size

Location and size shall be as shown on the Regulating Plan (Appendix A).

Typical Uses

Unstructured and passive recreation

Casual seating/picnicking

11.3 Green Standards



The required green, as designated on the Regulating Plan, will serve as an important public space for the Bush Central Station. The green will be available for civic purposes, commercial activity, unstructured recreation and other passive uses. Greens shall primarily be naturally landscaped with many shaded places to sit. Appropriate paths, civic elements, fountains or open shelters may be included and shall be formally placed within the green. TOD Mixed Use buildings, Arterial Mixed Use buildings or Neighborhood Frontage buildings shall front onto and activate this space.

Typical Characteristics

General Character

Open space

Spatially defined by landscaping and building frontages

Lawns, trees and shrubs naturally disposed

Open shelters and paths formally disposed

Location and Size

Location and size shall be as shown on the Regulating Plan (Appendix A) and shall range between 0.38 – 0.75 acres.

Typical Uses

Unstructured and passive recreation

Casual seating

Commercial and civic uses

Residential address

11.4 Square Standards



The required square provides an important terminated vista and sight line from the station platform. The square will serve as an open space available for civic purposes, commercial activity, unstructured recreation and other passive uses. The square should have a more urban, formal character and be defined by the surrounding building frontages and adjacent tree-lined streets. All buildings adjacent to the square shall front onto the square. Adjacent streets shall be lined with appropriately scaled trees that help to define the square. The landscape shall consist of lawns, trees, and shrubs planted in formal patterns and furnished with paths and benches. Shaded areas for seating should be provided. A civic element or small structure such as an open shelter, pergola, or fountain may be provided within the square.

Typical Characteristics

General Character

Formal open space

Spatially defined by buildings and tree-lined streets.

Open shelters, paths, lawns, and trees formally arranged

Walkways and plantings at all edges

Located at important intersection

Location and Size

Location and size shall be as shown on the Regulating Plan (Appendix A).

Typical Uses

Unstructured and passive recreation

Casual seating

Commercial and civic uses

11.5 Plaza Standards



Plazas add to the vibrancy of streets within the more urban zones and create formal open spaces available for civic purposes and commercial activity. Building frontages shall define these spaces. The landscape should consist primarily of hardscape. If trees are included, they should be formally arranged and of appropriate scale. Casual seating, along with tables and chairs, should be provided. Plazas typically should be located at the intersection of important streets.

Typical Characteristics

General Character

- Formal open space
- Primarily hardscape surfaces
- Trees and shrubs optional
- Spatially defined by building frontages

Location and Size

- Location and size shall be as shown on the Regulating Plan (Appendix A).
- Shall front on at least one (1) street.

Typical Uses

- Commercial and civic uses
- Casual seating
- Tables and chairs for outdoor dining
- Retail and food kiosks

11.6 Pedestrian Passage Standards



Pedestrian passages create intimate passageways through buildings at designated locations. These paths provide direct pedestrian access to residential addresses and create unique spaces for frontages to engage and enter off of. Pedestrian passages allow for social and commercial activity to spill into the public realm. Pedestrian passages should consist of a hardscape pathway activated by frequent entries and exterior stairways. The edges may simply be landscaped with minimal planting and potted plants.

Typical Characteristics

General Character

- Hardscape pathway
- Frequent entries and frontages
- Exterior stairways
- Defined by building frontages
- Minimal planting and potted plants
- Maintain the character of surrounding buildings

Standards

Min. Width 12 feet

Location shown on the Regulating Plan (Appendix A).

Typical Uses

- Pedestrian connection and access
- Casual seating

11.7 Multi-Use Trail Standards



The multi-use trail provides an important place for active recreation and creates a connection to regional paths and biking trails. The multi-use trail will run continuously from the station platform, through the required park, and along Renner Road. The multi-use trail will help activate connections between the open spaces and the uses throughout the Bush Central Station. The multi-use trail may have a different character as it passes through the park and as it is adjacent to Renner Road. Within the required park, the trail shall be naturally disposed with low impact paving materials so there is minimal impact to the existing creek bed and landscape. Along Renner Road, the trail shall have a more formal disposition with a paved trail and regular landscaping.

Typical Characteristics

General Character

Park Multi-Use Trail:

Naturally disposed landscape

Low impact paving

Trees lining trail for shade

Appropriately lit for safety

Renner Road Multi-Use Trail:

Hardscape Path

Formally disposed pedestrian furniture, landscaping and lighting

Trees lining trail for shade

Standards

Min. Width 12 feet

Location shown on the Regulating Plan (Appendix A).

Typical Uses

Active and passive recreation

Casual seating

11.8 Playground Standards



Playgrounds shall be permitted in parks and greens to provide open space designed and equipped for the recreation of children. These playgrounds should serve as quiet, safe places -- protected from the street and typically located where children do not have to cross major to access. Playgrounds may be fenced. An open shelter, play structures or interactive art and fountains may be included with landscaping between. Shaded areas and seating shall be provided. Playground equipment and design must be reviewed and approved by the City prior to installation.

A larger playground may be incorporated into the park, whereas a more intimate playground may be incorporated into the green.

Typical Characteristics

General Character

- Focused toward children of all ages
- Fenced with minimal exits (non mandatory)
- Open shelter
- Shade and seating provided
- Play structure, interactive art or fountains

Standards

- Min. Size N/A
- Max. Size N/A
- As described by civic space type in which playground is located
- Protected from traffic
- No service or mechanical equipment

Typical Uses

- Active and passive recreation
- Unstructured recreation
- Casual seating

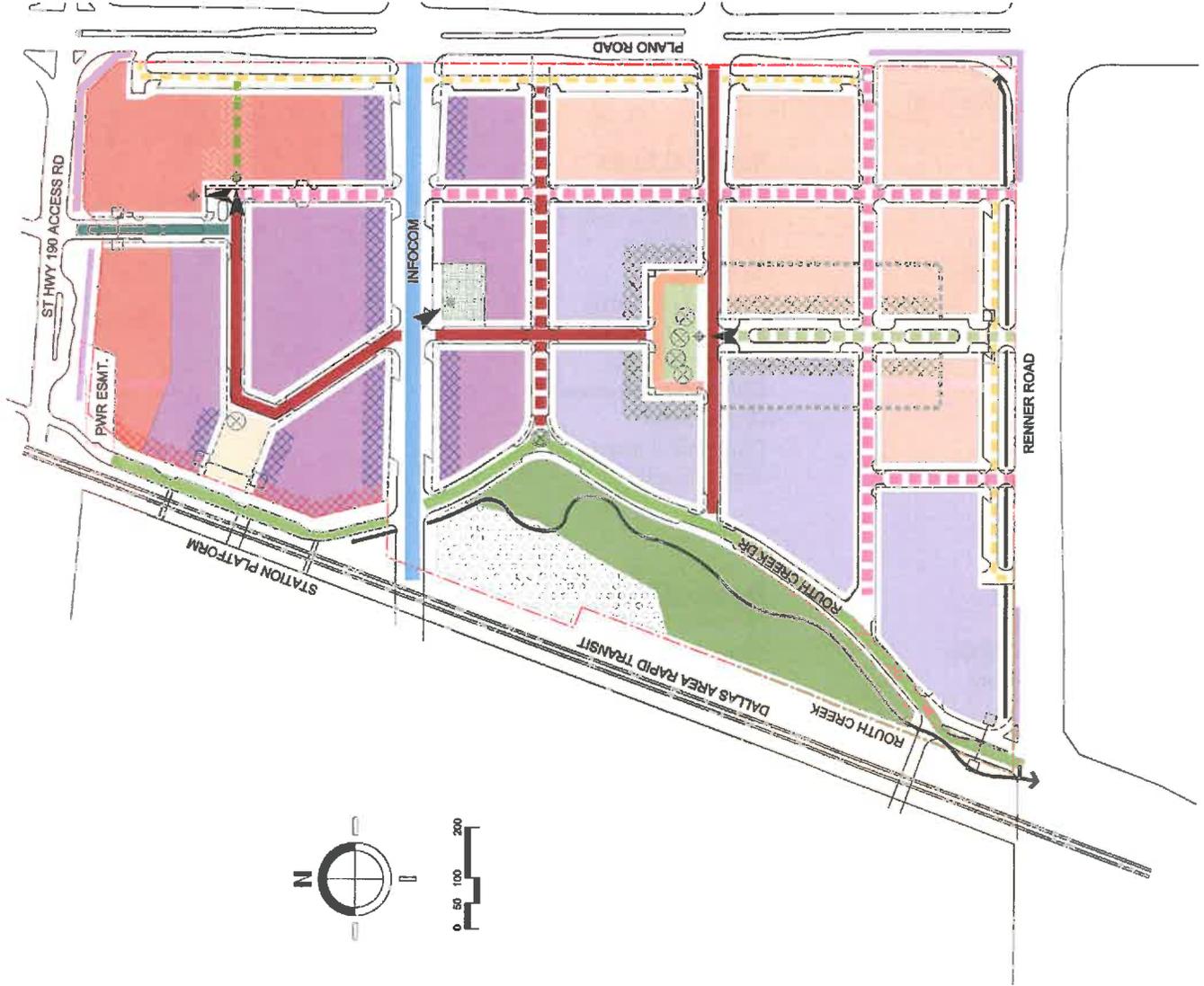
11.9 Ancillary Structure Standards



Ancillary structures should be formal in character and generally related to but clearly subordinate to surrounding buildings. Each individual structure should keep in character with the style of nearby buildings. Typically, these structures are located at prominent locations within an appropriate civic space. Ancillary structures located in more urban zones may have minor commercial uses, such as small food or news vendors, but may also serve as civic elements for general public use with more passive activities.

Typical Characteristics	
General Character	
Formal character	
Relating to style of surrounding buildings	
One or more open sides	
Covered or providing shade	
Small, stand alone structure	
Located within Park, Green, Square or Plaza	
Standards	
Min. Size	N/A
Max. Size	N/A
Typical Uses	
Civic purposes	
Minor commercial uses	
Casual seating/picnicking	

APPENDIX A



REGULATING PLAN

Bush Central Station Planned Development Code

1/14/2010

Appendix B Planting List

The following lists contain all species approved for use in the Bush Central Station. It contains native and acceptable adapted species. Other species that are drought tolerant and adaptive may be used for planting within the Bush Central Station. The use of alternative species may be permitted with the approval of the City.

CANOPY/STREET TREE LIST

<u>Common Name</u>	<u>Botanical Name</u>
Live Oak	<i>Quercus virginiana</i>
Red Oak	<i>Quercus shumardi</i>
Bald Cypress	<i>Taxodium distichum</i>
Sweetgum	<i>Liquidambar styraciflua</i>
Cedar Elm	<i>Ulmus crassifolia</i>
Lacebark Elm	<i>Ulmus parvifolia</i>
Bigtooth Maple	<i>Acer grandidentatum</i>
Caddo Maple	<i>Acer saccharum 'Caddo'</i>
Texas Ash	<i>Fraxinus velutina 'Rio Grande'</i>
Bur Oak	<i>Quercus macrocarpa</i>
Chinquapin Oak	<i>Quercus muhlenbergii</i>
Escarpment Live Oak	<i>Quercus fusiformis</i>
Ginkgo	<i>Ginkgo biloba</i>

ORNAMENTAL TREE LIST

<u>Common Name</u>	<u>Botanical Name</u>
Yaupon Holly	<i>Ilex vomatoria</i>
Crape Myrtle	<i>Lagerstromia indica</i>
Deciduous Yaupon	<i>Ilex decidua</i>
Southern Crabapple	<i>Malus app.</i>
Chinese Pistache	<i>Pistacia chinensis</i>
Mexican Plum	<i>Prunus Mexicana</i>
Wax Myrtle	<i>Myrica carifera</i>
Chitalpa	<i>Chitalpa tashkentensis</i>
Deciduous Holly	<i>Ilex decidua</i>
Desert Willow	<i>Chilopsis linearis</i>
Eve's Necklace	<i>Sophora affinis</i>

SHRUBS LIST

<u>Common Name</u>	<u>Botanical Name</u>
Dwarf Nandina	<i>Nandina domestica 'nana'</i>
Dwarf Burford Holly	<i>Ilex cornuta 'burfordi nana'</i>
Abelia Grandiflora	<i>Abelia grandiflora</i>
Barberry	<i>Barberry spp.</i>
Yucca (Red, Yellow or Soft Tip)	<i>Hesperaloe parviflora</i>
Texas Sage	<i>Leucophyllum frutescans</i>
Indian Hawthorn	<i>Raphiolepis indica</i>
Dwarf Crape Myrtle	<i>Lagerstromia indica 'nana'</i>
Dwarf Yaupon Holly	<i>Ilex vomitorria 'nana'</i>

Pampas Grass	<i>Cortaderia selloana</i>
Black-Eyed Susan	<i>Rudbeckia hirta</i>
Dwarf Wax Myrtle	<i>Myrica pusilla</i>
Needlepoint Holly	<i>Ilex cornuta 'Needle Point'</i>
Knockout Rose	<i>Rosa 'Knock Out'</i>
Rosemary	<i>Rosmarinus officinalis</i>

GROUND COVER/VINES LIST

<u>Common Name</u>	<u>Botanical Name</u>
Asian Jasmine	<i>Trachelospermum Asiaticum</i>
Big Blue Liriope	<i>Liriope muscari</i>
Mondograss	<i>Ophiopogon japonicus</i>
Purple Winter Creeper	<i>Euonymum coloratus</i>
Santolina	<i>Santolina virens</i>
Trumpet Vine	<i>Campsis radicans</i>
Virginia Creeper	<i>Parthenocissus quinquefolia</i>
Lady Banks Rose	<i>Rosa banksiaw lutea</i>
Confederate Jasmine	<i>Trachelospermum jasminoides</i>
Crossvine	<i>Bignonia capreolata</i>
Evergreen Wisteria	<i>Millettia reticulata</i>
Lantana 'New Gold'	<i>Lantana camara 'New Gold'</i>
Liriope 'Silver Dragon'	<i>Liriope muscari 'Silver Dragon'</i>
Prostrate Rosemary	<i>Rosmarinus officinalis prostrata</i>
Sweet Autumn Clematis	<i>Clematis terniflora</i>

ORNAMENTAL GRASSES LIST

<u>Common Name</u>	<u>Botanical Name</u>
Dwarf Fountain Grass 'Little Bunny'	<i>Pennisetum alopecuroides 'Little Bunny'</i>
Dwarf Maiden Grass	<i>Miscanthus sinensis 'Adagio'</i>
Fountain Grass	<i>Pennisetum alopecuroides</i>
Inland Seaoats	<i>Chasmanthium latifolium</i>
Maiden Grass	<i>Miscanthus sinensis 'Gracillimus'</i>
Mexican Feather Grass	<i>Stipa tenuissima</i>
Muhly Grass	<i>Muhlenbergia capillaris</i>
Weeping Lovegrass	<i>Eragrostis curvula</i>

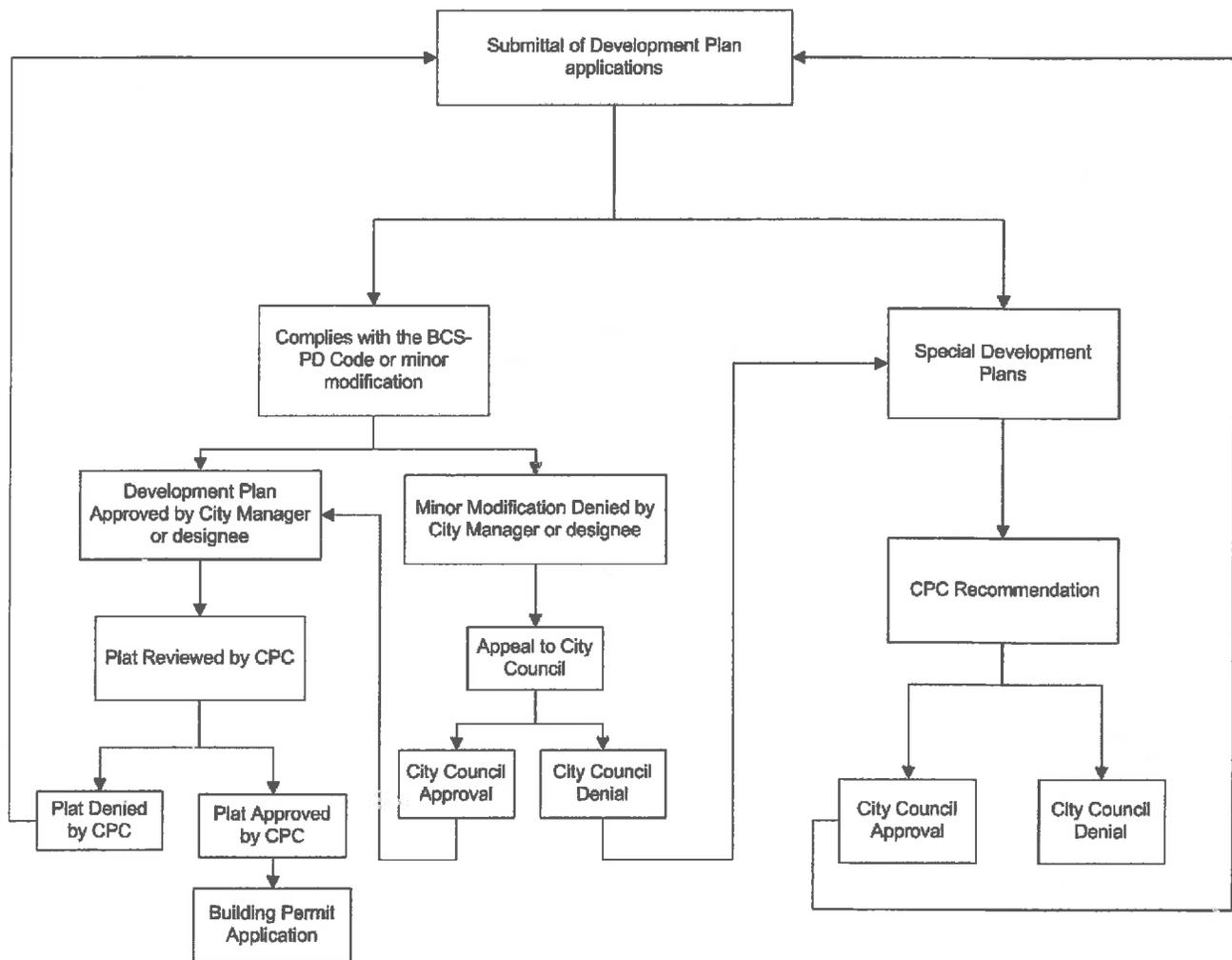
TURF

<u>Common Name</u>	<u>Botanical Name</u>
Bermuda	<i>Cynodon dactylon</i>
St. Augustine	<i>Stenotaphrum secundatum</i>
Zoysia	<i>Zoysia tenuifolia</i>

These plantings may be placed in Civic/Open Spaces or used to meet the private landscaping requirements of the Code. The applicant shall select drought tolerant, low maintenance, and adaptable shrubs and ground cover based on the placement on the site subject to approval by the City.

Appendix C

Bush Central Station
Development Review Process



BCS-PD: Bush Central Station Planned Development
 CPD: City Plan Commission
 CC: City Council
 SDP: Special Development Plan

Note: Development Plans include site plans, building elevations, landscape plans, civil engineering plans, and all plats



**City of Richardson
City Council Meeting
Agenda Item Summary**



City Council Meeting Date: Monday, January 31, 2011

Agenda Item: ZF 10-20 – Caruth Transit Oriented Planned Development

Staff Resource: Sam Chavez, Assistant Director of Development Services SC

Summary: This request was continued by the City Council at their January 17, 2011 meeting.

The applicant's request is to rezone approximately 85.9 acres of land (which includes an approximate 0.2-acre tract - City of Plano) to a Planned Development District with modified development standards under Form Based elements. The 0.2 acre tract of land located on the east side of US 75, just north of Spring Creek is owned by the City of Plano. The City of Richardson is the applicant for the tract for the purpose of establishing zoning authority (see letter from City of Plano) because the applicant does not have written authority to rezone the tract. The site encompasses two (2) tracts of land. The East Caruth Tract, located on the east side of US 75 is a 54.5-acre tract of land and the West Caruth Tract, located on the west side of US 75 is a 31.4-acre tract of land. When combined the total acreage is approximately 85.9 acres. Of that acreage, approximately 52.5 acres of land is designated for development due to the location of Spring Creek which bisects both tracts.

Numerous citizens spoke in opposition to the proposed request.

Board/Commission Action: The request was considered by the City Plan Commission on December 7, 2010. The Commission voted 6-1 to recommend approval of the request with amendments.

Action Proposed: The City Council may approve the request as presented, approve with conditions, or deny the request.

Staff Update - Caruth

Applicant's Revisions as of December 21, 2010

1. Added an appeals procedure to the City Managers decisions on minor modifications (Page 5, Section 3.8.6) and modified the associated Development Review Process flow chart accordingly (Page 57). **(CPC Recommendation)**
2. Added a periodic review provision – 3 year review, 60 months after first building permit (Page 5, Section 3.9). **(Applicant Proposed)**
3. Excluded multi-family from the Arterial Mixed Use Character Zone (Page 14, Schedule of Use Table.) **(Applicant Proposed)**
4. Added 50' minimum building height to TOD Core Character Zone (Page 17). **(Applicant Proposed)**
5. Reduced the minimum building height in TOD Freeway Hi-Rise Character Zones adjacent to the DART Light Rail Line from 80 feet to 50 feet (Page 26). **(CPC Recommended 80 feet for all TOD Freeway Hi-Rise Character Zones)**
6. Eliminated the minimum building height of 80 feet from the TOD Freeway Character Zone located on the west side of US 75 (Page 26). **(CPC Recommended 80 feet for all of the TOD Freeway Hi-Rise Character Zones)**

Proposed by Applicant at City Council's January 17, 2011 Meeting

1. Limit the number of multi-family units to 1,500.

Applicant's Additional Proposed Revisions Following City Council's January 17, 2011 Meeting

1. Change the minimum floor height for upper floors from 10 feet floor-to-floor to 10 feet minimum floor-to-ceiling in all Character Zones.
2. Restrict use of EIFS for decorative molding only.
3. Increase the amount of masonry on non-public street facing building elevations to 80%, which conforms with the City's standards for the percentage of masonry on each elevation.

Items Pending Clarification

1. Prohibit multi-family uses on the west side of US 75.
2. No minimum building height in TOD Freeway Hi-Rise west side of US 75.
3. Minimum first floor heights-Podium (minimum 15 feet floor-to-floor)



Staff Report

TO: City Council

FROM: Sam Chavez, Assistant Director of Development Services SC

DATE: January 31, 2011

RE: Zoning File 10-20: Caruth Transit Oriented Planned Development

REQUEST:

To rezone approximately 85.9 acres of land (including an approximate 0.2-acre tract owned by the City of Plano) to a Planned Development District with modified development standards under Form Based elements. This request was continued at the City Council's January 17, 2011 meeting.

The "Staff Update" outlines current and applicant proposed amendments subsequent to the Commission's December 21, 2010 meeting.

APPLICANT:

Brian E. Moore / GFF Planning
City of Richardson (with respect to the property owned by the City of Plano)

PROPERTY OWNER:

Pamela A. Spadaro / U.S. Trust, Bank of America
Vester T. Hughes, Jr. / Estate of W.W. Caruth, Jr.
City of Plano

TRACT SIZE AND LOCATION:

Approximately 85.9 acres located at the NEC and NWC of US Hwy 75 and Renner Road

EXISTING DEVELOPMENT:

The subject tracts are undeveloped.

ADJACENT ROADWAYS:

PGBT: Freeway/Turnpike; 69,000 vehicles per day on all lanes, eastbound and westbound, at the Shiloh Road Toll Plaza; 155,000 vehicles per day on all lanes, eastbound and westbound at the Coit Road toll Plaza (December 2009).

US Hwy 75: Freeway/Turnpike; 267,000 vehicles per day on all lanes, northbound and southbound, north of Campbell Road (December 2009).

Renner Road: Six-lane, divided arterial; 21,800 vehicles on all lanes, eastbound and westbound between Renner Parkway and US Hwy 75 (April 2009). 27,900 vehicles on all lanes, eastbound and westbound between Plano Road and DART Light Rail (March 2009). No traffic counts available between US Hwy 75 and DART Light Rail.

SURROUNDING LAND USE AND ZONING:

Caruth East

- North:** City of Plano and Undeveloped; R-1500-Temp
- South:** Office and Parks/Open Space; TO-M Technical Office
- East:** Undeveloped and existing Bush Turnpike Station; I-M(1) Industrial, TO-M Technical Office and PD Planned Development
- West:** Undeveloped; R-1500-Temp and TO-M Technical Office

Caruth West

- North:** City of Plano and Undeveloped; R-1500-Temp
- South:** Public/Institutional/School; TO-M Technical Office
- East:** Undeveloped; R-1500-Temp and TO-M Technical Office
- West:** Multi-Family/Group Quarters; PD Planned Development

FUTURE LAND USE PLAN:

Caruth East

Transit Village and Regional Employment

Mixed or multiple land uses built around small-scale pedestrian blocks located at the City's rail stations. Uses include medium- to high-density residential, retail, entertainment, hospitality and offices.

Higher density development is appropriate with the primary use being high-rise office. Secondary uses include retail centers and entertainment venues.

Future Land Uses of Surrounding Area:

- North:** City of Plano
- South:** Parks & Open Space and Regional Employment
- East:** Transit Village and Regional Employment
- West:** Regional Employment

Caruth West
Regional Employment

Higher density development is appropriate with the primary use being high-rise office. Secondary uses include retail centers and entertainment venues.

Future Land Uses of Surrounding Area:

North: City of Plano
South: Regional Employment
East: Transit Village and Regional Employment
West: Multi-Family Residential

EXISTING ZONING:

Caruth East

The subject property is zoned TO-M Technical Office (Ord. 2564-A, February 1987 and Ord. 2618-A, August 1987) and R-1500-Temp.

Caruth West

The subject property is zoned TO-M Technical Office (Ord. 2564-A, February 1987).

APPLICANT'S STATEMENT

(Please refer to the complete Applicant's Statement.)

TRAFFIC IMPACT:

Please see Exhibit B

STAFF COMMENTS:

Applicant's Request:

The applicant's request is to rezone approximately 85.9 acres of land (which includes an approximate 0.2-acre tract - City of Plano) to a Planned Development District with modified development standards with Form Based elements. The 0.2 acre tract of land located on the east side of US 75, just north of Spring Creek is owned by the City of Plano. The City of Richardson is the applicant for the tract for the purpose of establishing zoning authority (see letter from City of Plano) because the applicant does not have written authority to rezone the tract. The site encompasses two (2) tracts of land. The East Caruth Tract, located on the east side of US 75 is a 54.5-acre tract of land and the West Caruth Tract, located on the west side of US 75 is a 31.4-acre tract of land. When combined the total acreage is approximately 85.9 acres. Of that acreage, approximately 52.5 acres of land is designated for development due to the location of Spring Creek which bisects both tracts.

The table below represents the current zoning entitlements for the subject properties and the land use assumptions used for the purpose of analyzing traffic impacts associated with the request:

East Caruth Tract

Current Zoning Entitlement Rights		TIA Land Use Assumptions		Difference
General Office	1,507,006 SF	General Office	767,500 SF	(739,506 SF)
Shopping Center/Retail	0	Shopping Center/Retail	161,000 SF	161,000 SF
Hotel	0	Hotel	0	0
Multi-Family	0	Multi-Family	1,365 Units	1,365 Units

West Caruth Tract

Current Zoning Entitlement Rights		TIA Land Use Assumptions		Difference
General Office	1,024,946 SF	General Office	839,500 SF	(185,446 SF)
Shopping Center/Retail	0	Shopping Center/Retail	25,000 SF	25,000 SF
Townhome	0	Townhome	75 Units	75 Units

In summary, the amount of square footage for non-residential uses anticipated in the subject request is reduced by 738,952 square feet under what is currently allowed in the current zoning entitlements. In addition, 1,365 multi-family units and 75 townhome units are being added.

The Traffic Impact Analysis (TIA) submitted by the applicant examined the subject request and the Bush Central Station-PD application (Zoning File 10-21) in a single analysis. The table below represents the current zoning entitlements for the subject properties and land use assumptions for both requests.

All Tracts Combined	Unit	Current Entitlement	TIA Land Use Assumptions	Difference
General Office	SF	5,172,584	2,207,000	(2,965,584)
Shopping Center/Retail	SF	293,403	386,000	92,597
Hotel	Rooms	0	400	400
Multi-Family	Units	426	3,765	3,339
Townhome	Units	0	175	175
Bush Central Station				
General Office	SF	2,640,632	600,000	(2,040,632)
Shopping Center/Retail	SF	293,403	200,000	(93,403)
Hotel	Rooms	0	400	400
Multi-Family	Units	426	2,400	1,974
Townhome	Units	0	100	100
East Caruth Tract				
General Office	SF	1,507,006	767,500	(739,506)
Shopping Center/Retail	SF	0	161,000	161,000
Multi-Family	Units	0	1,365	1,365
West Caruth Tract				
General Office	SF	1,024,946	839,500	(185,446)
Shopping Center/Retail	SF	0	25,000	25,000
Townhome	Units	0	75	75

In summary, the amount of square footage for non-residential uses anticipated in the proposed overall zoning scenarios is reduced by a total of 2,872,987 square feet and the number of multi-family units is increased by 3,340 units over what is currently allowed in the existing zoning entitlements. In addition, 400 hotel rooms and 175 townhome units are being added.

The proposed Planned Development includes the following Form Based elements:

- Administration
- Definitions
- Regulating Plan
- Schedule of Permitted Uses
- Building Form Standards
- Architectural Standards
- Street and Streetscape Design Standards
- Signage Standards
- Civic Space Standards
- Appendix A: Regulating Plan
- Appendix B: Planting List
- Appendix C: Development Process Flow Chart

Administration (Section 3) establishes the provisions for reviewing and approving development applications to ensure that all development is consistent with the PD. *In addition to the PD requirements, all development plans are also subject to all other applicable City of Richardson codes and ordinances, such as the Building Code, Fire Code, Engineering Design Standards, Thoroughfare Plan, Subdivision and Development Code and Sign Code.*

The PD outlines the basic steps for review of a development application. The outline serves as a checklist to ensure that the development plan complies with the applicable PD requirements. The following steps are to be taken:

- Locate the property on the Regulating Plan
- Identify
 - Character Zone
 - Street Type
 - Special Frontage Standards
- Review the Schedule of Uses by Character Zone
- Examine the Building Form and Development Standards
- Refer to Building Design Standards
- Refer to Street Type and Streetscape Standards

If the development plan does not comply with the PD, the plan is deferred to the City Manager or designee for interpretation, for review as a minor modification as prescribed in the PD, if appealed-forwarded to City Council for consideration or forwarded to the City Plan Commission and City Council for consideration as a Special Development Plan.

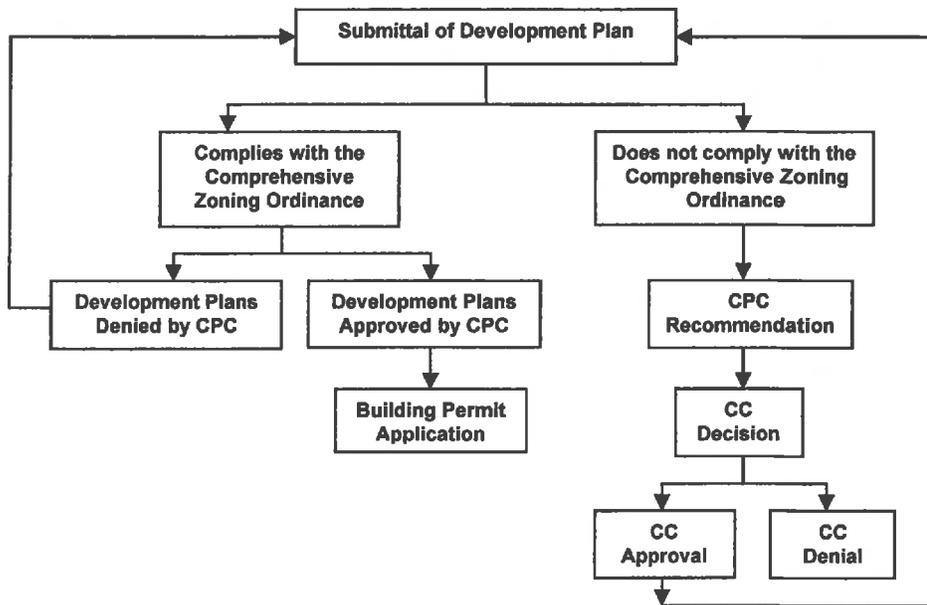
Minor Modifications (Subsection 3.8) designates the approving authority with regard to minor modifications to the PD and establishes thresholds (Table 3.1) for specific allowed modifications, including an appeal process for uses not listed in the PD.

- Minor Modifications (approving authority City Manager or designee)
 - Does not materially change the circulation and building location
 - Does not increase the building area permitted
 - Does not change the relationship between the building and streets
 - Does not allow a prohibited use
 - Does not increase building height or reduce required parking
 - Limited changes to a street cross section and Street and Streetscape Design Standards
 - Uses not listed, but are substantially similar (appeals are forwarded directly to the City Council)

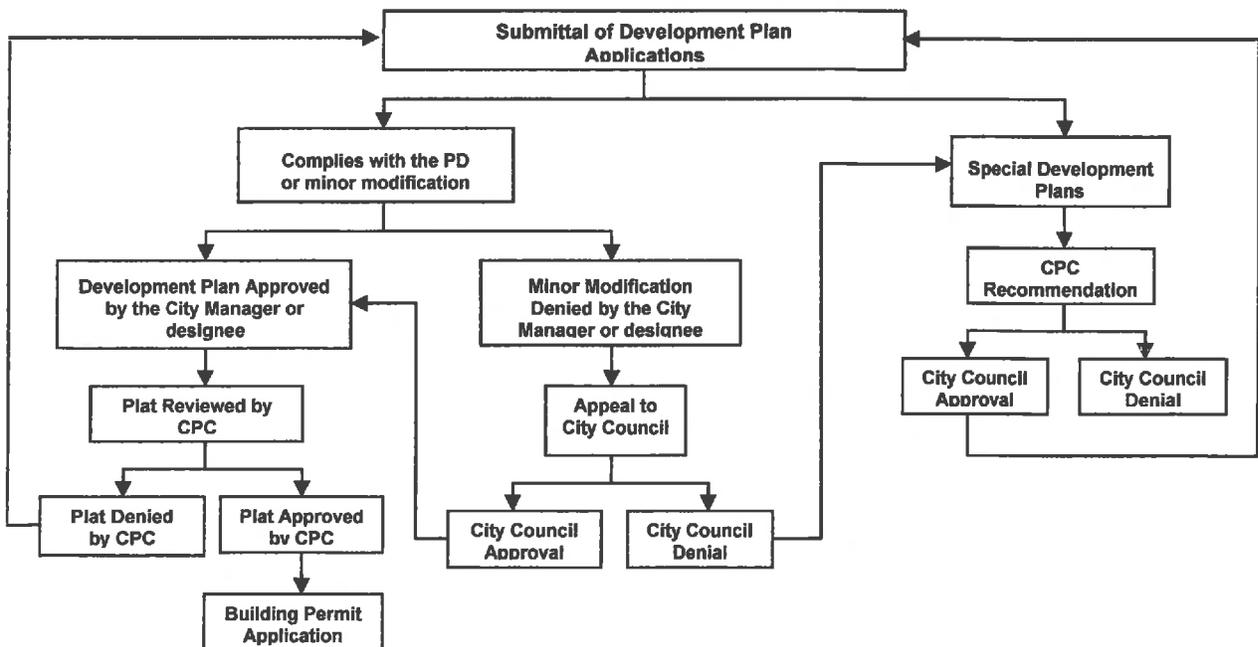
Special Development Plans (Subsection 3.7) establishes the approving authority for any proposed modification to the PD; other than those permitted as minor modification, and are consider major modifications and thus treated as Special Development Plans. Special Development Plans require the request to be reviewed and considered by the City Plan Commission and City Council.

The flow chart below depicts the City’s current development review process as a comparison with what is proposed by the applicant.

City of Richardson Development Review Process



Proposed Development Review Process



The proposed review and approval process defers from the City's current process in that site plans, building elevations, landscape plan, and civil engineering will be approved at a staff level if the development application conforms with the PD. Currently, these require approval by the Commission. Development plats will continue to be reviewed and approved by the Commission.

The PD

The Form Based elements include a Regulating Plan and the Development Standards.

Regulating Plan (Appendix A) represents the zoning map for the subject sites and creates distinct character zones which are different from other areas/zones within the subject site. Other elements represented on the Regulating Plan include Street Designations, Special Frontages, Civic/Open Space and Other Designations.

The Regulating Plan also depicts a future alignment for the Cotton Belt. The Cotton Belt Rail Line alignment has not been finalized, but the City's preferred route will take the rail line through the Caruth property west of US75, across US75, and north through the Caruth property east of US75 to the existing Bush Turnpike Station. The Cotton Belt should serve as an "economic engine" for the area with its east – west connection to the region via the existing Bush Turnpike Station.

The following five (5) Character Zones and a general description of each and general allowable uses are identified below. For a detailed list of proposed allowed uses in each Character Zone please refer to the Schedule of Permitted Uses (Section 6, Table 6.1) in the PD.

- **Character Zones**

- TOD Core (High Pedestrian Activity, Highest Density, Greatest Variety of Uses)
- TOD Mixed Use (High Intensity Commercial and Residential with supporting retail uses)
- Arterial Mixed Use (Transition to Regional Streets for Auto-Oriented Sites, Gateway to Development)
- TOD Freeway Hi-Rise (Transition from PGBT and US 75, High Intensity Development)
- Urban Neighborhood (Primarily residential with commercial at street intersection)

General Allowable Uses (by Character Zone)

TOD Core, TOD Mixed, Arterial Mixed, TOD Freeway Hi-Rise

- Retail Sales or Service, Food Service, Office, Research
- Arts, Entertainment, and Recreation
- Education, Public Administration, Health Care, Institutions
- Home Occupation, Multi-Family, Residential Lofts, Live-Work Units,
- Hotel, Surface Parking (accessory), Structured Parking, Sales from Kiosk, Community Garden, Roof-Mounted Antenna, Special Event, Equipment (utility, wind, solar, rain harvesting)

Additional Uses (by Character Zone)

TOD Mixed Arterial Mixed TOD Freeway Hi-Rise

- Drive-Through
- Private Attached Garage
- Veterinary Clinic
- Townhome

Arterial Mixed

- Drive-Thru, Auto-Related Sales and Service
- Townhome
- Veterinary Clinic

TOD Freeway Hi-Rise

- Drive-Thru
- Auto-Related Sales and Service
- Veterinary Clinic
- Surface Parking

Urban Neighborhood

- Retail Sales or Service, Office, Food Service
- Art Gallery, Studio (art, antique, furniture or electronic), Open Space
- Child Day Care and Preschool, Religious Institution
- Home Occupation, Multi-Family, Residential Lofts, Live-Work Unit, Townhome, Residential (detached), Accessory Residential Unit
- Hotel, Surface Parking (accessory), Structured Parking, Private Garage, Community Garden, Roof-Mounted Antenna, Special Event, Equipment (utility, wind, solar, rain harvesting)

- **Street Designations** (Mandatory and Non-Mandatory Streets) illustrate the design, configuration and development standards for all streets and includes the following street designations:
 - TOD Main
 - TOD Street
 - Parkway
 - TOD Street Promenade
 - TOD Station
 - TOD Trail Connector
 - Park Avenue G-1
 - Park Avenue G-2
- **Special Frontage** (Mandatory) are applied to a certain block to address specific requirements and transitions, and includes the following designation:
 - Station Platform
- **Civic/Open Space** (Mandatory and Non-Mandatory) creates a network of open spaces that provide passive and active recreational opportunities, and includes the following designations:
 - Mandatory Park
 - Non-Mandatory Plazas
- **Other Designations** (Mandatory) indicates an area for a specific use with applicable standards, and includes the following designation:
 - Multi-Use Trail

The **Development Standards** are established in text and graphic form for each Character Zone, and include Building Form and Development Standards, Building Design Standards, Street & Streetscape Standards, Signage, Civic/Open Space and related standards for all streets, and public and private development. All development plans will be reviewed for compliance with each standard.

Building Form and Development Standards (Section 7) are established in text and graphic form for each Character Zone, and includes following standards:

- Building Placement
 - Street-Setback Line
 - Build-To Zone
 - Setback
 - Building Frontage
- Block Standards
 - Block Face Dimension
 - Block Perimeter
- Building Height
 - TOD Core
 - TOD Mixed Use
 - Arterial Mixed Use
 - TOD Freeway Hi-Rise
 - Urban Neighborhood
- Commercial Frontage Requirement
 - TOD Core (Ground floors of all buildings fronting on Type ‘A’ Streets built to Retail Ready standards)
 - Arterial Mixed Use (Ground floors of all buildings fronting on Type ‘A’ Streets built to Retail Ready standards)
 - TOD Freeway Hi-Rise (Ground floors of all buildings fronting on PGBT and US 75 service roads built to Retail Ready standards)
- Special Frontage Requirements
 - TOD Core – East Caruth (Station Platform Frontage requirements)
 - TOD Freeway Hi-Rise – East Caruth (Station Platform Frontage requirements)
- Parking & Service Access
 - Parking Location
 - Off-Street Parking Standards
 - Driveways and Services
- Encroachments
 - Allowed for specific elements, require 8’ vertical clearance

- Applicability
 - Various additional development standards

Building Design Standards (Section 8) are used to establish a coherent urban character, which will be reviewed for compliance and includes the following standards:

- Building Orientation
- Design of Parking Structure
- Design of Automobile Related Building Site Elements
- Roof Form
- Façade Composition
- Windows and Doors
- Commercial and Mixed Use Building Materials
- Standards Specific to the Urban Neighborhood

The tables below depict the proposed building materials for the development (tables do not reflect the applicant's latest proposed revision, see "Staff Update").

TOD Core, TOD Mixed Use, Arterial Mixed Use & TOD Freeway Hi-Rise Building Materials

Proposed Building Material	Permitted by City Code	Caruth Properties
Building Façade		
Masonry (brick, stone, stucco, cast stone, glass, glass block)	Min. 85% of entire façade area (min. 80% per elevation) • Proposed	<ul style="list-style-type: none"> • Min. 85% along streets • Masonry or accent materials when not along streets
Accent material (wood, architect metal panel, split-face concrete block, tile, pre-cast concrete panels and EIFS)	Max. 15% of entire façade area (max. 20% per elevation) • Proposed • EIFS not permitted below a height of 8'	<ul style="list-style-type: none"> • Max. 15% along streets • 100% when not along streets • EIFS must be a least 8' above ground floor
Roof material (copper, standing seam metal, slate, synthetic slate or similar materials)	Class 'C' Fire Classification (asphalt shingles, copper, standing seam metal roof, slate, synthetic slate or similar materials)	<ul style="list-style-type: none"> • When visible from streets and alley

Urban Neighborhood Building Materials (Commercial and Mixed Use)

Proposed Building Material	Permitted by City Code	Caruth Properties
Building Façade		
Masonry (brick, stone, stucco, stone, cast stone, glass block, tilt-up gridded concrete panel, architectural panels)	Min. 85% of entire façade area (min. 80% per elevation)	<ul style="list-style-type: none"> • Min. 85% along streets • Masonry and/or accent material when not along streets
Accent material (wood, metal, cementitious-fiber clapboard, EIFS)	Max. 15% of entire façade area (max. 20% per elevation) • EIFS not permitted below 8 feet	<ul style="list-style-type: none"> • Max. 15% along streets • 100% when not along streets • EIFS must be a least 8' above ground floor

Urban Neighborhood Building Materials (Residential)

Building Material	Permitted by City Code	Caruth Properties
Building Façade		
Masonry (brick, stone, cementitious-fiber clapboard, cast stone,)	Min. 85% of entire façade area (min. 80% per elevation) • Proposed (-cementitious-fiber clapboard)	<ul style="list-style-type: none"> • Min. 85% along street frontage • (+and stucco), 0% when not along streets • EIFS not allowed on ground floors
Accent material (architectural metal panels, rock, glass block, tile, EIFS)	Max. 15% of entire façade area (max. 20% per elevation) • Proposed • EIFS not permitted below 8 feet	<ul style="list-style-type: none"> • Max. 15% along streets • (+ EIFS must be a least 8' above ground floor and EI
Roof material (asphalt shingle, copper, standing seam metal, slate, synthetic slate or similar materials)	Class 'C' Fire Classification (asphalt shingles, copper, standing seam metal roof, slate, synthetic slate or similar materials)	<ul style="list-style-type: none"> • When visible from streets and alley (+ terra cotta)

- ❖ The sides and rear facades in the Urban Neighborhood are to be of finished quality and the same color that blend with the public street facades of the building.

Street & Streetscape Design Standards (Section 9) specify typical street configurations for the development in text and graphic form. The standards include specifications for:

- Right-of-way widths
- Vehicular lane width
- Parkway widths
- Number of travel lanes
- On-street parking
- Pedestrian accommodations

Signage Standards (Section 10) are established to ensure compatibility throughout the development. Except as specially listed in the proposed Sign Standards section of the PD, all other signage shall comply with the City's Sign Code. For a detail list, please refer to Table 10.1 of the proposed PD. In general the following types of sign are proposed:

- Wall (Building)
- Monument
- Window
- Building Blade
- Tenant Blade
- Marquee
- For Sale/For lease
- Address
- Temporary
- Banner
- Sandwich Board
- Light Pole Banner
- Directory
- LED Signage (requires lens covers or diffuser)

Civic/Open Space (Section 11) creates a network of open spaces and standards that recognizes the natural qualities of the area while providing a range of both passive and active recreational opportunities.

- Park
- Plaza
- Pedestrian Passage
- Multi-Use Trail
- Playground
- Ancillary Structure

On November 7, 2010, the Commission and City Council met at a Joint Work Session to receive a presentation from staff and the applicants on the proposed project. As a result of the work session, the following items were discussed:

The following items were addressed and the PD has been amended accordingly.

- Require major modifications to be reviewed by the City Council (CC), after a City Plan Commission (CPC) recommendation. *The Code and Development Process Flow Chart have been amended to reflect the required action..*
- Prohibit EIFS as an allowed building material...*The Code has been amended to prohibit along ground floors and allow a maximum of 15% EIFS 8 feet above ground level.*
- Permit LED lighting for lighting and signage...*The Code has been amended to require LED's to be covered by a lens or diffuser.*
- "Supergraphics" allowance...*The City's Sign Ordinance which does not allow "supergraphics" will supersede.*

The following were discussed with the applicant; however, they remain items for discussion:

- Require minor modification to be approved by CPC
- Incorporate "green" design standards. *(Applicant Response: appropriate for CC&Rs)*
- Incorporate iconic elements/branding for the development. *(Applicant Response: appropriate for CC&Rs)*
- Incorporate additional architectural/design controls. *(Applicant Response: appropriate for CC&Rs)*

The following elements have been identified by staff that may require further clarification:

- Development Rights as they relate to the Traffic Impact Analysis (TIA)
The PD does not designate density and/or intensity values, which is common for a Form Based Code not to include. However, the appropriateness of the roadway network for the development is based on the land use assumption used in the TIA's findings. Establishing maximum number of units and maximum non-residential square footage could be considered as assumed in the applicant's TIA.
- Civic/Open Space Standards - Park Standards
The applicant intends to utilize the Spring Creek flood plain area to meet the need of the residents of the development for active recreational opportunities. A trail is under construction along the flood plain that will provide passive recreational opportunities; however, an active recreational area may not be feasible within flood plain area.

Throughout the development passive recreational opportunities will be provided, which will be in the forms of urban plazas/opens space. The details of either active or passive recreational area will be further defined at the time of development.

Conclusion

Based on staff's review of the applicant's request, the proposal of a PD with Form Based elements appears to be appropriate. The PD achieves a predictable community vision through its regulatory nature. It achieves a predicable physical result by its concentration on the visual aspect of the development through building heights, façade treatment and the relationship of the building to the street through compact, walkable urbanism. If applied appropriately, it could provide the community with the opportunity to respond to market demands in an expedient and predictable manner.

Motion: On December 21, 2010, the Commission on a vote of 5-2 recommended approval of the request, subject to the following special conditions:

1. The subject site shall be zoned PD Planned Development and shall be developed in accordance with the Caruth Properties Transit Oriented Planned Development Code attached hereto as Exhibit "D".
2. In addition to the maximum building heights in the TOD Freeway Hi-Rise Character Zone, a minimum 80 foot building height shall be included.

Exhibit B

TRAFFIC IMPACT:

As part of the application, a Traffic Impact Analysis (TIA) was submitted by the applicants. The TIA was conducted by Kimley-Horn & Associates, Inc., a traffic engineering and planning consulting firm hired by the applicants to evaluate the combined impact of the developments on the roadway system. As part of the review process, the City hired an independent traffic engineering consultant, Lee Engineering, to perform a comprehensive evaluation of the TIA, its assumptions, and results.

The TIA examined both the Bush Central Station and Caruth TOPD requests in a single analysis. The applicant's consultant utilized the City's standard TIA guidelines and prior Kimley-Horn studies conducted in Richardson. As part of the study, new traffic counts were conducted at signalized intersections and on all surrounding arterial roadway and frontage roads.

The following scenarios were initially analyzed per the City TIA guidelines:

- 2020 Background Traffic (existing 2010 plus typical growth)
- 2020 Background Traffic plus Full Site build-out Traffic
- 2035 Background Traffic (existing 2010 plus typical growth)
- 2035 Background Traffic plus Full Site build-out Traffic

At the December 9, 2010 City Planning Commission meeting, members of the commission requested additional information pertaining to the existing traffic conditions of the roadway network so the public could better understand how the proposed development would potentially impact the roadway network. Following the CPC meeting and subsequent meetings with the public, City staff requested that Kimely-Horn conduct analysis of additional scenarios including the following:

- 2010 Existing Traffic
- 2010 Existing Traffic plus Full Site build-out Traffic
- 2035 Traffic plus Full Site build-out without any of the applicant's proposed mitigation
- 2035 Traffic with other potential intersection improvements at the Renner and US75 intersections which could be entertained by the City and TxDOT to combat the congestion generated by background traffic and future background traffic growth not associated with the development.
- 2035 Traffic plus Full Site build-out with additional background improvements and the developer's mitigation at the Renner and US75 intersections

The results of all these studies were be presented at the January 17, 2011 City Council meeting.

The existing 2010 traffic data collected by Kimley-Horn and the historical daily traffic volume counts conducted by the City are attached to this exhibit for review. The bar graph shows historical and current traffic volume counts for Renner Road and Plano

Road in the area of the development, and includes traffic volume counts for other arterial roadways in Richardson for comparison purposes.

To assess traffic impacts associated with the proposed development on the adjacent roadway network, basic land use assumptions were established by the applicants. The table below depicts land use intensity assumptions utilized in the TIA for the proposed zoning, as well as currently allowed land use intensities allowed by the existing zoning.

TIA Land Use Assumptions and Existing vs Proposed Zoning Comparison

All Tracts Combined		Units	Existing	Proposed	Difference
General Office	SF		5,172,584	2,207,000	(2,965,584)
Shopping Center/Retail	SF		293,403	386,000	92,597
Hotel	Rooms		0	400	400
Apartment/Multifamily	DU		426	3,765	3,339
Townhome	DU		0	175	175
Bush Central Station Tract		Units	Existing	Proposed	Difference
General Office	SF		2,640,632	600,000	(2,040,632)
Shopping Center/Retail	SF		293,403	200,000	(93,403)
Hotel	Rooms		0	400	400
Apartment/Multifamily	DU		426	2,400	1,974
Townhome	DU		0	100	100
East Caruth Tract		Units	Existing	Proposed	Difference
General Office	SF		1,507,006	767,500	(739,506)
Shopping Center/Retail	SF		0	161,000	161,000
Apartment/Multifamily	DU		0	1,365	1,365
West Caruth Tract		Units	Existing	Proposed	Difference
General Office	SF		1,024,946	839,500	(185,446)
Shopping Center/Retail	SF		0	25,000	25,000
Townhome	DU		0	75	75

In summary, the amount of square footage for non-residential uses anticipated in the proposed zoning scenario is reduced by a total of 2,965,584 square feet and the number of multi-family units is increased by 3,339 units over what is currently allowed in the existing zoning entitlements. In addition, 400 hotel room and 175 townhome units are being added.

The table below depicts new automobile trip generations associated with the proposed development and trip generations associated with the currently allowed land uses.

Trip Generation Table (existing and proposed zoning)

New Auto Trips - Proposed Zoning

Proposed Tract	Daily Total	AM Peak Hour			PM Peak Hour		
		IN	OUT	TOTAL	IN	OUT	TOTAL
Bush Central Station	28,255	1,066	1,103	2,169	1,363	1,435	2,798
Caruth - East	19,237	966	634	1,600	866	1,232	2,098
Caruth - West	9,082	898	166	1,064	287	917	1,204
Totals	56,574	2,930	1,903	4,833	2,516	3,584	6,100

New Auto Trips - Existing Zoning

Proposed Tract	Daily Total	AM Peak Hour			PM Peak Hour		
		IN	OUT	TOTAL	IN	OUT	TOTAL
Bush Central Station	27,004	2,484	592	3,076	1,321	3,275	4,596
Caruth - East	9,696	1,301	177	1,478	270	1,320	1,590
Caruth - West	7,606	1,009	138	1,147	199	967	1,166
Totals	44,306	4,794	907	5,701	1,790	5,562	7,252

28% -39% 110% -15% 41% -36% -17%²
 Increase Decrease Increase Decrease Increase Decrease Decrease

Based on the findings of the TIA, the following roadway improvements were recommended as mitigation measures to be implemented by the developer. The figure attached graphically displays the location of each improvement.

- Renner Road:
 1. Westbound Auxiliary Lane – Plano Road to Routh Creek Parkway
 2. Westbound Deceleration Lanes at All Other Driveways
 3. Westbound to Northbound free-flow right-turn lane and auxiliary lane at US75 leading north at least to the first driveway
 4. Median Opening with Turn Lanes at Drive P2 (proposed street designated as “Park Avenue” on the Bush Central Station Regulating Plan, located between Plano Road and proposed Routh Creek Parkway)
 5. Traffic Signal at Drive P2
 6. Eastbound Left Turn to Routh Creek Parkway

- Plano Road:
 1. Southbound Right Turn bays / Auxiliary Lane – Bush Turnpike to Renner Road
 2. Northbound Left Turn Lanes at Infocom and Drive P5
 3. Traffic Signals at Infocom Drive and Drive P5 (proposed street designated as “TOD Street” on the Bush Central Station Regulating Plan, located on Plano Road, between Infocom and Renner Road)
 4. Improved Right Turn Lane to Renner Road with free-flow into auxiliary lane on Renner Road

- US-75 Frontage Roads
 1. Deceleration Lanes at All Driveways (All driveways and right turn bays to satisfy TxDOT Access Management Requirements)

- Other Transportation Recommendations:
 1. Cotton Belt Transit Line Connection to Bush Turnpike Station
 2. Plano Road at Renner Road Intersection to remain as an At-Grade Intersection in the City’s Master Transportation Plan.

While the TIA identifies the measures which should be the responsibility of the developer to return traffic operations to the background conditions, the following modifications are also recommended if the City desires to improve the background operating conditions shown in the 2035 background scenarios. With these measures in place, there are no LOS F conditions in 2035. These recommendations would not be the responsibility of the developers and are also shown in an attached figure.

Renner Road at US 75 Frontage Roads

1. Modify the existing five-lane SBFR approach to Renner Road to provide two left-turn lanes (one shared with the U-turn lane), two through lanes, and one right-turn lane.
2. Modify the existing five-lane NBFR approach to Renner Road to provide two left-turn lanes (one shared with the U-turn lane), two through lanes, and one right-turn lane.

3. Convert the existing northbound right-turn lane to be a free right turn by the addition of an eastbound auxiliary lane on Renner Road between the NBFR and the eastbound right-turn bay at Routh Creek Parkway.

Renner Road at Plano Road

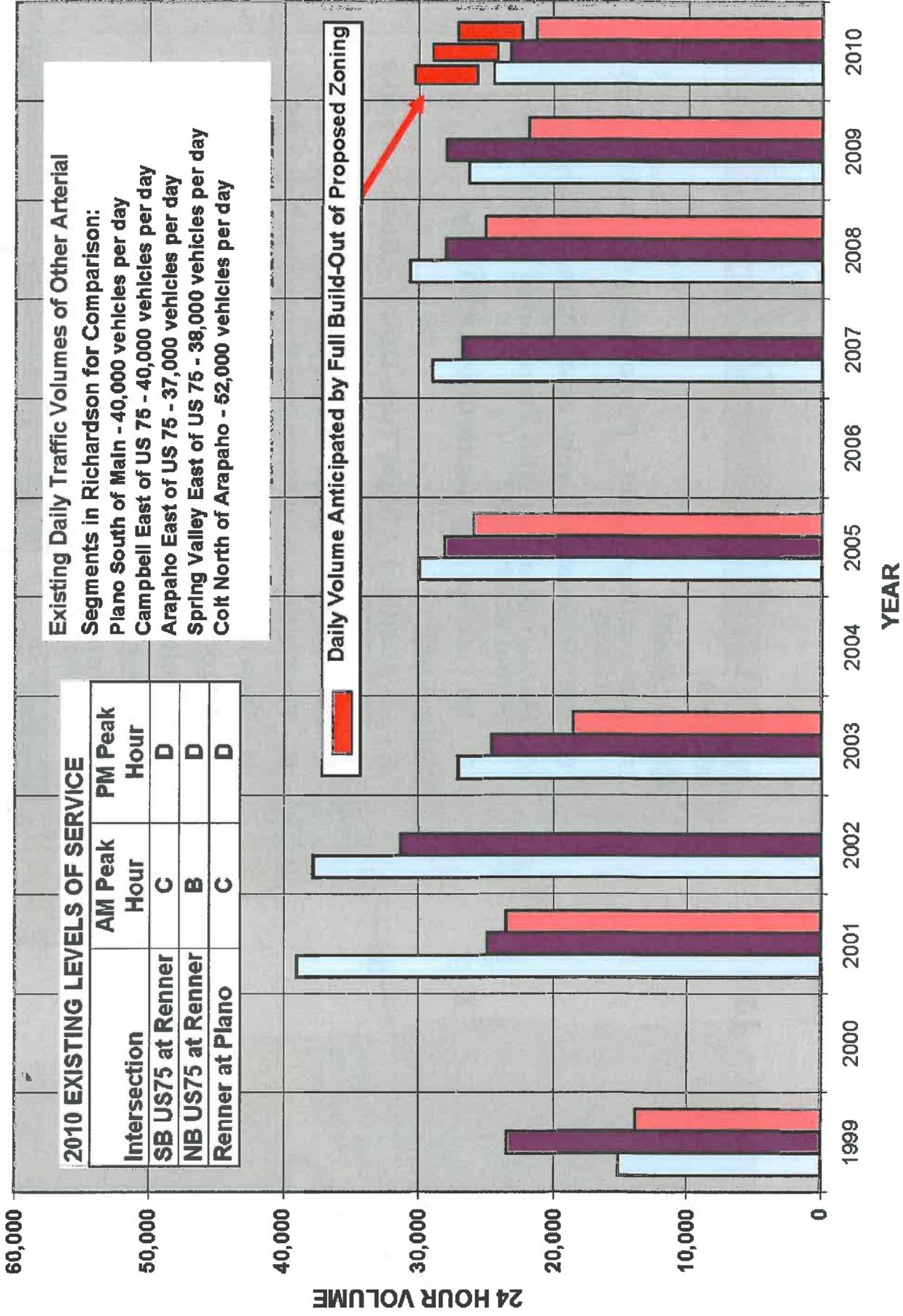
1. Convert the existing northbound right-turn lane to be a free right turn by the addition of an acceleration lane on eastbound Renner Road.

After a thorough review of the TIA and its recommendations, City staff and staff's traffic consultant (Lee Engineering) were able to conclude the following:

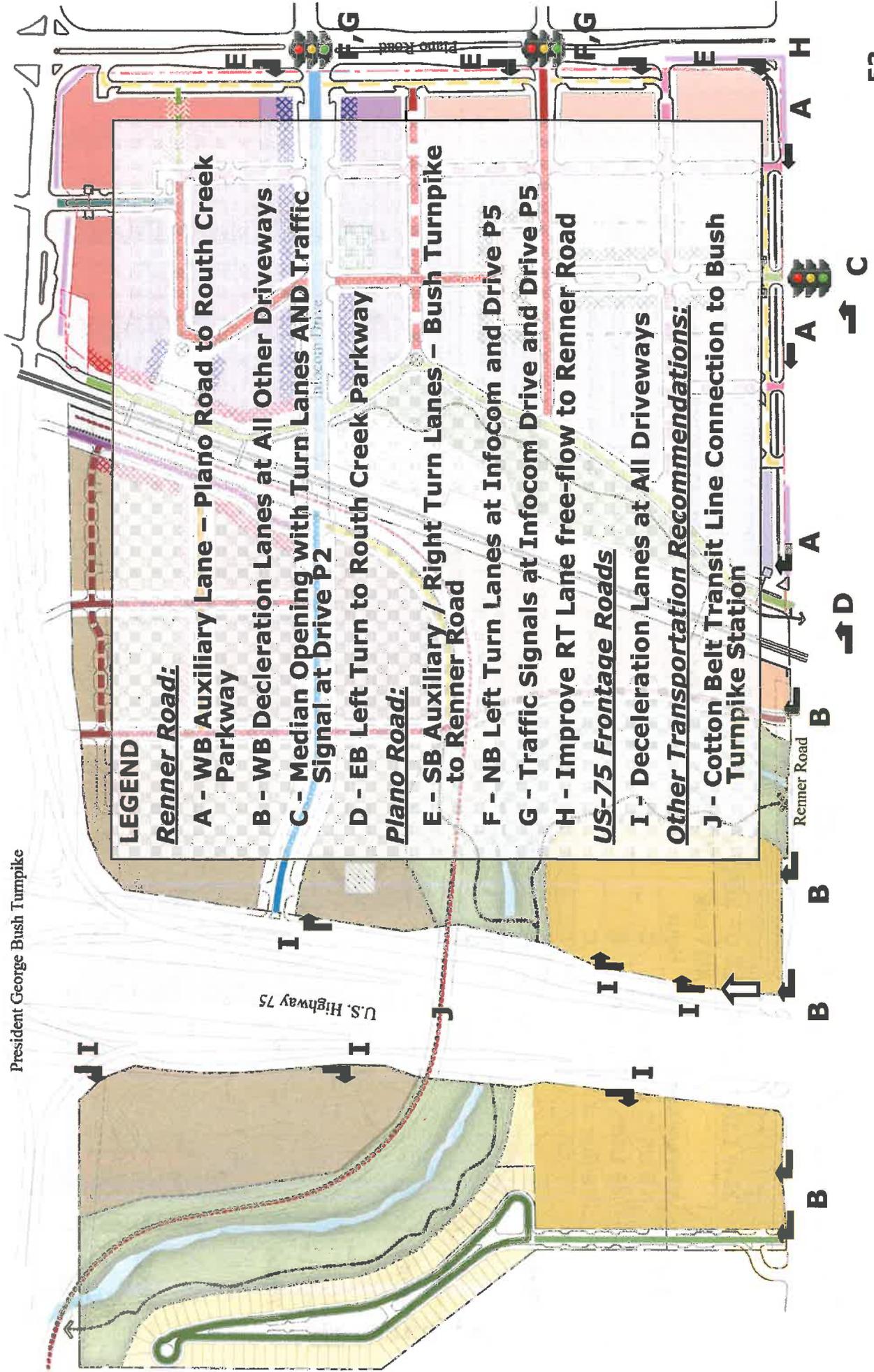
- The proposed TOD Mixed Use Zoning results in more daily traffic than the existing zoning entitlements, but it is more spread out over the entire day due to the mixture of proposed uses and the Peak Hour traffic volumes will be lower than the existing zoning allows with a better distribution of inbound and outbound traffic.
- Adequate provision of access and circulation drives will evenly distribute the traffic to the Arterial and Freeway frontage road system minimizing the impact to any specific link on the roadway network.
- The Cotton Belt Transit Line Connection to Bush Turnpike Station will greatly benefit the development and reduce the impact on traffic even more than depicted in the results of the TIA because staff limited the study's assumption of Transit usage to only a 10% reduction of trip capture rate.
- Significant levels of roadway capacity enhancements including additional turn bays and auxiliary lanes on the US 75 Frontage Roads, Plano Road and Renner Road are proposed by the applicant as part of the regulating plan to maximize the efficiency of the roadway network.
- The Plano Road at Renner Road Intersection can remain as an At-Grade Intersection. The Thoroughfare Plan will need to be amended to reflect this change at a future time.

The above conclusions were based on the land use assumptions utilized in the TIA and the applicants have agreed to cap the intensity of some landuses; however, if a significant increase in land use intensity or a major modification to the proposed mix of land use are proposed in the future, staff recommends that a revised TIA be required prior to any future PD amendment similar to the process in which other major modifications must be submitted through the City Council.

2010 Traffic Analysis and Development Traffic with Historical Data

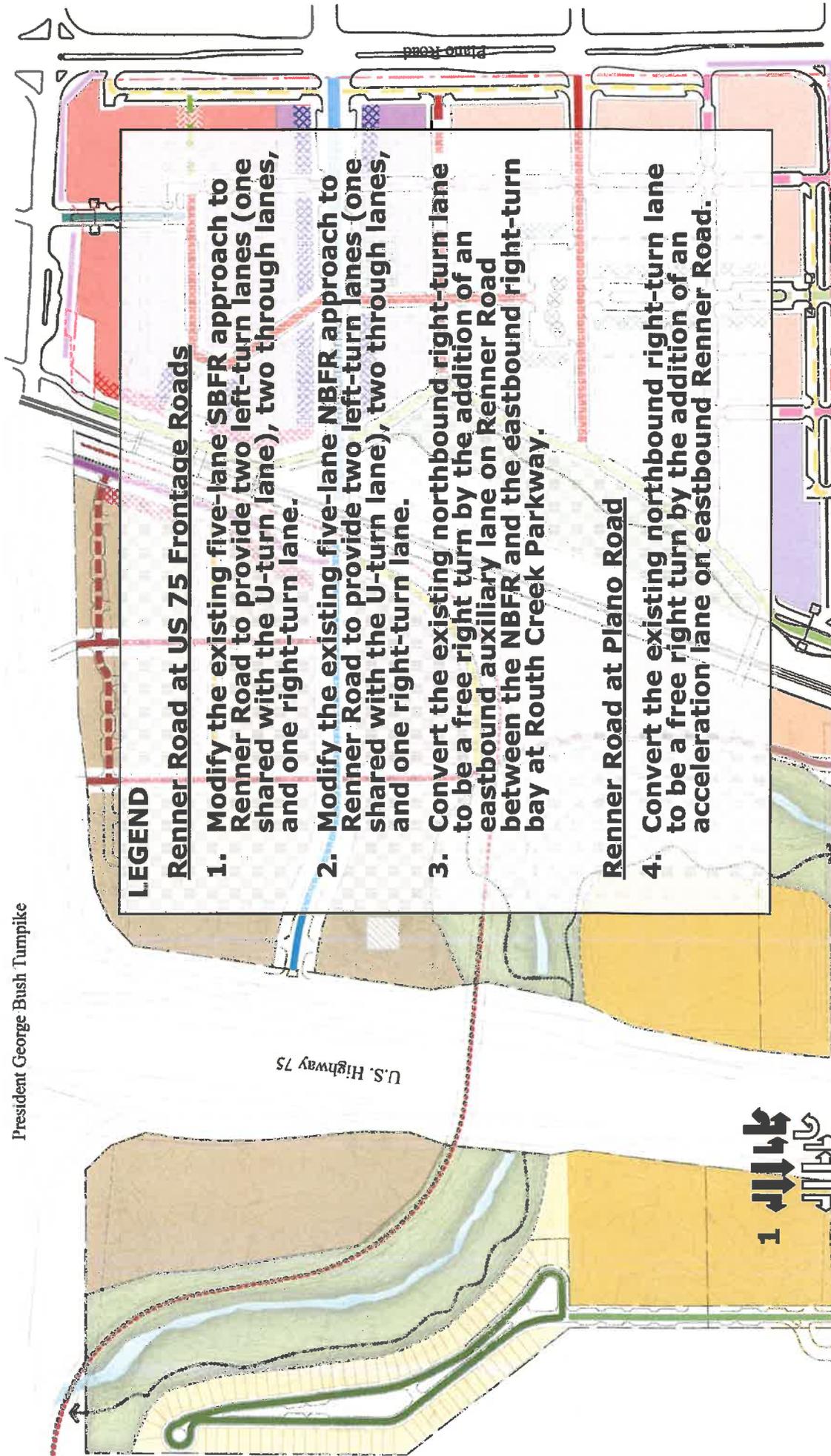


Traffic Impact Analysis Recommended Developer Mitigation Measures



Traffic Impact Analysis Recommended Potential Future CIP Improvements

President George Bush Turnpike



LEGEND

Renner Road at US 75 Frontage Roads

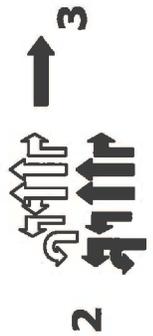
1. Modify the existing five-lane SBFR approach to Renner Road to provide two left-turn lanes (one shared with the U-turn lane), two through lanes, and one right-turn lane.
2. Modify the existing five-lane NBFR approach to Renner Road to provide two left-turn lanes (one shared with the U-turn lane), two through lanes, and one right-turn lane.
3. Convert the existing northbound right-turn lane to be a free right turn by the addition of an eastbound auxiliary lane on Renner Road between the NBFR and the eastbound right-turn bay at Routh Creek Parkway.

Renner Road at Plano Road

4. Convert the existing northbound right-turn lane to be a free right turn by the addition of an acceleration lane on eastbound Renner Road.

LEGEND

- Existing Lane Configuration
- Proposed Lane Configuration



Regards,
GOOD FULTON & FARRELL PLANNING

A handwritten signature in black ink, appearing to read "Brian E. Moore". The signature is stylized with a large, sweeping initial "B" and a long horizontal stroke at the end.

Brian E. Moore, AICP

Cc: Pamela A. Spadaro, CCIM
Paul Moser
R. Lawrence Good, FAIA, AICP



ZF 10-20

SUBJECT PROPERTY FOR ZONE CHANGE

SUBJECT PROPERTY FOR ZONE CHANGE

ZF 10-20 Aerial Map

Updated By: shacklett, Update Date: November 17, 2010
File: DSI\mapping\Cases\ZF1020\ZF1020ZF1020 ortho.mxd





President George Bush Tpk

Subject Property

DART Light Rail

Renner Rd

US HWY 75

Subject Property

Subject Property
Looking Northwest

CARUTH PROPERTIES TRANSIT ORIENTED PLANNED DEVELOPMENT CODE

January 13, 2011



GFF Planning



Gateway
Planning Group

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Section 9. Street and Streetscape Design Standards

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Appendix A: Regulating Plan

Appendix B: Planting List

Appendix C: Development Process Flow Chart

Section 1. Purpose and Intent:

The purpose of the Caruth Properties Transit Oriented Planned Development Code (hereinafter, “the C-TOPD Code”) is to support development of a pedestrian-oriented, mixed-use urban development environment, with convenient access to rail transit, shopping, employment, housing, and regional retail services. The goal of the Caruth Properties Transit Oriented Planned Development District (hereinafter, “the C-TOPD District”) is to promote an efficient, compact land use pattern; encourage pedestrian activity; reduce the reliance on private automobiles; and provide a more functional and attractive community through the use of recognized principles of urban design.

- 1.1 Economic Development – The C-TOPD District and corresponding standards are created to support economic development, sustainable tax base, and job creation by establishing adjacency predictability of private development that supports and leverages investment in and around the C-TOPD.
- 1.2 Implement the Design Goals of the Regulating Plan – The objective of the C-TOPD District is to foster a major regional employment center with significant regional retail and residential uses within convenient walking distance from the existing transit station and potential transfer station to the Cotton Belt rail line. The existing transit station provides a connection to the twelve (12) member cities of Dart, including Dallas. Development within this area would be of high intensity, accommodating large scale office and retail users while providing for transitions to adjoining uses and neighborhoods.
- 1.3 Establish Specific Development Standards – The C-TOPD Code implements the vision for Transit Oriented Development along the DART rail corridor as established in the City’s Comprehensive Plan and Caruth Properties Regulating Plan, hereafter known as the Regulating Plan (Appendix A). The Regulating Plan shall provide guidance to property owners, developers, and the City on the form, character, and intensity of future development in the C-TOPD. Creation of different Character Zones within the C-TOPD District enables specific site and locational standards to be enumerated and applied. Clear graphic standards are provided for location, height, and building elements. Such standards promote sustainability, public welfare, walkable mixed use development, housing variety and transportation choice.

Section 2. Components of the Code:

- 2.1 This C-TOPD Code shall apply to the C-TOPD District unless otherwise specified in this Code. Development of property within the C-TOPD shall comply with the respective development standards set forth in the C-TOPD Code. The components of this C-TOPD consist of:
 - 2.1.1 C-TOPD District Regulating Plan: The C-TOPD District Regulating Plan, hereafter known as the “Regulating Plan”, is its official zoning map. It identifies the applicable character zones within the C-TOPD District including:
 - i. Character Zones – The C-TOPD is divided into different “Character Zones”. A Character Zone creates a distinct urban form within that Zone which is different from urban forms in other Character Zones. Each Character Zone shall establish use and development standards including height, bulk, building and parking location, and functional design. The Regulating Plan classifies all lots within the C-TOPD into one of five (5) Character Zones.
 - ii. Special Frontage Standards – The Special Frontage Standards establish exceptions and special conditions for all buildings along designated frontages. These Special Frontage standards shall apply in addition to the underlying Character Zone standards.
 - iii. Street Designations by Street Type – The Street Designations illustrate the design, configurations and development context for all streets within the C-TOPD. The Street

Classification addresses vehicular lane widths, number of lanes, pedestrian accommodation, street tree requirements, on-street parking, and parkway and median standards (streetscape standards). Street designations on the Regulating Plan include Type 'A' and Type 'B' Streets, Mandatory and Non-Mandatory Street Networks. The Mandatory Street Network specifies the future streets needed to implement the C-TOPD Regulating Plan, and shall be required and shall generally meet the locational and connectivity goals of the Regulating Plan. Their design shall be guided by the Street Type Specifications. The Non-Mandatory Street Network indicates locations of suggested, but not required streets within the C-TOPD Regulating Plan. These streets are not mandatory, but at the election of the developer and their design shall be guided by the Street Type Specifications.

- iv. Civic/Open Space Designations – Civic/Open Space Designation include Mandatory and Non-Mandatory Civic/Open Spaces. The Mandatory Civic/Open Space areas shown on the Regulating Plan designate the locations of proposed civic/open spaces (including parks, plazas, greens, and squares). The Non-Mandatory Civic/Open Space designation indicates the locations of desired, but not required civic/open spaces (including parks, plazas, greens, and squares) to implement the Regulating Plan. Recommended Civic/Open Spaces are not mandatory, but at the election of the developer.

- 2.1.2 Development Standards: The C-TOPD Code (the text portion of this Code) enumerates the development standards with text and graphics for Character Zones, building form, civic/open space, landscape, architectural, signage, lighting, and all related standards for all streets, public and private development.

Section 3. Administration

This section sets forth the provisions for reviewing and approving development applications within the C-TOPD District. The intent is to ensure that all development is consistent with the provisions of this Code. All sections of this Code shall be applied during the review process.

- 3.1 The development standards under Articles XIII-A thru XVI and Article XXI-C of the City of Richardson Comprehensive Zoning Ordinance, as amended, shall not apply to the C-TOPD except as specifically referenced herein. Development standards not addressed in the C-TOPD Code shall be governed by the City of Richardson Comprehensive Zoning Ordinance to the extent they are not in conflict with the intent or text of the C-TOPD Code.

- 3.2 Sign Standards under Chapter 18, as amended, of the City of Richardson Code of Ordinances, shall not apply to the C-TOPD except as specifically referenced herein.

3.3 Using this Code:

The following basic steps should be followed to determine the uses and development standards applicable to all properties within the C-TOPD District:

- 3.3.1 Locate the subject property on the Regulating Plan.

3.3.2 Identify:

- i. the Character Zone in which the property is located;
- ii. the Street Type designation along all its street frontages; and,
- iii. any Special Frontage Requirements that may be applicable to the subject property.

- 3.3.3 Review the Schedule of Uses by Character Zone as listed in Table 6.1 to determine allowed uses.

- 3.3.4 Examine the corresponding zone standards in the Building Form and Development Standards in Section 7 to determine the applicable development standards.

- 3.3.5 Refer to Section 5.5 for Special Frontage Standards.

- 3.3.6 Refer to Section 8 for Building Design Standards.

- 3.3.7 Refer to Section 9 for Street Type and Streetscape Standards.

The information from the above listed steps explains where the building will sit on the lot, the limits on its three dimensional form, the range of uses, and the palette of materials that will cover it.

- 3.4 Development within the C-TOPD District that complies with the provisions of this Code shall follow the City's development process as outlined in Chapter 21, Article II of the City of Richardson's Code of Ordinances and shall be approved by the City Manager or designee (see Appendix C for flow chart of the review process). In addition to complying with applicable City regulations that are not in conflict with this Code, the applicant shall provide the information required to adequately show compliance with this Code.
- 3.5 Standard for Approval of development plans: If a development plan conforms to the standards set forth in this Code and applicable City regulations not in conflict with this Code, the development plan shall be approved. Upon request by an applicant, the authority charged with approving the development plan shall certify the reason for an action taken on a development plan.
- 3.6 The City Manager or designee shall be responsible for the following:
- 3.6.1 Reviewing development plan applications for compliance with the requirements of C-TOPD.
 - 3.6.2 Approving development plan applications that are in compliance with the requirements of the C-TOPD Code.
 - 3.6.3 Making determinations on the applications and interpretations of standards in this Code.
 - 3.6.4 Approving revisions to previously approved development plans that comply with this Code and all applicable city ordinances.
 - 3.6.5 Approving any minor modifications to the approved Regulating Plan per Section 3.8.
 - 3.6.6 Recommendations on any SDP applications to the City Plan Commission (CPC) and City Council.
- 3.7 Special Development Plans: A request for a modification to any of the standards of this Code other than minor modifications permitted under Sections 3.8 shall be reviewed and processed as Special Development Plans.
- 3.7.1 Special Development Plans (SDP) are intended to allow applicants development flexibility to address specific market opportunities and/or contexts. An application for a Special Development Plan shall be processed as an amendment to the zoning ordinance under Article XXIX of the City of Richardson Zoning Ordinance and may only be considered by the City Council (CC) after the CPC has made a recommendation. The City Manager or designee shall review, make recommendations on any SDPs, and shall forward all SDP applications to the CPC. In evaluating an SDP, CC may consider the extent to which the application meets any of the following:
- i. provides an alternative "Master Plan" approach by consolidating multiple properties to create a predictable, market responsive development for the area; or
 - ii. fits the adjoining context by providing appropriate transitions.
- 3.8 Minor Modifications to the C-TOPD Code:
- The City Manager or designee shall have the authority to approve a request for minor modifications to C-TOPD Code that:
- 3.8.1 Does not materially change the circulation and building location on the site;
 - 3.8.2 Does not increase the building area permitted under this Code;
 - 3.8.3 Does not change the relationship between the buildings and the street;
 - 3.8.4 Does not allow a use not otherwise authorized in this Code;
 - 3.8.5 Does not allow greater height of any building or reduction of any parking requirement established in this Code; or
 - 3.8.6 Changes to established street cross sections per Table 3.1 below and Section 9 of this Code.

The City Manager or designee shall also have the authority to approve minor modifications outlined in Table 3.1. Any appeals to the decisions of the City Manager on minor modifications shall be heard by the City Council. Any City Council denials of minor modifications or any changes beyond those that meet the criteria above, the thresholds established in Table 3.1, shall be processed as an amendment to this Code under Article XXIX of the City of Richardson Comprehensive Zoning Ordinance.

Table 3.1 Minor Modification Thresholds

<i>Standard</i>	<i>Minor Modification Threshold</i>	<i>Comments</i>
Area/boundary of Character Zones (including any Mandatory Civic/Open Spaces)	No more than a 15% change (increase or decrease) in the area of the TOD Freeway Hi-Rise, TOD Arterial Mixed Use and Urban Neighborhood Zone (aggregate or per block). No more than 25% change (increase or decrease) in the area of the TOD Core and TOD Mixed Use Zone (aggregate or per block).	<ul style="list-style-type: none"> • Shall not eliminate any Character Zone • 15% or 25% measurement shall be based on the total area of that specific Character Zone within the entire C-TOPD District
Location of any Mandatory Street	Location shall not move more than 100' in any direction. (This standard does not apply to the TOD Trail Connector Street Type)	<ul style="list-style-type: none"> • Shall maintain the connectivity intended by the Regulating Plan
Building Form and Development Standards		
<ul style="list-style-type: none"> • Street Setback Line 	No more than a 10% change in the SSL as established in the corresponding Character Zone Building Form and Development Standards	<ul style="list-style-type: none"> • Changes to the Street Setback Lines may only be due to any changes to the street cross sections or change in the width of a sidewalk • In no case shall the sidewalk be less than 6 feet in width.
<ul style="list-style-type: none"> • Build to zones/setbacks 	No more than a 20% change in the maximum or minimum setback.	
<ul style="list-style-type: none"> • Building Frontage 	No more than a 15% reduction in the required building frontage along each block of a Type 'A' Street	<ul style="list-style-type: none"> • Any reduction in the required building frontage shall be to accommodate Porte-cocheres for drop-off and pick-up.
<ul style="list-style-type: none"> • Street screen 	Waiver of Street Screen requirement along a Type 'B' Street	<ul style="list-style-type: none"> • Requirement for a street screen may only be waived along a Type 'B' Street along the frontage of any interim surface parking lot (off-street) that is intended to be in-filled with a parking structure. • In no case shall any portion of the surface parking have frontage along a Type 'A' Street without a required street screen • In no case shall the (off-street) surface parking lot be located at a street intersection for a minimum depth of 30' along each street (regardless of the Street Type).
Street Cross Sections	Cross sections of new streets may be adjusted with respect to number of lanes, lane widths, on-street parking configuration, pedestrian accommodation, and street tree planting	<ul style="list-style-type: none"> • Any changes in the street cross sections shall be based on specific development context such as vegetation, natural features, drainage, and fire access and is subject to approval by the City.

3.9 Requirement for Periodic Review:

3.9.1 60 months after the approval of the first building permit for a structure on the property, and at a frequency of each 36 months thereafter, the City Manager or designee shall convene a meeting of the

owner or owners of all properties within the planned development for the purpose of review and assessment of the effectiveness of the ordinance in achieving intended development forms and patterns. The agenda for this periodic review might include assessment of building appearance, quality of construction, mix of land uses and traffic conditions. It is intended that these reviews would provide an opportunity for the property owners and the City of Richardson to determine if modifications to the ordinance would be mutually beneficial to achieve desired results.

Section 4. Definitions

In addition to Definitions in Article I of the City of Richardson Comprehensive Zoning Ordinance, the following terms shall have the corresponding interpretations.

Arcade means a portion of the main façade of the building that is at or near the Street-Setback Line and a colonnade supports the upper floors of the building. Arcades are intended for buildings with ground floor commercial or retail uses and the arcade may be one or two stories.

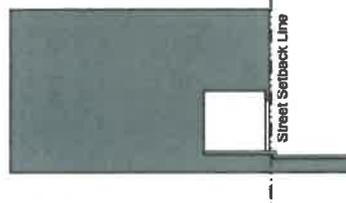


Image of an arcade

Attics/Mezzanines means the interior part of a building contained within a pitched roof structure or a partial story between two main stories of a building.

Auto-Related Sales and Service Uses means establishments that provide retail sales and services related to automobiles including, but not limited to, tires, batteries, gasoline, etc.

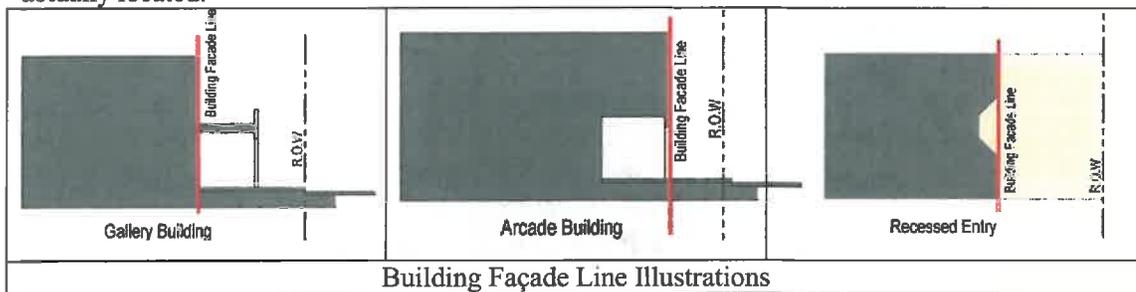
Block means the aggregate of lots, pedestrian passages and rear alleys, circumscribed by streets.

Block Face means the linear dimension of a block along one of its street frontages.

Build-to Line means the line at which the principal building’s front façade shall be built.

Build-to Zone (BTZ) means the area within which the principal building’s front façade is to be built.

Building Façade Line means the horizontal plane along a lot where the building’s front façade is actually located.



Building Façade Line Illustrations

Building Form Standards means the standards established for each Character Zone that specify the height, bulk, orientation, and elements for all new construction and redevelopment.

Building Frontage means the percentage of the building’s front façade that is required to be located at the front Build-to Line or Zone as a proportion of the block frontage along that public street. Parks, plazas, squares, improved forecourts, and pedestrian breezeway frontages shall be considered as buildings for the calculation of building frontage.

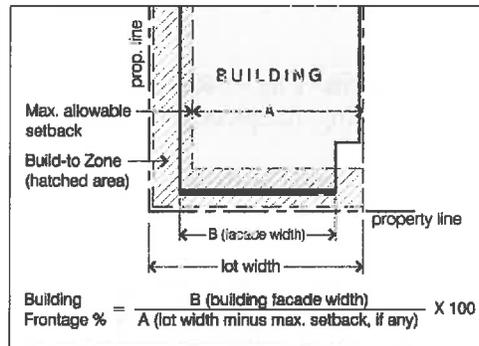


Image showing how a lot's building frontage is calculated.

Character Zone means an area within the C-TOPD that creates a distinct urban form different from other areas within the C-TOPD. Character Zones are identified in the Regulating Plan.

City Manager means the City Manager of the City of Richardson or his/her designee.

Civic/Open Space means publicly accessible open space in the form of parks, courtyards, forecourts, plazas, greens, pocket parks, playgrounds, etc. They may be privately or publicly owned. For all residential uses, privately accessible open spaces such as courtyards, porches, and balconies may also be considered as Civic/Open Space for the purposes of the C-TOPD Code.

Comprehensive Plan means the City of Richardson Comprehensive Plan that establishes the blueprint for the long-term growth and development of the City as adopted on the effective date of this Code.

Encroachment means any structural or non-structural element such as a sign, awning, canopy, terrace, or balcony that breaks the plane of a vertical or horizontal regulatory limit, extending into a Setback, into the Public R.O.W, or above a height limit.

Gallery means an extension of the main façade of the building that is at or near the front property line and the gallery may overlap the public sidewalk.

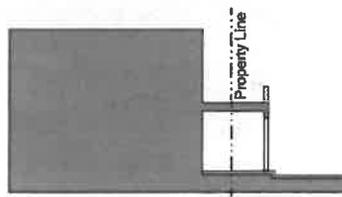


Image of a Gallery

Kiosk means a small, roofed structure, often open on one or more sides, used as a newsstand or booth. This structure could be temporary or permanent.

Live-Work Unit means a dwelling unit that is also used for work purposes, provided that the 'work' component is restricted to the uses of professional office, artist's workshop, studio, or other similar uses and is located on the street level and constructed as separate units under a condominium regime or as a single unit. The 'live' component may be located on the street level (behind the work component) or any other level of the building. Live-work unit is distinguished from a home occupation otherwise defined by the C-TOPD Code in that the work use is not required to be incidental to the dwelling unit, non-resident employees may be present on the premises and customers may be served on site.

Living Screen means a Street Screen composed of landscaping in the form of vegetation.

Minor Modification means any changes to the C-TOPD Code that meets the criteria established in Section 3.8 and Table 3.1.

Master Sign Plan means a unique sign plan to implement a specific vision for a portion or all of the development that meets Section 10.2 of this Code.

Non-Mandatory Civic/Open Space means plaza, green, square, or park area designated on the Regulating Plan which is shown as a suggested feature within the C-TOPD. The Non-Mandatory Plaza is not required and is at the election of the developer.

Non-Mandatory Pedestrian Passage means a pedestrian access area designated on the Regulating Plan which is shown as a suggested feature within the C-TOPD. The Non-Mandatory Pedestrian Passage is not required and is at the election of the developer.

Non-Mandatory Street means a street that is designated on the Regulating Plan which is shown as a suggested street within the C-TOPD. The Non-Mandatory Street is not required and is at the election of the developer.

Park means a civic/open space that is a preserve available for unstructured recreation.

Plaza means a primarily hardscaped civic/open space with formal landscaping, available for civic purposes and commercial activities. A plaza shall be spatially defined by buildings.

Playground means a civic/open space designed and equipped for children's recreation. A playground may be fenced and may include an open shelter. Playgrounds may be located within residential areas and may be placed within a block. They may be included in other civic/open spaces.

Pedestrian Easement means the area between the curb face of the street and the Street-Setback Line (SSL). This area contains the sidewalk, street trees, lighting and pedestrian furniture.

Regulating Plan means the Zoning Map attached hereto as Appendix A that shows the Character Zones, Civic Spaces, location of Streets, maximum height permitted and other Special Requirements applicable to the C-TOPD subject to the standards in the C-TOPD Code. For the purposes of this Code, the Regulating Plan shall also be the Concept Plan for the C-TOPD District.

Retail Ready means space constructed at a minimum interior height of 14 feet which may be used for noncommercial uses and can be converted into retail/commercial use. Prior to the issuance of a certificate of occupancy for a retail/commercial use in a Retail-Ready space, the space must comply with all building and construction codes for that use. The intent of Retail-Ready space is to provide the flexibility of occupying a space in accordance with market demand and allowing the use in such space to change to retail/commercial uses accordingly.

Retail Sales Retail establishments are the final step in the distribution of merchandise. They are organized to sell in small quantities to many customers. Establishments in stores operate as fixed point-of-sale locations, which are designed to attract walk-in customers. Retail establishments often have displays of merchandise and sell to the general public for personal or household consumption, though they may also serve businesses and institutions. Some establishments may further provide after-sales services, such as repair and installation. Included in, but not limited to this category, are durable consumer goods sales and service, consumer goods, other grocery, food, specialty food, beverage, dairy, etc, and health and personal services.

Service Uses means a category for limited personal service establishments which offer a range of personal services that include (but not limited to) clothing alterations, shoe repair, dry cleaners, laundry, health and beauty spas, tanning and nail salons, hair care, etc.

Sign, Building Blade means a pedestrian-oriented sign that is affixed perpendicular to the corner of a building or to the front façade of a building above the ground floor to provide identification for the whole building.



Image of a Building Blade Sign

Sign, Marquee means a sign structure placed over the entrance to a theatre or other public gathering venue. It has signage stating either the name of the establishment or the name of the event, artist, and other details of the event appearing at that venue. The marquee is often identifiable by a surrounding cache of light bulbs, usually yellow or white, that flash intermittently or as chasing lights. Marquee signs may often be combined with Building Blade signs.



Image of a Marquee sign with a Building Blade Sign

Sign, Monument means any sign which is connected to the ground and which has no clear space for the full width of the sign between the bottom of the sign and the surface of the ground. A monument sign may include a sign face and sign structure, and may also include a sign base and sign cap.



Image of a Monument Sign

Sign, Sandwich Board means a portable sign consisting of two panels of equal size, which are hinged at the top or one panel with a support and placed on the ground or pavement so as to be self-supporting.



Images of sandwich board signs.

Sign, Tenant Blade means a smaller pedestrian-oriented sign that is affixed perpendicular to the building façade under a canopy or awning or immediately over a tenant space and provides identification for individual tenants within a building.



Image of a Tenant Blade Signs

Special Development Plan means a development application that meets Section 3.7 of this Code.

Station Platform Frontage: means the special frontage requirement on blocks with frontage along the rail station platform as indicated in the Regulating Plan.

Street Screen means a freestanding wall or living screen built along the BTZ or in line with the building façade line along the street. It may mask a parking lot or a loading/service area from view or provide privacy to a side yard and/or strengthen the spatial definition of the public realm.



Image of a combination masonry and living street screen

Street-Setback Line (SSL) means the imaginary line located at the outside edge of the sidewalk and measured from the center line of the street from which all build-to lines and build-to zones are measured from.

Street Network means the Mandatory and Non-Mandatory network for new and existing streets within the C-TOPD as established in the Regulating Plan.

Street Type means a specific designation for streets that establish a certain character and cross-sections to improve walkability within the C-TOPD.

Tree Well means an unpaved area around the trunk of a tree within the sidewalk area that is either landscaped with ground cover or covered with a tree grate.



Example of a tree well with a tree grate



Example of a tree well with landscaping

Type 'A' Street means the streets identified as such on the Regulating Plan. Type 'A' Streets are the primary pedestrian streets.

Type 'B' Street means the streets identified as such on the Regulating Plan. Type 'B' Streets are intended to primarily accommodate access to parking, service, and other auto-related functions.

Section 5. The Regulating Plan

- 5.1 The Regulating Plan (Appendix A) is hereby adopted as the official zoning map for the C-TOPD District.
- 5.2 Character Zones Established – the following Character Zones are established. The boundaries of the specific Character Zones shall be established in the Regulating Plan.
- 5.2.1 TOD Core: The TOD Core provides the most opportunity for the highest intensity development. It is the area that has significant development impact and the highest pedestrian activity due to its adjacency to the transit station. The TOD Core consists of the highest density, with the greatest variety of uses. Development within the TOD Core Zone shall meet the Building Form and Development Standards in Section 7.1 of this Code.
- 5.2.2 TOD Mixed Use: The TOD Mixed Use is the area adjacent to the TOD Core that is intended for high intensity commercial and residential uses in addition to supporting retail and restaurant uses. Development within the TOD Mixed Use Zone shall meet the Building Form and Development Standards in Section 7.2 of this Code.
- 5.2.3 Arterial Mixed Use: Arterial Mixed Use is intended to provide appropriate transitions to major regional roadways while taking advantage of the arterial frontage for limited auto-oriented sites. The Arterial Mixed Use frontage also acts as a gateway into the C-TOPD by providing an attractive street frontage for the overall development. Development within the Arterial Mixed Use Zone shall meet the Building Form and Development Standards in Section 7.3 of this Code.
- 5.2.4 TOD Freeway Hi-Rise: TOD Freeway Hi-Rise is intended to provide an appropriate transition into the C-TOPD from the President George Bush Turnpike (SH 190) and US 75 access roads. This area is also intended for high intensity development. Development within the TOD Freeway Hi-Rise Zone shall meet the Building Form and Development Standards in Section 7.4 of this Code.
- 5.2.5 Urban Neighborhood: The Urban Neighborhood consists primarily of a residential fabric. The area has a mix of Townhomes and Live-Work units with commercial activity concentrated at street intersections. Development within the Urban Neighborhood Zone shall meet the Building Form and Development Standards in Section 7.5 of this Code.
- 5.3 Street Designations D-1 Street Type Established – The Regulating Plan shall establish the following Street Designations:
- 5.3.1 Type “A” Streets Established – Type “A” Streets are intended to be the primary pedestrian streets. The Type “A” Streets are TOD Main Street, TOD Street Promenade and TOD Station.
- 5.3.2 Type “B” Streets Established – Type “B” Streets are intended to balance pedestrian orientation with automobile orientation (service access, driveways, drive-through lanes, etc.). The Type “B” Streets are TOD Street, Parkway, TOD Trail Connector, and Park Avenue.
- 5.4 Mandatory Streets by Street Type – The Street Network indicates Mandatory and Non-Mandatory streets needed to implement the C-TOPD Regulating Plan. The Regulating Plan designates the type, classification and location of streets. All new streets in the C-TOPD shall meet the street design standards established in Section 9 herein.
- 5.5 Special Frontage Requirements – In order to address specific requirements and transitions based on street frontage and adjacency, the following Special Frontage Requirement as established in the Regulating Plan shall apply:
- 5.5.1 Station Platform Frontage: Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.

5.6 Street Network by Street Type – The Street Network indicates Mandatory and Non-Mandatory streets needed to implement the C-TOPD Regulating Plan. The Regulating Plan designates the type and location of streets. All new streets in the C-TOPD shall meet the street design standards established in Section 9 herein

5.7 Civic/Open Space – The Regulating Plan indicates Mandatory and Non-Mandatory Civic/Open Spaces. The specific standards for Civic/Open Space are established in Section 11.

5.8 Building Height – The Regulating Plan also indicates the maximum building height permitted within each character zone of the C-TOPD District.

Section 6. Schedule of Permitted Uses

6.1 Generally: Due to the emphasis on urban form over land uses in the C-TOPD, general use categories have been identified by Character Zone. Uses not listed in the following schedule, but are substantially similar, may be permitted upon the approval of the City Manager or designee, subject to appeal directly to the City Council.

6.2 Schedule of Uses:

Table 6.1

Character Zone	TOD Core	TOD Mixed Use	Urban Neighborhood	Arterial Mixed Use	TOD Freeway Hi-Rise
Land Use					
Commercial Uses (Office, Retail, Sales & Service Uses)					
Retail Sales or Service with <u>no drive through facility</u> (includes alcohol sales, which shall meet Chapter 4, Alcohol Beverages of the City of Richardson Cod of Ordinances). Excluded from this category are Auto-Retail Sales and Service Uses (see Section 3 of the Code for Definition of Retail, Service uses, and Auto-related Sales and Service)	P	P	P/C	P	P
Finance, Insurance, and Real Estate establishments including banks, credit unions, real estate, and property management services, <u>with no drive through facility</u>	P	P	P/C	P	P
Offices for business, professional, and technical uses such as accountants, architects, lawyers, doctors, etc.	P	P	P/C	P	P
Research laboratory headquarters, laboratories and associated facilities	P	P	NP	P	P
Food Service Uses such as full-service restaurants, cafeterias, bakeries and snack bars with <u>no drive through facilities</u> Included in this category is café seating within a public or private sidewalk area with no obstruction of pedestrian circulation. Also included in this category is the sale of alcoholic beverages which shall meet Chapter 4, Alcoholic Beverages of the City of Richardson Code of Ordinances.	P	P	P/C	P	P
Live-Work Unit	P	P	P	P	P
Any use with a drive through facility	NP	P/C	NP	P/C	NP
Auto-related Sales and Service	NP	NP	NP	P/C	NP
Arts, Entertainment, and Recreation Uses					
Art galleries	P	P	P/C	P	P
Art, antique, furniture or electronics studio (retail, repair or fabrication; excludes auto electronics sales or service)	P	P	P/C	P	P
Games arcade establishments	P	P	NP	P	P
Theater, cinema, dance, music or other entertainment	P	P	NP	P	P

Character Zone	TOD Core	TOD Mixed Use	Urban Neighborhood	Arterial Mixed Use	TOD Freeway Hi-Rise
establishment					
Museums and other special purpose recreational institutions	P	P	NP	P	P
Fitness, recreational sports, gym, or athletic club	P	P	NP	P	P
Parks, greens, plazas, squares, and playgrounds	P	P	P	P	P
Educational, Public Administration, Health Care and Other Institutional Uses					
Business associations and professional membership organizations	P	P	NP	P	P
Child day care and preschools	P	P	P/C	P	P
Schools, libraries, and community halls	P	P	NP	P	P
Universities and Colleges	P/C	P/C	NP	P	P
Hospital	P	P	NP	P	P
Civic uses	P	P	P/C	P	P
Social and fraternal organizations	P	P	NP	P	P
Social services and philanthropic organizations	P	P	NP	P	P
Religious Institutions	P	P	P	P	P
Funeral homes	P	P	NP	P	P
Residential Uses					
Home Occupations	P/A	P/A	P/A	P/A	P/A
Multi-family residential					
Ground Floor	P/C	P	P	NP	P/C
Upper Floors	P	P	P	NP	P
Residential Lofts	P	P	P	P	P
Live-Work Unit	P	P	P	P	P
Single-family residential attached dwelling unit (Townhomes)	NP	P/C	P	NP	NP
Single-family residential detached dwelling unit	NP	NP	P	NP	NP
Accessory residential unit	NA	NA	P/A	NA	NA
Other Uses					
Model homes for sales and promotion**	P	P	P	P	P
Hotels	P	P	P	P	P
Parking, surface (primary use of property)	NP	NP	NP	NP	NP
Parking, surface (accessory use of property)	P	P	P	P	P
Parking, structured	P	P	P	P	P
Private attached garage	NP	P	P	NP	NP
Private detached garage	NP	NP	P	NP	NP
Sales from kiosks	P	P	NP	P	P
Veterinary clinic	NP	P	NP	P	NP
Community garden	P	P	P	P	P
Incidental Outdoor Display (subject to standards in Section 7 of the Code)	P/A	P/A	P/A	P/A	P/A
Antennas including cell, accessory, and mounted on top of buildings.	P/A/C	P/A/C	P/A/C	P/A/C	P/A/C
Utility infrastructure	P/A/C	P/A/C	P/A/C	P/A/C	P/A/C
Rain water harvesting equipment	P/A/C	P/A/C	P/A/C	P/A/C	P/A/C
Wind energy equipment	P/A	P/A	P/A	P/A	P/A
Solar energy equipment	P/A	P/A	P/A	P/A	P/A
Special Event	P	P	P	P	P

P= Permitted by right NP= Not Permitted P/C= Permitted with design criteria per Table 6.2 P/A = Permitted Accessory Use NA= Not applicable

A* = Accessory use to not exceed 25% of the primary use building square footage

** Model homes are limited to a time period until all the homes are sold in the neighborhood.

6.3 Use Criteria: All uses listed as P/C in Table 6.1 shall also meet the following standards in Table 6.2

Table 6.2 -- Use Criteria		
<i>Use</i>	<i>Zone</i>	<i>Location & Design Criteria</i>
Non-Residential Uses		
Retail Sales & Service (Excluded from this category are Auto-Retail Sales and Service)	Urban Neighborhood	<ul style="list-style-type: none"> Shall be limited to ground floor uses on corner lots in the Urban Neighborhood Zone.
Finance, Insurance, and Real Estate establishments including banks, credit unions, real estate, and property management services (no drive through facility).		
Food Service Uses such as full-service restaurants, cafeterias, bakeries and snack bars (no drive-through facility). Included in this category is café seating within a public or private sidewalk area with no obstruction of pedestrian circulation. Also included in this category is the sale of alcoholic beverages which shall meet Chapter 4 Alcohol Beverages of the City of Richardson Code of Ordinances.		
Offices for business, professional, and technical uses such as accountants, architects, lawyers, doctors, etc. Art, antique, furniture or electronic studio (retail, repair, or fabrication) Art galleries	Urban Neighborhood	<ul style="list-style-type: none"> Shall be limited to ground floor uses on any lot in the Urban Neighborhood Zone.
Any permitted use with a drive through facility	TOD Mixed Use & Arterial Mixed Use	<ul style="list-style-type: none"> All drive through access (driveways) shall be from Type 'B' Streets. Drive through lanes and/or canopies shall not have frontage along on or be located along any Type 'A' Streets. Drive through areas screened by a 4' high Street Screen.
Universities and Colleges	TOD Core & TOD Mixed Use	<ul style="list-style-type: none"> Shall be required to provide structured parking as part of the build-out for the university/college campus
Civic uses	Urban Neighborhood	<ul style="list-style-type: none"> Shall be limited to ground floor uses on corner lots in the Urban Neighborhood.
Child day care and preschools	Urban Neighborhood	<ul style="list-style-type: none"> Shall be located on corner lots only Pre-schools shall be no greater than 15,000 sq. feet Daycares shall be no greater than 10,000 sq. feet
Auto-related Sales and Service	Arterial Mixed Use	<ul style="list-style-type: none"> No more than 50% of a block's frontage along a Type 'B' Street shall be occupied by gas pumps, canopies, and/or service bays. No outdoor storage of vehicles or other products sold shall be permitted. All auto related sales display shall be inside storefronts.
Residential Uses		
Multi-family residential Ground Floor	TOD Core & TOD Freeway Hi-Rise	<ul style="list-style-type: none"> All ground floors along all Type 'A' Streets shall be built to Retail Ready standards.
Single-family residential attached dwelling unit (Townhomes)	TOD Mixed Use	<ul style="list-style-type: none"> Shall not be permitted along Renner Road
Other Uses		
Antennas including cell, accessory and mounted (Excluded from this category are freestanding and commercial antennas and equipment buildings)	All Zones	<ul style="list-style-type: none"> Antennas shall be permitted on rooftops. Antennas shall be screened entirely with a screen of same color as the principal building. Antennas shall not be visible from adjacent Type 'A' Street.

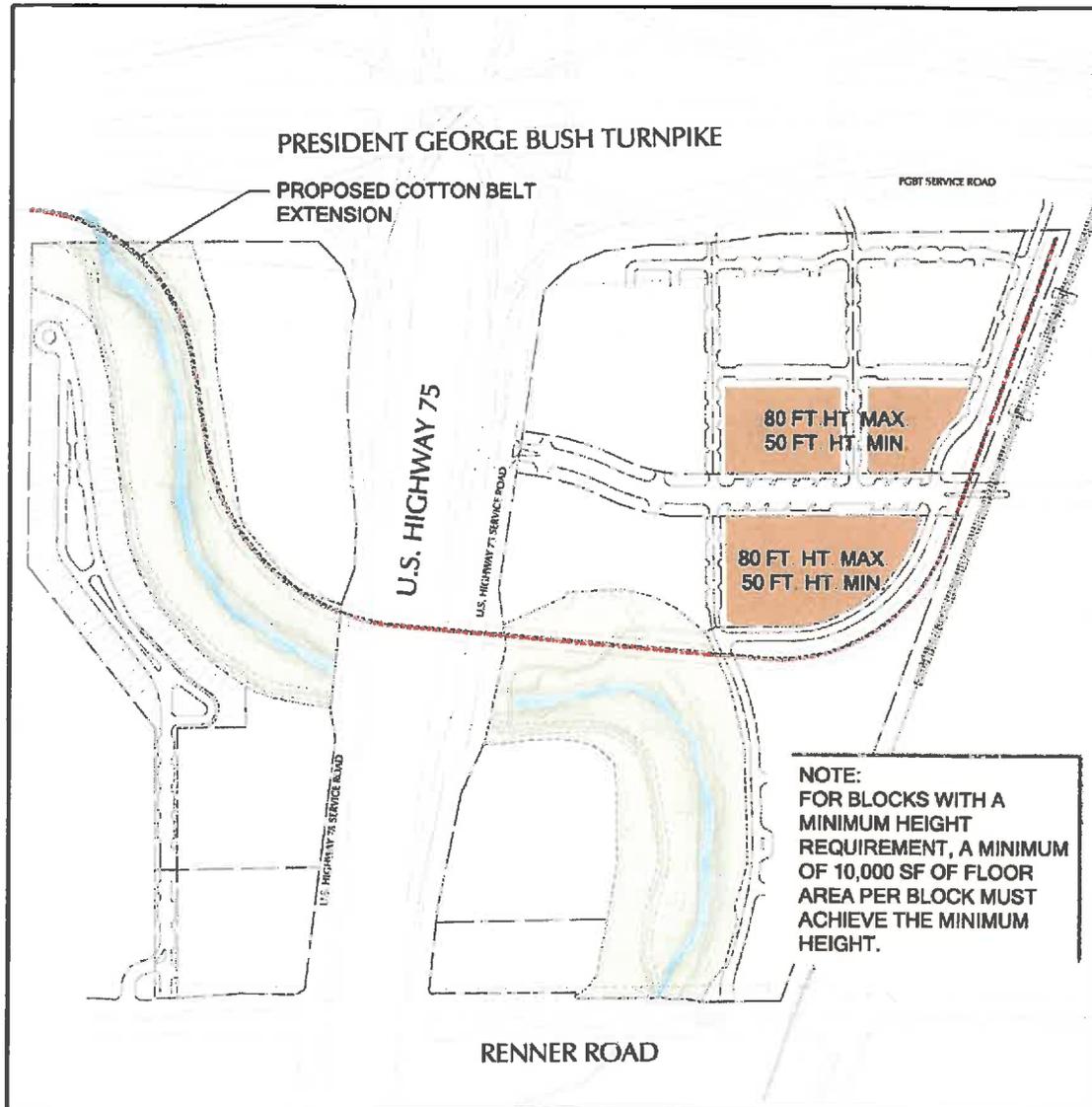
Table 6.2 – Use Criteria		
<i>Use</i>	<i>Zone</i>	<i>Location & Design Criteria</i>
Utility equipment (includes electrical transformers, gas meters, etc)	All Zones	<ul style="list-style-type: none"> • Utility equipment shall not be placed along a Type 'A' Street. • On all other frontages, they shall be screened with a Street Screen at least as high as the equipment being screened. • Utility equipment shall be permitted on rooftops. • Utility equipment shall be screened entirely with a screen of same color as the principal building.
Rain water harvesting equipment	All Zones	<ul style="list-style-type: none"> • Rain water harvesting equipment shall be permitted on rooftops. • Rain water harvesting equipment shall be screened entirely with a screen of same color as the principal building.

Section 7. Building Form and Development Standards

The following section establishes the Building Form and Development Standards for all Character Zones within the C-TOPD District. Diagrams and reference letters are used for illustrations purposes only. Reference letters may not be in continuous sequence.

7.1 TOD Core Zone

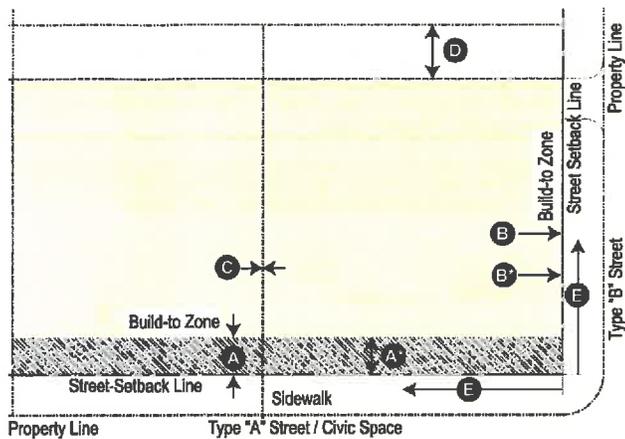
Character Zone: TOD Core



TOD Core Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements.

7.1.1 Building Placement



Legend

----- Property Line	----- Setback Line	----- Street-Setback Line
■ Building Area	▨ Build-to Zone	

Street-Setback Line (SSL)
(Distance from center line of street to edge of the BTZ)

TOD Main Street	61 feet
TOD Street	32 feet min.
TOD Street Promenade	32 feet min.
TOD Station	33.5 feet min.
TOD Trail Connector	24 feet min.

Build-To Zone (BTZ)
(Distance from edge of SSL to edge of the BTZ)

Front (Type "A" Street and Civic Space)	0 – 10 feet	A
Front (Type "B" Street)	0 feet (see #1)	B

Setback

Front (Type "A" Street and Civic Space – distance from SSL)	0 feet (min.) 10 feet (max.)	A*
Front (Type "B" Street – distance from SSL)	0 feet	B*
Side (from property line)	0 feet (see #2)	C
Rear (from property line)	5 feet (see #2)	D

Building Frontage

Building Frontage required along all street BTZ	90% (min.) (see #3 and #8)	E
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7.1.2 Block Standards

Block face dimensions	250 – 500 feet (see #4)
Block perimeter	1500 feet (maximum) (see #4)

7.1.3 Building Height

Principal Building Standards

Building maximum	Shall be established on the Regulating Plan (see #6 and #9)
First floor to floor height	15 feet min. (see #5)
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)
Upper floor(s) height (floor-to-floor)	10 feet min.

7.1.4 Commercial Frontage Requirements

Ground floors of all buildings fronting on Type "A" Streets shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

7.1.5 Station Frontage Requirements

Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.

Notes

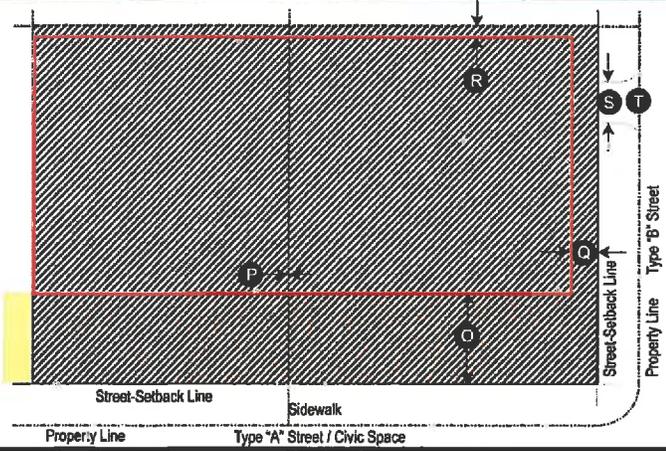
- #1 – The area between the building and the edge of the BTZ at the public sidewalk shall be paved flush with the sidewalk.
- #2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #3 – Corner building street facades shall be built to the BTZ for a minimum of 30' from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to zone standards.
- #4 – Blocks exceeding the maximum block face and perimeter dimensions may be permitted as follows:
 - (i) No more than two adjacent blocks may be combined based on the Regulating Plan.
 - (ii) An increase in block dimensions shall not eliminate or significantly move a required street or required civic/open space. Required streets and required civic/open spaces may move a maximum of 100 feet
 - (iii) The block shall maintain a continuous Type "A" Street frontage with adjoining blocks.
 - (iv) Ground floor internal pedestrian connectivity shall be provided through the block. Connectivity may be provided inside the building, through a parking garage or outside by way of a Pedestrian Passage.
- #5 – First floor heights shall not apply to parking structures.

#6 – Not used.

#7 – All buildings in the TOD Core Zone shall meet the Building Design Standards in Section 8.

#8 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen higher than the tallest piece of equipment. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen that provides an opaque screen at maturity or a combination of masonry and living screen. Species shall be selected from the C-TOPD Planting List in Appendix B of the C-TOPD Code. The required Street Screen shall be located within the BTZ along the corresponding frontage.

7.1.6 Parking & Service Access



Legend

Property Line	Building Footprint
Surface Parking Area	Street-Setback Line
Above Grade Parking Area	

(i) Parking Location

Surface/At Grade Parking

Type "A" Street and Civic Space setback	Shall be located behind the principal building	O
Type "B" Street setback	Min. of 3 feet behind the building facade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R

Above Grade Parking

Setback along Type "A" Street, Type "B" Street, and Civic Space (distance from SSL)	0 feet min.	O
Side and rear setbacks (distance from property line)	0 feet min.	
Upper Floors	May be built up to the building line	

(ii) Required Off-Street Parking Spaces

Non-residential uses	1 space/300 sq. feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	20 feet max. (except when drives may need to be wider to address service access or fire lane standards)	S
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Driveways and off-street loading and unloading shall not be located on a Type "A" Streets. **T**

Porte cocheres may be permitted on Type "A" Streets to provide drop-off and valet service.

Shared driveways and cross access easements are encouraged between lots to minimize curb cuts.

If driveway and/or off-street service loading and unloading access is provided from a Type "A" Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.

7.1.7 Encroachments

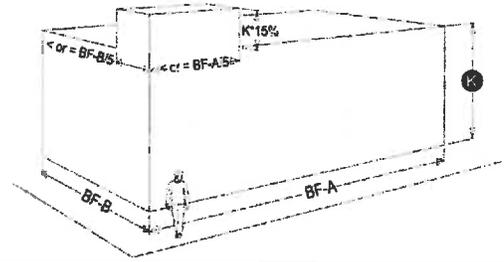
Canopies, signs, awnings and balconies may encroach over the BTZ, setback, and, sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.1.8 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

Notes

#9 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



#10 - Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.

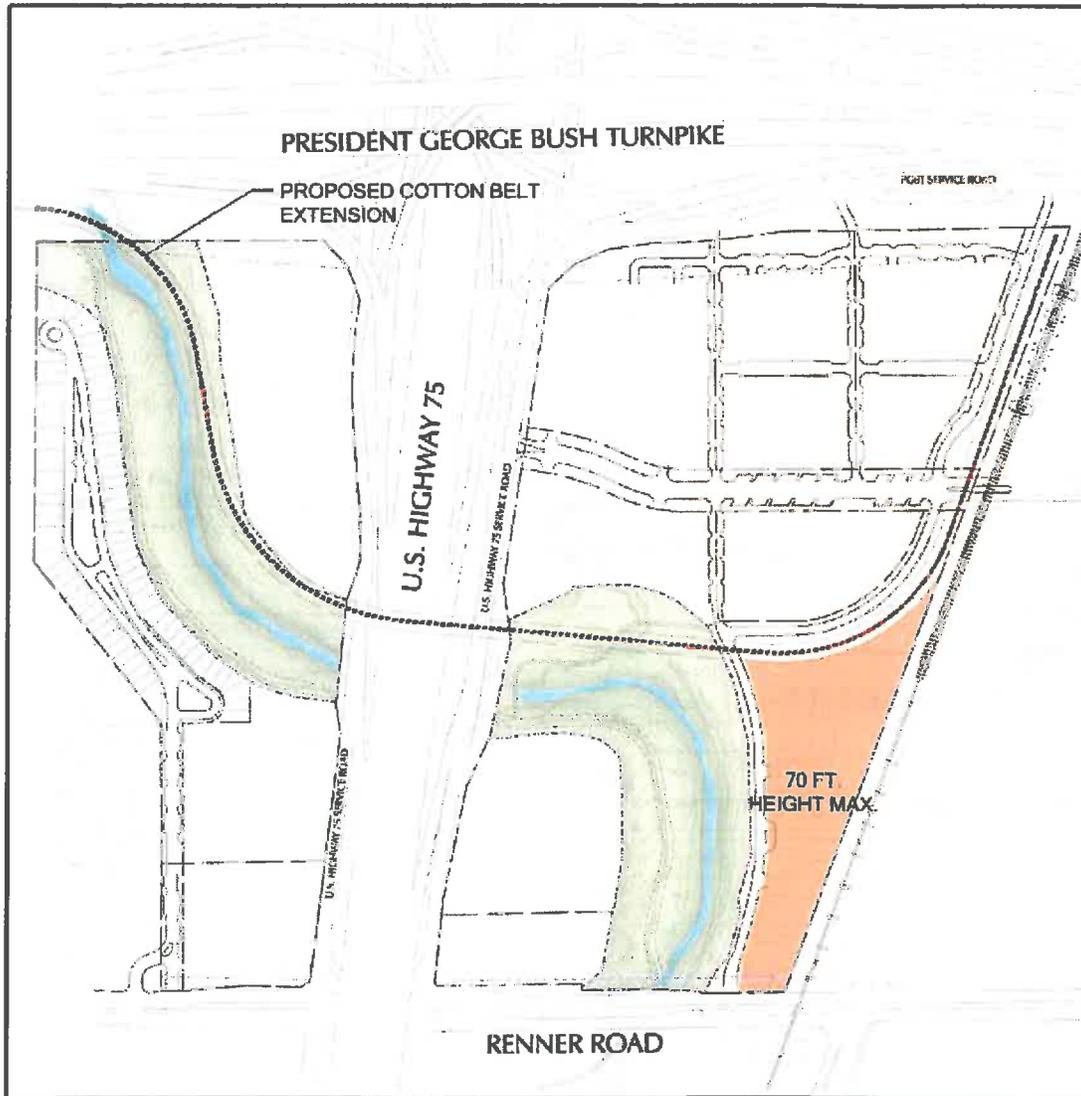
#11 – Setbacks and build-to lines for recessed entry and arcade buildings shall be measured from the building façade line which contains the recessed entry or arcade.

#12 – Required parking may be provided anywhere within the C-TOPD.

#13 – Article III, Chapter 21, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-stre parking areas.

7.2 TOD Mixed Use Zone

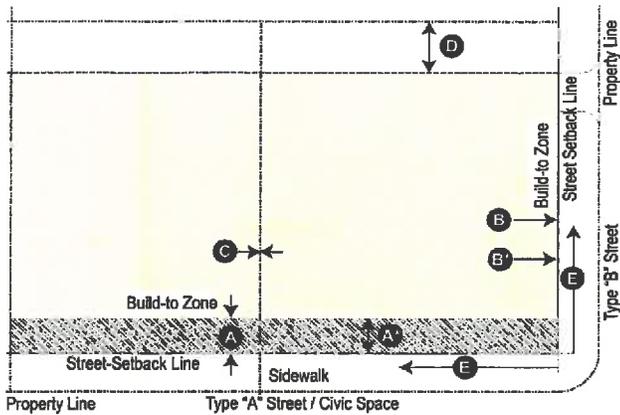
Character Zone: TOD Mixed Use



TOD Mixed Use Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements.

7.2.1 Building Placement



Legend

----- Property Line	----- Setback Line	----- Street-Setback Line
■ Building Area	■ Build-to Zone	

Street-Setback Line (SSL)
(Distance from center line of street cross section to edge of the BTZ)

Parkway	38 feet
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Build-To Zone (BTZ)
(Distance from edge of SSL to edge of the BTZ)

		A
Front (Type "B" Street)	0 - 10 feet (see #1)	B

Setback

		A*
Front (Type "B" Street – distance from SSL)	0 - 20 feet	B*
Side (distance from property line)	0 feet (see #2)	C
Rear (distance from property line)	0 feet (see #2)	D

Building Frontage

Building Frontage required along all street BTZ	90% (min.) (see #3 and #8)	E
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7.2.2 Block Standards

Block face dimensions	250 – 500 feet (see #4)
Block perimeter	1500 feet (maximum) (see #4)

7.2.3 Building Height

Principal Building Standards

Building maximum	Shall be established as on the Regulating Plan (see #6 and #9)
First floor to floor height	10 feet min. (see #5)
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)
Upper floor(s) height (floor-to-floor)	10 feet min.

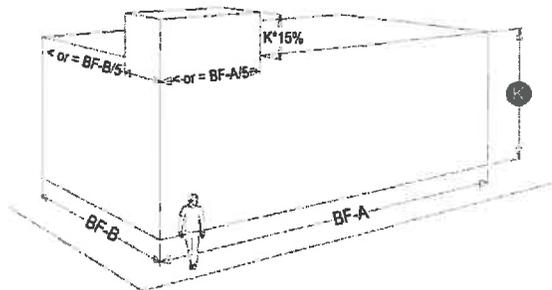
Notes

- #1 – The area between the building and the edge of the BTZ at the public sidewalk shall be paved flush with the sidewalk.
- #2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #3 – Corner building street facades shall be built to the BTZ for a minimum of 30' from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to zone standards.
- #4 – Blocks exceeding the maximum block face and perimeter dimensions may be permitted as follows:
 - i. No more than two adjacent blocks may be combined based on the Regulating Plan.
 - ii. An increase in block dimensions shall not eliminate or significantly move a required street or required civic/open space. Required streets and required civic/open spaces may move a maximum of 100 feet
 - iii. The block shall maintain a continuous Type "A" Street frontage with adjoining blocks.
 - iv. Ground floor internal pedestrian connectivity shall be provided through the block. Connectivity may be provided inside the building, through a parking garage or outside by way of a Pedestrian Passage
- #5 – First floor heights shall not apply to parking structures.
- #6 – Not used.

#7 – All buildings in the TOD Mixed Use Zone shall meet the Building Design Standards in Section 8.

#8 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen higher than the tallest piece of equipment. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen that provides an opaque screen at maturity or a combination of masonry and living screen. Species shall be selected from the C-TOPD Planting List in Appendix B of the C-TOPD Code. The required Street Screen shall be located within the BTZ along the corresponding frontage.

#9 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



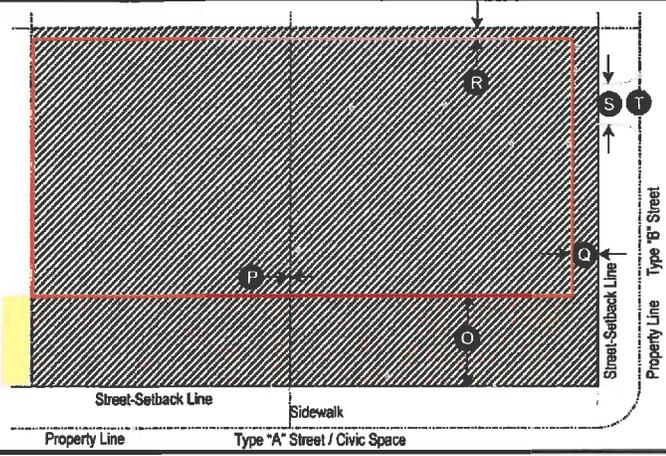
#10 – Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.

#11 – Setbacks and build-to lines for recessed entry and arcade buildings shall be measured from the building façade line which contains the recessed entry or arcade.

#12 – Required parking may be provided anywhere within the C-TOPD.

#13 – Article III, Chapter 21, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas

7.2.5 Parking & Service Access



Legend

Property Line	Building Footprint
Surface Parking Area	Street-Setback Line
Above Grade Parking Area	

(i) Parking Location

Surface/At Grade Parking

		O
Type "B" Street setback	Min. of 3 feet behind the building facade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R
Above Grade Parking		
Setback along Type "B" Street (distance from SSL)	0 feet min.	O
Side and rear setbacks (distance from property line)	0 feet min.	
Upper Floors	May be built up to the building line	

(ii) Required Off-Street Parking Spaces

Non-residential uses	1 space/250 sq. feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	20 feet max. (except service drives which may be a max. of 30 feet wide)	S
Driveways and off-street loading and unloading shall not be located on a Type "A" Streets. Porte cocheres may be permitted on Type "A" Streets to provide drop-off and valet service. Shared driveways and cross access easements are encouraged between lots to minimize curb cuts. If driveway and/or off-street service loading and unloading access is provided from a Type "A" Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.		
		T

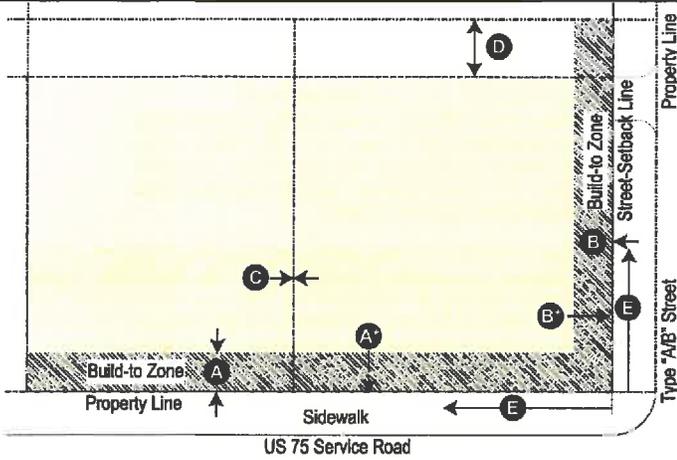
7.2.6 Encroachments

Canopies, signs, awnings and balconies may encroach over the BTZ, setback, and, sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.2.7 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

7.3.1 Building Placement



Legend

----- Property Line	----- Setback Line	----- Street-Setback Line
Building Area	Build-to Zone	

Street-Setback Line (SSL)
(Distance from center line of street cross section to edge of the BTZ)

Park Avenue G-2	44 feet
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Build-To Zone (BTZ)
(Distance from Property Line to edge of the BTZ)

Front (U.S. 75 Service Road)	None
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(Distance from SSL to edge of the BTZ)

Front (Civic Space)	0 – 10 feet	A
Front (Type "B" Street)	0 feet (see #1)	B

Setback

Front (U.S. 75 Service Road – distance from property line or edge of utility easement)	15 feet (min.)
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Front (Civic Space – distance from SSL)	0 feet (min.) – 10 feet (max)	A*
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Front (Type "B" Street – distance from SSL)	0 feet	B*
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Side (distance from property line)	0 feet (see #1)	C
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Rear (distance from property line)	0 feet (see #1)	D
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Building Frontage

Building Frontage required along all street BTZ	40% (min.) (see #5)	E
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Building Frontage required along PGBT and U.S. 75 Service Road	0% (min.) (see #5)	
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7.3.2 Block Standards

Block face dimensions	No Maximum
Block perimeter	No Maximum

7.3.3 Building Height

Principal Building Standards

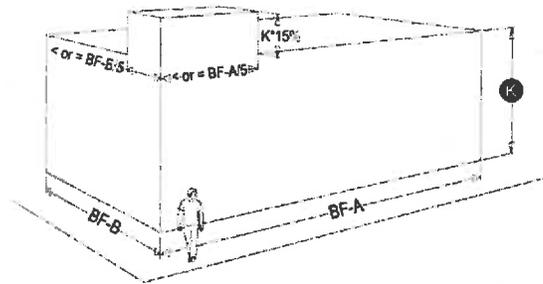
Building maximum	Shall be as established on the Regulating Plan (see #3 and #6)
First floor to floor height	15 feet min. (see #2)
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)
Upper floor(s) height (floor-to-floor)	10 feet min.

7.3.4 Commercial Frontage Requirements

Ground floors of all buildings fronting on Type "A" Streets shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

Notes

- #1 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #2 – First floor heights shall not apply to parking structures.
- #3 – Not used.
- #4 – All buildings in the Arterial Mixed Use Zone shall meet the Building Design Standards in Section 8.
- #5 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen higher than the tallest piece of equipment. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen that provides an opaque screen at maturity or a combination of masonry and living screen. Species shall be selected from the C-TOPD Planting List in Appendix B of the C-TOPD Code. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #6 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.

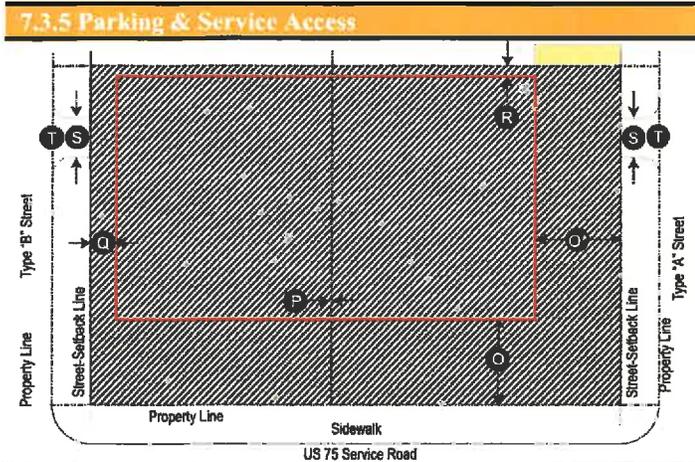


#7 – Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.

#8 – Setbacks and build-to lines for recessed entry and arcade buildings shall be measured from the building façade line which contains the recessed entry or arcade.

#9 – Required parking may be provided anywhere within the C-TOPD.

#10 – Article III, Chapter 21, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas



Legend

Property Line	Building Footprint
Surface Parking Area	Street-Setback Line
Above Grade Parking Area	

(i) Parking Location

Surface/At Grade Parking

U.S. 75 Service Road setback (distance from property line)	Min. 10 feet	
Civic Space setback (distance from SSL)	Min. of 3 feet behind the Building façade line	O
Type "B" Street setback (distance from SSL)	Min. of 3 feet behind the building façade line along that street	Q
Side setback (distance from property line)	0 feet	P
Rear setback (distance from property line)	0 feet	R

Above and Below Grade Parking (Distance from SSL)

Setback along Type "B" Street, and Civic Space (distance from SSL)	0 feet min.	O
Setback along U.S. 75 Service Road (distance from property line)	0 feet min.	
Side and rear setbacks (distance from property line)	0 feet min.	
Upper Floors	May be built up to the building line	

(ii) Required Off-Street Parking Spaces

Non-residential uses	1 space, 200 sq. feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

Parking driveway width	City standards on Arterial Roadways and 24 feet max on all C-TOPD Streets, except when drives may need to be wider to address service access or fire lane standards.	S
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Driveways and off-street loading and unloading shall not be located on a Type "A" Streets. **T**

Porte cocheres may be permitted on Type "A" Streets to provide drop-off and valet service.

Shared driveways and cross access easements are encouraged between lots to minimize curb cuts.

If driveway and/or off-street service loading and unloading access is provided from a Type "A" Street, such access shall be deemed as temporary and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped.

7.3.6 Encroachments

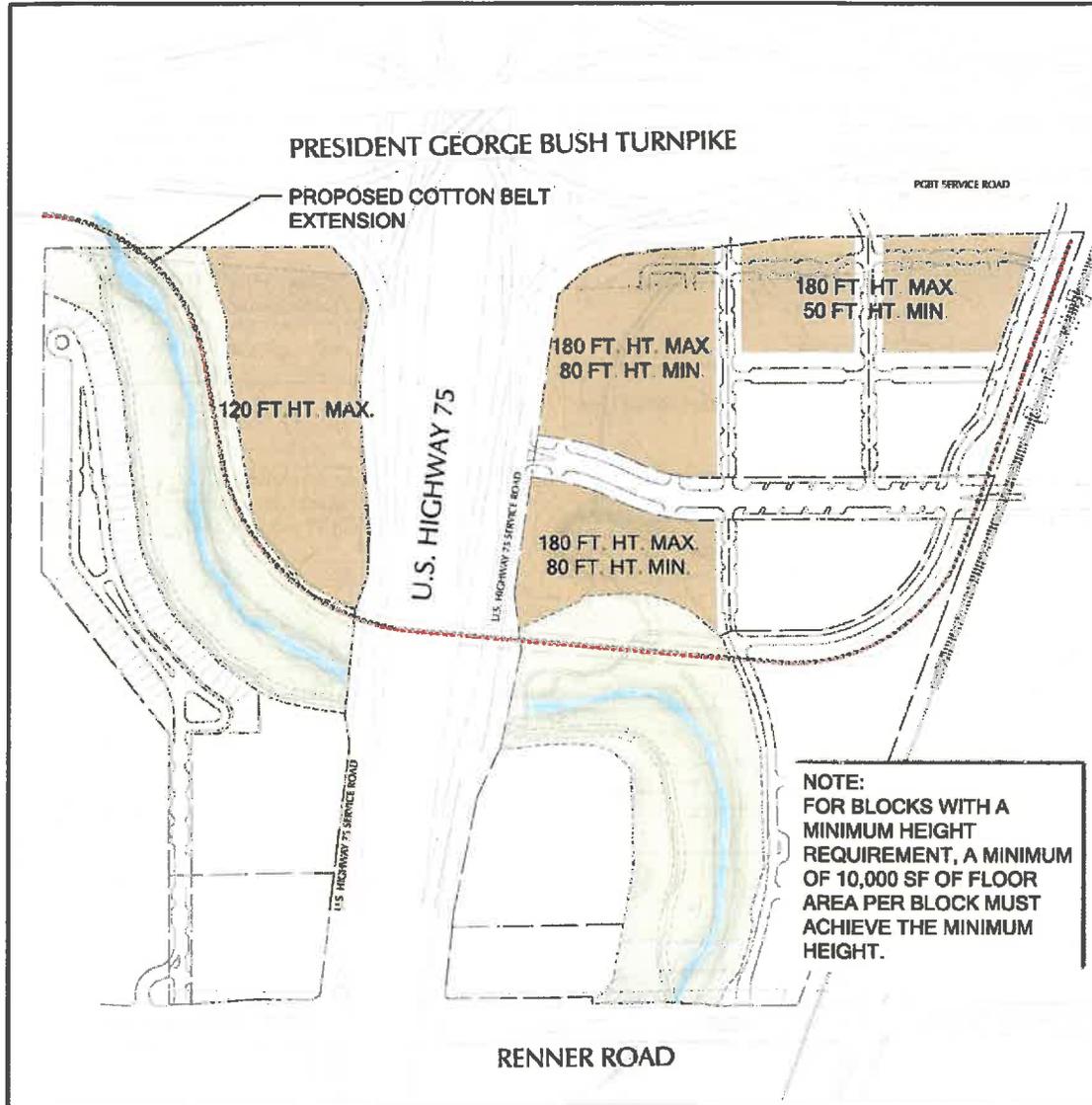
Canopies, signs, awnings and balconies may encroach over the BTZ, setback, and sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.3.7 Applicability

Building Form and Development Standards in this Section shall apply to all development in this Character Zone.

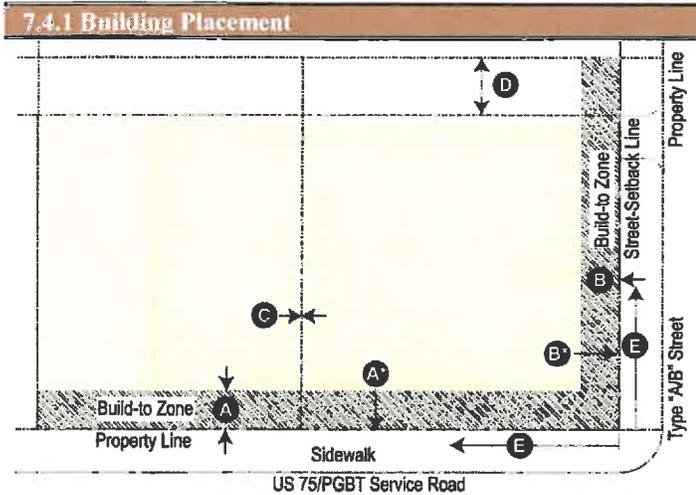
7.4 TOD Freeway Hi-Rise

Character Zone: TOD Freeway Hi-Rise



TOD Freeway Hi-Rise Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements.



Legend

----- Property Line	----- Setback Line	----- Street-Setback Line
▨ Building Area	▨ Build-to Zone	

Street-Setback Zone (SSL)
(Distance from center line of street cross section to edge of the BTZ)

TOD Main Street	61 feet
TOD Street	32 feet min.
TOD Street Promenade	42 feet min.
TOD Station	33.5 feet min.

Build-To Zone (BTZ)
(Distance from Property Line to edge of the BTZ)

Front (PGBT and U.S. 75 Service Road)	None	A
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(Distance from SSL to edge of the BTZ)

Front (Type "A" Street and Civic Space)	0 – 25 feet	B
Front (Type "B" Street)	0 feet	

Setback

Front (PGBT and U.S. 75 Service Road – distance from property line or edge of utility easement)	15 feet (min.)	A*
Front (Type "A/B" Street and Civic Space – distance from SSL)	0 feet	B*
Side (distance from property line)	0 feet (see #1)	C
Rear (distance from property line)	0 feet	D

Building Frontage

Building Frontage required along all street BTZ	90% (min.) (see #2 and #6)	
Building Frontage required along PGBT and U.S. 75 Service Road	0% (min.) (see #2 and #6)	E

7.4.2 Block Standards

Block face dimensions	No Maximum
Block perimeter	No Maximum

7.4.3 Building Height

Principal Building Standards

Building maximum	Shall be as established on the Regulating Plan (see #4 and #7)
First floor to floor height	15 feet min. (see #3)
Ground floor finish level	12 inches max. above sidewalk (for ground floor Retail Ready buildings)
Upper floor(s) height (floor-to-floor)	10 feet min.

7.4.4 Commercial Frontage Requirements

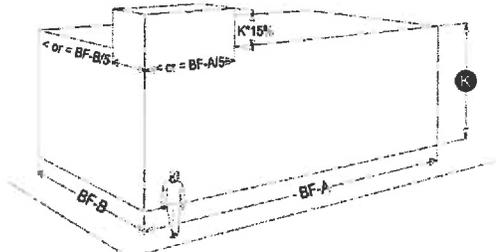
Ground floors of all buildings fronting on Type "A" streets shall be built to Retail Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.

7.4.5 Station Frontage Requirements

Ground floors of all buildings designated as Station Frontage on the Regulating Plan shall provide shaded areas to a minimum depth of 6 feet. Shaded devices may include arcades, galleries, awnings, canopies, etc.

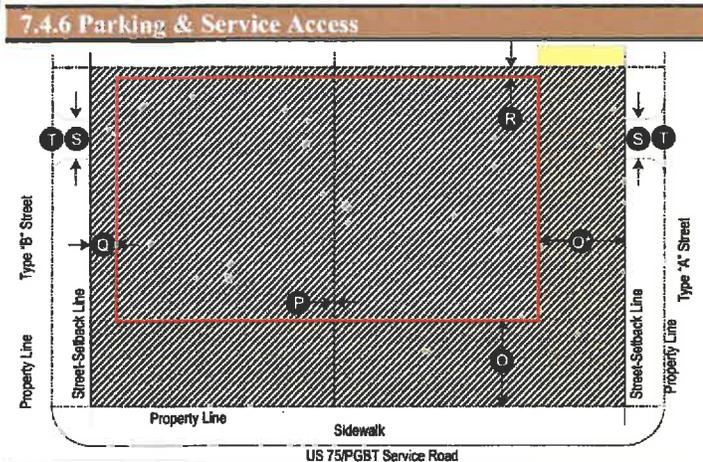
Notes

- #1 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #2 – Corner building street facades shall be built to the BTZ for a minimum of 15' from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the build-to zone standards.
- #3 – First floor heights shall not apply to parking structures.
- #4 – Not used.
- #5 – All buildings in the TOD Freeway Hi-Rise shall meet the Building Design Standards in Section 8.
- #6 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen higher than the tallest piece of equipment. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen that provides an opaque screen at maturity or a combination of masonry and living screen. Species shall be selected from the C-TOPD Planting List in Appendix B of the C-TOPD Code. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #7 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



- #8 – Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.
- #9 – Setbacks and build-to lines for recessed entry and arcade buildings shall be measured from the building façade line which contains the recessed entry or arcade.
- #10 – Required parking may be provided anywhere within the C-TOPD.

#11 – Article III, Chapter 21, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas.



Legend

Property Line	Building Footprint
Surface Parking Area	Street-Setback Line
Above Grade Parking Area	

(i) Parking Location

Surface/At Grade Parking

PGBT Service Road and U.S. 75 setback (see #12)	Shall be located behind the principal building	O
Type "A" Street and Civic Space (see #12)	Shall be located behind the principal building	O*
Type "B" Street setback	Min. of 3 feet behind the building façade line along that street	Q
Side setback (distance from property line)	0 feet min.	P
Rear setback (distance from property line)	0 feet min.	R

Above Grade Parking

Setback along Type "A" Street, Type "B" Street, and Civic Space (distance from SSL)	0 feet min.
PGBT Service Road, U.S. 75 Service Road, side and rear setbacks (distance from property line)	0 feet min.
Upper Floors	May be built up to the building line

(ii) Required Off-Street Parking Spaces

Non-residential uses	1 space/300 sq. feet (gross)
Residential uses	1.5 space/unit

(iii) Driveways and Service Access

	TXDOT standards on service road and 24 feet max on all C-TOPD	S
Parking driveway width	Streets, except when drives may need to be wider to address service access or fire lane standards.	

Driveways and off-street loading and unloading shall not be located on PGBT Service Road. Porte cocheres may be permitted on Type "A" Streets to provide drop-off and valet service. Shared driveways and cross access easements are encouraged between lots to minimize curb cuts. If driveway and/or off-street service loading and unloading access is provided from PGBT Service Road, such access shall be deemed as temporary and cross access easements

along the rear of the property shall be required when adjoining properties are undeveloped.

7.4.7 Encroachments

Canopies, signs, awnings and balconies may encroach over the BTZ, setback, and, sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.

7.4.8 Applicability

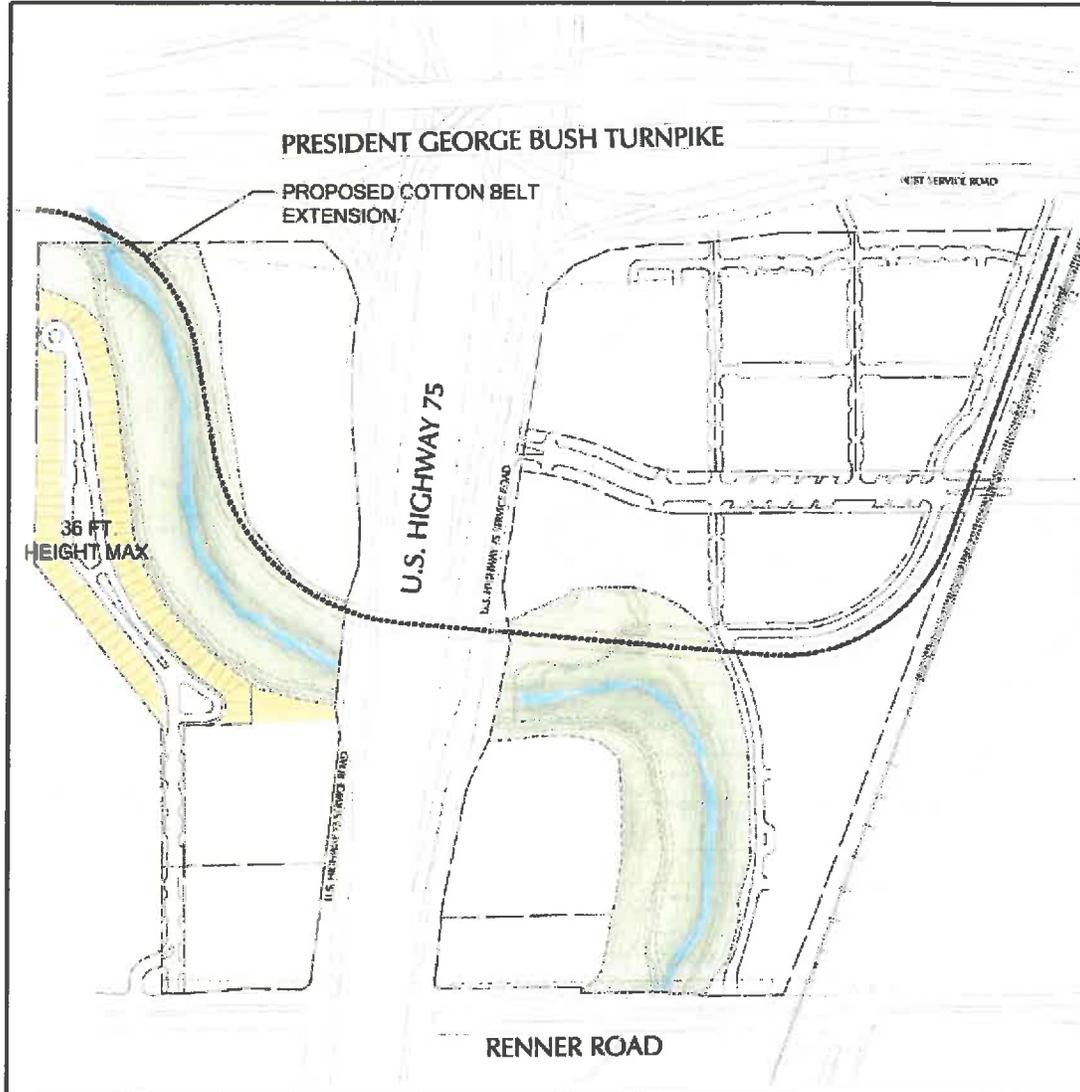
Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

Notes

#12 – Surface parking as an ancillary use with an office motor court shall be permitted with a 20-foot setback.

7.5 Urban Neighborhood Zone

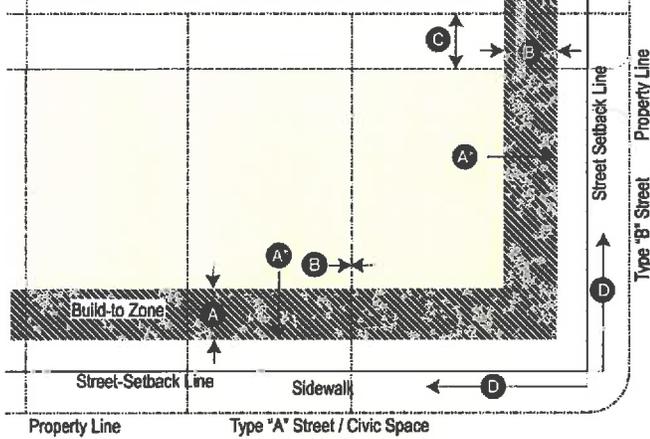
Character Zone: Urban Neighborhood



Urban Neighborhood Zone Location Map

Note: This map is for reference only. Refer to the Regulating Plan (Appendix A) for all requirements.

7.5.1 Building Placement



Legend

----- Property Line	----- Setback Line	----- Street-Setback Line
■ Building Area	▨ Build-to Zone	

Street-Setback Line (SSL)
(Distance from center line of street cross section to edge of the BTZ)

Park Avenue G-1	26 feet
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Build-To Zone (BTZ)
(Distance from SSL to edge of the BTZ)

Front (Type Civic Space)	10 – 20 feet	A
Front (Type "B" Street)	10 – 20 feet	B

Setback

Front (distance from SSL)	10 feet	A*
Side (distance from property line)	0 feet (see #1)	B
Rear (distance from property line)	10 feet	C

Building Frontage

Building Frontage required along all streets BTZ	70% (min.) (see #4)	D
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7.5.2 Block Standards

Block face dimensions	No Maximum
Block perimeter	No Maximum

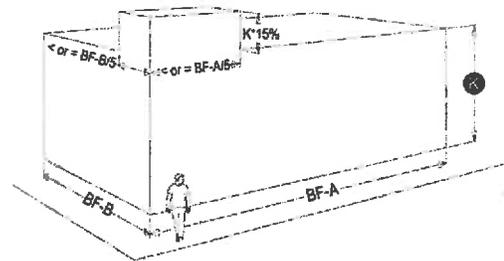
7.5.3 Building Height

Principal Building Standards

Building maximum	Shall be as established on the Regulating Plan (see #2 and #5)
Accessory building maximum	20 feet (see #2)
First floor to floor height	15 feet min. for non-residential 10 feet min for residential
Ground floor finish level	12 in. max. above finished sidewalk (for Retail Ready buildings); 18 in. min. (for residential uses)
Upper floor(s) height (floor-to-floor)	10 feet min.

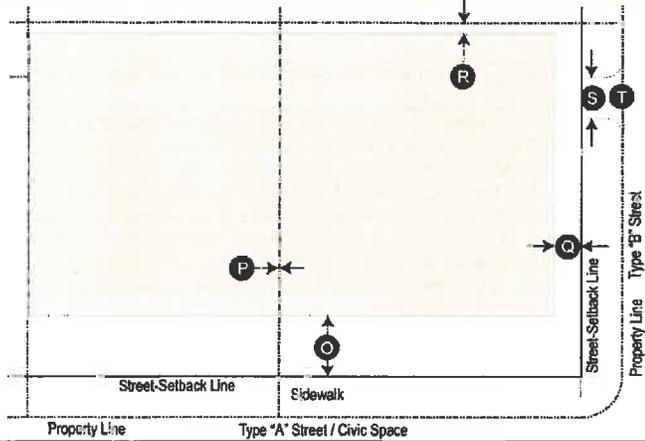
Notes

- #1 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
- #2 – Not used.
- #3 – All buildings in the Urban Neighborhood Zone shall meet the Building Design Standards in Section 8.
- #4 – Any frontage along all streets (except alleys) not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen, furthermore service areas shall be defined by a Street Screen higher than the tallest piece of equipment. The Street Screen shall be of either the same building material as the principal structure on the lot or masonry or a living screen that provides an opaque screen at maturity or a combination of masonry and living screen. Species shall be selected from the C-TOPD Planting List in Appendix B of the C-TOPD Code. The required Street Screen shall be located within the BTZ along the corresponding frontage.
- #5 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.



- #6 - Ground and roof mounted mechanical equipment shall be screened from direct ground level view from adjoining public rights-of-way. In addition to a parapet wall no lower than 36 inches, the perimeter of any visible roof mounted mechanical equipment shall be circumscribed by a wall or permanent screen that is at least as tall as the equipment itself.
- #7 – Setbacks and build-to lines for recessed entry and arcade buildings shall be measured from the building façade line which contains the recessed entry or arcade.
- #8 – Article III, Chapter 21, Section 21-52 of the City of Richardson Subdivision and Development Ordinance shall apply for design of off-street parking areas

7.5.4 Parking & Service Access



Legend

- Property Line
- Parking Area

(iv) Parking Location

Surface/At Grade Parking

Civic Space setback (distance from SSL)	21 feet	O
Type "B" Street setback (distance from SSL)	10 feet	Q
Side setback (distance from property line)	5 feet	P
Rear setback (distance from property line)	5 feet	R

(v) Required Off-Street Parking Spaces

Non-residential uses	1 space/300 sq. feet (gross)
Residential uses	1.5 space/unit

(vi) Driveways and Service Access

Parking driveway width	20 feet max.	S
Shared driveways and cross access easements are encouraged between lots to minimize curb cuts.		T

7.5.5 Encroachments

Porches, stoops, awnings, signs, canopies, balconies, bay windows and other architectural features may encroach into required yards, provided they do not encroach over the front property line.

7.5.6 Applicability

Building Form and Development Standards in this Section shall apply to all development within this Character Zone.

Section 8. Building Design Standards

The Building Design Standards and Guidelines for the C-TOPD shall establish a coherent urban character and encourage enduring and attractive development. Development plans shall be reviewed by the City Manager or designee for compliance with the standards below.

The key design principles establish essential goals for development in the C-TOPD to ensure the preservation, sustainability, and visual quality of this unique environment. Buildings shall be located and designed so that they provide visual interest and create enjoyable, human-scaled spaces. The key design principles are:

- a. New buildings shall utilize building elements and details to achieve a pedestrian-oriented public realm.
- b. Compatibility is not meant to be achieved through uniformity, but through the use of variations in building elements to achieve individual building identity.
- c. Building facades shall include appropriate architectural details and ornament to create variety and interest.
- d. Open space(s) shall be incorporated to provide usable public areas integral to the urban environment.

8.1 General to all Character Zones

8.1.1 Building Orientation

- i. Buildings shall be oriented toward public streets, where the lot has frontage along public streets.
- ii. Primary entrance to buildings shall be located on the street along which the building is oriented. At intersections, corner buildings may have their primary entrances oriented at an angle to the intersection.
- iii. All primary entrances shall be oriented to the public sidewalk for ease of pedestrian access. Secondary and service entrances may be located from internal parking areas or alleys.

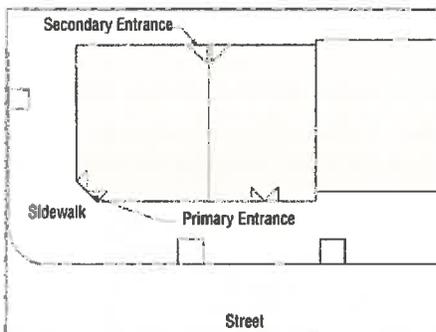


Figure showing required building orientation and location of primary entrances

8.1.2 Design of Parking Structures

- i. All frontages of parking structures located on Type "A" Streets shall not have parking uses on the ground floor to a minimum depth of 30 feet.
- ii. Parking structure facades on all public streets shall be designed with both vertical (façade rhythm of 20 feet to 30 feet) and horizontal (aligning with horizontal elements along the block) articulation.

- iii. Where above ground structured parking is located at the perimeter of a building with frontage along any public street; it shall be screened in such a way that cars on all levels are completely hidden from view.
- iv. When parking structures are located at corners, corner architectural elements shall be incorporated such as corner entrance, signage and glazing.



Images showing appropriate design of Parking Structures

8.1.3 Design of Automobile Related Building Site Elements

- i. Drive-through lanes for commercial uses shall not be located along or visible from any public street within all Character Zones except Arterial Mixed Use and TOD Mixed Use. Drive-through lanes shall be hidden behind a Street Screen within Arterial Mixed Use and TOD Mixed Use.
- ii. All off-street loading, unloading, and trash pick up areas shall be located along alleys or Type 'B' Streets unless permitted in the specific building form and development standards in Section 7. Any off-street loading, unloading, or trash pick up areas shall be screened using a Street Screen that is at least as tall as the trash containers and/or service equipment it is screening at the BTZ. The Street Screen shall be made up of (i) the same material as the principal building or (ii) a living screen or (iii) a combination of the two.

8.2 Standards Specific to the TOD Core, TOD Mixed Use, Arterial Mixed Use and TOD Freeway Hi-Rise Character Zones:

8.2.1 Roof Form

- i. Buildings shall have simple, flat fronts with minimal articulations with flat or low pitched roofs with parapets. Corner hip roof elements and gable accents at the parapet may be permitted. Projecting mansard roofs shall not be permitted.



8.2.2 Façade Composition

- i. Storefronts on facades that span multiple tenants shall use architecturally compatible materials, colors, details, awnings, signage, and lighting fixtures.
- ii. Building entrances may be defined and articulated by architectural elements such as lintels, pediments, pilasters, columns, porticos, porches, overhangs, railings, balustrades, and

others as appropriate. All building elements should be compatible with the architectural style, materials, colors, and details of the building as a whole. Entrances to upper level uses may be defined and integrated into the design of the overall building facade.

- iii. Buildings shall generally maintain the alignment of horizontal elements along the block.
- iv. Corner emphasizing architectural features, pedimented parapets, cornices, awnings, blade signs, arcades, colonnades and balconies may be used along commercial storefronts to add pedestrian interest.



Buildings with architectural features and storefront elements that add interest along the street.

- v. Buildings which are located on axis with a terminating street or at the intersection of streets shall be considered as feature buildings. Such buildings shall be designed with features which take advantage of that location, such as an accentuated entry and a unique building articulation which is off-set from the front wall planes and goes above the main building eave or parapet line.

8.2.3 Windows and Doors

- i. Windows and doors on public street (except alleys) fronting facades shall be designed to be proportional and appropriate to the architectural style of the building. First floor windows shall NOT be opaque, tinted or mirrored glass.
- ii. All ground floor front facades of buildings along public streets or Civic/Open Space shall have transparent storefront windows covering no less than 50% of the façade area.



Images showing appropriate window designs and proportions.

8.2.4 Building Materials

- i. At least 85% of each building’s façade (excluding doors and windows) along any public street shall be finished in one of the following materials:
 - Masonry (brick, stone, stucco utilizing a three-step process, cast stone, glass or glass block)
- ii. No more than 15% of each façade along any public street shall use accent materials such as wood, architect metal panel, split-face concrete block, tile, pre-cast concrete panels, or

Exterior Insulating Finishing System (EIFS). EIFS may only be used 8 feet above the ground floor.



Images showing appropriate building materials within TOD Core, TOD Mixed Use, and TOD Freeway Hi-Rise.

- iii. Roofing materials visible from any public right-of-way shall be copper, factory finished standing seam metal, slate, synthetic slate, or similar materials.

8.3 Standards Specific to the Urban Neighborhood Character Zone:

8.3.1 Building Orientation

- i. Garages for Residential Buildings shall be located on alleys at the rear of residential buildings; pull-through garages are allowed if the garage door is set back behind the rear façade of the main structure. If front-loaded garages are utilized for residential uses, the garages shall be no greater than 12 feet wide and set back at least 20 feet measured from the face of the main structure closest to the garage or rotated 90 degrees with windows on the wall facing the street.
- ii. All garage doors shall be divided into single bays separated by at least a 16 in. pier or column.
- iii. Front-loaded garages shall only be permitted on lots that back up to Spring Creek Corridor. Front-loaded garages on residential lots less than 40 feet wide shall not be permitted.

8.3.2 Building Massing and Scale

- i. Commercial and Mixed Use Buildings shall be simple, rectilinear forms with flat or low pitched roofs with parapets.
- ii. Residential buildings shall have few, if any, articulations and simple roofs (gable, hip, combination) with most building wing articulations set at the rear of the structure. Window projections, bay windows, stoops, porches, balconies, and similar extensions shall be exempt from this standard.
- iii. Gable roofs, if provided for residential buildings, shall have a minimum pitch of 5/12. When hipped roofs are used, the minimum pitch shall be 5/12. Other roof types shall be appropriate to the architectural style of the building. Porch roofs may be a minimum pitch of 3/12.
- iv. Projecting mansard roofs shall be prohibited.



Images showing appropriate massing and scale for Residential Buildings

8.3.3 Façade Composition

- i. Buildings shall maintain a façade rhythm of 20 feet to 30 feet along Type 'B' streets. This rhythm may be expressed by changing materials, or color, or by using design elements such as columns and pilasters, or by varying the setback of portions of the building façade.
- ii. Awnings, blade signs, arcades, colonnades, café seating, and balconies should be used along commercial storefronts to add pedestrian interest along all public streets.
- iii. For retail storefronts, a transom, display window area, and bulkhead at the base shall be utilized.
- iv. Buildings shall generally maintain the alignment of horizontal elements along the block.
- v. Porches, stoops, eaves and balconies shall be added along the front residential facades to add pedestrian interest along public streets.
- vi. For residential buildings the grade of the slab or first floor elevation shall be elevated at least 18 inches above the grade of the sidewalk.
- vii. Alley facing facades shall be of finished quality and of the same color that blend with the public facades of the building.



Residential buildings with porches, balconies, and stoops to add interest along the street.

8.3.4 Windows and Doors

- i. Windows and doors shall be designed to be proportional and appropriate to the architectural style of the building.
- ii. Windows may have jack arch, keystone arch, flat arch, or ornamental arches.



Images showing appropriate window designs and proportions.

- iii. All ground floor building facades for commercial and mixed use buildings along public streets shall have transparent storefront windows covering no less than 50% of the façade area. Each upper floor of the same building façades facing a public street or Civic/Open Space shall contain transparent windows covering at least 30% of the façade area.
- iv. All building facades of residential buildings fronting on public streets or civic / open spaces, except alleys, shall have transparent windows covering at least 25% of each façade.

8.3.5 Commercial and Mixed Use Building Materials

- i. The following materials shall NOT be permitted on any façade:
 - Use of lap or shingle siding of any material including wood, vinyl, or painted or corrugated metal, or roofing materials.
- ii. At least 85% of public street facing facades of all new buildings (excluding doors and windows) shall be finished in one or more of the following materials:
 - Masonry (brick, stucco utilizing a three-step process, stone, terra cotta, cast stone, glass block and/or tile).
 - Tilt-up concrete panels that have a grid like appearance
 - Architectural metal panels
- iii. No more than 15% of public street facing facades shall use accent materials such as wood, metal, cementitious-fiber clapboard (not sheet) with at least a 50-year warranty only on upper floors, or Exterior Insulating Finishing System (EIFS). EIFS may only be used 8 feet above the ground floor.
- iv. Side facades and rear facades (that do not front on any streets) shall be of finished quality and of the same color and materials that blend with the front of the building. Building materials for these facades may be any of the primary and accent façade materials listed above. Ground floors of side and rear facades shall not be EIFS.

8.3.6 Residential Building Materials

- i. At least 85% of all public street fronting facades (excluding doors and windows) shall be finished in one or more of the following materials. No more than three different materials shall be used on any single residential façade:
 - Cementitious-fiber clapboard (not sheet) with at least a 50-year warranty.
 - Masonry (brick; stone; cast stone, or stucco utilizing a three-step process).
- ii. The following may only be allowed up to 15% as an accent material:
 - Exterior Insulating Finishing System (EIFS), architectural metal panels or similar material over a cementitious base, rock, glass block and tile. EIFS may only be used 8 feet above the ground floor.
- iii. Rear facades shall be of finished quality and of the same color that blend with the public street facades of the building. Ground floors of all facades shall not be EIFS.
- iv. Roofing materials (visible from any public right-of-way): copper, factory finished painted metal, slate, synthetic slate, terra cotta, and asphalt shingles.
- v. An enclosed garage shall be designed and constructed of the same material as the primary building.

Section 9. Street & Streetscape Design Standards

- 9.1 Generally: Streets in the C-TOPD District need to support the overall goal of a mixed use, compact, pedestrian oriented district. They should balance all forms of mobility while maximizing convenience for residents and visitors.

The Regulating Plan designates the required and recommended street network within the C-TOPD District. This section specifies the typical configuration of streets within the C-TOPD District. The specifications address vehicular lane width, parkway widths, R.O.W widths, number of travel lanes, on-street parking, and pedestrian accommodation. The character of streets in the C-TOPD District will vary based on the location. The service/access roads of President George Bush Turnpike (State Highway 190) and US 75 are under the purview of Texas Department of Transportation (TxDOT) while the remaining streets are city streets.

- 9.2 New Streets: This section specifies standards for all new streets in the C-TOPD District. New streets shall be based on the Mandatory or Non-Mandatory designation on the Regulating Plan.

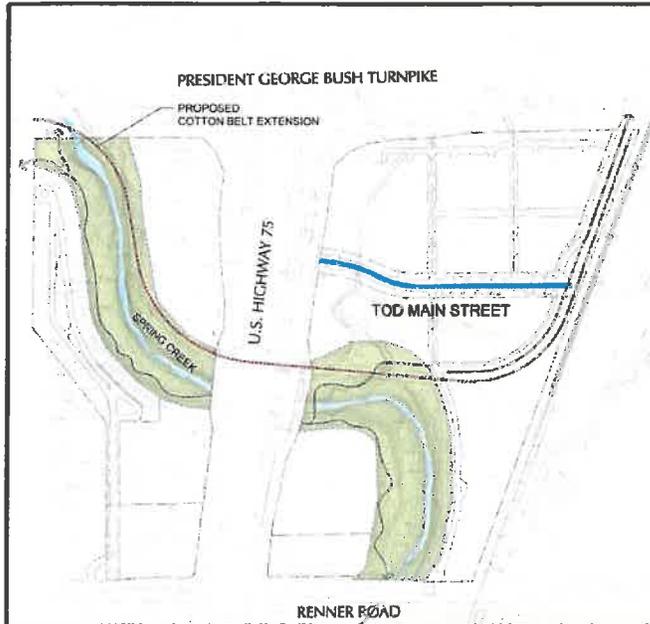
9.3 Street Classifications Established: Table 9.1 and associated cross sections shall establish the cross sections for each street type. The cross sections may be adjusted to fit existing contexts with the approval of the City Engineer. In addition, the proposed cross sections may be adjusted to meet the needs of the Uniform Fire Code as adopted by the City.

Table 9.1

Elements Street Classifications	Street Width (Recommended Minimum)		Number of Vehicular Lanes	Lane Widths for Vehicular Lanes	Number of Bike Lanes	On-Street Parking	Pedestrian Sidewalk Width (min.)	Parkway/ Tree Well
	R.O.W	Pedestrian Easement (both sides)						
TOD Main Street	90 feet	16 feet	4	10.5 feet	2	Yes, both sides, angled	16 feet	Tree well (5 feet X 5 feet)
TOD Street	38 feet	13 feet	2	11 feet		Yes, both sides, parallel	8 feet	Parkway (5 feet) Tree well (5 feet X 5 feet)
Parkway	30 feet	13 feet	2	15 feet		No	8 feet	Parkway (5 feet)
TOD Street Promenade	38 feet	13-23 feet	2	11 feet		Yes, both sides, parallel	8 feet; 10 feet plaza/outdoor dining	Tree well (5 feet X 5 feet) Parkway (5 feet)
TOD Station	Variable	13 feet (west side)	2	10.5 feet		Yes, west side, parallel	13 feet (west)	Tree well (5 feet X 5 feet)
TOD Trail Connector	22 feet	13 feet (north); 14.5 feet (south)	2	11 feet		No	8 feet (north) 14.5 feet (south)	Tree well (5 feet X 5 feet) Parkway (5 feet – north)
Park Avenue (G-1)	Variable	11 feet	2	14 feet		Yes, both sides, parallel	6 feet	Tree well (5 feet X 5 feet) Parkway (5 feet)
Park Avenue (G-2)	57 feet	11 feet	2	15 feet		Yes, both sides, parallel	6 feet	Tree well (5 feet X 5 feet) Parkway (5 feet)

9.4 TOD Main Street

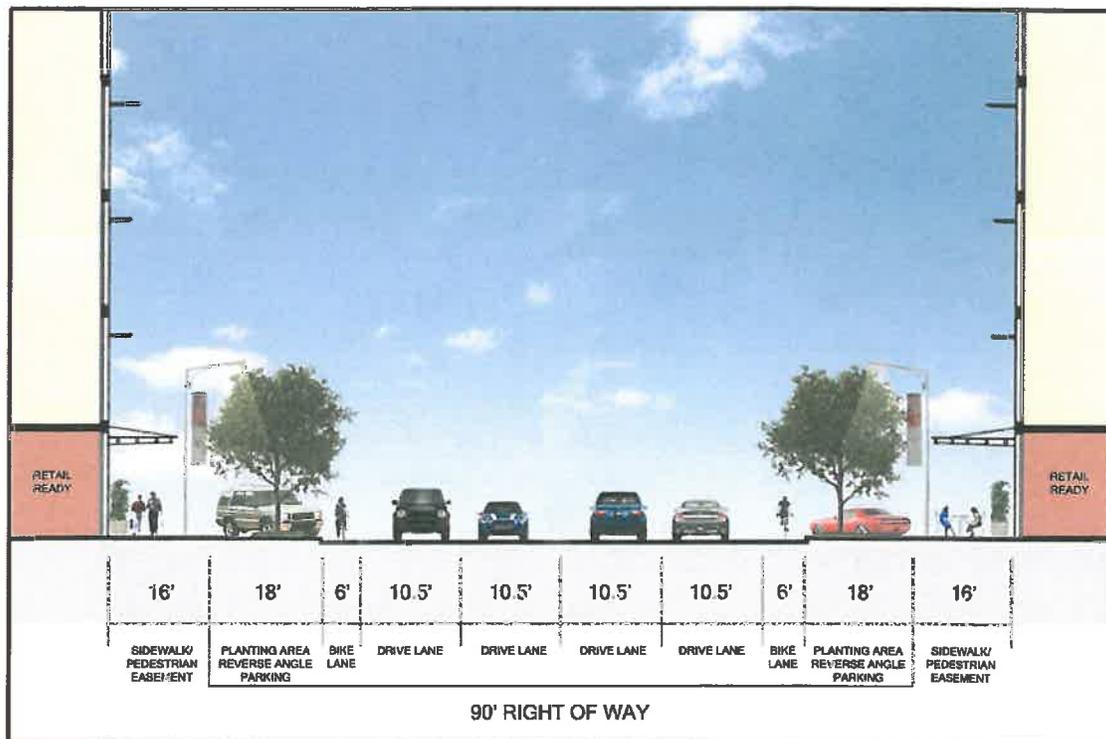
Location Map



TOD Main Street

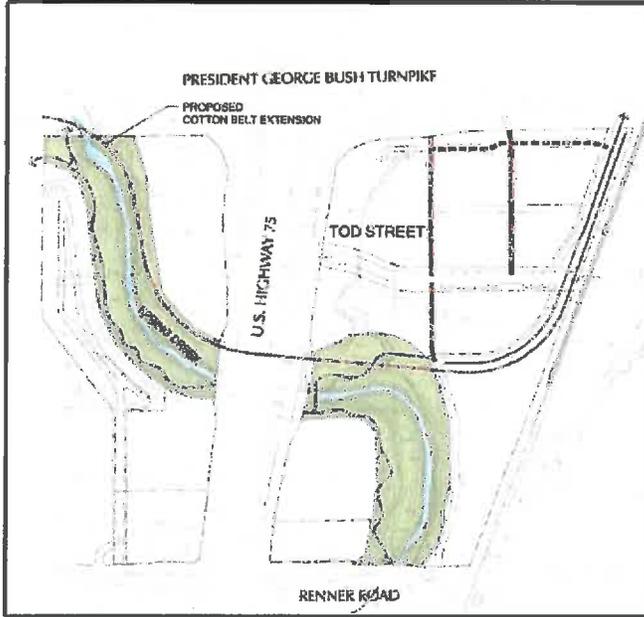
Street Type: A
ROW: 90 ft.
Design Speed: 25 MPH
Parking: Reverse Angle on both sides
Number of Vehicular Lanes: 4
Number of Bike Lanes: 2
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



9.5 TOD Street

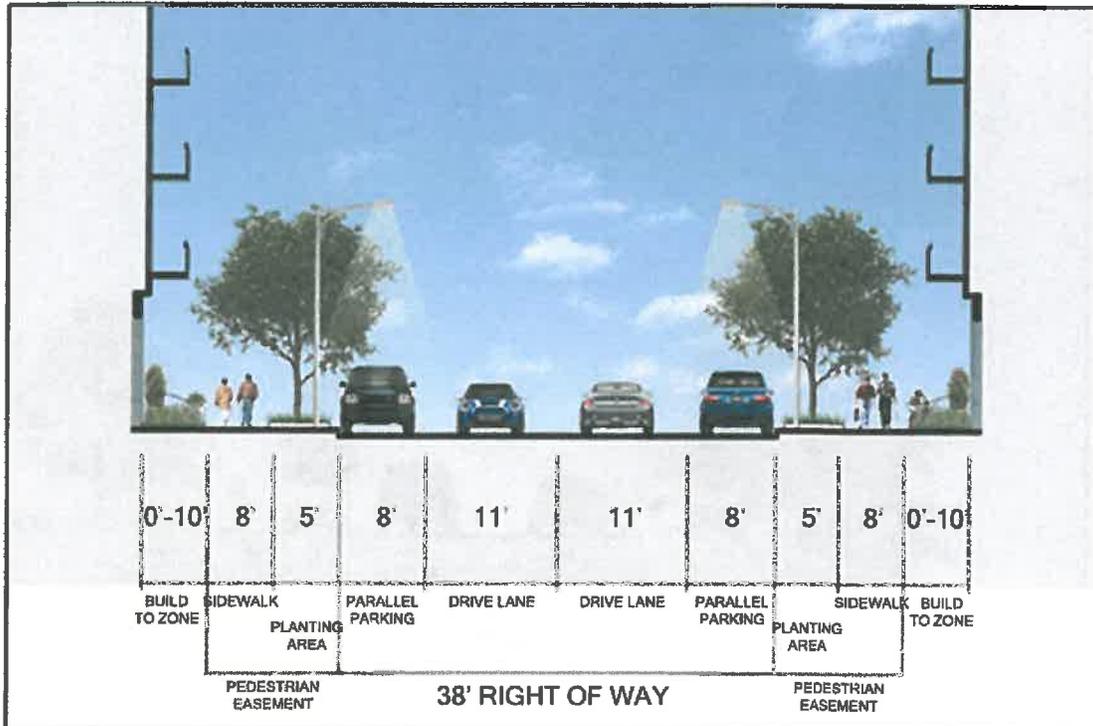
Location Map



TOD Street

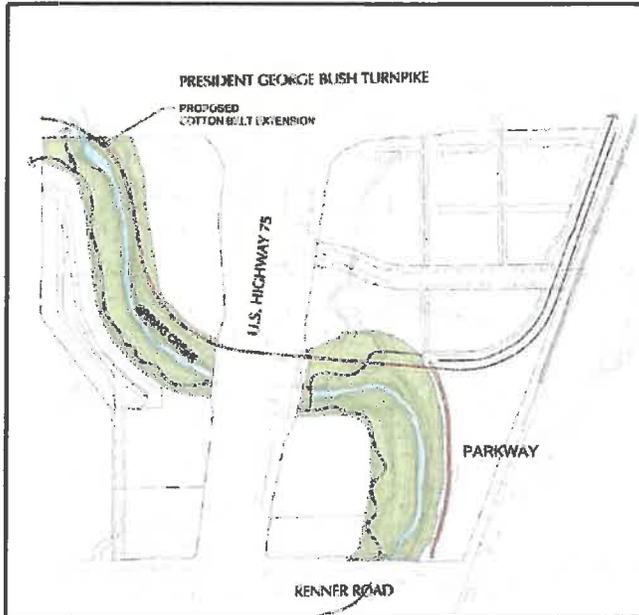
Street Type: B
ROW: 38 ft.
Design Speed: 25 MPH
Parking: Parallel on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



9.6 Parkway

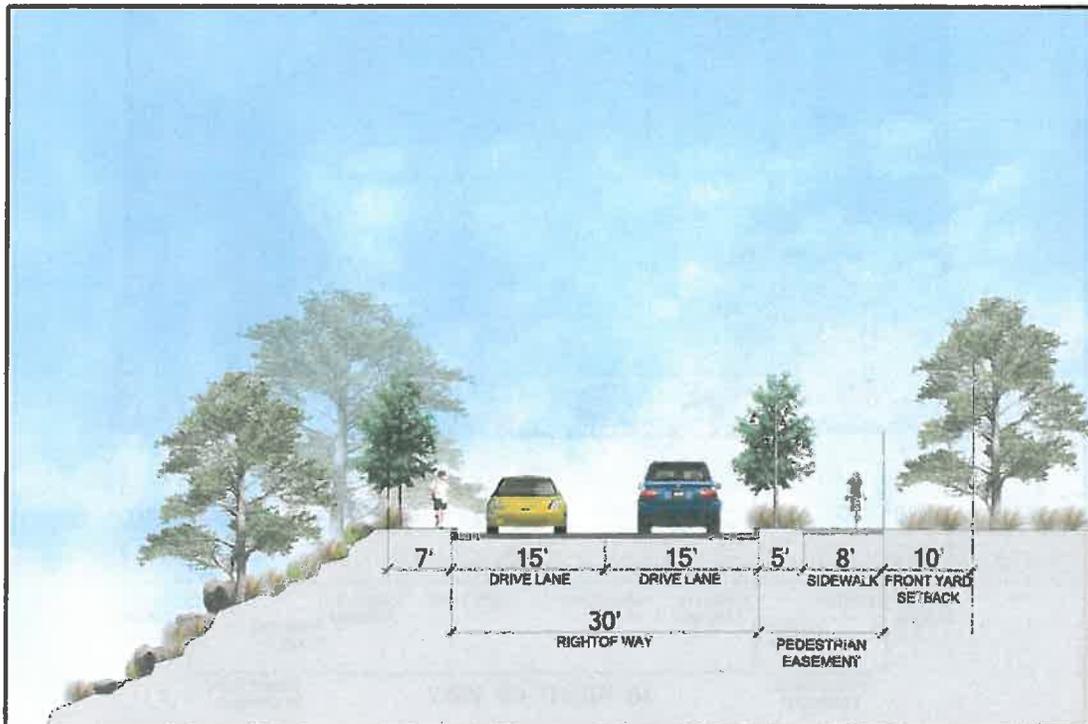
Location Map



Parkway

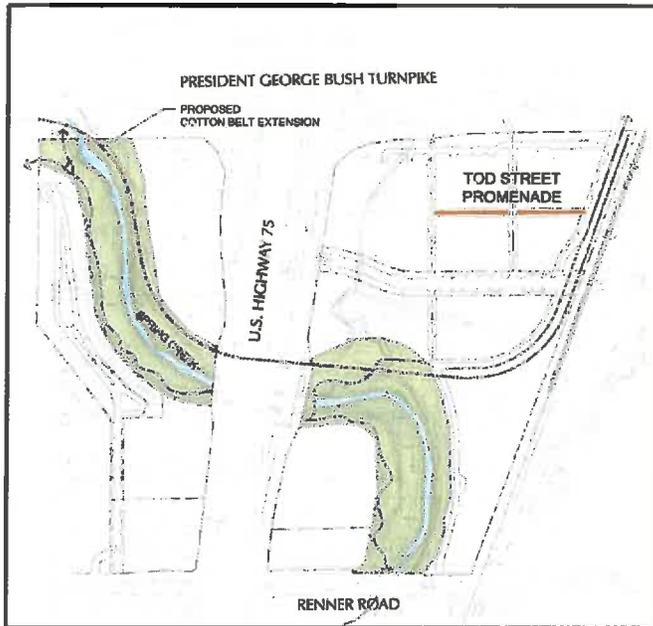
Street Type: B
ROW: 30 ft.
Design Speed: 25 MPH
Parking: None
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



9.7 TOD Street Promenade

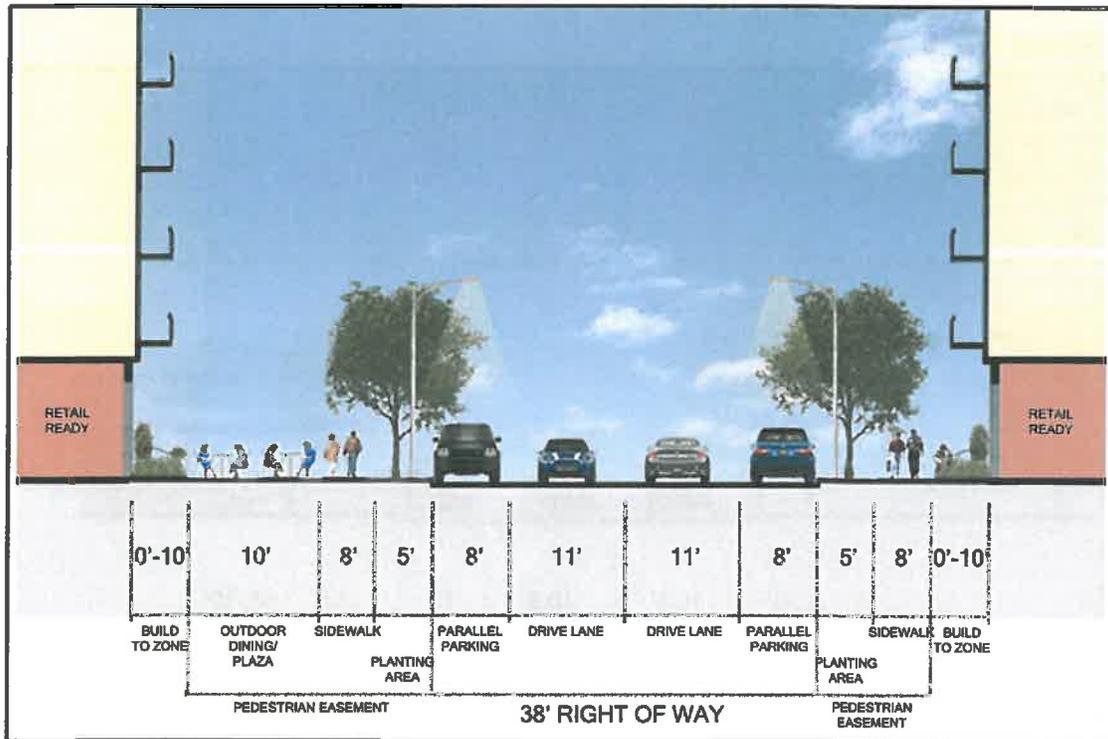
Location Map



TOD Street Promenade

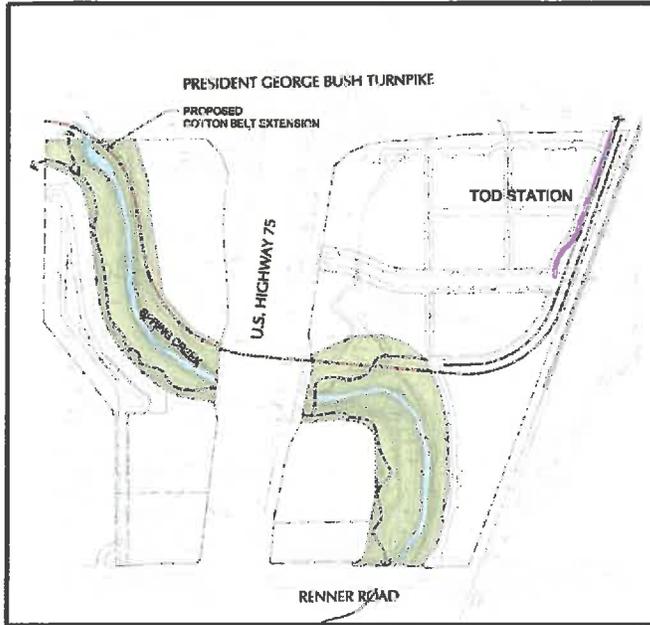
Street Type: A
ROW: 38 ft.
Design Speed: 25 MPH
Parking: Parallel parking on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



9.8 TOD Station

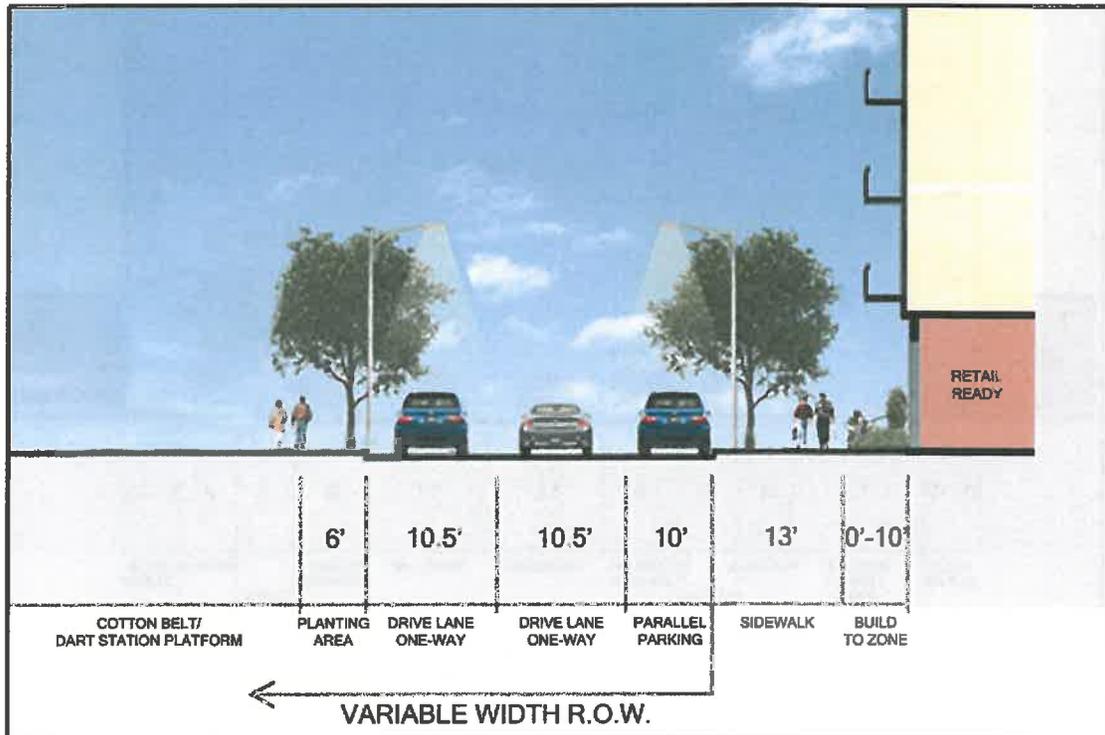
Location Map



TOD Station

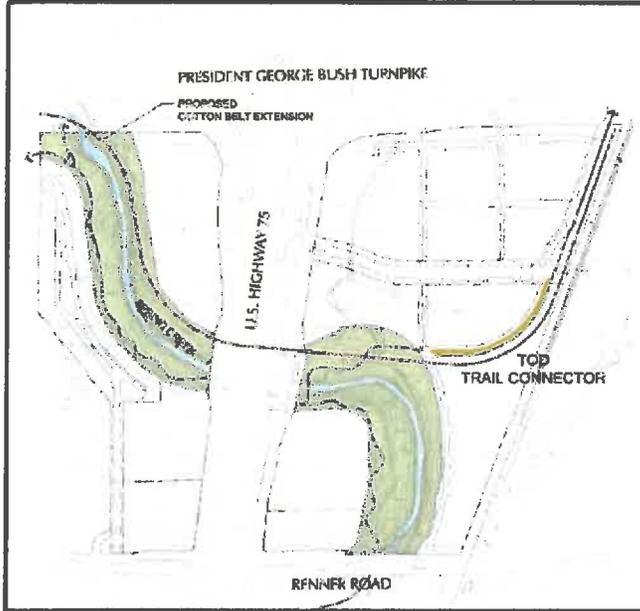
Street Type: A
ROW: Variable
Design Speed: 25 MPH
Parking: Parallel parking on west side
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



9.9 TOD Trail Connector

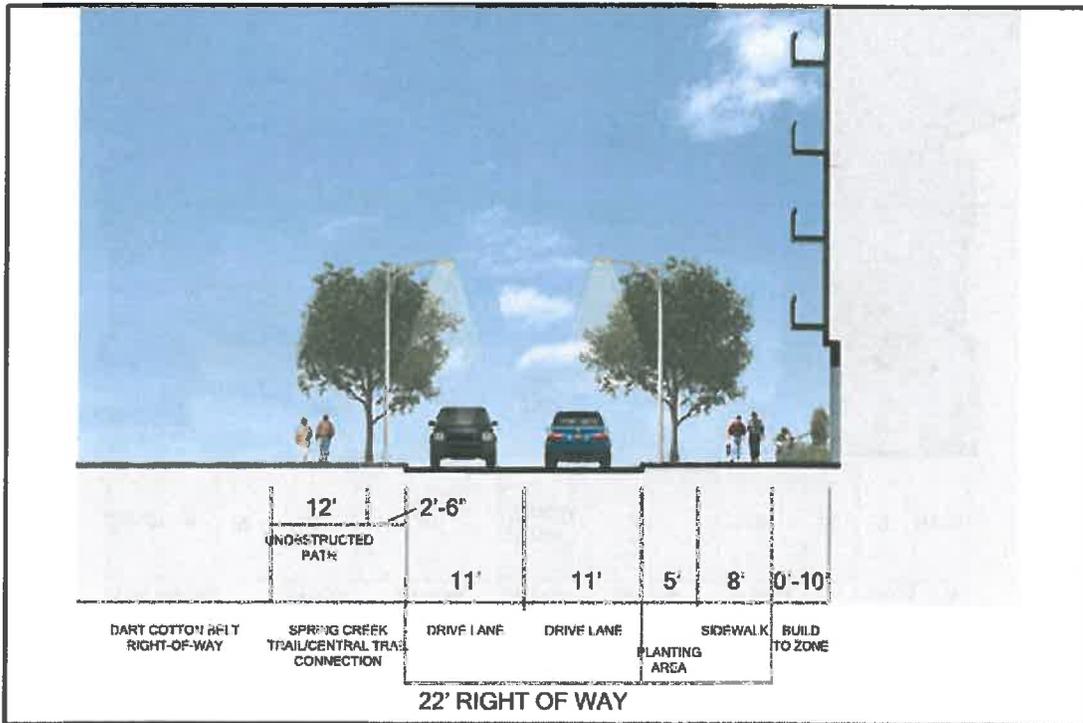
Location Map



TOD Trail Connector

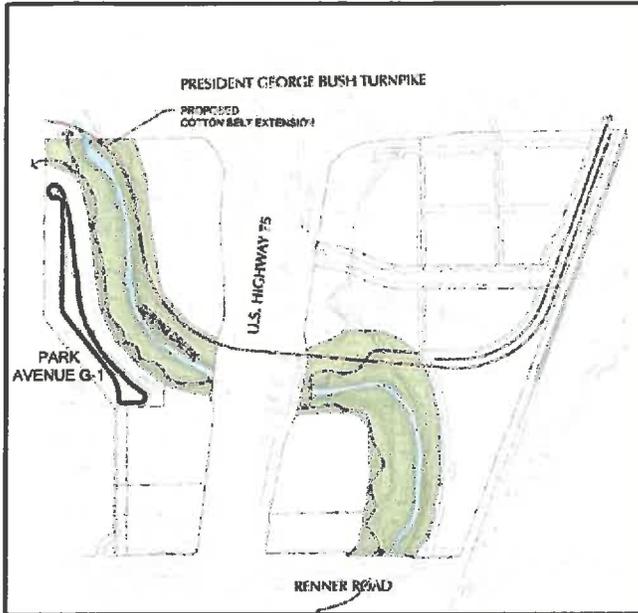
Street Type: B
ROW: 22 ft.
Design Speed: 25 MPH
Parking: None
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



9.10 Park Avenue G-1

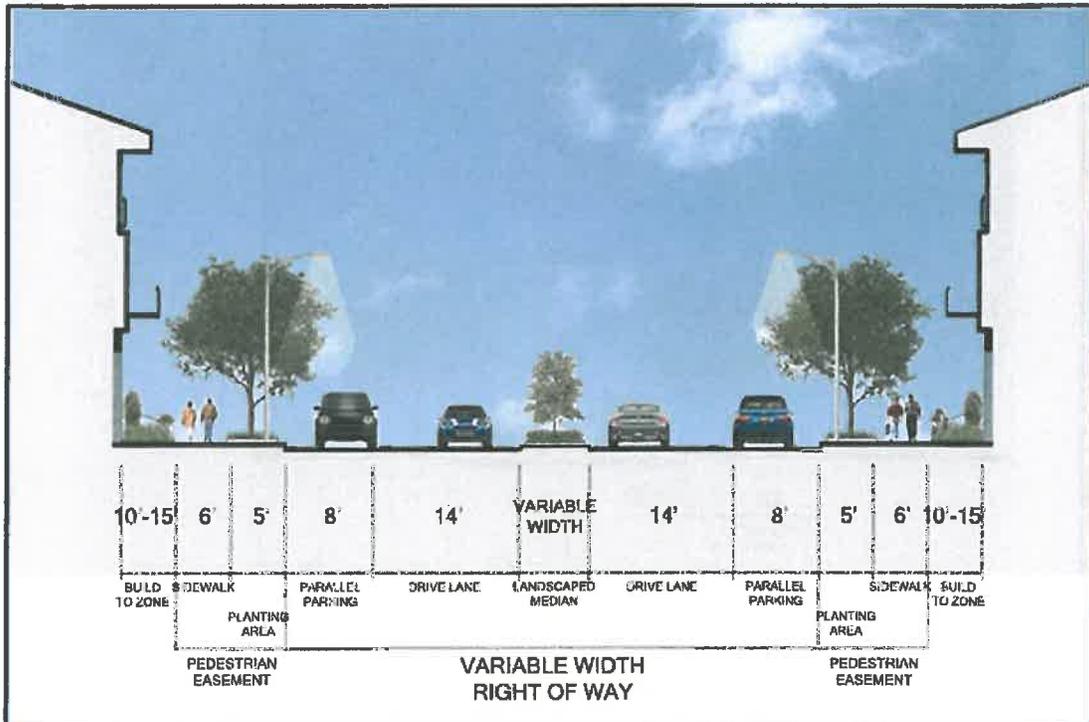
Location Map



Park Avenue G-1

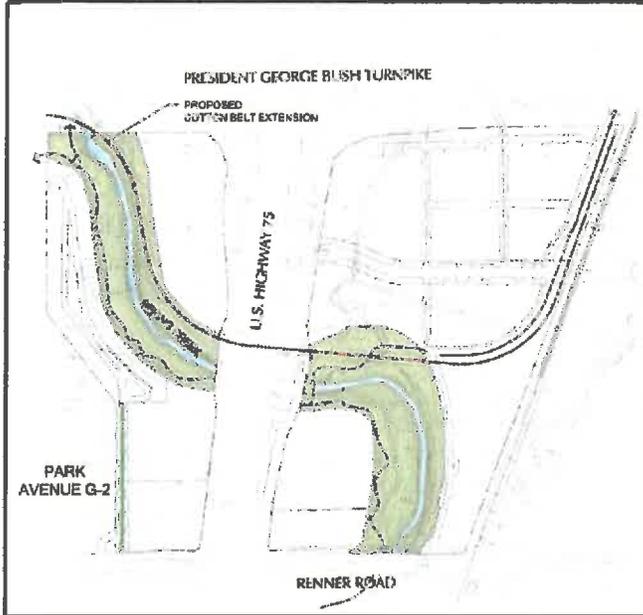
Street Type: B
ROW: Variable
Design Speed: 25 MPH
Parking: Parallel Parking on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



9.11 Park Avenue G-2

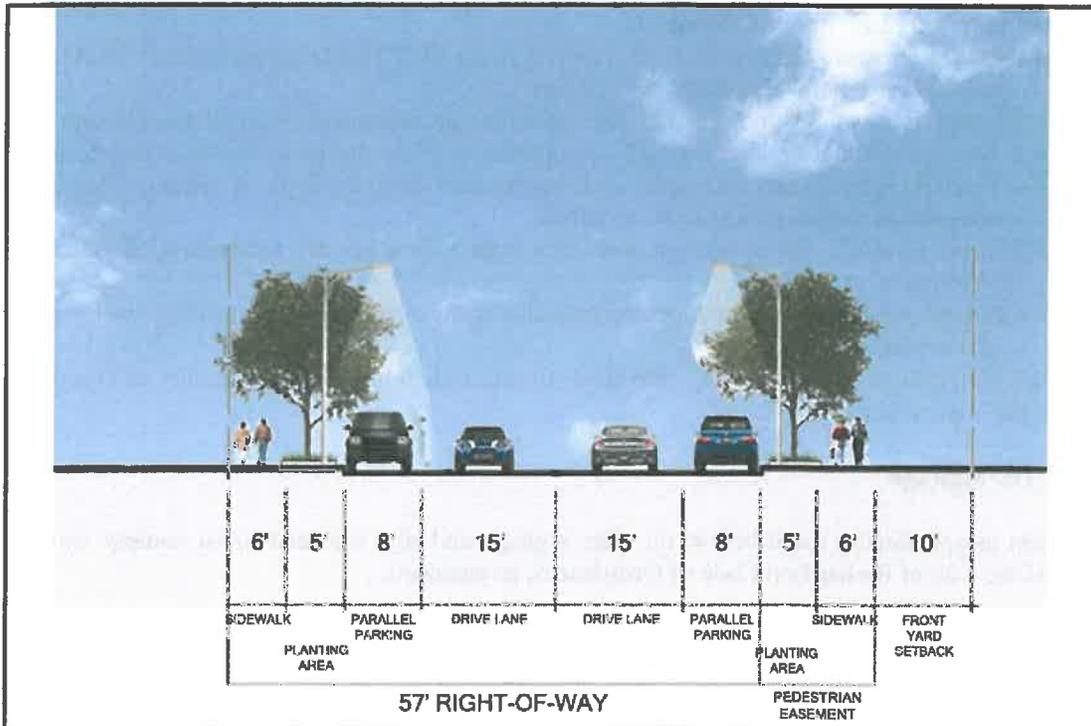
Location Map



Park Avenue G-2

Street Type: B
ROW: 57 ft.
Design Speed: 25 MPH
Parking: Parallel Parking on both sides
Number of Vehicular Lanes: 2
Number of Bike Lanes: None
Curb Radius: 20 ft.
Walkway Type: Pedestrian Esmt.
Landscape Type: Street Trees between 40 ft. & 50 ft. avg.

Street Section



- 9.12 Streetscape & Landscape Standards: Streetscape standards shall apply to all streets within the C-TOPD. Streetscape standards shall address all elements between the building face and edge of the curb. Typical streetscape elements addressed are street trees, lighting, street furniture and pedestrian amenities, and materials. Maintenance of all landscape within the rights-of-way shall be by the property owners association established for the C-TOPD.
- 9.13 Street Trees and Landscaping (within the pedestrian easement):
- 9.13.1 Street trees shall be required on all C-TOPD Streets (except on alleys and PGBT access road.)
 - 9.13.2 Street trees shall be planted approximately 3 feet behind the curb line.
 - 9.13.3 Spacing shall be an average of 40 feet on center (measured per block face) along all streets.
 - 9.13.4 The minimum caliper size for each tree shall be 3 in. and shall be a minimum of 12 feet in height at planting. Each tree shall be planted in a planting area no less than 36 sq. feet; however, the tree well area may be no smaller than 25 sq. feet.
 - 9.13.5 Along the Park Avenue G-1 trees shall be required in the median and spacing and species shall be the same as the trees in the parkway.
 - 9.13.6 Turf and groundcover: When clearly visible from the street and alleys, all unpaved ground areas shall be planted with low growing shrubs or ground cover, ornamental grasses, or a combination thereof. Turf grass must be installed as solid sod and not seeded on.
 - 9.13.7 Species shall be selected from the C-TOPD Planting List in Appendix B of this ordinance.
 - 9.13.8 Maintenance of all landscape materials shall meet the requirements of the City of Richardson Landscape Ordinance Requirements.
 - 9.13.9 Along arterials and highway access roads, street trees shall be planted within the required landscape parkway as per the City of Richardson Landscape Ordinance/Policies.
- 9.14 Street Furniture, Lighting, and Materials:
- 9.14.1 Pedestrian scale lighting shall be required along all C-TOPD streets (except on PGBT access road). They shall be no taller than 20 feet.
 - 9.14.2 Street lights shall be placed at 50 feet on center, approximately 3 feet behind the curb line.
 - 9.14.3 The light standard selected shall be compatible with the design of the street and buildings.
 - 9.14.4 Trash receptacles and bike racks shall be required along all Type 'A' Streets. A minimum of one each per block face shall be required.
 - 9.14.5 Street furniture and pedestrian amenities such as benches are recommended along all Type 'A' Streets.
 - 9.14.6 All street furniture shall be located in such a manner as to allow a clear sidewalk passageway of a minimum of 6 feet.
 - 9.14.7 Materials selected for paving and street furniture shall be of durable quality and require minimal maintenance.

Section 10. Signage

Except as specifically listed below, all other signage and sign standards must comply with Chapter 18 of the City of Richardson Code of Ordinances, as amended.

- 10.1 For new signs, the standards in Table 10.1 shall apply and sign permits shall be approved administratively by the City of Richardson Building Official unless specifically noted in this section.

Table 10.1

Character Zone	TOD Core	TOD MU	Arterial MU	TOD Freeway	Urban Neighborhood	Standard
Sign Type						
Wall (Building) Signs	P	P	P	P	P (comm. uses only)	<ul style="list-style-type: none"> For all ground floor commercial uses (retail, office, and restaurant): One sign per tenant space; area to be calculated at 1.5 sq. feet per linear foot of public street frontage for the tenant space with a maximum of 100 sq. ft per tenant. Second and upper floor commercial uses may also be permitted one second floor wall sign per tenant space per public street frontage; area to be calculated at 1.5 sq. feet per linear foot of second or upper floor frontage along that public street with a maximum of 125 sq. feet Institutional uses (non-profits and churches): One sign per tenant space; area to be calculated at 1.5 sq. feet per linear foot of public street frontage with a maximum of 100 sq. feet Live-Work and Home occupations: One sign limited to an area of 20 sq. feet max. Building sign may encroach a maximum of 12" on to a sidewalk while maintaining a vertical clearance of 8 feet from the finished sidewalk. Building signs may be internally or externally lit. Marquee signs as only permitted as specified below.
Monument Signs	NP	NP	P	P	NP	<ul style="list-style-type: none"> One monument sign per lot per lot street frontage (no more than 2 per lot separated by at least 100 feet) limited to a maximum of 50 sq. feet per sign face and 6 feet in height. Permitted only along PGBT access road, U.S 75 access road, and streets along TOD Freeway Hi-Rise.
Window Signs	P	P	P	P	P (comm. uses only)	<ul style="list-style-type: none"> Limited to 10% of the window area. In the Urban Neighborhood Zone, window signs are only permitted for commercial uses (including the "work" component of live-work uses). <p>The following shall be exempt from this limitation:</p> <ul style="list-style-type: none"> Addresses, closed/open signs, hours of operation, credit card logos, real estate signs, and now hiring signs. Mannequins and storefront displays of merchandise sold. Interior directory signage identifying shopping aisles and merchandise display areas.
Building Blade Signs	P	P	P	P	P (comm. uses only)	<ul style="list-style-type: none"> One per building (commercial and mixed use buildings only), always on separate building face Area = 30 sq. feet maximum per sign face. May encroach a maximum of 6 feet over a sidewalk, but shall not encroach over any parking or travel lane. Building blade signs may be attached to the building at the corners of building or

Character Zone	TOD Core	TOD MU	Arterial MU	TOD Freeway	Urban Neighborhood	Standard
						along any street facing façade above the first floor facade.
Tenant Blade Signs	P	P	P	P	P (comm. uses only)	<ul style="list-style-type: none"> One per commercial tenant space (retail, office, or restaurant use) Area = 16 sq.feet maximum per sign face May encroach a maximum of 4 feet over a public sidewalk, but shall not encroach over any parking or travel lane. Tenant blade signs shall be oriented perpendicular to the building façade and hung under the soffit of an arcade or under a canopy/awning or attached to the building façade immediately over the ground floor tenant space while maintaining a vertical clearance of 8 feet from the finished sidewalk.
Marquee Signs	P	P	P	P	NP	<ul style="list-style-type: none"> Permitted for theatres, auditoriums, and other public gathering venues of 100 persons or more Marquee signs shall be attached to the building or located above or below a canopy only Area = 100 sq.feet maximum Message board may be changeable copy (electronic and non-electronic). Electronic message boards shall be non-flashing. Marquee signs shall not be permitted along Renner Road, PGBT access road and U.S. 75 access road.
For sale/for lease signs	P	P	P	P	P	<ul style="list-style-type: none"> Size is limited to 32 sq. feet per sign face All other standards are the same as City or Richardson Sign Regulations.
Address signs	P	P	P	P	P	Same as City of Richardson Sign Regulations
Temporary construction signs	P	P	P	P	P	One (1) free standing sign per lot during construction only; limited to 32 sq. feet
Banners	P	P	P	P	P	Same as City of Richardson Sign Regulations
Sandwich board signs	P	P	P	P	P	<ul style="list-style-type: none"> Permitted only for retail, service, or restaurant uses Limited to 12 sq. feet per sign face per storefront; Sign may not exceed 4 feet in height. A minimum of 6 feet of sidewalk shall remain clear. Chalkboards may be used for daily changing of messages. Readerboards (electronic and non-electronic) shall be prohibited. Sign shall be removed every day after the business is closed.

Character Zone	TOD Core	TOD MU	Arterial MU	TOD Freeway	Urban Neighborhood	Standard
Light Pole Banners	P	P	P	P	NP	<ul style="list-style-type: none"> Permitted only with approval of the Building Official. Max. 10 sq. feet per sign face. Limited to one per light pole All light pole banners shall be approved by the appropriate utility company prior to consideration by the Sign Control Board. Light pole banners shall be limited to publicize community-wide events, holiday celebrations, public art, and other city sponsored events.
Directory signs	P	P	P	P	P	<ul style="list-style-type: none"> Shall be allowed for all multi-tenant commercial and mixed use buildings only One directory sign per multi-tenant building limited to 12 sq. feet in area Design of the sign shall be integral to the façade on which the sign is to be affixed.
LED signs	P	P	P	P	P	<ul style="list-style-type: none"> Shall be covered by a lens or diffuser. Shall only be permitted as part of a Master Sign Plan.
Pole signs	NP	NP	NP	NP	NP	
Off-premises signs	NP	NP	NP	NP	NP	

10.2 An applicant has the option to establish unique sign standards including size, color, type, design, and location. Such applications shall be reviewed as “Master Sign Plans” by the City of Richardson Building Official and are subject to approval of the Sign Control Board. In evaluating a Master Sign Plan, the Sign Control Board shall consider the extent to which the application meets the proposed Sign Plan:

- 10.2.1 Promotes consistency among signs within a development thus creating visual harmony between signs, buildings, and other components of the property;
- 10.2.2 Enhances the compatibility of signs with the architectural and site design features within a development;
- 10.2.3 Encourages signage that is in character with planned and existing uses thus creating a unique sense of place; and
- 10.2.4 Encourages multi-tenant commercial uses to develop a unique set of sign regulations in conjunction with development standards.

Section 11. Civic/Open Space Standards

11.1 The design of Civic/Open Space shall be regulated by the Civic/Open Space standards herein which shall create a network of open spaces that recognizes the natural qualities of the area while providing a range of both passive and active recreational opportunities. These opportunities may be accommodated in a variety of spaces ranging from large regional parks to neighborhood-scaled plazas. The open space network will be serviced by an interconnected network of trails and paths for pedestrians and bicyclists alike.

11.2 Open Space Standards – Spring Creek Corridor



The required open space, Spring Creek Corridor, as designated on the Regulating Plan, will create an important public space that connects the community within the C-TOPD and allows for active and passive recreation. Spring Creek Corridor shall primarily be naturally landscaped with many places to sit on benches or low walls. Appropriate civic elements, fountains or open shelters may be included. TOD Freeway Hi-Rise, Urban Neighborhood and Arterial Mixed Use buildings shall front onto and activate this space.

Typical Characteristics

General Character

Large, open space

Spatially defined by landscaping and building frontages

Paths, trails, open shelters, lawns, trees and shrubs naturally disposed

May be lineal, following the trajectories of natural corridors

Location and Size

Location and size shall be as shown on the Regulating Plan (Appendix A).

Typical Uses

Passive, and unstructured active recreation

Casual seating/picnicking

11.3 Plaza Standards



Plazas add to the vibrancy of streets within the more urban zones and create formal open spaces available for civic purposes and commercial activity. Building frontages shall define these spaces. The landscape should consist primarily of hardscape. If trees are included, they should be formally arranged and of appropriate scale. Casual seating, along with tables and chairs, should be provided. Plazas typically should be located at the intersection of important streets.

Typical Characteristics

General Character

Formal open space

Primarily hardscape surfaces

Trees and shrubs optional

Spatially defined by building frontages

Location and Size

Location and size shall be as shown on the Regulating Plan (Appendix A).

Typical Uses

Commercial and civic uses

Casual seating

Tables and chairs for outdoor dining

11.4 Pedestrian Passage Standards



Pedestrian passages create intimate passageways through buildings at designated locations. These paths provide direct pedestrian access to residential addresses and create unique spaces for frontages to engage and enter off of. Pedestrian passages allow for social and commercial activity to spill into the public realm. Pedestrian passages should consist of a hardscape pathway activated by frequent entries and exterior stairways. The edges may simply be landscaped with minimal planting and potted plants.

Typical Characteristics

General Character

- Hardscape pathway
- Frequent entries and frontages
- Exterior stairways
- Defined by building frontages
- Minimal planting and potted plants
- Maintain the character of surrounding buildings

Standards

Min. Width 12 feet

Typical Uses

- Pedestrian connection and access
- Casual seating

11.5 Multi-Use Trail Standards



The multi-use trail provides an important place for active recreation and creates a connection to regional paths and biking trails. The multi-use trail will help activate connections between the open spaces and the uses throughout the C-TOPD. The multi-use trail may have different character as it passes through Spring Creek Corridor and as it aligns with public streets. Within Spring Creek Corridor, the trail shall be naturally disposed with low impact paving materials so there is minimal impact to the existing creek bed and landscape. Along public streets, the trail shall have a more formal disposition with a paved trail and regular landscaping.

Typical Characteristics

General Character

Spring Creek Corridor Multi-Use Trail:

Naturally disposed landscape

Low impact paving

Trees lining trail for shade

Appropriately lit for safety

Public Street Multi-Use Trail:

Hardscape Path

Formally disposed pedestrian furniture, landscaping and lighting

Trees lining trail for shade

Standards

Min. Width 12 feet

Location shown on the Regulating Plan (Appendix A).

Typical Uses

Active and passive recreation

Casual seating

11.6 Playground Standards



Playgrounds shall be permitted in parks to provide open space designed and equipped for the recreation of children. These playgrounds should serve as quiet, safe places – protected from the street and typically located where children are not required to cross major roads to access. Playgrounds may be fenced. An open shelter, play structures or interactive art and fountains may be included with landscaping between. Shaded areas and seating shall be provided.

A large playground may be incorporated into the park.

Playground equipment shall serve all ages, based on City of Richardson Parks and Recreation Standards, as amended. Playground equipment and design shall be reviewed and approved by the City of Richardson, including the Parks and Recreation Department.

Typical Characteristics

General Character

- Focused toward children
- Fenced with minimal exits (non-mandatory)
- Open shelter
- Shade and seating provided
- Play structure, interactive art or fountains

Standards

- Min. Size N/A
- Max. Size N/A
- As described by civic space type in which playground is located
- Protected from traffic
- No service or mechanical equipment

Typical Uses

- Active and passive recreation
- Unstructured recreation
- Casual seating

11.7 Ancillary Structure Standards



Ancillary structures should be formal in character and generally related to but clearly subordinate to surrounding buildings. Each individual structure should keep in character with the style of nearby buildings. Typically, these structures are located at prominent locations within an appropriate civic space. Ancillary structures located in more urban zones may have minor commercial uses, such as small food or news vendors, but may also serve as civic elements for general public use with more passive activities. Other ancillary structures located within the Urban Neighborhood Zone should be more modest in use and character, ranging from a simple, public pavilion or pergola to a neighborhood kiosk or mail pavilion.

Typical Characteristics

General Character

Formal character

Relating to style of surrounding buildings

One or more open sides

Covered or providing shade

Small, stand alone structure

Located within Park, Green, Square or Plaza

Standards

Min. Size N/A

Max. Size N/A

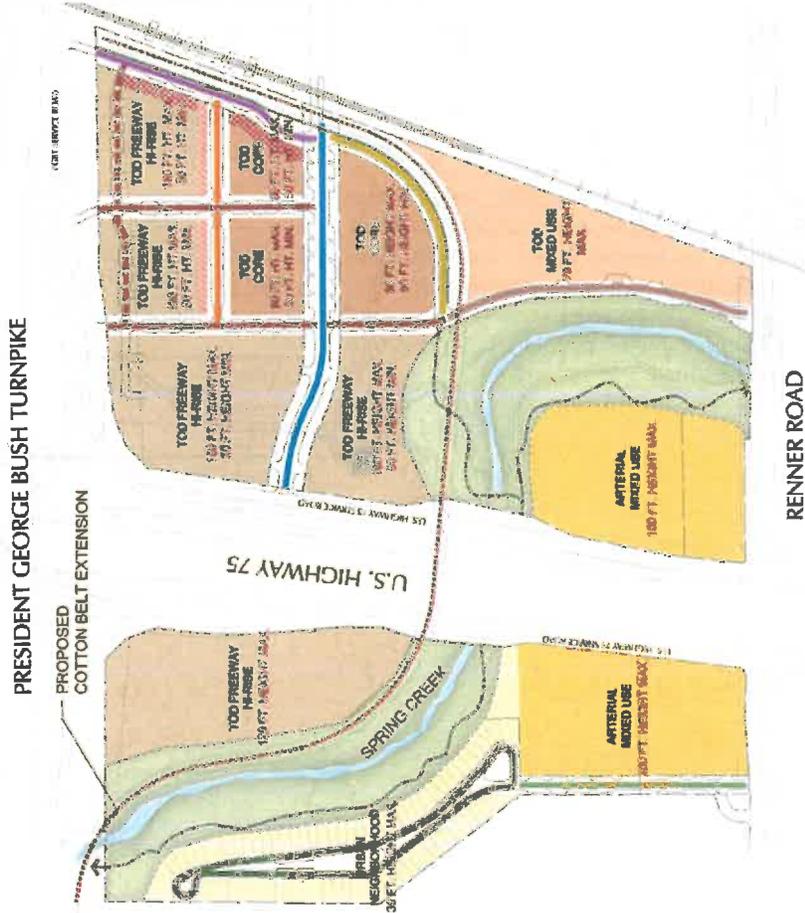
Typical Uses

Civic purposes

Minor commercial uses

Casual seating/picnicking

LEGEND	
CHARACTER ZONES	STREETS - GENERAL
<ul style="list-style-type: none"> TOD Core -- 5.9 Acres TOD Mixed Use -- 5.3 Acres TOD Freeway H-Rise -- 21.2 Acres Arterial Mixed Use -- 13.1 Acres Urban Neighborhood -- 7.0 Acres <p>Note: Acreages are approximate</p>	<ul style="list-style-type: none"> Solid line denotes mandatory street Dashed line denotes non-mandatory street
SPECIAL REQUIREMENTS	STREET TYPES
<ul style="list-style-type: none"> Station Platform Frontage <p>NOTE: For blocks with a minimum height requirement, a minimum of 10,000 sq ft of floor area per block must achieve the minimum height.</p>	<ul style="list-style-type: none"> Street Type 'A' - TOD Main Street Street Type 'A' - TOD Street Promenade Street Type 'A' - TOD Station Street Type 'B' - Parkway Street Type 'B' - TOD Toll Connector Street Type 'C' - Park Avenue G-1 Street Type 'C' - Park Avenue G-2 Street Type 'D' - TOD Street
	OTHER
	<ul style="list-style-type: none"> Multi-Use Trail
	OPEN SPACE TYPES
	<ul style="list-style-type: none"> Non-Mandatory Plaza Mandatory Open Space



APPENDIX A: REGULATING PLAN
Caruth Properties
Transit Oriented Planned Development



GFF Planning

Appendix B Planting List

The following lists contain all species approved for use in the C-TOPD District. It contains native and acceptable adapted species. Other species that are drought tolerant and adaptive may be used for planting within the C-TOPD District. The use of alternative species may be permitted with the approval of the Zoning Administrator.

CANOPY/STREET TREE LIST

<u>Common Name</u>	<u>Botanical Name</u>
Live Oak	<i>Quercus virginiana</i>
Red Oak	<i>Quercus shumardi</i>
Bald Cypress	<i>Taxodium distichum</i>
Sweetgum	<i>Liquidambar styraciflua</i>
Cedar Elm	<i>Ulmus crassifolia</i>
Lacebark Elm	<i>Ulmus parvifolia</i>
Bigtooth Maple	<i>Acer grandidentatum</i>
Caddo Maple	<i>Acer saccharum 'Caddo'</i>
Texas Ash	<i>Fraxinus texensis</i>
Bur Oak	<i>Quercus macrocarpa</i>
Chinquapin Oak	<i>Quercus muhlenbergii</i>
Escarpment Live Oak	<i>Quercus fusiformis</i>
Ginkgo	<i>Ginkgo biloba</i>

ORNAMENTAL TREE LIST

<u>Common Name</u>	<u>Botanical Name</u>
Yaupon Holly	<i>Ilex vomatoria</i>
Crape Myrtle	<i>Lagerstromia indica</i>
Deciduous Yaupon	<i>Ilex decidua</i>
Southern Crabapple	<i>Malus app.</i>
Chinese Pistache	<i>Pistacia chinensis</i>
Mexican Plum	<i>Prunus Mexicana</i>
Wax Myrtle	<i>Myrica carifera</i>
Chitalpa	<i>Chitalpa tashkentensis</i>
Deciduous Holly	<i>Ilex decidua</i>
Desert Willow	<i>Chilopsis linearis</i>
Eve's Necklace	<i>Sophora affinis</i>

SHRUBS LIST

<u>Common Name</u>	<u>Botanical Name</u>
Dwarf Nandina	<i>Nandina domestica 'nana'</i>
Dwarf Burford Holly	<i>Ilex cornuta 'burfordi nana'</i>
Abelia Grandiflora	<i>Abelia grandiflora</i>
Barberry	<i>Barberry spp.</i>
Yucca (Red, Yellow or Soft Tip)	<i>Hesperaloe parviflora</i>
Texas Sage	<i>Leucophyllum frutescans</i>
Indian Hawthorn	<i>Raphiolepis indica</i>
Dwarf Crape Myrtle	<i>Lagerstromia indica 'nana'</i>

Dwarf Yaupon Holly	<i>Ilex vomitoria 'nana'</i>
Pampas Grass	<i>Cortaderia selloana</i>
Black-Eyed Susan	<i>Rudbeckia hirta</i>
Dwarf Wax Myrtle	<i>Myrica pusilla</i>
Needlepoint Holly	<i>Ilex cornuta 'Needle Point'</i>
Knockout Rose	<i>Rosa 'Knock Out'</i>
Rosemary	<i>Rosmarinus officinalis</i>

GROUND COVER/VINES LIST

<u>Common Name</u>	<u>Botanical Name</u>
Asian Jasmine	<i>Trachelospermum Asiaticum</i>
Big Blue Liriope	<i>Liriope muscari</i>
Mondograss	<i>Ophiopogon japonicus</i>
Purple Winter Creeper	<i>Euonymum coloratus</i>
Santolina	<i>Santolina virens</i>
Trumpet Vine	<i>Campsis radicans</i>
Virginia Creeper	<i>Parthenocissus quinquefolia</i>
Lady Banks Rose	<i>Rosa banksiaw lutea</i>
Confederate Jasmine	<i>Trachelospermum jasminoides</i>
Crossvine	<i>Bignonia capreolata</i>
Evergreen Wisteria	<i>Millettia reticulata</i>
Lantana 'New Gold'	<i>Lantana camara 'New Gold'</i>
Liriope 'Silver Dragon'	<i>Liriope muscari 'Silver Dragon'</i>
Prostrate Rosemary	<i>Rosmarinus officinalis prostrata</i>
Sweet Autumn Clematis	<i>Clematis terniflora</i>

ORNAMENTAL GRASSES LIST

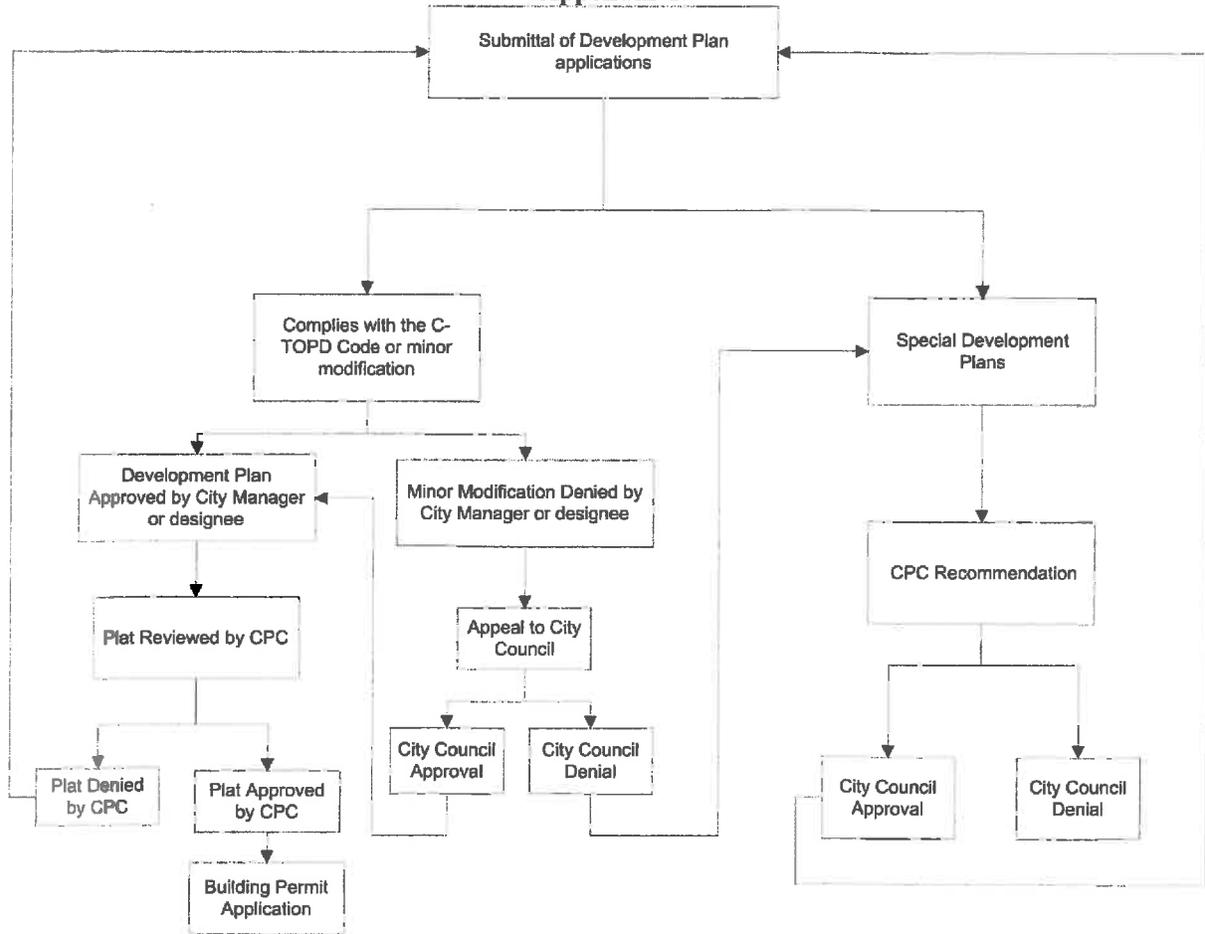
<u>Common Name</u>	<u>Botanical Name</u>
Dwarf Fountain Grass 'Little Bunny'	<i>Pennisetum alopecuroides 'Little Bunny'</i>
Dwarf Maiden Grass	<i>Miscanthus sinensis 'Adagio'</i>
Fountain Grass	<i>Pennisetum alopecuroides</i>
Inland Seaoats	<i>Chasmanthium latifolium</i>
Maiden Grass	<i>Miscanthus sinensis 'Gracillimus'</i>
Mexican Feather Grass	<i>Stipa tenuissima</i>
Muhly Grass	<i>Muhlenbergia capillaris</i>
Weeping Lovegrass	<i>Eragrostis curvula</i>

TURF

<u>Common Name</u>	<u>Botanical Name</u>
Bermuda	<i>Cynodon dactylon</i>
St. Augustine	<i>Stenotaphrum secundatum</i>
Zoysia	<i>Zoysia tenuifolia</i>

These plantings may be placed in Civic/Open Spaces or used to meet the private landscaping requirements of the Code. The applicant shall select drought tolerant, low maintenance, and adaptable shrubs and ground cover based on the placement on the site subject to approval by the City.

Appendix C



C-TOPD: Caruth Properties Transit Oriented Planned Development
 CPD: City Plan Commission
 CC: City Council
 SDP: Special Development Plan

Note: Development Plans include site plans, building elevations, landscape plans, civil engineering plans, and all plats



**City of Richardson
City Council Meeting
Agenda Item Summary**



Meeting Date: Monday, January 31, 2011

Agenda Item: Items of Community Interest

Staff Resource: Bill Keffler, City Manager

Summary: The City Council will have an opportunity to address items of community interest, including:

Expressions of thanks, congratulations, or condolence; information regarding holiday schedules; an honorary or salutary recognition of a public official, public employee, or other citizen; a reminder about an upcoming event organized or sponsored by the City of Richardson; information regarding a social, ceremonial, or community event organized or sponsored by an entity other than the City of Richardson that was attended or is scheduled to be attended by a member of the City of Richardson or an official or employee of the City of Richardson; and announcements involving an imminent threat to the public health and safety of people in the City of Richardson that has arisen after the posting of the agenda.

Board/Commission Action: NA

Action Proposed: No action will be taken.